

PLEASE QUOTE

Your Ref:

Our Ref: DA 2024/41

Enquiries: Planning Department

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.au

We value your feedback on our service.
Tell us about it at www.burnie.tas.gov.au/feedback



NOTICE OF APPLICATION FOR LAND USE PERMIT

(Section 57(3) Land Use Planning and Approvals Act 1993)

Advice to Adjoining Land Owner or Occupier

Application No: - DA 2024/41
Development Site: - 11-16/1 Atkins Drive ROMAINE
CT: 162223/17
Proposal: - Multiple Dwellings

Notice of the above application is served on you as an adjoining land owner or occupier.

The application may be viewed at -

Burnie City Council Customer Services Counter
Ground Floor, City Offices,
80 Wilson Street, Burnie

Between the hours of 8.45 am – 4.45 pm Monday to Friday inclusive (excluding public holidays) or on Council's website at www.burnie.tas.gov.au/permits

You are entitled to make representation in writing on any aspect of the proposal addressed to: -

General Manager,
Burnie City Council,
PO Box 973, Burnie 7320

or burnie@burnie.tas.gov.au by no later than 5.00 pm on **12 March 2026**. Council must have regard to any written representation received during the exhibition period when considering its decision on the application.

All persons who make representation will be notified within seven (7) days of the Council's decision. Any persons who made representation and is not satisfied with the Council decision may, under Section 61(5) of the *Land Use Planning and Approvals Act 1993*, lodge an appeal against that decision within fourteen (14) days of the date of that notice to: -

The Tasmanian Civil and Administrative Tribunal,
GPO Box 1311,
HOBART TAS 7001.

Should you have any enquiries regarding this development proposal, please do not hesitate to contact the Planning Department on (03) 6430 5700.

Troy McCarthy
EXECUTIVE MANAGER – DEVELOPMENT SERVICES
Date of Notice: - **25 February 2026**



Land Use Planning and Approvals Act 1993

Tasmanian Planning Scheme

PERMIT APPLICATION

Office use only

Application No _____

Date Received _____

Permit Pathway - *Permitted/Discretionary*

Use or Development Site:

Street Address

1 ATKINS DR ROMAINE TAS 7320

Certificate of
Title Reference

162223/17

Applicant

First Name

Payal Patel of PDA

Second
Name

Surname

Postal Address:

Phone No:

Mobile:

Email Address:

I/we consent for all giving of information and the serving of notices in relation to this application to be delivered electronically to the above email address?

YES



NO



Applicants Signature:

Payal

Owner (note – if more than one owner, all names must be indicated)

First Name

ATKINS DRIVE PTY LTD

Second Name

Surname

Postal Address:

Phone No:

Instruction for making a permit application

a) *Use or development?*

The application must provide a full description of the proposed use and/or development and of the manner in which the use and/or development is to operate.

“Use” is the purpose or manner for which land is utilised. “Development” is any site works (including any change in natural condition or topography of land and the clearing or conversion of vegetation), and the construction, alteration, or removal of buildings, structures and signs, required in order to prepare a site for use or to change existing conditions within a site. Subdivision is development.

Clause 6.2 Tasmanian Planning Scheme provides the use classes by which all use or development must be described. Development must be categorised by reference to the use class it is to serve.

b) *Required Information*

Adequate statements, plans and specifications must be included within the permit application to address and demonstrate compliance with all applicable requirements of the planning scheme, including any site analysis, impact report and recommendation, and advice, consent or determination required from a State agency or utility entity.

The application must clearly identify the documents relied upon for determination.

Section 51(1AC) *Land Use Planning and Approvals Act 1993* provides that a permit application is not valid unless it includes all of the information required by a planning scheme. Clause 6.1 Tasmanian Planning Scheme prescribes the minimum information that is necessary in order to complete a valid permit application.

S54 *Land Use Planning and Approvals Act 1993* provides that the planning authority may require the applicant to supply further information before it considers a permit application. If the planning authority requires further information to more particularly address one or more of the applicable requirements of the Tasmanian Planning Scheme, the statutory period for determination of a permit application does not run until that information is answered to the satisfaction of the planning authority

c) *Applicable Provisions and Standards*

The permit application must be assessed against the applicable provisions and standards of the Tasmanian Planning Scheme. The application is to identify by reference the clauses it relies upon to demonstrate compliance. (eg *clause 8.4.3 (A1 – A4, and P5)*)

d) *Discretionary Permits*

If a permit is discretionary the permit application must be notified for a period of 14 days to allow opportunity for any interested person to consider the proposed use and/or development and to provide comment on the discretionary matter.

If a permit application relies on performance criteria to satisfy an applicable standard or is discretionary under another provision of the interim planning scheme, the permit is discretionary only with respect to that standard.

The Council must have regard to all representations received during the notification period on a discretionary matter when determining whether to grant or refuse a permit.

e) *If the applicant is not the landowner*

If the applicant is not the owner of the land in the use or development site, the applicant is required to notify all of the owners either prior to or within 7 days from the date of making the permit application.

The permit application must identify all of the landowners; and the applicant must sign the application form to acknowledge the obligation to advise such landowners that the permit application has been made.

If the site includes land owned or administered by the Burnie City Council or by a State government agency, the consent in writing from the Council or the Minister responsible for Crown land must be provided at the time of making the application.

f) *Applicant declaration*

It is an offence for a person to do any act that is contrary to a compliance requirement created under the section 63 *Land Use Planning and Approvals Act 1993*. The applicant is required to complete a declaration that the information given in the permit application is true and correct.

g) *Payment of Fees*

The Council is not required to take any action on the permit application until all the relevant fees have been paid.

Permit Information

(NB If insufficient space, please attach separate document)

Proposed Use:

Use Class

Documents included with the permit application to describe the Use

[Please see attached documents and cover letter](#)

Proposed Development

Use class to which the development applies [residential](#)

Documents included with the permit application to describe the Development

[Please see attached documents and cover letter](#)

Provisions and Standards relied upon for grant of a Permit


[Please see attached documents and cover letter](#)

Value of use and/or development

Notification of Landowner/s

If land is not in applicant's ownership

I, **Payal Patel of PDA**, declare that the owner/each of the owners of the land has been notified of the intention to make this permit application.

Signature of Applicant  Date **10/05/2024**

If the permit application involves land owned or administered by the BURNIE CITY COUNCIL

Burnie City Council consents to the making of this permit application.

General Manager (Signature) Date


If the permit application involves land owned or administered by the CROWN

I, the Minister responsible for the land, consent to the making of this permit application.

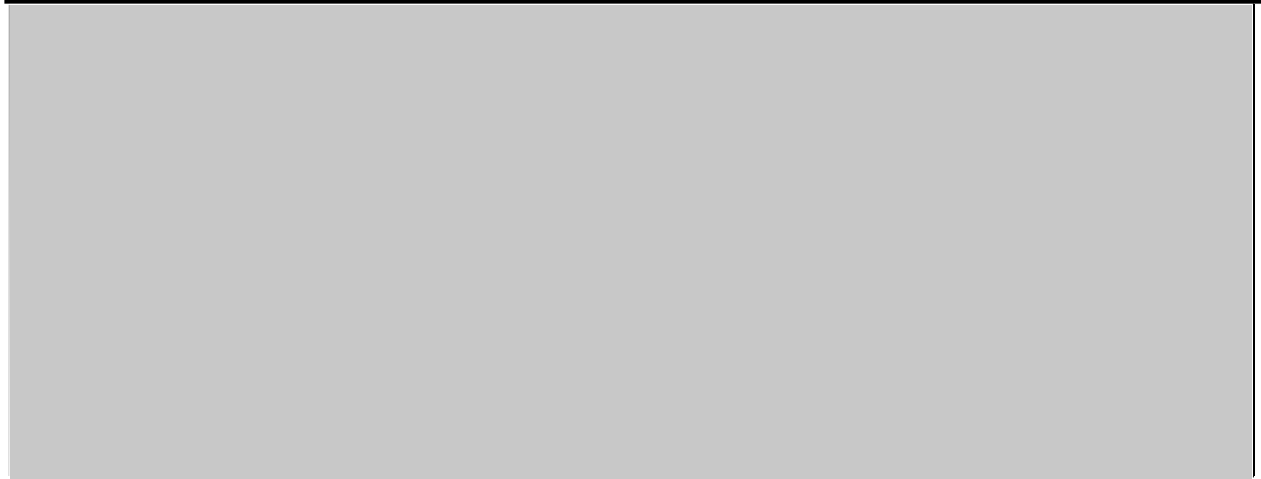
Minister (Signature) Date

Applicant Declaration

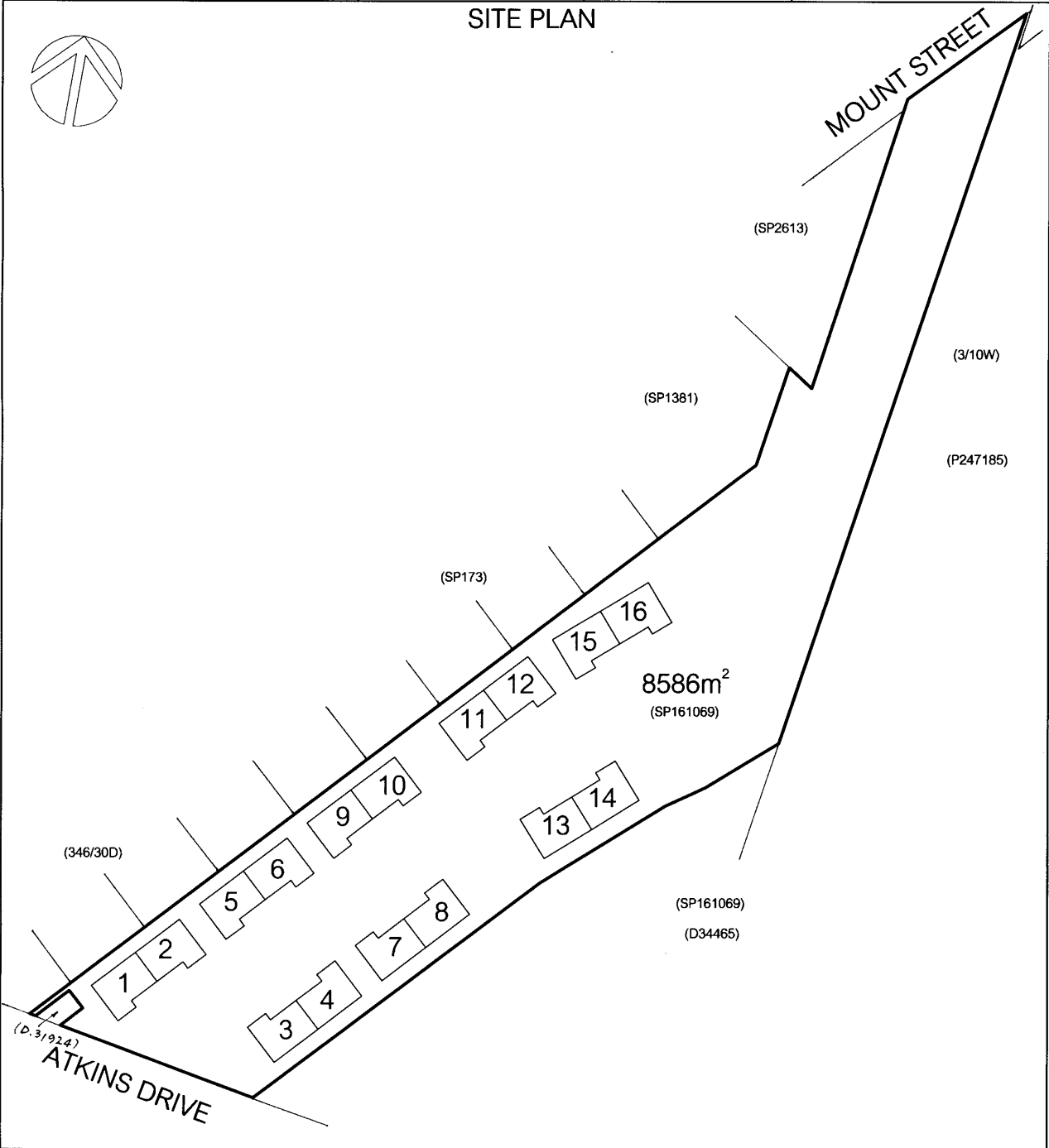
I, **Payal Patel of PDA**, declare that the information I have given in this permit application to be true and correct to the best of my knowledge.

Signature of Applicant  Date **10/05/2024**

Office use only



CITY/TOWN : CITY OF BURNIE SUBURB/LOCALITY:ROMAINE FOLIO REFERENCE: F.R. 161069-1 SITE COMPRISES THE WHOLE OF LOT 1 ON PLAN No. SP161069		STRATA PLAN SHEET 1 OF 4 SHEETS		REGISTERED NUMBER 162223
NAME OF STRATA SCHEME 1 ATKINS DRIVE, ROMAINE		STRATA TITLES ACT 1998 14 JUL 2011		
MAPSHEET MUNICIPAL CODE No. 105	LAST UPI No. FVA99	SCALE 1:750	LENGTHS IN METRES	REGISTERED BY <i>Alice Kawa</i> Recorder of Titles



NOTES: (i) ALL BUILDINGS ON THE SITE TO BE SHOWN ON SHEET 1. (ii) BUILDING TO SITE BOUNDARY OFFSETS OF LESS THAN 2.00 METRES TO BE SHOWN ON SHEET 1.	<i>[Signature]</i> Council Delegate	27/6/11 Date	<i>[Signature]</i> Registered Land Surveyor	4-1-2010 Date
	STAGED/COMMUNITY DEVELOPMENT. SCHEME No. (IF APPLICABLE) (C.992339)		LODGED BY ROCK PROPERTY DEVELOPMENTS	

STRATA PLAN

SHEET 2 OF 4 SHEETS

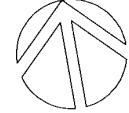
STRATA TITLES ACT 1998

Registered Number

Howland 27/6/11
Council Delegate Date

162223

SCALE 1:400

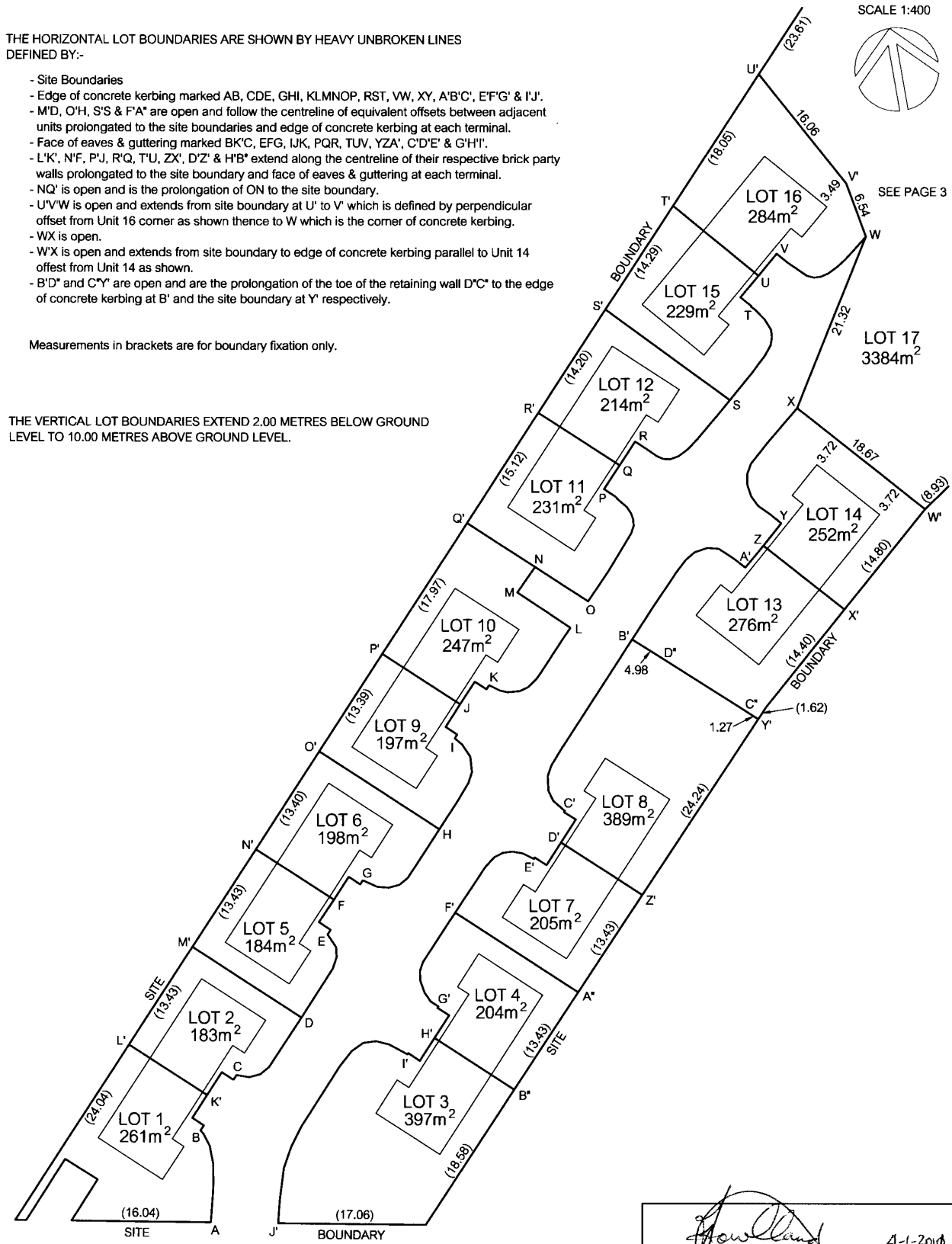


THE HORIZONTAL LOT BOUNDARIES ARE SHOWN BY HEAVY UNBROKEN LINES DEFINED BY:-

- Site Boundaries
- Edge of concrete kerbing marked AB, CDE, GHI, KLMNOP, RST, VW, XY, A'B'C', E'F'G' & I'J'.
- M'D, O'H, S'S & F'A* are open and follow the centreline of equivalent offsets between adjacent units prolonged to the site boundaries and edge of concrete kerbing at each terminal.
- Face of eaves & guttering marked BK'C, EFG, IJK, PQR, TUV, YZA', C'D'E' & G'H'I'.
- L'K', N'F, P'J, R'Q, T'U, Z'X', D'Z' & H'B* extend along the centreline of their respective brick party walls prolonged to the site boundary and face of eaves & guttering at each terminal.
- NQ' is open and is the prolongation of ON to the site boundary.
- U'V'W is open and extends from site boundary at U' to V' which is defined by perpendicular offset from Unit 16 corner as shown thence to W which is the corner of concrete kerbing.
- WX is open.
- W'X is open and extends from site boundary to edge of concrete kerbing parallel to Unit 14 offset from Unit 14 as shown.
- B'D* and C*Y* are open and are the prolongation of the toe of the retaining wall D'C* to the edge of concrete kerbing at B' and the site boundary at Y' respectively.

Measurements in brackets are for boundary fixation only.

THE VERTICAL LOT BOUNDARIES EXTEND 2.00 METRES BELOW GROUND LEVEL TO 10.00 METRES ABOVE GROUND LEVEL.



Howland
Registered Land Surveyor
A-1-2018
Date

STRATA PLAN

SHEET 3 OF 4 SHEETS

STRATA TITLES ACT 1998

Registered Number

Paul 27/6/11
Council Delegate Date

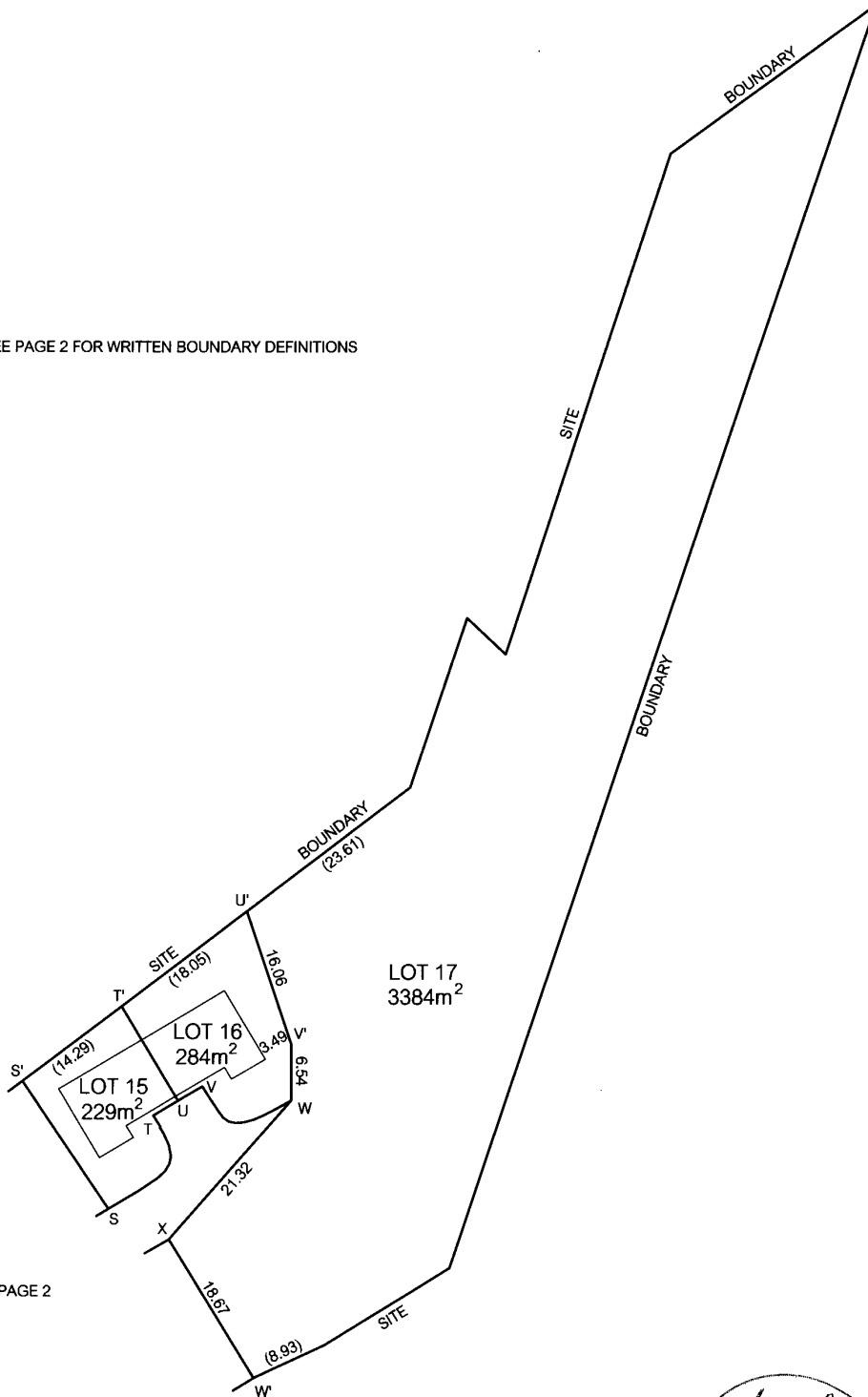
162223

SCALE 1:500



SEE PAGE 2 FOR WRITTEN BOUNDARY DEFINITIONS

SEE PAGE 2



Paul
Registered Land Surveyor Date 4-1-2011

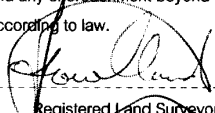
<p>STRATA PLAN</p> <p>SHEET 4 OF 4 SHEETS</p>	<p>STRATA TITLES ACT 1998</p>	<p>Registered Number</p> <p style="font-size: 24pt;">162223</p>
--	-------------------------------	--

NAME OF BODY CORPORATE: Strata Corporation No. 162223, 1 ATKINS DRIVE, ROMAINE

ADDRESS FOR THE SERVICE OF NOTICES: 1 ATKINS DRIVE
ROMAINE, TAS 7320

SURVEYORS CERTIFICATE

I, Timothy Leigh Gowland of OTAGO BAY
a surveyor registered under the Surveyors Act 2002 certify that
the building or buildings erected on the site and drawn on sheet 1 of
this plan are within the site boundaries of the folio stated on sheet 1
and any encroachment beyond those boundaries is properly authorised
according to law.


 Registered Land Surveyor

4-1-2011
Date

100902
Ref No.

COUNCIL CERTIFICATE

I certify that the BURNIE CITY Council has:

(a) approved the lots shown in this plan and
(b) issued this certificate of approval in accordance
with section 31 of the Strata Titles Act 1998


 Council Delegate

27/6/11
Date

DA 2011/8
Ref No.

GENERAL UNIT ENTITLEMENTS

LOT	UNIT ENTITLEMENT
1	1
2	1
3	1
4	1
5	1
6	1
7	1
8	1
9	1
10	1
11	1
12	1
13	1
14	1
15	1
16	1
17	6
TOTAL	22

SEARCH OF TORRENS TITLE

VOLUME 162223	FOLIO 17
EDITION 3	DATE OF ISSUE 30-Nov-2023

SEARCH DATE : 08-May-2024

SEARCH TIME : 04.36 PM

DESCRIPTION OF LAND

City of BURNIE

Lot 17 on Strata Plan 162223 and a general unit entitlement operating for all purposes of the Strata Scheme being a 6 undivided 1/22 interest

Derived from Strata Plan 162223

Derivation : Part of 50,000 Acres Granted to the Van Diemens Land Company

SCHEDULE 1

M975296 TRANSFER to PROFITIFY PTY LTD Registered
30-Nov-2023 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
The registered proprietor holds the lot and unit entitlement

subject to any interest noted on common property

Folio of the Register volume 162223 folio 0

SP161069 EASEMENTS in Schedule of Easements

SP161069 FENCING COVENANT in Schedule of Easements

B13018 COVENANTS set forth in Transfer

C992339 APPLICATION for registration of a staged development scheme Registered 14-Jul-2011 at noon

UNREGISTERED DEALINGS AND NOTATIONS

N185697 PRIORITY NOTICE reserving priority for 90 days
TRANSFER Profitify Pty Ltd to Atkins Drive Pty Ltd as trustee for the Atkins Drive Trust Lodged by SIMMONS WOLFHAGEN on 23-Feb-2024 BP: N185697

N173358 TRANSFER to ATKINS DRIVE PTY LTD Lodged by SIMMONS WOLFHAGEN on 22-Mar-2024 BP: N173358

SEARCH OF TORRENS TITLE

VOLUME 162223	FOLIO 0
EDITION 1	DATE OF ISSUE 14-Jul-2011

SEARCH DATE : 08-May-2024

SEARCH TIME : 04.36 PM

DESCRIPTION OF LAND

City of BURNIE
 The Common Property for Strata Scheme 162223
 Derivation : Part of 50,000 Acres Granted to the Van Diemens
 Land Company
 Prior CT 161069/1

SCHEDULE 1

STRATA CORPORATION NUMBER 162223, 1 ATKINS DRIVE, ROMAINE

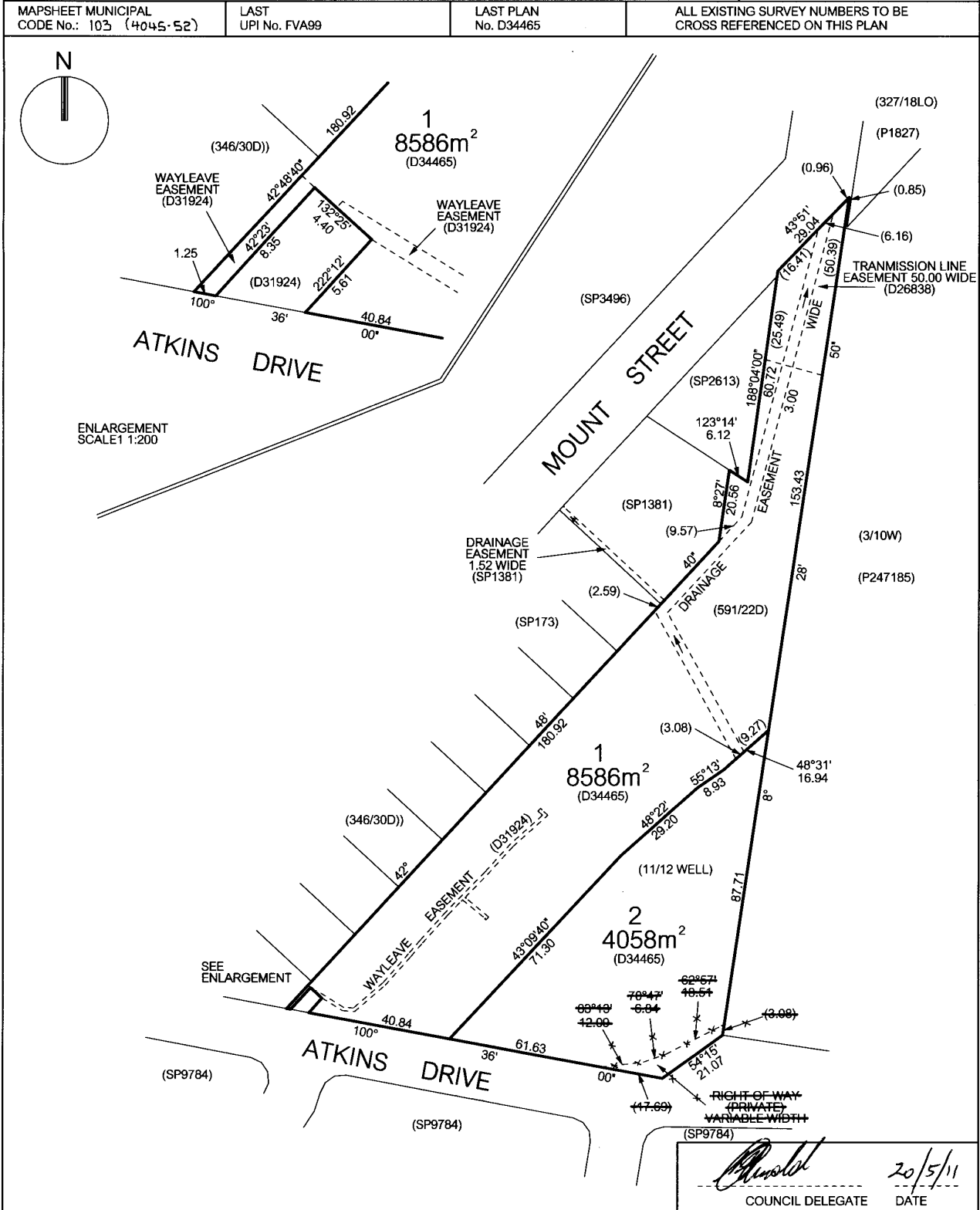
SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP161069 EASEMENTS in Schedule of Easements
 SP161069 FENCING COVENANT in Schedule of Easements
 B13018 COVENANTS set forth in Transfer
 C992339 APPLICATION for registration of a staged development
 scheme Registered 14-Jul-2011 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

OWNER: ATKINS RESERVE PTY. LTD.	PLAN OF SURVEY	REGISTERED NUMBER SP161069
FOLIO REFERENCE: F.R. 34465-1		BY SURVEYOR: TIMOTHY LEIGH GOWLLAND
GRANTEE: PART OF 50,000 ACRES GRANTED TO THE VAN DIEMANS LAND COMPANY.	LOCATION CITY OF BURNIE	APPROVED EFFECTIVE FROM 14 JUL 2011 <i>Alice Kawa</i> Recorder of Titles
MAPSHEET MUNICIPAL CODE No.: 103 (4045-52)	LAST UPI No. FVA99	LAST PLAN No. D34465
SCALE 1: 1000 LENGTHS IN METRES		
ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN		



SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 16 1069

PAGE 1 OF 3 PAGE/S
3

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

FENCING COVENANT

The owner of each Lot on the Plan covenants with Atkins Reserve Pty Ltd (the Vendor) that the Vendor shall not be required to fence.

RIGHT OF WAY EASEMENTS

Lot 2 is ~~SUBJECT TO~~ a Right of Carriageway marked "Right of Way (PRIVATE) Variable Width" on the Plan appurtenant to Certificate of Title Volume 247486 Folio 4.

WAYLEAVE EASEMENT

Lots 1 and 2 are ^{is} ~~are~~ SUBJECT TO as per B138572 the full free right and liberty for the Transend Networks Pty Ltd (formerly Hydro Electric Commission) and successors and their servants, agents and workmen at all times;

- a) To lay and maintain cables for the transmission of electrical energy under and along the strip of land marked "Wayleave Easement" on the Plan (called "the servient land") with the right for they Transend Networks Pty Ltd (formerly Hydro Electric Commission) servants workmen and others authorised at all times to enter into and upon the servient land for the purpose of laying inspecting repairing and maintaining any such cables without doing unnecessary damage to the said servient land and making good any damage occasioned by such entry.
- b) To enter into and upon the servient land for all or any of the above purposes with or without all necessary plant equipment machinery and vehicles of every kind.

Lots 1 and 2 are ^{is} ~~are~~ SUBJECT TO the Easement described in B682828 FIRSTLY ALL THAT the full and free right and liberty for the Transend Networks Pty Ltd (formerly Hydro Electric Commission) and its successors and its and their servants agents and workmen at all times

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: Atkins Reserve Pty Ltd FOLIO REF: Certificate of Title Volume 34465 Folio 1 SOLICITOR & REFERENCE: Butler McIntyre & Butler	PLAN SEALED BY: <i>Burnie City Council</i> DATE: <i>20/5/11</i> SD 2010/1170 REF NO. <i>[Signature]</i> Council Delegate
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	

ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 2 OF $\frac{2}{3}$ PAGES	Registered Number SP 16 10 69
SUBDIVIDER: ATKINS RESERVE PTY LTD FOLIO REFERENCE: 34465 / 1	
50.00 WIDE ON THE PLAN	

- a) to clear the lands indicated as "Transmission Line Easement" ^{on (D.26838)} (hereafter called "the servient lands") and to erect construct place inspect alter repair renew maintain and use in upon over and along and remove from the servient lands towers poles wires cables apparatus appliances and other ancillary works (all of which are hereinafter collectively referred to as 'the said lines') for the transmission and distribution of electrical energy and for purposes incidental thereto;
- b) to cause or permit electrical energy to flow or be transmitted through and along the said lines;
- c) to cut away and keep clear of the said lines all trees and all other obstructions or erections of any nature whatsoever which may at any time overhang encroach or be in or on the servient lands and which may in any way endanger or interfere with the proper operation of the said lines; and
- d) to enter into and upon the servient lands for all or any of the above purposes with or without all necessary plant equipment machinery and vehicles of every kind

TRANSMISSION LINE EASEMENT

Lot~~s~~ 1 ^{is} ~~and 2 are~~ SUBJECT TO the Easement described in B13018 and being the full and free right and liberty for the Transend Networks Pty Ltd (formerly Hydro Electric Commission) and its successors and its and their servants agents and workmen at all times;

- ^{the plan}
- a) to clear the land marked "Transmission Line Easement 50.00 wide" ^{on} ~~Diagram No. 34465~~ (the said lands being portion of the land comprised in lot~~s~~ 1 ~~and 2~~ and situate in the Town of Burnie being called "the servient land") and to erect construct place inspect alter repair renew maintain and use in upon over and along and remove from the servient land towers poles wires cables apparatus appliances and other ancillary works (all of which are hereinafter collectively referred to as "the said lines") for the transmission and distribution of electrical energy and for purposes incidental;
 - b) to cause or permit electrical energy to flow or be transmitted through and along the said lines;
 - c) to cut away remove and keep clear of the said lines all trees and all other obstructions or erections of any nature whatsoever which may at any time overhang encroach or be in or on the servient lands and which may in any way endanger or interfere with the proper operation of the said lines; and
 - d) to enter into and upon the servient land for all or any of the above purposes with or without all necessary plant equipment machinery and vehicles of every kind

AND SECONDLY the benefit of a covenant with the Transend Networks Pty Ltd (formerly Hydro Electric Commission) and its successors that the servient land will not erect any building on any part of the servient lands to the intent that the burden of the covenant may run with and bind the subject lands and every part and that the benefit may be annexed to the easement above.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

PIN OR STAPLE HERE
DO NOT GUM THIS
FORM TO THE INSTRUMENT

ANNEXURE PAGE

PAGE 3 of 3 PAGES

Vol. 34465 Fol. 1

SP 16 1 0 6 9

EXECUTED by Atkins Reserve Pty Ltd ACN
142 266 180 in accordance with section 127 of
the Corporations Act 2001:

Signature: [Handwritten Signature]

Name: PETER H. SYPKES

Director

PLEASE PRINT

Signature: [Handwritten Signature]

Name: KENT L MEDWIN

Director/Secretary *
* Delete as appropriate

PLEASE PRINT

National Australia Bank A.C.N 142 266 180
Being the Proprietor of Mortgage Number C960321
Hereby consents to the schedule of easements herein

Signed Sealed and Delivered for and on behalf of the
National Australia Bank by its Attorney [Handwritten Signature]

Under Registration of Power of Attorney Number
Who certifies that he/she is a Conveyancing Officer of the
National Australia Bank and declares that she has
Received no notice of revocation of the said Power of Attorney
And in the presence of [Handwritten Signature]

Witness Signature: [Handwritten Signature]

Witness Full Name: CAROL MICHAEL SIMON

Witness Address: 101/16 CORNWALL ST

Witness Occupation: HOUSING TRD

NOTE:-Every annexed page shall be signed by the parties to the dealing, or where the party is a corporate body, be signed by the persons who have attested the affixing of the seal of that body to the dealing.

Version 1

THE BACK OF THIS PAGE MUST NOT BE USED



Disclosure Statement
Section 35(2) of the *Strata Titles Act 1998*

STAGED DEVELOPMENT SCHEME DOCUMENT

This disclosure statement contains details of a staged development scheme which is proposed to be developed in2..... stages on the land of the developer.

This disclosure statement forms part of a staged development scheme and should be read in conjunction with the master plan.

1. **Name and address of Developer:** Albert Trinh Atkins Drive Pty Ltd
.....
.....

Name and address of Staged Development Scheme: 1 Atkins Drive, Romaine, TAS, 7320
.....
.....

2. **A description of the proposed development and the stages in which it is to be carried out:**
..... 24 residential units
..... Stage 1 - Units 1 to 16 constructed
..... Stage 2 - Units 17 to 24 inclusive with additional common property
.....

3. **Schedule of times, or the happening of specified events, for the commencement and completion of each stage:**
..... Stage 1 - complete
..... Stage 2 - Commencement - June 2024
..... Stage 2 - Completion - June 2026
.....

4. Schedule of working hours during which work is to proceed for the proposed stages:

Monday to Friday 7:00am to 5:00pm

Saturday 9:00am to 4:00pm

5. Description of (any) amenities to be provided:

N/A

6. Purposes for which amenities are to be provided:

N/A

7. Extent for which amenities are to be available for use by owners or occupiers or other invitees:

N/A

8. Arrangements for providing and maintaining the amenities and defraying the cost of their provision or maintenance:

N/A

9. A schedule of materials and finishes to be used in the building work involved in carrying out proposed development work:

(As per drawings provided by N Plus B ref: A100 - U301)

Roof: Colorbond

Window material: Aluminium

Wall material: Brick

10. A schedule of the proposed unit entitlements for each proposed lot after the completion of each stage of the proposed development:

Stage 1: Lots 1 to 16, unit entitlement 1 each

Stage 2: Lots 17 to 24, unit entitlement 1 each

Note: Strata amendment required to increase total unit entitlements to 24. All units 1 each.

11. Any other details relevant to the disclosure statement forming part of the scheme:

The hatched portion of Lot 16 on the master development plan forms part of a construction zone until completion of Stage 2 of the Staged Strata Scheme.

WARNING

The information contained in the Master Plan, Management Statement and Constituent Documents is of a general nature only as to how the development may affect you. If you require further information you should contact the developer. You may also wish to obtain your own independent advice.

Date: 26/04/24

Signed by the Developer

Signed by the Council

Albert Trinh
ALBERT TRINH
Sole Director and Secretary

NAME OF SCHEME: 1 Atkins Drive Romaine

PAGE 3 OF 3 PAGES

THE BACK OF THIS FORM MUST NOT BE USED

CITY/TOWN CITY OF BURNIE	NEW MASTER PLAN SHEET 1 OF 1 SHEET		Registered Number
SUBURB/LOCALITY ROMAINE			STRATA TITLES ACT 1998
FOLIO REFERENCE FR161069-1	NAME OF STRATA SCHEME 1 ATKINS DRIVE, ROMAINE		REGISTERED
SITE COMPRISES THE WHOLE OF LOT 1 ON SP161069	SCALE 1:750	LENGTHS IN METRES	Recorder of Titles

AMENDMENT TO STRATA DEVELOPMENT SCHEME: STR162223



Unit Entitlements:
Each lot is to have 1 entitlement

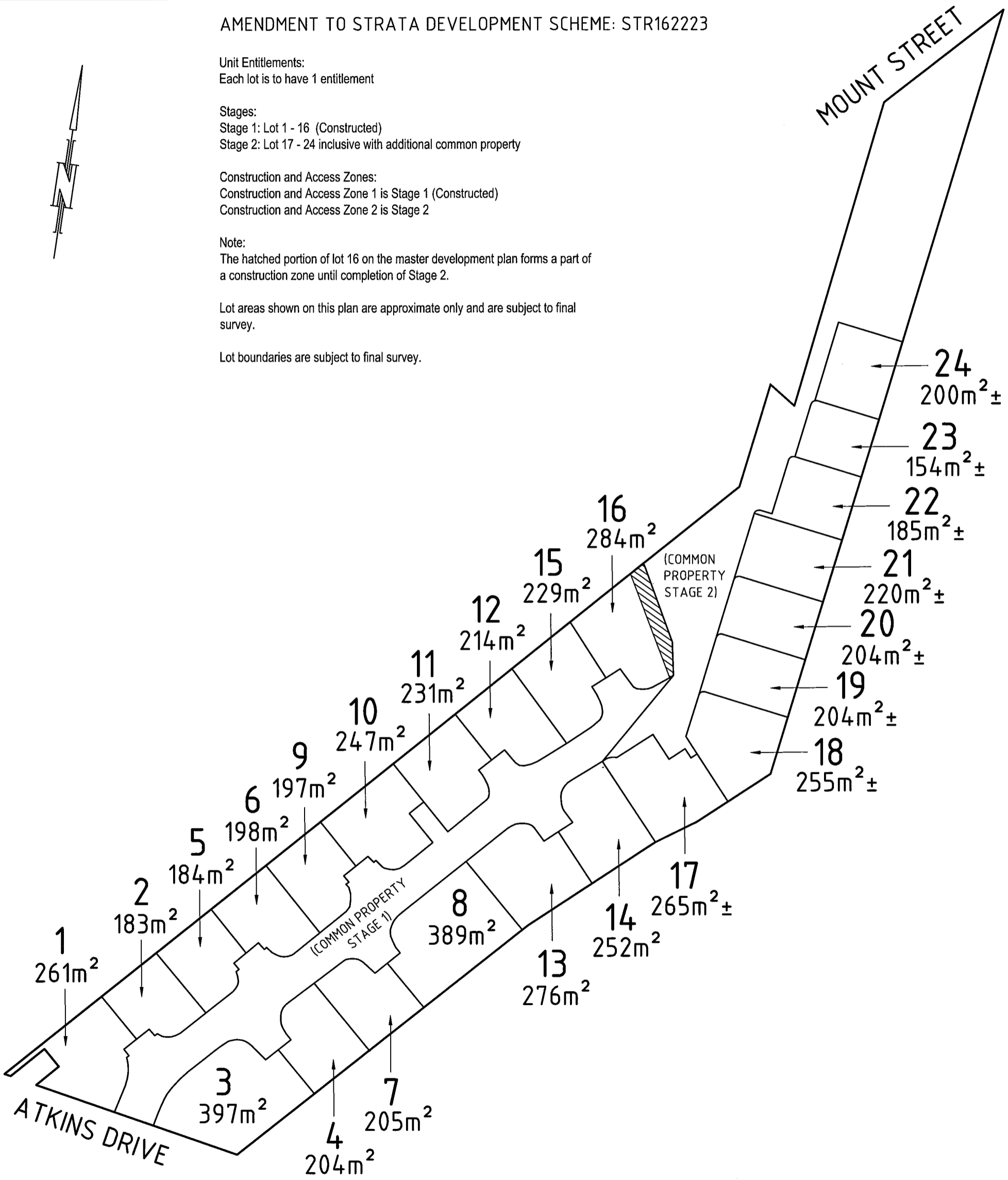
Stages:
Stage 1: Lot 1 - 16 (Constructed)
Stage 2: Lot 17 - 24 inclusive with additional common property

Construction and Access Zones:
Construction and Access Zone 1 is Stage 1 (Constructed)
Construction and Access Zone 2 is Stage 2

Note:
The hatched portion of lot 16 on the master development plan forms a part of a construction zone until completion of Stage 2.

Lot areas shown on this plan are approximate only and are subject to final survey.

Lot boundaries are subject to final survey.



NOTES: (i) ALL BUILDINGS ON THE SITE TO BE SHOWN ON SHEET 1.
(ii) BUILDING TO SITE BOUNDARY OFFSETS OF LESS THAN 2.00 METRES TO BE SHOWN ON SHEET 1.

STAGED/COMMUNITY DEVELOPMENT.
SCHEME No. (IF APPLICABLE)

_____	_____		27/03/2024
Council Delegate	Date	Registered Land Surveyor	Date
LODGED BY			

STRATA TITLES ACT 1998 (TAS)
Minutes of Special General Meeting
Strata Corporation 162223
1 Atkins Drive Romaine

Minutes of Special General Meeting for Strata Corporation 162223 being 1 Atkins Drive Romaine held on 11th April 2024 at Coast to Coast Strata, 38-40 Wilmot Street at 9.00am

- Joshua Jones was appointed Chair
- Present: Vince Tamburo and Shane McFarling
- Proxies were received from: Atkins Drive Pty Ltd – Proxy to Joshua Jones
- In attendance: Joshua Jones – Coast to Coast Strata
- A quorum was declared.

Motions:

1. **Resolved** that the Body Corporate approved the proposed staged development plans as tabled to the meeting for the construction of new dwellings on lot 17 and the manager authorised to have the common seal applied to any and all necessary documents as required throughout the process.

Meeting Closed 9.21am

Coast to Coast Strata

Our Locations

38-40 Wilmot Street Burnie (Head Office)

By Appointment Only

5/5 Fitzroy Street Sorell

48 Belhaven Crescent Newstead

Phone: 1800 222 787

admin@c2cstrata.com.au

www.c2cproperty.com.au

PLEASE QUOTE

Your Ref:

Our Ref: 31/13/4; 24/25857; DA 2024/41
DD001.2024.00000041.001

Enquiries: Jon Randall (JR)

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690
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2 September 2024

Payal Patel

Email: [REDACTED]

Dear Ms Patel

**COUNCIL ADVICE TO DEVELOPER
PROPOSED MULTI UNIT DEVELOPMENT – DA 2024/41
UNIT 11-16, 1 ATKINS DRIVE, ROMAINE**

I write in relation to your recent Development Application (DA 2024/41) seeking grant of a permit under the *Land Use Planning and Approvals Act 1993* for the proposed development of units on land at 1 Atkins Drive, Romaine.

This letter provides formal communication regarding the proposed use and development as follows:

- Part A of this letter contains written advice from Burnie City Council as a “Road Authority”, and contains the information required for assessment and determination of a permit application made under the *Land Use Planning and Approvals Act 1993* against relevant requirements of the Tasmanian Planning Scheme (TPS).

Council’s Land and Environmental Services Department has been provided with a copy of Part A to assist the Council in its role as a planning authority.

- Part B of this letter contains general information from Council, acting as a Road Authority under the *Local Government (Highways) Act 1982* and the *Roads and Jetties Act 1935*, and as a Road Manager under the *Heavy Vehicle National Law (Tasmania) Act 2013*.

It also addresses the Council’s requirements as a Stormwater Service Provider under the *Urban Drainage Act 2013*.

Part B is to inform you of matters outside of the development application process that may affect the use or development. Provision of this information at an early stage of the development process will enable informed decisions to progress the approvals process as smoothly as possible.

The information and advice in this letter are not exhaustive and does not cover matters relating to approvals that may be required from other authorities, such as the Water and Sewerage Authority (TasWater) and the Electricity Entity (TasNetworks), etc.

PART A

“Road Authority” Advice:

In reference to the relevant Tasmanian Planning Scheme Codes:

- **C3.0 Road and Railway Assets Code**

- C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

We understand that you wish to rely on acceptable solution A1.2 to satisfy section C3.5.1 of the State Planning Provisions. The Road Authority therefore provides the following advice in this regard for access via Atkins Drive.

Access ***is already be provided*** to the road network at Atkins Drive to allow access to your proposed development, subject to the following:

A ‘*Permit to Conduct Work in a Council Street*’ is required for any construction work within the road reserve. An application form is available on Council’s website <https://burnie.tas.gov.au/ROP>, together with the ‘*General Terms and Conditions*’ and ‘*Requirements, Guidelines and Conditions of Constructing a Driveway to a Council Street*’.

Once a permit has been approved, Council must be notified a minimum of 48 hours prior to pouring concrete and at the completion of pour, to undertake inspections and ensure all works conform to all relevant standards and specifications.

This Road Authority advice is valid for a period of 12 months from the date of this letter.

“Council” Advice:

Council is not aware of any matters that are pertinent to this development application process in relation to coastal inundation, coastal erosion, flood hazard or landslide.

The proposed development is within the urban drainage district and connections to the urban stormwater system can be provided. Detailed requirements under the *Urban Drainage Act 2013* for stormwater connection are set out in Part B.

PART B – GENERAL INFORMATION

Road Authority Information:

We draw your attention to Burnie City Council Highways By-Law No. 3 of 2013, available on the Burnie City Council website <https://burnie.tas.gov.au/Council/By-laws>. Part 3 of this By-law is relevant to the proposed use and development.

Any works in the road reserve must be undertaken at the developer’s expense and in accordance with *Infrastructure Assets – Standards Policy CP-CBS-SG-031* available on Council’s website <https://burnie.tas.gov.au/Policies>, and the Tasmanian and Burnie City Council Standard Drawings, and to the satisfaction of Burnie City Council.

Stormwater Service Provider Information:

In accordance with Section 14 of the *Urban Drainage Act 2013*, permission must be obtained from the Stormwater Service Provider in order to connect new works to the public stormwater system.

This process does not form part of a permit application under the Tasmanian Planning Scheme.

However, it is recommended you consider the requirements of the Stormwater Service Provider prior to lodging a permit application, otherwise it is possible that changes required to satisfy drainage requirements may result in the need for an amendment to the development permit.

Council has reviewed the submitted drawings and provides the following preliminary feedback in relation to the proposed stormwater discharge arrangements.

The development site currently has a connection to the stormwater network. The new development may utilize this existing connection subject to the following:

- a) Due to downstream network limitations, the following requirements shall apply:
 - i. A Permissible Site Discharge (PSD) rate shall be calculated for the development site (Lot 17) using the rational method and the following data:
 - Run-off coefficient 0.55.
 - Intensity 100mm/hr (or as calculated by a suitably qualified person based on the time of concentration of the entire development site and a 20% AEP event).
 - Area shall be the entire area of the site (Lot 17).
 - ii. The Design Site Discharge (DSD) rate shall be calculated based on the critical storm duration of the entire development catchment (Lot 17) calculated by a suitably qualified person.
 - iii. Where the DSD is greater than the PSD the increase in site stormwater discharge shall be accommodated by providing on-site detention (OSD) to limit stormwater runoff to the satisfaction of Council.
 - iv. In calculating the DSD and any required OSD, the following design criteria are to be used:
 - For the purposes of calculating the DSD, a runoff coefficient of no less than 0.9 shall be used for impervious areas and a coefficient of no less than 0.4 shall be used for pervious areas unless otherwise approved by Council.
 - Roofs, driveways and carports, and other impervious hard standing areas shall be 100% impervious for drainage calculation purposes.
 - Driveways constructed with gravel or "grass-crete" or pervious pavers shall be 80% impervious for drainage calculation purposes.
 - Courtyards and paths paved with pervious pavers and with a proper subsurface drainage system constructed underneath shall be 25% impervious for drainage calculation purposes.
 - Unroofed decks constructed with open timber decking shall also be considered 25% impervious.
 - The Site Storage Requirement (SSR) for detention systems shall be determined by calculating runoff volumes for the full range of site storm durations for the 10% AEP storm event to determine the maximum SSR using Boyd's formula, computer modelling, or other approved methods.
 - Calculations must be submitted to the Council for review.
 - v. Where OSD is provided; the following three (3) main elements shall be included in the OSD system design:
 - Temporary storage: Wherever reasonably practicable temporary storage is to consist of an open surface pond or an in-ground tank. Where these types of storage are not reasonably practicable, Council may consider allowing the use of above ground tanks. The storage must be designed to contain the excess volume of stormwater resulting from limits on the peak discharge flow rate.

- Discharge control pit or weir: A flow control pit or weir with a litter and sediment removal component must be included in the OSD design. An outlet/orifice shall be a minimum internal diameter or width of at least 25mm and protected by an approved mesh screen. An overflow structure must be designed to cater for flows exceeding the capacity of the OSD. The overflow structure must direct excess flows in a manner to minimise any detrimental effects on downstream properties.
 - Maintenance Scheme: A maintenance schedule/plan for the OSD must be submitted to the Council with the OSD engineering design. The cleaning of below-ground storage facilities shall be conducted in accordance with the requirements and risk control measures specified in AS 2865-2009 Confined Spaces.
- vi. All upgrades to the Council stormwater network shall be designed by a suitably qualified person to Council requirements.

Neither this letter nor any land use permit issued constitutes permission to connect the development to the stormwater network. This permission needs to be arranged separately by contacting Council at stormwater@burnie.tas.gov.au or call (03) 6430 5770.

The following basic information is required to enable Council to assess the request for permission to connect to the stormwater network:

Information in relation to stormwater quantity and conveyance requirements:

- Locations and sizes of pipes, pits, spoon drains etc.
- Invert levels for pipes, manholes, drains and culverts.
- Location and relative level of proposed connection to the stormwater network.
- Details of any proposed detention system and associated calculations (as aforementioned).
- 1% AEP storm event drainage path locations, direction, widths, and flow depths indicated on plans, including any existing flow paths that cross or are in close proximity to the proposed development.

Should you have any further queries, please contact Council's Technical Officer, Jon Randall on (03) 6430 5736 or email: jrandall@burnie.tas.gov.au

Yours faithfully



Jarrod Denny

MANAGER TECHNICAL SERVICES

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Jon Randall, Technical Officer – Burnie City Council

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Sustainable Transport Surveys Pty Ltd

ABN: 18 439 813 274

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19 September 2024

Albert Trinh
Director



Dear Albert

Re: 11-16/1 Atkins Drive, Romaine
Project No: 24506

I refer to your request for a Traffic Impact Assessment in relation to the proposed residential development at the above address. Burnie City Council has issued an RFI in response to the town planning application, which requests the following information:

4. Code 3.0 Road and Railway Asset Code

The proposal will result in an increase in vehicle traffic using an existing vehicle crossing.

Please provide a response to the relevant standards of the C3.0 Road and Railway Assets Code.

Our assessment is provided as follows. This takes into account the above.

1 EXISTING CONDITIONS

1.1 LOCATION & LAND USE

The subject site is in Romaine with frontage onto Mount Street in the north. It is bordered by vacant land in the south, Parklands High School in the east, and residential lots in the west. The subject site is part of a larger property and is included within a 'Staged Strata Scheme' on the property.

The overall property is irregular in shape, currently accommodates 16 existing dwellings and covers a total area of approximately 8,586 m². Conversely, the subject site covers an area of approximately 3,384 m² and is currently vacant. An accessway is centrally located within the existing development and connects to Atkins Drive via a single crossover.

The location of the site with respect to the surrounding road network is shown in **Figure 1**, which is followed by an aerial view of the site in **Figure 2**.

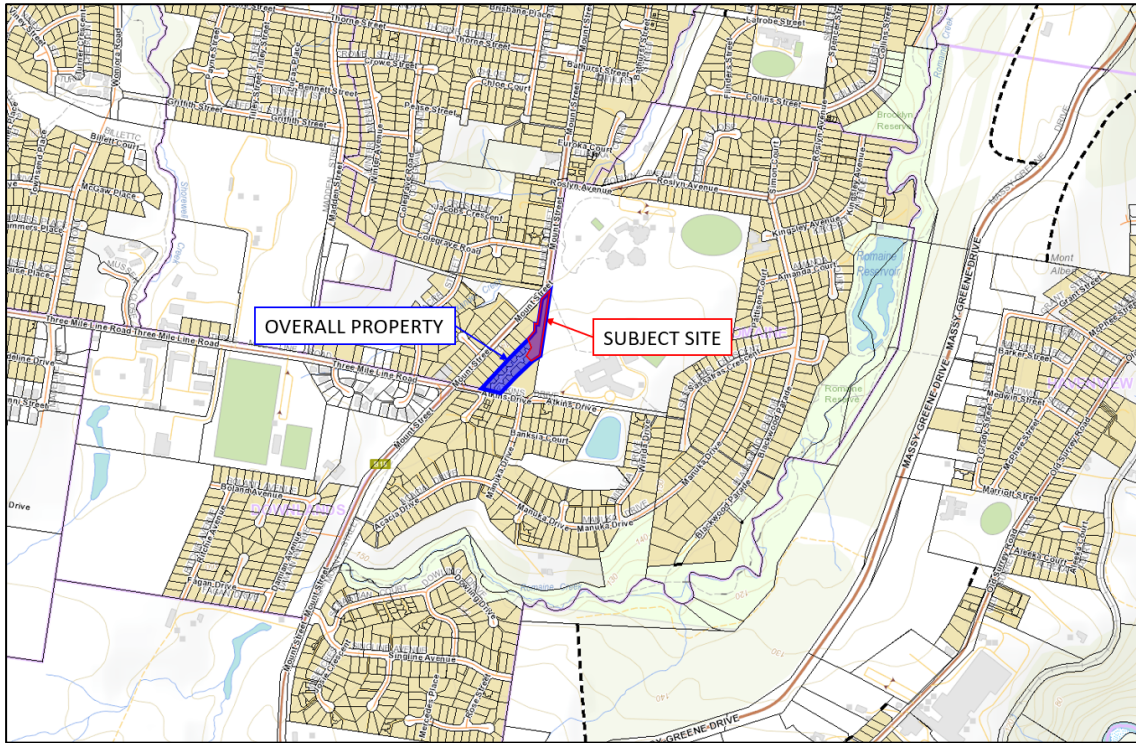


Figure 1 Subject site location (Source: The LIST)



Figure 2 Aerial view of subject site (Source: The LIST)

1.2 ZONING & OVERLAYS

Burnie City Council is the responsible authority, and the zoning of the land is 'General Residential'. The site is subject to the following overlays and codes:

- Inner protection area (Electricity Transmission Infrastructure Protection Code); and
- Electricity transmission corridor (Electricity Transmission Infrastructure Protection Code).

1.3 ROAD NETWORK

Atkins Drive is a sealed collector road that follows an east-west alignment and is under the care and management of Council. The carriageway is approximately 8.9m wide and accommodates two-way vehicular traffic with parking permitted on both sides. Paved footpaths that are about 1.8m wide have been provided on both sides. There is no posted speed limit, therefore the default speed limit of 50 km/h in urban areas applies; however, a school zone speed limit of 40 km/h applies between 8:00 – 9:00am and 2:30 – 3:30pm on school days. Atkins Drive is a short section of road that runs between Parklands High School in the east and Mount Street in the west.

Mount Street is a sealed arterial road that follows a northeast-southwest alignment in the vicinity of the site and is under the care and management of DSG. The carriageway is approximately 12.5m wide and provides one traffic lane in each direction with parking permitted on both sides. Paved footpaths that are about 1.8m wide have been provided on both sides. The posted speed limit is 60 km/h.

1.4 SUSTAINABLE TRANSPORT

1.4.1 Walkability

The site has very good access to pedestrian facilities with paved footpaths provided on both sides of most of the surrounding roads and the wider area encompassing a well-connected footpath network. Additionally, a pedestrian crossing with refuge has been provided on Mount Street near the subject site.

1.4.2 Cycling

There are no bicycle lanes or paths in the vicinity of the site and cycling occurs as a shared mode of transport on the roadways and footpaths.

1.4.3 Public Transport

The site has very good access to the bus network with the nearest bus stops located only a couple of metres from the property's access. The following Metro Tasmania bus routes operate in the area:

- 191 – Emu Heights & Havenview to Burnie via Shorewell Park;
- 194 – Hospital, Shorewell Plaza, Acton (Thorne St), Upper Burnie Circular; and
- 196 – Brooklyn & Romaine to Burnie via Aquatic Centre & Hillcrest.

1.5 CRASH HISTORY

A review of the Tasmanian vehicle crash data for the most recent 5-year period, ending 31 May 2023, has shown the following in terms of crashes in the area:

- 5 x property damage only crashes:
 - 2 x DCA 110: Cross traffic;
 - 1 x DCA 113: Right near;
 - 1 x DCA 149: Other manoeuvring; and
 - 1 x DCA 189: Other curve.
- 3 x first aid crashes:
 - 1 x DCA 110: Cross traffic;
 - 1 x DCA 121: Right through; and
 - 1 x DCA 160: Parked.
- 3 x minor crashes:
 - 3 x DCA 110: Cross traffic.

The crash trend during the most recent 5-year period shows that the recorded crashes generally had low levels of severity. Nevertheless, crashes that involved first aid and minor injuries are prevalent. Most of the recorded crashes occurred at the Mount Street / Atkins Drive intersection.

The crash history review area is shown in **Figure 3**. It is noted that the crash history review area includes data that precedes the 5-year period up to 31 May 2023; this data was excluded from the crash history review.

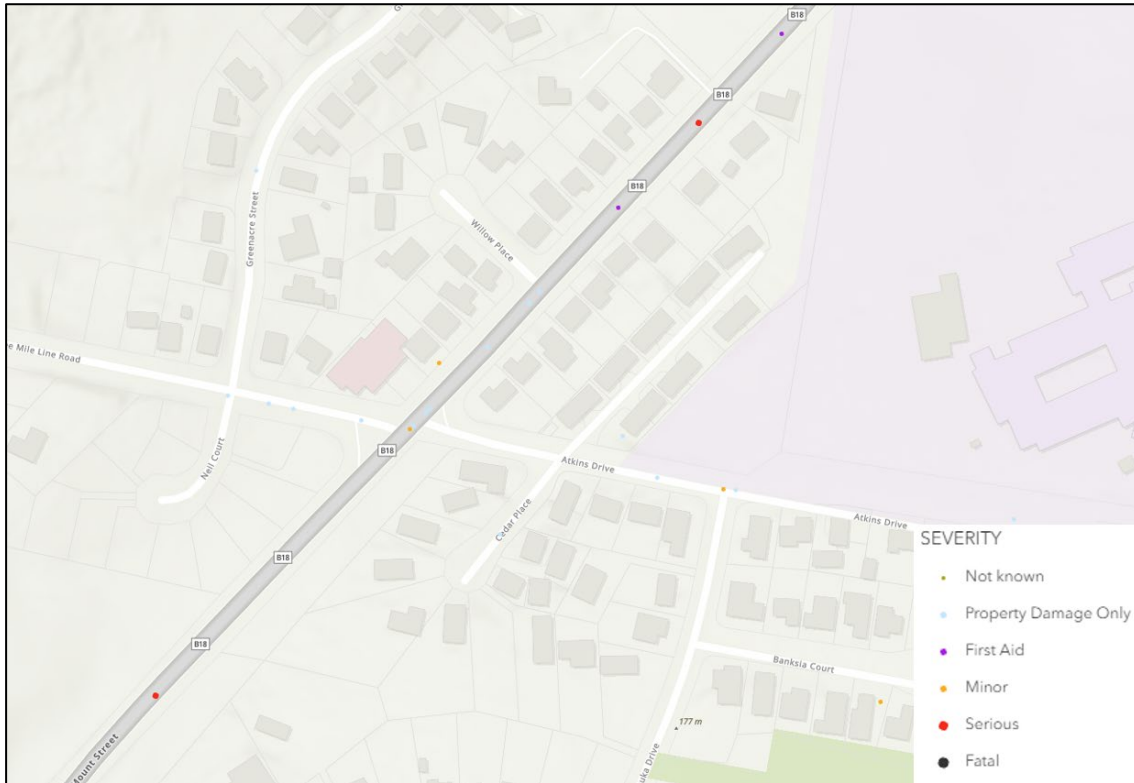


Figure 3 Crashes since 1 January 2009 (Source: ArcGIS / Department of State Growth)

2 PROPOSAL

It is proposed to develop eight (8) 2-bedroom residential dwellings on the subject site.

Each dwelling unit will be provided with two (2) parking spaces, including a single garage and an 'open' parking space. Four (4) additional parking spaces will be provided for visitors; the total on-site parking provision will thus be 20 spaces.

Access to the dwellings is proposed via an extension of the current accessway that serves the existing adjoining dwelling units in the southwest. This accessway connects to the external/public road network via a crossover to Atkins Drive.

The proposed site layout is attached as **APPENDIX 1** at the end of this letter.

3 CAR PARKING MATTERS

Statutory car parking requirements are specified in Code C2.0 (Parking and Sustainable Transport Code) of the Tasmanian Planning Scheme. The application triggers Clause C2.5 (Use Standards) and Clause C2.6 (Development Standards for Buildings and Works), and accordingly assessments against the relevant Controls under Clause C2.5 and Clause C2.6 are provided in **Table 1** and **Table 2**, respectively.



Table 1 Clause C2.5 (Use Standards)

Control C2.5.1 – Car parking numbers	
Objective: That an appropriate level of car parking spaces are provided to meet the needs of the use.	
Acceptable Solution	Performance Criteria
<p>A1</p> <p>The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme, excluding if:</p> <ul style="list-style-type: none"> the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; the site is contained within a parking precinct plan and subject to Clause C2.7; the site is subject to Clause C2.5.5; or it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or; the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: $N = A + (C - B)$ <p>N = Number of on-site car parking spaces required A = Number of existing on site car parking spaces B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1 C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.</p> 	<p>P1.1</p> <p>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> the availability of off-street public car parking spaces within reasonable walking distance of the site; the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> variations in car parking demand over time; or efficiencies gained by consolidation of car parking spaces; the availability and frequency of public transport within reasonable walking distance of the site; the availability and frequency of other transport alternatives; any site constraints such as existing buildings, slope, drainage, vegetation and landscaping; the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; the effect on streetscape; and any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development. <p>P1.2</p> <p>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> the nature and intensity of the use and car parking required; the size of the dwelling and the number of bedrooms; and the pattern of parking in the surrounding area.
<p>Response</p> <p>A1</p> <p>As per Table C2.1, the car parking requirements are 2 spaces per dwelling for a 2 or more bedroom dwelling in the General Residential Zone (including all rooms capable of being used as a bedroom) plus 1 dedicated space per 3 dwellings on an internal lot (rounded up to the nearest whole number) for visitor parking for multiple</p>	

dwelling in the General Residential Zone. Accordingly, the proposal has a statutory requirement to provide the following number of parking spaces:

- 8 2-bedroom dwellings – 16 spaces; and
- 8 dwellings – 3 visitor spaces.

The proposal therefore has a statutory requirement to provide a total of 19 parking spaces on-site. As 20 spaces will be provided, including 4 visitor spaces, the proposal complies with Acceptable Solution A1.

Control C2.5.2 – Bicycle parking numbers

Objective:

That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.

Acceptable Solution	Performance Criterion
<p>A1</p> <p>Bicycle parking spaces must:</p> <ul style="list-style-type: none"> • be provided on the site or within 50m of the site; and • be no less than the number specified in Table C2.1. 	<p>P1</p> <p>Bicycle parking spaces must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> • the likely number of users of the site and their opportunities and likely need to travel by bicycle; and • the availability and accessibility of existing and any planned parking facilities for bicycles in the surrounding area.

Response

A1

As per Table C2.1, there are no bicycle parking requirements for dwellings in the General Residential Zone.

Control C2.5.3 – Motorcycle parking numbers

Objective:

That the appropriate level of motorcycle parking is provided to meet the needs of the use.

Acceptable Solution	Performance Criterion
<p>A1</p> <p>The number of on-site motorcycle parking spaces for all uses must:</p> <ul style="list-style-type: none"> • be no less than the number specified in Table C2.4; and • if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained. 	<p>P1</p> <p>Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> • the nature of the proposed use and development; • the topography of the site; • the location of existing buildings on the site; • any constraints imposed by existing development; and • the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area.

Response

A1

As per Table C2.4, a use that requires 0–20 car parking spaces does not have a requirement to provide any motorcycle parking spaces. Given that the proposal is required to provide 19 car parking spaces, it does not have any motorcycle parking requirements.

Table 2 Clause C2.6 (Development Standards for Buildings and Works)

Control C2.6.1 – Construction of parking areas	
Objective: That parking areas are constructed to an appropriate standard.	
Acceptable Solution	Performance Criterion
<p>A1</p> <p>All parking, access ways, manoeuvring and circulation spaces must:</p> <ul style="list-style-type: none"> ▪ be constructed with a durable all weather pavement; ▪ be drained to the public stormwater system, or contain stormwater on the site; and; ▪ excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement. 	<p>P1</p> <p>All parking, access ways, manoeuvring and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to:</p> <ul style="list-style-type: none"> ▪ the nature of the use; ▪ the topography of the land; ▪ the drainage system available; ▪ the likelihood of transporting sediment or debris from the site onto a road or public place; ▪ the likelihood of generating dust; and ▪ the nature of the proposed surfacing.
<p>Response</p> <p><u>A1</u></p> <p>All parking areas, access ways, and manoeuvring and circulation spaces will be constructed with durable all-weather pavement and will be drained to the public stormwater system. The proposal therefore complies with Acceptable Solution A1.</p>	
Control C2.6.2 – Design and layout of parking areas	
Objective: That parking areas are designed and laid out to provide convenient, safe and efficient parking.	
Acceptable Solutions	Performance Criterion
<p>A1.1</p> <p>Parking, access ways, manoeuvring and circulation spaces must either:</p> <ul style="list-style-type: none"> ▪ comply with the following: <ul style="list-style-type: none"> – have a gradient in accordance with Australian Standard AS 2890 – Parking facilities, Parts 1–6; – provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces; – have an access width not less than the requirements in Table C2.2; – have car parking space dimensions which satisfy the requirements in Table C2.3; – have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces; 	<p>P1</p> <p>All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:</p> <ul style="list-style-type: none"> ▪ the characteristics of the site; ▪ the proposed slope, dimensions and layout; ▪ useability in all weather conditions; ▪ vehicle and pedestrian traffic safety; ▪ the nature and use of the development; ▪ the expected number and type of vehicles; ▪ the likely use of the parking areas by persons with a disability; ▪ the nature of traffic in the surrounding area; ▪ the proposed means of parking delineation; and ▪ the provisions of Australian Standards AS 2890.1:2004 – Parking facilities, Part 1: Off-street

- have a vertical clearance of not less than 2.1m above the parking surface level; and
- excluding a single dwelling, be delineated by line marking or other clear physical means; or
- comply with Australian Standard AS 2890- Parking facilities, Parts 1-6.

car parking and AS 2890.2:2002 – Parking facilities, Part 2: Off-street commercial vehicle facilities.

A1.2

Parking spaces provided for use by persons with a disability must satisfy the following:

- be located as close as practicable to the main entry point to the building;
- be incorporated into the overall car park design; and
- be designed and constructed in accordance with Australian/New Zealand Standards AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities.

Response

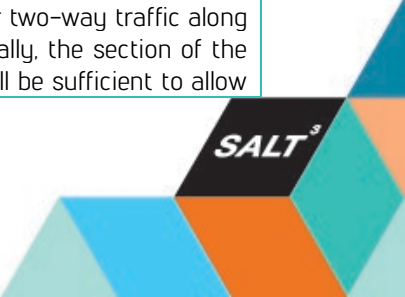
A1.1

- SALT has assessed the proposed site layout in terms of gradients, and we are satisfied that gradients on the site comply with the relevant parts of AS 2890.1:2004 at all parking areas, access ways, and manoeuvring spaces. This is subject to the final detailed design achieving the maximum gradients shown on the 'Driveway and Parking Setout Plan'.
- Vehicles will be able to enter and exit the site in a forward direction as demonstrated by the swept path diagrams attached as **APPENDIX 2** at the end of this letter. It is noted that vehicles may be required to perform 3-point turns to access proposed unit 8's parking area; however, this is considered acceptable for the proposed land use and is compliant with AS 2890.1:2004 for User Class 1A (residential) car parking;
- Table C2.2 specifies the following access width requirements:

Table C2.2 Internal Access Way Widths for Vehicles

Number of parking spaces served	Internal access way widths	Passing bay dimensions for two-way traffic in addition to the access way width
1 to 5	A width not less than 3m.	2m wide by 5m long, plus entry and exit tapers, every 30m, unless on land within the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone or Open Space Zone.
6 to 20	(a) A width not less than 4.5m for the first 7m from the road carriageway and 3m thereafter, and (b) At changes of direction or intersections have: (i) an internal radius of not less than 4m, or (ii) a width more than 4.2m.	2m wide by 5m long, plus entry and exit tapers, every 30m.
21 and over	A width not less than 5.5m.	Not applicable

- The existing access way that connects to the road carriageway (Atkins Drive) has a minimum width of approximately 6.2m along its entire length (~135m). The proposed extension of this access way will have a minimum width of about 4.2m along its entire length.
- The section of the access way adjoining the visitor car parking area will effectively be widened to about 5.7m over a length of 14m, which is suitable to serve as a passing bay for two-way traffic along the access way – refer to the swept path diagrams at **APPENDIX 2**. Additionally, the section of the access way next to proposed units 6 and 7 will be at least 6.0m wide, which will be sufficient to allow



two cars to pass each other. Refer to the proposed site layout attached as **APPENDIX 1** at the end of this letter.

- Table C2.3 specifies the following car parking space dimensions:

Angle of car spaces to manoeuvring space	Combined access and manoeuvring width	Car park widths	Car park length
Parallel	3.6m	2.3m	6.7m
45 degrees	3.5m	2.6m	5.4m
60 degrees	4.9m	2.6m	5.4m
90 degrees	6.4m	2.6m	5.4m
90 degrees	5.8m	2.8m	5.4m
90 degrees	5.2m	3m	5.4m
90 degrees	4.8m	3.2m	5.4m

Notes to Table C2.3:

(1) If entry to the car space is from a road, the combined access and manoeuvring width may include the road.

- The visitor car parking spaces will be angled at 90°, be 5.4m long x 3.0m wide and have a combined access and manoeuvring width of about 6.2m;
- All garages will comply with the relevant parts of AS 2890.1:2004 for the design of enclosed car parking. Parking spaces in front of garages will be at least 6.5m long x 3.4m wide. Swept path diagrams have been prepared that show suitable vehicle access to key parking spaces – see **APPENDIX 2**.
- All garages will have vertical clearances of at least 2.1m above the parking surface level.
- The visitor car parking spaces will be delineated with appropriate line marking.

A12

The proposed dwellings fall under the Class 1a buildings classification as per the Building Code of Australia. According to the Code, Class 1a buildings do not have a requirement to provide accessible car parking spaces.

Control C2.6.3 – Number of accesses for vehicles

Objective:

That:

- access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;
 - accesses do not cause an unreasonable loss of amenity of adjoining uses; and
- the number of accesses minimise impacts on the streetscape.

Acceptable Solutions	Performance Criteria
<p>A1</p> <p>The number of accesses provided for each frontage must:</p> <ul style="list-style-type: none"> • be no more than 1; or • no more than the existing number of accesses whichever is the greater. <p>A2</p> <p>Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.</p>	<p>P1</p> <p>The number of accesses for each frontage must be minimised, having regard to:</p> <ul style="list-style-type: none"> • any loss of on-street parking; and • pedestrian safety and amenity; • traffic safety; • residential amenity on adjoining land; and • the impact on the streetscape. <p>P2</p> <p>Within the Central Business Zone or in a pedestrian priority street, any new accesses must:</p>

	<ul style="list-style-type: none"> ▪ not have an adverse impact on: <ul style="list-style-type: none"> – pedestrian safety and amenity; or – traffic safety; and <p>be compatible with the streetscape.</p>
--	---

Response

A1

The proposed development will use one (1) existing access and will not require provision of any additional accesses. The proposal therefore complies with Acceptable Solution A1.

A2

Not applicable to the proposal.

Control C2.6.5 – Pedestrian access

Objective:

That pedestrian access within parking areas is provided in a safe and convenient manner.

Acceptable Solutions	Performance Criterion
<p>A1.1</p> <p>Uses that require 10 or more car parking spaces must:</p> <ul style="list-style-type: none"> ▪ have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by: <ul style="list-style-type: none"> – a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or – protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and ▪ be signed and line marked at points where pedestrians cross access ways or parking aisles. <p>A1.2</p> <p>In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.</p>	<p>P1</p> <p>Safe and convenient pedestrian access must be provided within parking areas, having regard to:</p> <ul style="list-style-type: none"> ▪ the characteristics of the site; ▪ the nature of the use; ▪ the number of parking spaces; ▪ the frequency of vehicle movements; ▪ the needs of persons with a disability; ▪ the location and number of footpath crossings; ▪ vehicle and pedestrian traffic safety; ▪ the location of any access ways or parking aisles; and ▪ any protective devices proposed for pedestrian safety.

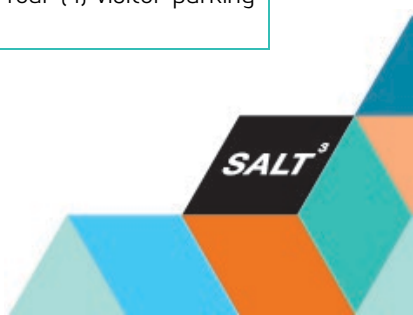
Response

A1.1

The proposed footpaths will not be separated from the access ways by either 2.5m horizontal clearances or protective devices. Assessment against the performance criteria is therefore required.

P1

- The site constitutes a vacant lot that is the balance within an existing strata development and is characterised by a low-speed environment.
- The proposed use is residential, which is similar to the surrounding land uses and properties.
- Each dwelling unit will be provided with two (2) designated parking spaces, while four (4) visitor parking spaces will also be provided, thus equalling a total of 20 parking spaces.



- The proposed development is expected to generate low levels of vehicular traffic (see **Section 5.1**) and as such will lend itself to safe interaction between pedestrians and vehicles.
- The proposed residential use of the site means that use of the site by people with a disability will be low compared to other uses (e.g., commercial, retail, etc.). Suitable driveway widths will be provided to ensure that a vehicle and pedestrian with a disability will be able to pass each other. Furthermore, each dwelling unit's designated parking spaces will also be situated in very close proximity to its entrance, which will provide safe and convenient access for all users.
- Footpaths will be provided alongside the access way – these footpaths will have finished surface levels of 150mm above the adjoining access way to provide separation between these elements. Line marked pedestrian crossings will also be provided in key areas – refer to the proposed site layout attached as **APPENDIX 1** at the end of this report.
- The low levels of vehicular traffic that the site is expected to generate, the low-speed environment of the proposed development, and the accommodations for pedestrians will collectively ensure a safe environment for both pedestrians and vehicles.
- The proposed layout of the access ways / parking aisles will ensure a safe and convenient shared environment for all users. The proposed layout will also ensure adequate sight distances at key areas.
- No protective devices are proposed given the nature of the site, which comprises an environment characterised by low levels of vehicular speeds and traffic volumes. The proposed 150mm difference in levels between the footpaths and access way will also contribute to pedestrian safety.

A12

Not applicable to the proposal.

4 WASTE COLLECTION

Kerbside waste collection is undertaken by a Council waste truck for the existing development on the property and this arrangement will continue for the proposed development.

5 TRAFFIC IMPACT

5.1 TRAFFIC GENERATION & DISTRIBUTION

Transport for New South Wales (TfNSW, formerly Roads and Maritime Services [RMS], Roads & Traffic Authority [RTA]) has published updated traffic generation rates in Technical Direction TDT 2013/04a, which are based on traffic surveys at developments within the Sydney urban area as well as within regional NSW. Given Romaine's locality with respect to Burnie and more broadly TAS, adopting the regional traffic generation rates is considered appropriate. The applicable traffic generation rates are as follows:

- Daily vehicle trips – 7.4 per dwelling;
- Average weekday AM peak hour vehicle trips – 0.71 per dwelling; and
- Average weekday PM peak hour vehicle trips – 0.78 per dwelling.

Applying the above rates to the proposed eight (8) dwellings means that the development is expected to generate approximately 59 vehicle trips per day and about 6 vehicle trips during both the weekday AM and PM peak hours.

The following inbound / outbound splits are typical for residential developments:

- Weekday AM peak hour: 20% inbound / 80% outbound
- Weekday PM peak hour: 60% inbound / 40% outbound

Applying these splits to the expected traffic generation of 6 vehicle trips results in the following:

- Weekday AM peak hour: 1 inbound / 5 outbound
- Weekday PM peak hour: 4 inbound / 2 outbound

Regarding the directional split of traffic, it is anticipated that most of the generated traffic (~85 – 90%) will travel to/from the north on Mount Street, while the remainder (~10 – 15%) will travel to/from the south on Mount Street and to/from the west on Three Mile Line Road.

The proposed development will evidently exhibit traffic generation that is considered very low from a traffic engineering perspective, with the 6 vehicle trips equating to 1 vehicle movement, on average, every 10 minutes during the peak hour.

Accordingly, we are satisfied that the traffic likely to be generated by the proposed development will be readily accommodated by Atkins Drive, Mount Street, and the surrounding road network and intersections without resulting in any detrimental impacts.

5.2 ROAD AND RAILWAY ASSESTS CODE

The application triggers Clause C3.5 (Use Standards) under Code C3.0 Road and Railway Assets Code and an assessment against Clause C3.5 is provided in **Table 3**.

Table 3 Clause C3.5 (Use Standards)

Control C3.5.1 – Traffic generation at a vehicle crossing, level crossing or new junction	
Objective: To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.	
Acceptable Solutions	Performance Criterion
<p>A1.1</p> <p>For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:</p> <ul style="list-style-type: none"> • a new junction; • a new vehicle crossing; or • a new level crossing. <p>A1.2</p> <p>For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.</p> <p>A1.3</p> <p>For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.</p> <p>A1.4</p> <p>Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</p> <ul style="list-style-type: none"> • the amounts in Table C3.1; or • allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road. <p>A1.5</p> <p>Vehicular traffic must be able to enter and leave a major road in a forward direction.</p>	<p>P1</p> <p>Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</p> <ul style="list-style-type: none"> • any increase in traffic caused by the use; • the nature of the traffic generated by the use; • the nature of the road; • the speed limit and traffic flow of the road; • any alternative access to a road; • the need for the use; • any traffic impact assessment; and • any advice received from the rail or road authority.

Response

A1.1

Not applicable.

A1.2

No new junctions, vehicle crossings, or level crossings are proposed given that the proposed development will use an existing access. Written consent is therefore not required.

A1.3

Not applicable.

A1.4

Table C3.1 specifies the following acceptable increases in traffic volumes to and from the site – it is noted that according to Clause C3.3, annual average daily traffic means the number of vehicles per day averaged over all days in a calendar year.

Location of vehicular traffic	Amount of acceptable increase in annual average daily traffic to and from the site (total of ingress and egress)	
	Vehicles up to 5.5m long	Vehicles longer than 5.5m long
Vehicle crossing on major roads and private level crossings	10% or 10 vehicle movements per day, whichever is the greater	10%
Vehicle crossings on other roads	20% or 40 vehicle movements per day, whichever is the greater	20% or 5 vehicle movements per day, whichever is the greater

The part of Table C3.1 that is relevant to the proposal is vehicle crossings on other roads, for vehicles up to 5.5m long, thus *20% or 40 vehicle movements per day, whichever is the greater*. As outlined in **Section 1.1** and **Section 2**, the proposal comprises the addition of eight (8) new dwellings to the existing 16 dwellings on the property, thus representing a 50% increase in vehicle movements per day. As also outlined in **Section 5.1**, the proposed development is anticipated to generate on average approximately 59 vehicle movements per day. The expected increase in annual average daily traffic therefore exceeds the acceptable parameters specified in Table C3.1 and assessment against the performance criteria is therefore required.

A1.5

Vehicles will be able to enter and exit all roads in a forward direction.

P1

- As outlined in **Section 5.1**, the proposal is anticipated to result in approximately 59 additional vehicle trips per day added to the road network, with about 6 of those trips occurring in each commuter peak hour. This level of traffic generation represents a negligible increase in the context of the surrounding road network and broader area, which is largely zoned 'General Residential'. The proposed development and its associated traffic generation is therefore appropriate for the area.
- The proposed development is expected to generate predominantly light vehicle traffic, which is already provided for on the surrounding road network.
- The road is a short section (~375m total length) that serves a limited geographical area that primarily includes residential lots and a school – the road therefore has adequate spare capacity to accommodate the development traffic. There are also not many greenfield sites remaining in this area, which means that any significant increases in traffic demands on the road are unrealistic.
- The default urban speed limit of 50 km/h applies; however, a school zone speed limit of 40 km/h applies between 8:00 – 9:00am and 2:30 – 3:30pm on school days. The speed limits are thus consistent with the safe and efficient access to the surrounding area and the proposed development.
- There are no alternative accesses to the proposed development.

- The proposed development will provide additional residential dwellings in Romaine and is consistent with the surrounding developments as well as being an appropriate land use for the zoning in the area.
- This Traffic Impact Assessment has been prepared for the proposed development and concludes that the development is not expected to result in any adverse impacts in terms of the safety and operation of the surrounding road network.
- DSG owns and maintains Mount Street, whereas Burnie City Council owns and maintains all the other roads in the surrounding area. No written advice has been received from the rail or road authority.

6 RESPONSE TO COUNCIL RFI

As described at the start of this letter, Council has issued an RFI that requires the preparation of a Traffic Impact Assessment that addresses the relevant matters under Code C3.0 Road and Railway Assets Code of the Tasmanian Planning Scheme.

The relevant Acceptable Solutions and Performance Criteria have been comprehensively addressed in **Table 3** under **Section 5.2** of this Traffic Impact Assessment letter.

7 CONCLUSION

Based on the considerations outlined in this letter, it is concluded that:

- The proposal relates to the development of eight (8) 2-bedroom dwellings.
- The subject site is part of a larger property and is included within a 'Staged Strata Scheme' on the overall property.
- Access to the subject site will be via an extension of an existing accessway and crossover to Atkins Drive that serves the current development on the property.
- The proposed development has a statutory requirement to provide a total of 19 car parking spaces.
- With 20 car parking spaces provided on site, the proposed parking provision complies with the statutory requirement.
- The proposed car park layout and access arrangements comply with the relevant requirements of the Planning Scheme and Australian Standards and will provide for convenient and efficient access.
- The proposed development does not have a statutory requirement to provide bicycle parking.
- The proposed development does not have a statutory requirement to provide motorcycle parking.
- The traffic that is likely to be generated by the proposed development will be readily accommodated by Atkins Drive, Mount Street, and the surrounding road network and intersections without resulting in any detrimental impacts.
- In instances where the application does not comply with relevant Acceptable Solutions, it has been demonstrated that the associated Performance Criteria are satisfied.
- There are no traffic engineering reasons why a Planning Permit should not be issued for the proposed development, subject to appropriate conditions.

If there are any enquiries in relation to this assessment, I can be contacted on the number below.

Yours sincerely,



Jarrod Wicks
Director
SALT
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M: 0439 340 139
jarrod.wicks@salt3.com.au

APPENDIX 1 PROPOSED SITE LAYOUT



ATKINS DRIVE

MOUNT STREET



 **LOCATION PLAN**
Scale 1 : 500

n+b

22 Fieldings Way
Ulverstone, Tasmania
Australia
7315

m 0417 134 369 e nick@nplusb.com.au
License No. 047538582 ABN 946 222 219 16

Issued As
PRELIMINARY

Scale A2
1 : 500

Revision No.	Date	Description
A	17/11/23	Issued as PRELIMINARY

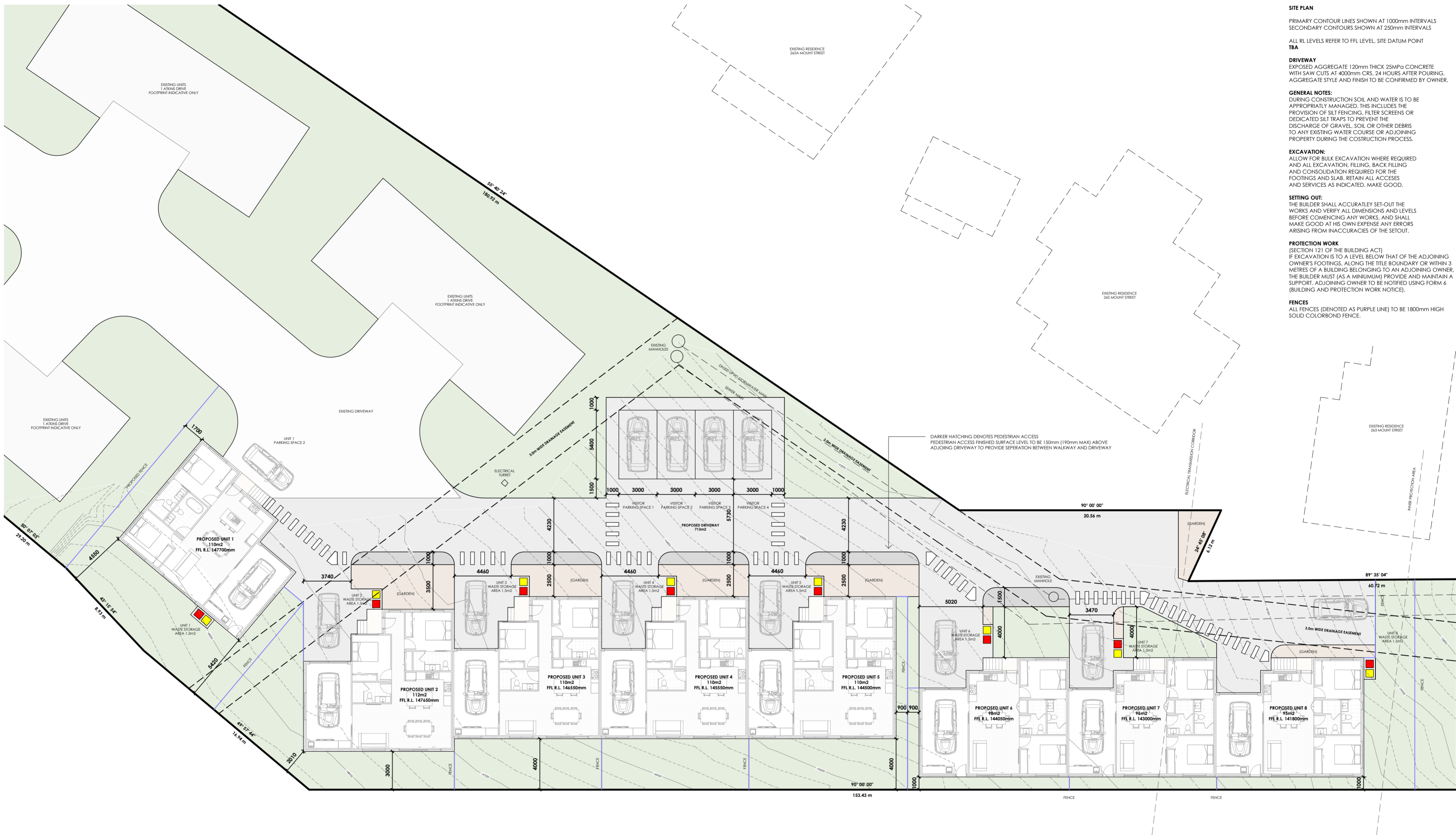
do not scale off plans
all dimensions are in millimeters
confirm all dimensions on site
all work relevant NCC & AS

Project
PROPOSED UNIT DEVELOPMENT
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

Sheet Title
LOCATION PLAN

Drawn	Issue Date	Project No.	Revision
NJB	17/11/23	TBA	A

Sheet Number
A101
/A111



SITE PLAN
 PRIMARY CONTOUR LINES SHOWN AT 1000mm INTERVALS
 SECONDARY CONTOURS SHOWN AT 250mm INTERVALS
 ALL RL LEVELS REFER TO FFL LEVEL, SITE DATUM POINT
 TBA

DRIVEWAY
 EXPOSED AGGREGATE 120mm THICK 25MPa CONCRETE
 WITH SAW CUTS AT 4000mm CRS, 24 HOURS AFTER POURING.
 AGGREGATE STYLE AND FINISH TO BE CONFIRMED BY OWNER.

GENERAL NOTES:
 DURING CONSTRUCTION SOIL AND WATER IS TO BE
 APPROPRIATELY MANAGED, THIS INCLUDES THE
 PROVISION OF SILT FENCING, FILTER SCREENS OR
 DEDICATED SILT TRAPS TO PREVENT THE
 DISCHARGE OF GRAVEL, SOIL OR OTHER DEBRIS
 TO ANY EXISTING WATER COURSE OR ADJOINING
 PROPERTY DURING THE CONSTRUCTION PROCESS.

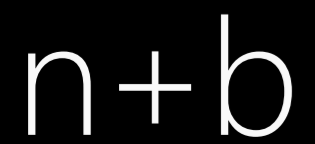
EXCAVATION:
 ALLOW FOR BULK EXCAVATION WHERE REQUIRED
 AND ALL EXCAVATION, FILLING, BACK FILLING
 AND CONSOLIDATION REQUIRED FOR THE
 FOOTINGS AND SLAB. RETAIN ALL ACCESSES
 AND SERVICES AS INDICATED. MAKE GOOD.

SETTING OUT:
 THE BUILDER SHALL ACCURATELY SET-OUT THE
 WORKS AND VERIFY ALL DIMENSIONS AND LEVELS
 BEFORE COMMENCING ANY WORKS, AND SHALL
 MAKE GOOD AT HIS OWN EXPENSE ANY ERRORS
 ARISING FROM INACCURACIES OF THE SETOUT.

PROTECTION WORK
 (SECTION 121 OF THE BUILDING ACT)
 IF EXCAVATION IS TO A LEVEL BELOW THAT OF THE ADJOINING
 OWNER'S FOOTINGS, ALONG THE TITLE BOUNDARY OR WITHIN 3
 METRES OF A BUILDING BELONGING TO AN ADJOINING OWNER,
 THE BUILDER MUST (AS A MINIMUM) PROVIDE AND MAINTAIN A
 SUPPORT, ADJOINING OWNER TO BE NOTIFIED USING FORM 6
 (BUILDING AND PROTECTION WORK NOTICE).

FENCES
 ALL FENCES (DENOTED AS PURPLE LINE) TO BE 1800mm HIGH
 SOLID COLORBOND FENCE.

SITE PLAN
 Scale 1 : 200



22 Fieldings Way
 Ulverstone, Tasmania
 Australia
 7315
 m 0417 134 369 e nick@nplusb.com.au
 License No. 047538582 ABN 946 222 219 16

Issued As
PRELIMINARY

Scale A2
1 : 200

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Revision
 No. 17/11/23 Description
 A Issued as PRELIMINARY

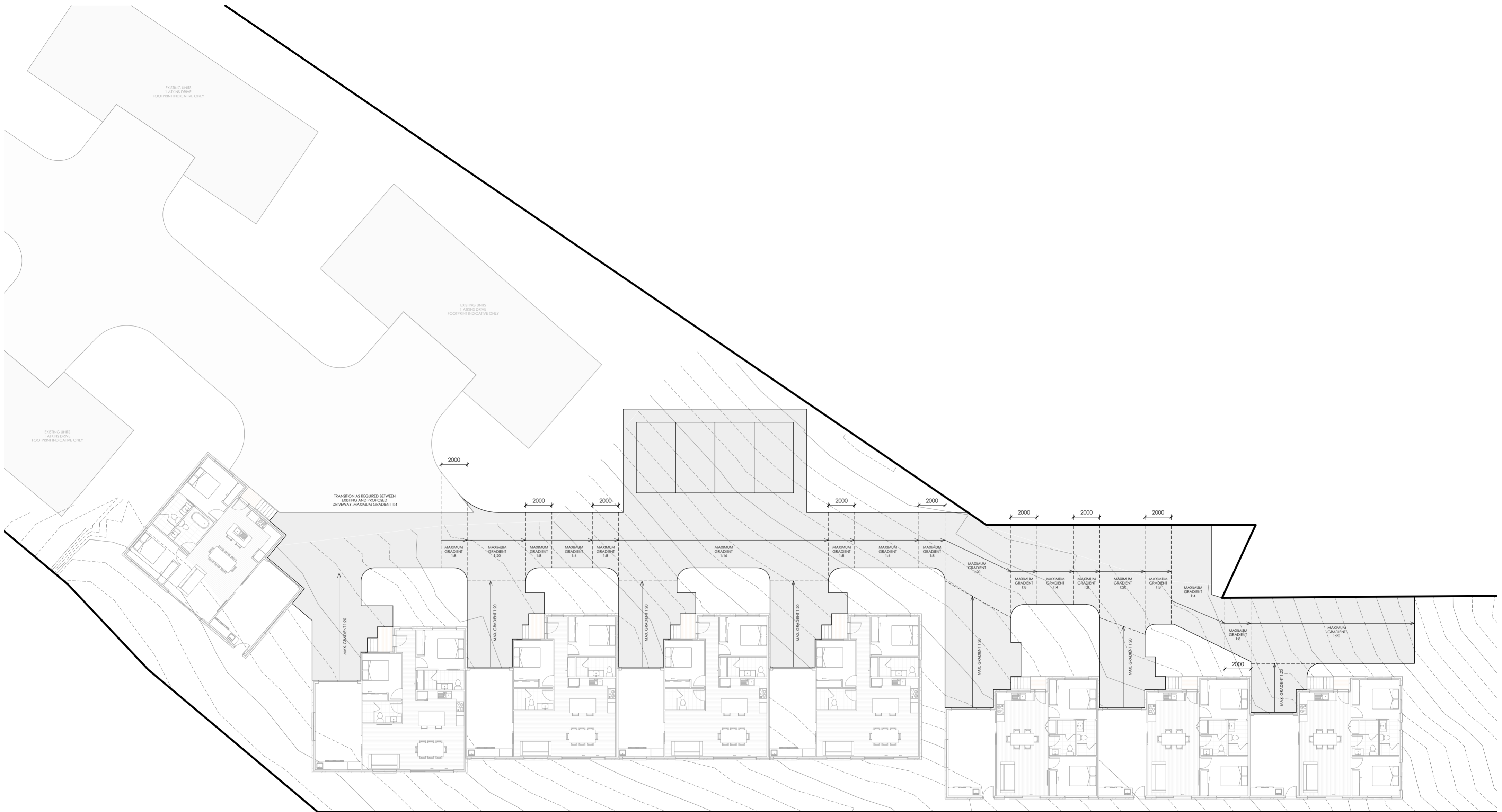
do not scale off plans
 all dimensions are in millimeters
 confirm all dimensions on site
 all work relevant NCC & AS

Project
PROPOSED UNIT DEVELOPMENT
 Location
11-1 ATKINS DRIVE, ROMAINE
 Client
ATKINS DRIVE PTY LTD

Sheet Title
SITE PLAN

Drawn Issue Date Project No. Revision
NJB 17/11/23 TBA A

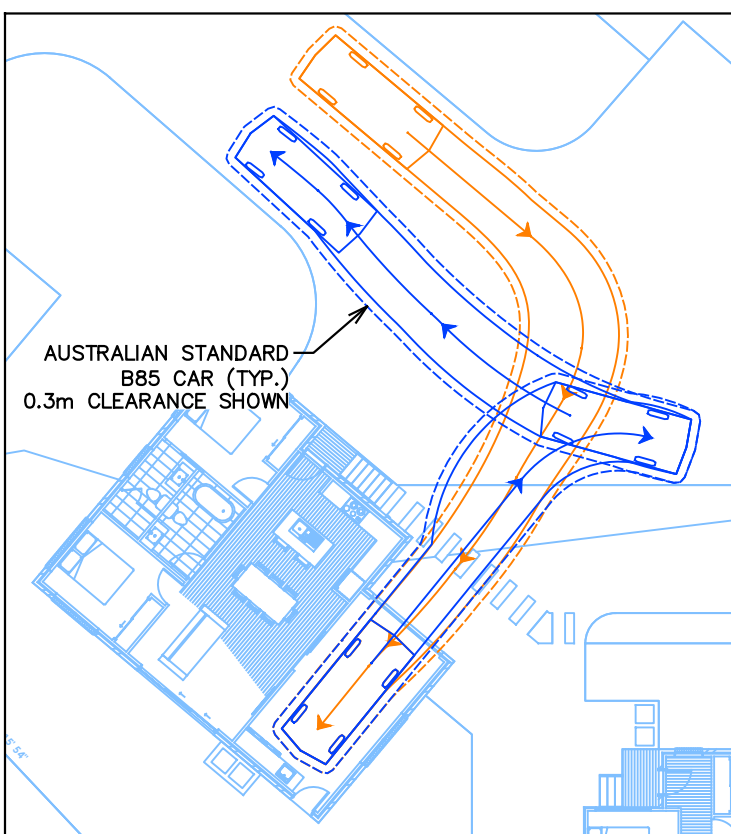
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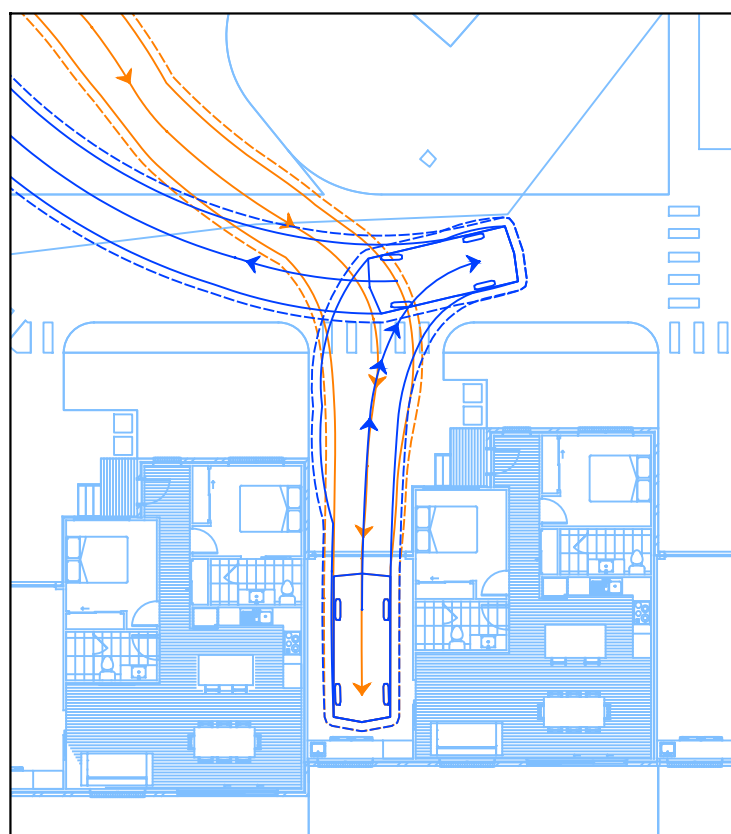
 **DRIVEWAY & PARKING SETOUT PLAN**
Scale 1 : 200

APPENDIX 2 SWEPT PATH DIAGRAMS

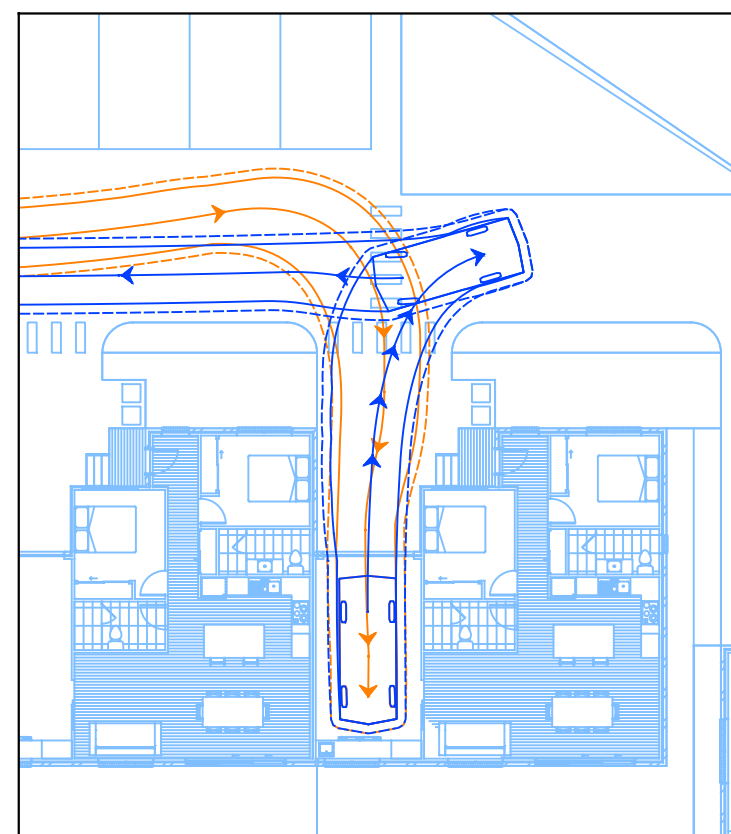




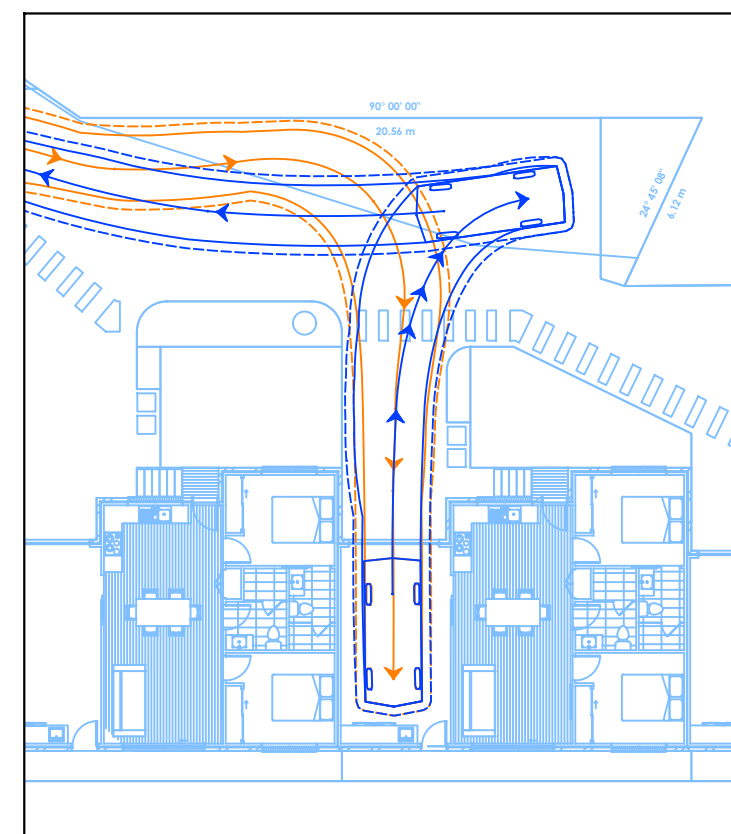
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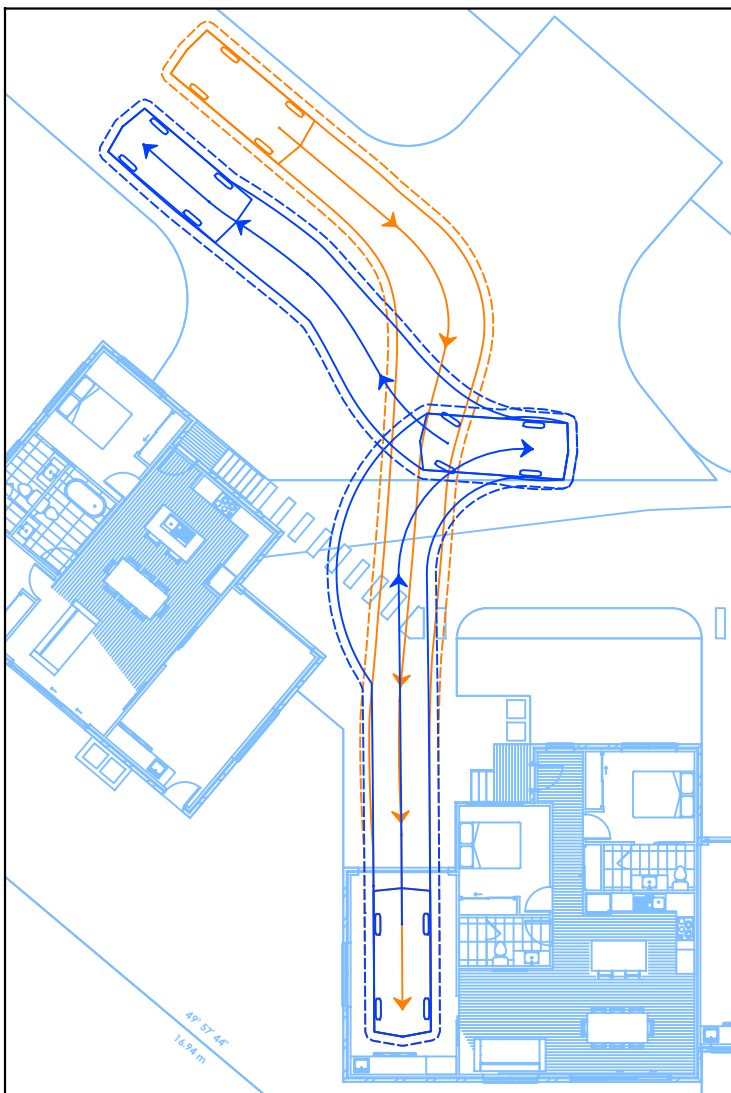
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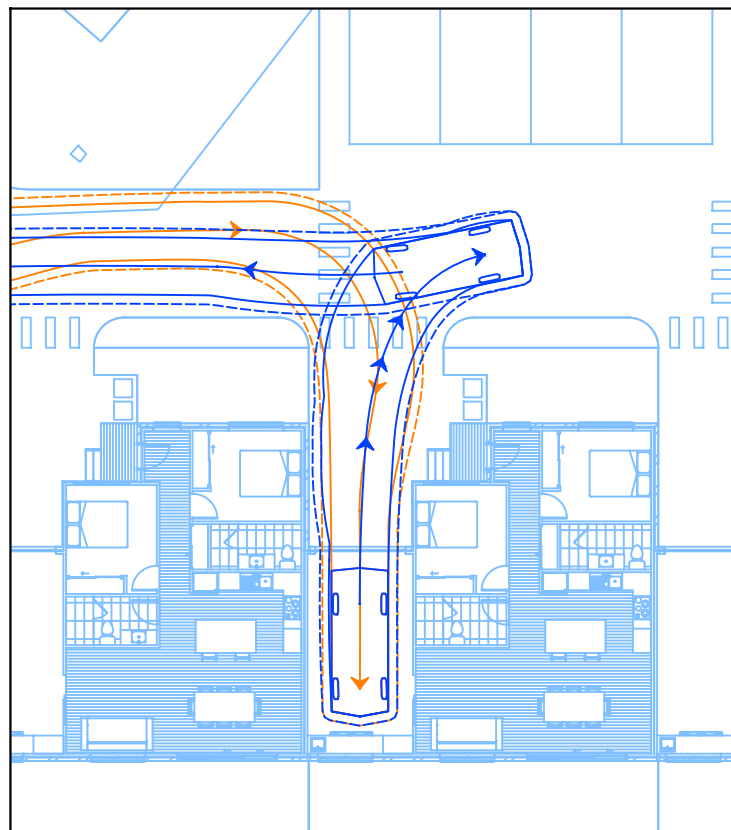
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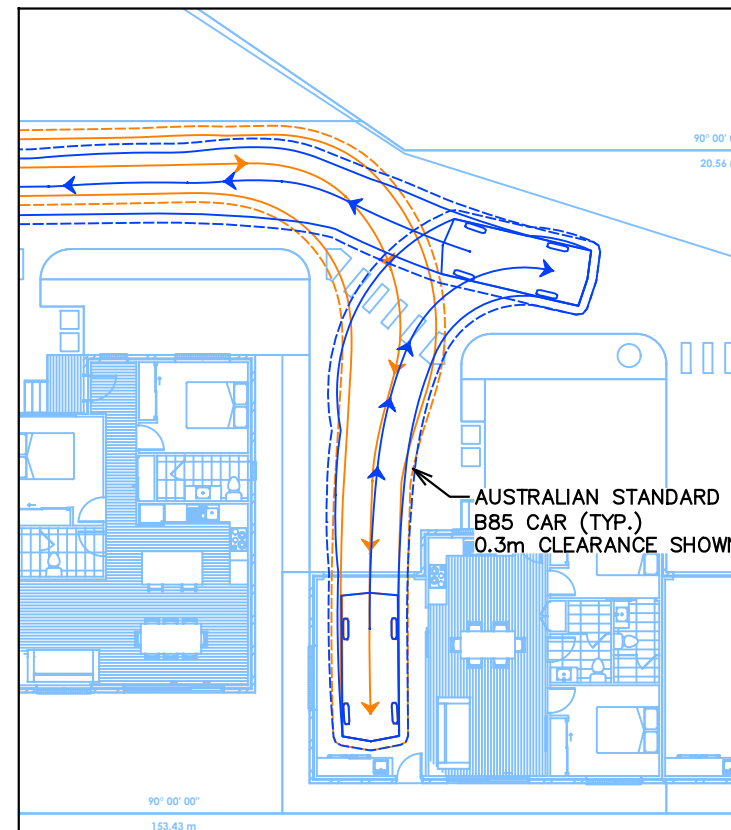
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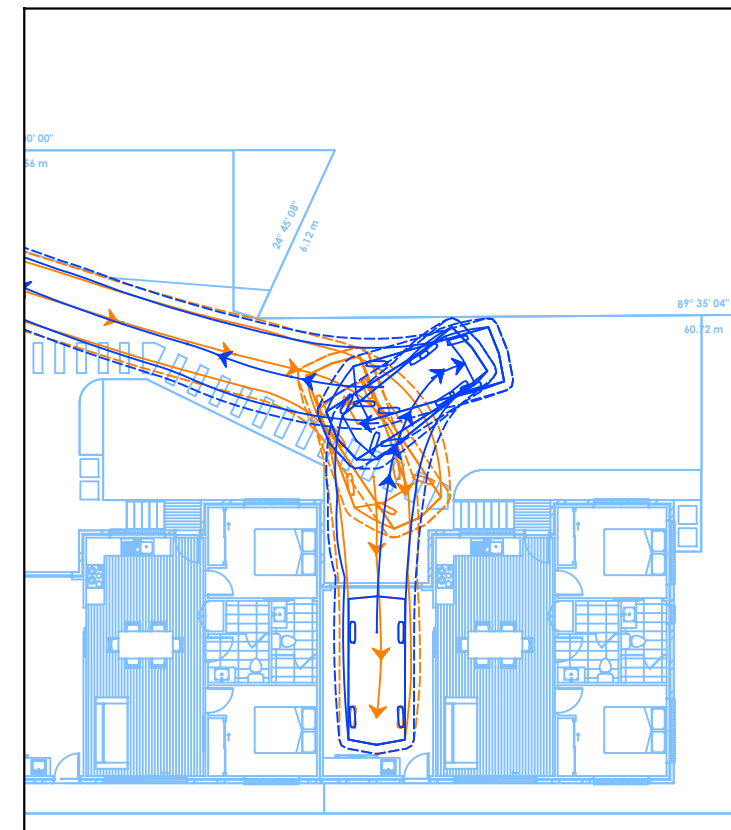
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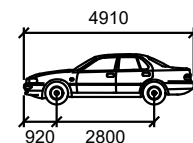
DWELLING UNIT 4 ACCESS



DWELLING UNIT 6 ACCESS



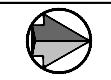
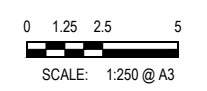
DWELLING UNIT 8 ACCESS



B85

Width	: 1870
Track	: 1770
Lock to Lock Time	: 6.0
Steering Angle	: 34.1

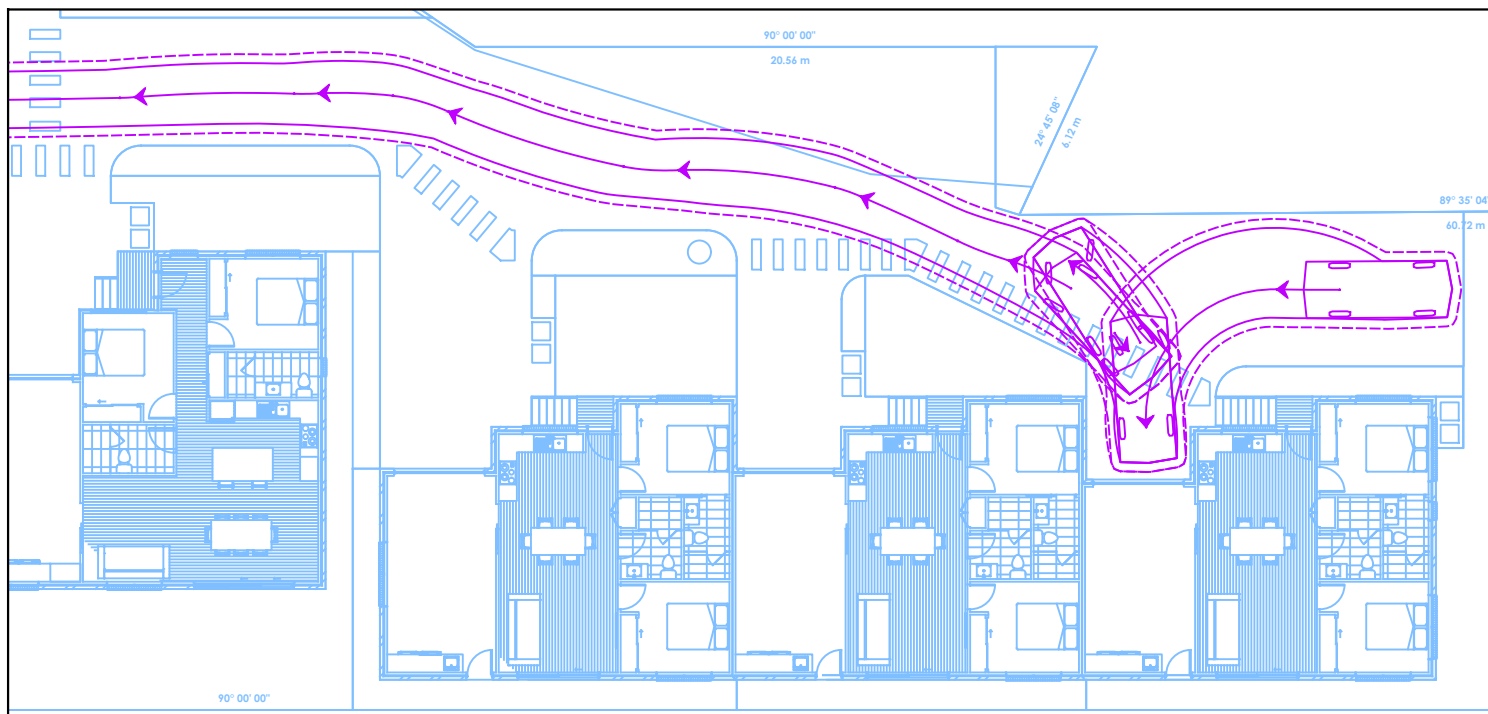
TRINH CAPITAL
 PROPOSED RESIDENTIAL DEVELOPMENT
 11-16/1 ATKINS DRIVE
 ROMAINE
 SWEEP PATH ANALYSIS



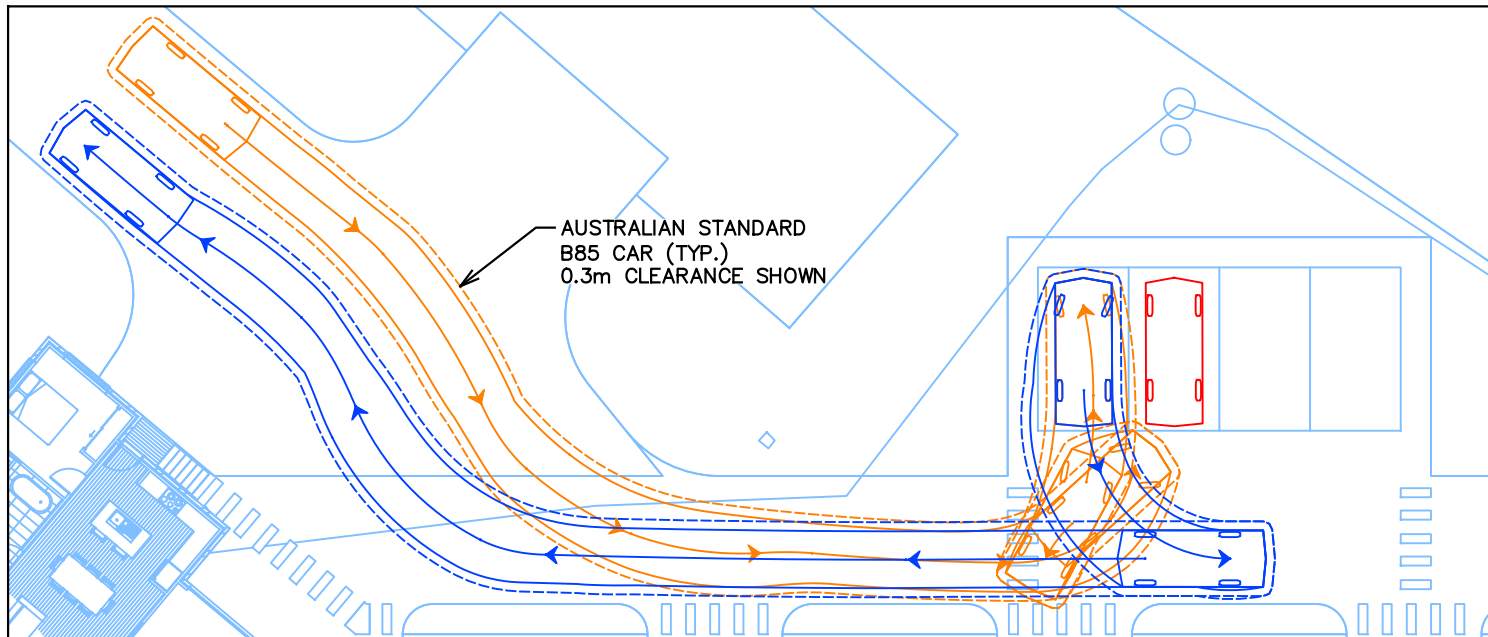
Service.
 Approachability.
 Loyalty.
 Transparency.

Level 3, 51 Queen St Melbourne VIC 3000
 • SYDNEY • CANBERRA • ADELAIDE • HOBART • DARWIN
 ABN: 18 439 813 274 Email: salt@salt3.com.au Ph: 03 9020 4225

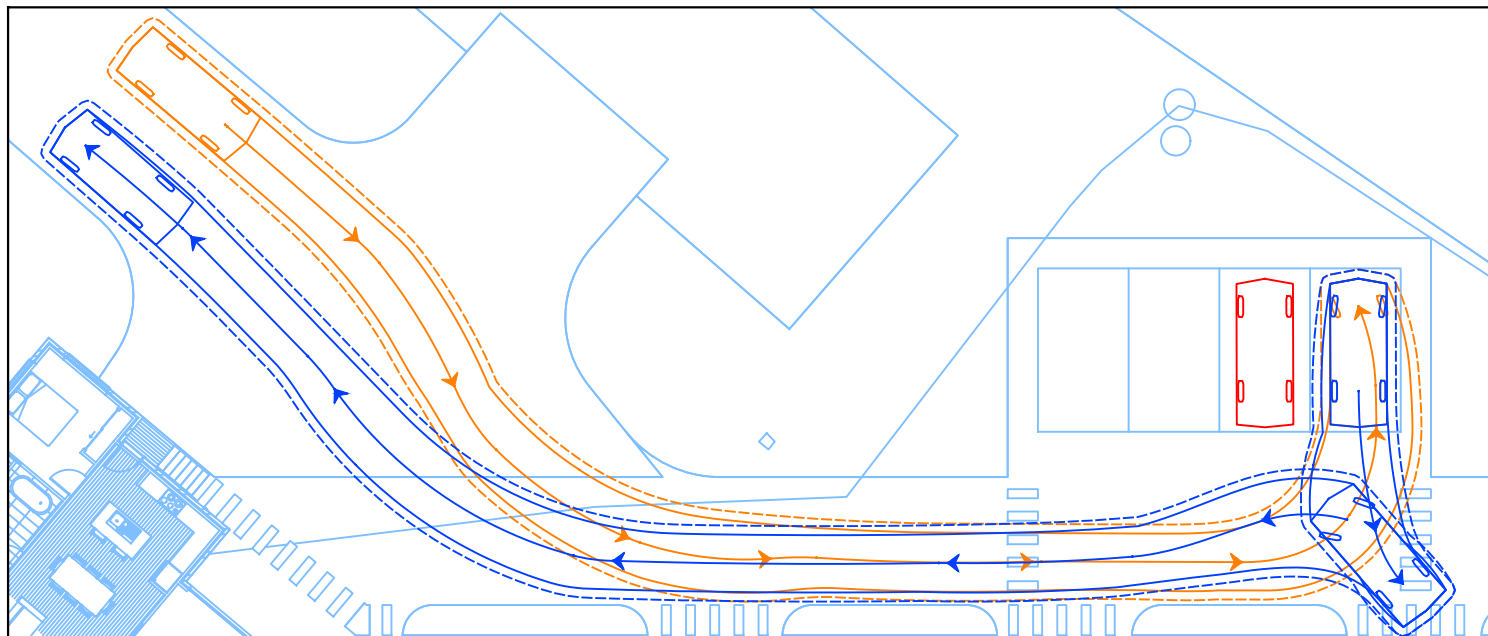
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DRAWING NUMBER	REVISION	
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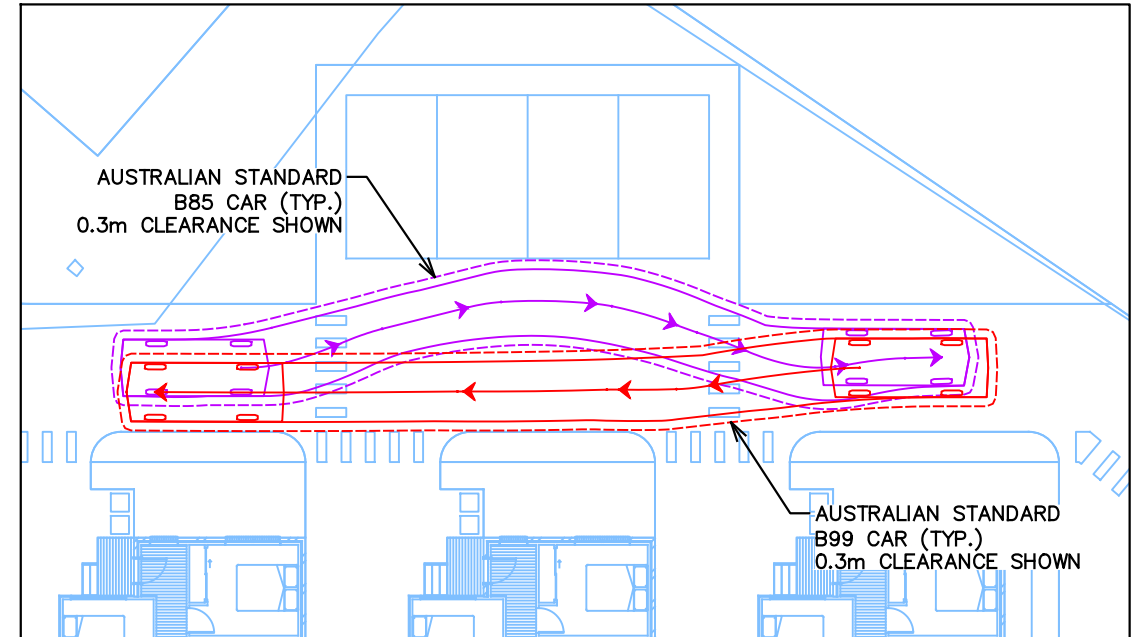
DWELLING UNIT 8 CAR PARK 2 ACCESS



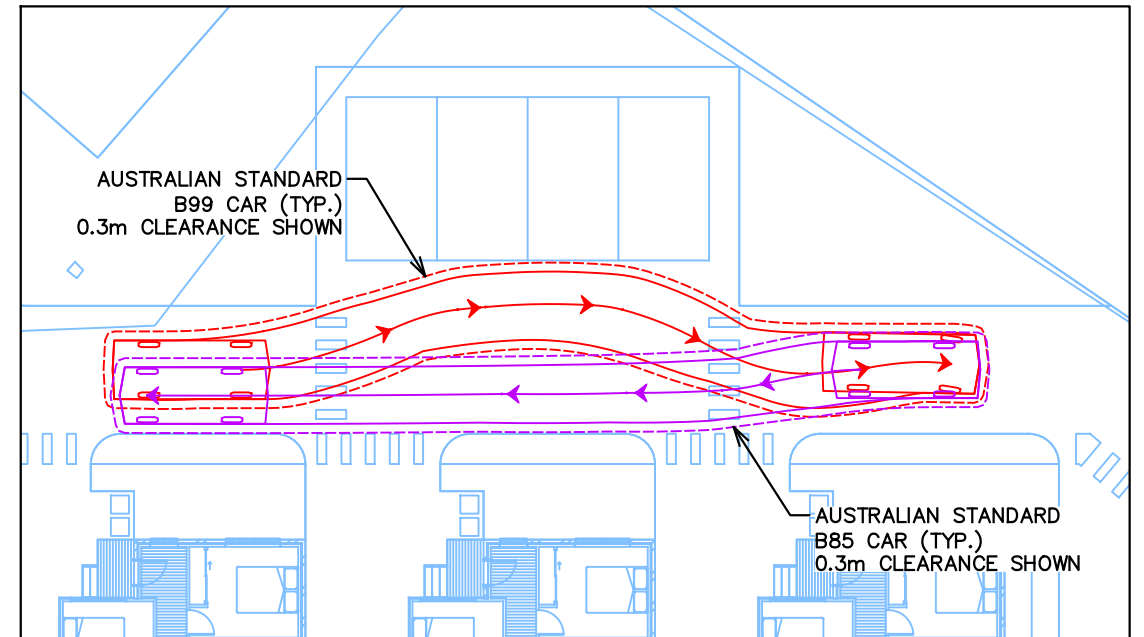
VISITOR CAR PARK ACCESS 1



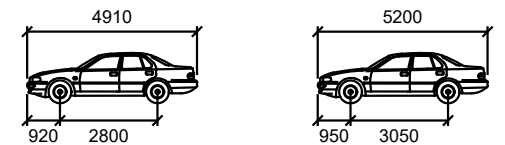
VISITOR CAR PARK ACCESS 2



CARS PASSING ALONG ACCESS WAY - 1



CARS PASSING ALONG ACCESS WAY - 2



	B85		B99	
		mm		mm
Width	: 1870		Width	: 1940
Track	: 1770		Track	: 1840
Lock to Lock Time	: 6.0		Lock to Lock Time	: 6.0
Steering Angle	: 34.1		Steering Angle	: 33.9

TRINH CAPITAL
 PROPOSED RESIDENTIAL DEVELOPMENT
 11-16/1 ATKINS DRIVE
 ROMAINE
 SWEEP PATH ANALYSIS

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		DRAWN / CHECKED	DATE	SIZE
		BW / JW	19-09-2024	A3
		DRAWING NUMBER	REVISION	
		SALT-24506-SK-002	1	

28 November 2024

Albert Trinh



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DARWIN
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Sustainable Transport Surveys Pty Ltd
ABN: 18 439 813 274

www.salt3.com.au

Dear Albert

**Re: 11-16/1 Atkins Drive, Romaine
Response to Council RFI**

We refer to Council Request for Further Information (RFI) dated 17 October 2024 and are pleased to provide the following response to the relevant traffic engineering matters raised.

Matter

C2.6.1 Construction of parking areas

The plans provided do not include the specification of the road surface type and the drainage of driveways, which are the requirements under clause C2.6.1.

Please provide plans demonstrating compliance with clause C2.6.1 (A1/P1) along with a written assessment.

Response

This is a matter that is typically addressed at the detailed design stage of the development by a Civil Engineer. If necessary, Council can place a permit condition to ensure that this is satisfied.

Matter

Table C2.2 Internal Access Way Widths for Vehicles

The TIA suggests that the number of parking spaces served is between "6 to 20" therefore the proposed internal access way width complies with Table 2.2. However the application is to be assessed with the total car parking spaces for all the dwellings using the internal driveway. In this aspect, the number of the parking spaces is over 21.

Please provide a revised access way plan demonstrating the width of the driveway of at least 5.5m or provide a written assessment against the performance criteria.

Response

Control C2.6.2 – Design and layout of parking areas

Objective:

That parking areas are designed and laid out to provide convenient, safe and efficient parking.

Acceptable Solutions

A1.1

Parking, access ways, manoeuvring and circulation spaces must either:

- comply with the following:
 - have a gradient in accordance with Australian Standard AS 2890 – Parking facilities, Parts 1-6;
 - provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;
 - have an access width not less than the requirements in Table C2.2;
 - have car parking space dimensions which satisfy the requirements in Table C2.3;
 - have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;
 - have a vertical clearance of not less than 2.1m above the parking surface level; and
 - excluding a single dwelling, be delineated by line marking or other clear physical means; or
- comply with Australian Standard AS 2890– Parking facilities, Parts 1-6.

Performance Criterion

P1

All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:

- the characteristics of the site;
- the proposed slope, dimensions and layout;
- useability in all weather conditions;
- vehicle and pedestrian traffic safety;
- the nature and use of the development;
- the expected number and type of vehicles;
- the likely use of the parking areas by persons with a disability;
- the nature of traffic in the surrounding area;
- the proposed means of parking delineation; and
- the provisions of Australian Standards AS 2890.1:2004 – Parking facilities, Part 1: Off-street car parking and AS 2890.2:2002 – Parking facilities, Part 2: Off-street commercial vehicle facilities.

A1.1 – Response

The proposed extension of the accessway will have a minimum width of about 4.2m along its entire length, with this extension serving less than 20 car spaces. Nonetheless as per Council's request, an assessment against the relevant performance criterion has been undertaken as follows:

- The accessway will have a minimum width of 4.23m which will operate in a single lane, two-way arrangement. However, the accessway will provide for passing opportunities along its length, including adjacent to the visitor car parking area and adjacent to Units 6 and 7.
- Given the low level of traffic that is likely to be generated along the proposed accessway, the fact that users will be long-term users familiar with the site and the slow speed environment within the site, we are of the opinion that the design of the accessway is appropriate for the proposed development.

Matter

C2.6.1:

- Road surface type not specified
- Drainage of driveways not demonstrated

Response

These matters are typically addressed at the detailed design stage of the development by a Civil Engineer. If necessary, Council can place a permit condition to ensure that this is satisfied.

Matter

C2.6.2:

- Gradients to AS2890 not demonstrated
- Line marking not noted for bays
- Vertical clearance to garages not demonstrated

Response

The plans attached at Appendix 1 show gradients along the proposed accessway in accordance with the requirements of AS/NZS 2890.1:2004. Specifically, a maximum grade of 1:4 is provided along ramped sections, with 1:8 transitions (2.0m long minimum) provided before and after. Furthermore, a maximum grade of 1:16 is to be provided past the visitor car spaces in accordance with the requirements of AS/NZS 2890.1:2004.

Linemarking is not considered necessary for the tandem spaces in front of the garages but is considered necessary for the visitor parking spaces. Council can place a permit condition to ensure that visitor parking spaces are linemarked to their satisfaction.

In relation to vertical clearance, we are instructed that all garages are to achieve a minimum headroom clearance of 2.1m, satisfying Clause C2.6.2 of the Planning Scheme. A plan has been prepared demonstrating this and will be submitted as part of the overall response to the RFI.

Matter

C2.5.2

- Question bicycle parking stated as not needed?

Response

As per Table C2.1 of the Planning Scheme, there are no bicycle parking requirements for dwellings in the General Residential Zone.

Matter

C2.6.5

- Footpaths shown do not link to Atkins Dr or Mount St – is this acceptable?

Response

The development is an extension of the existing site, and Atkins Drive does not have a footpath. It is common for pedestrians to share the driveways with vehicles in developments such as this, as they are a low speed environment.

We therefore do not consider it necessary to provide a footpath connection to Atkins Drive and/or Mount Street in this case.

We trust this is sufficient for now. Should there be any queries in relation to this letter, please don't hesitate to contact the undersigned.

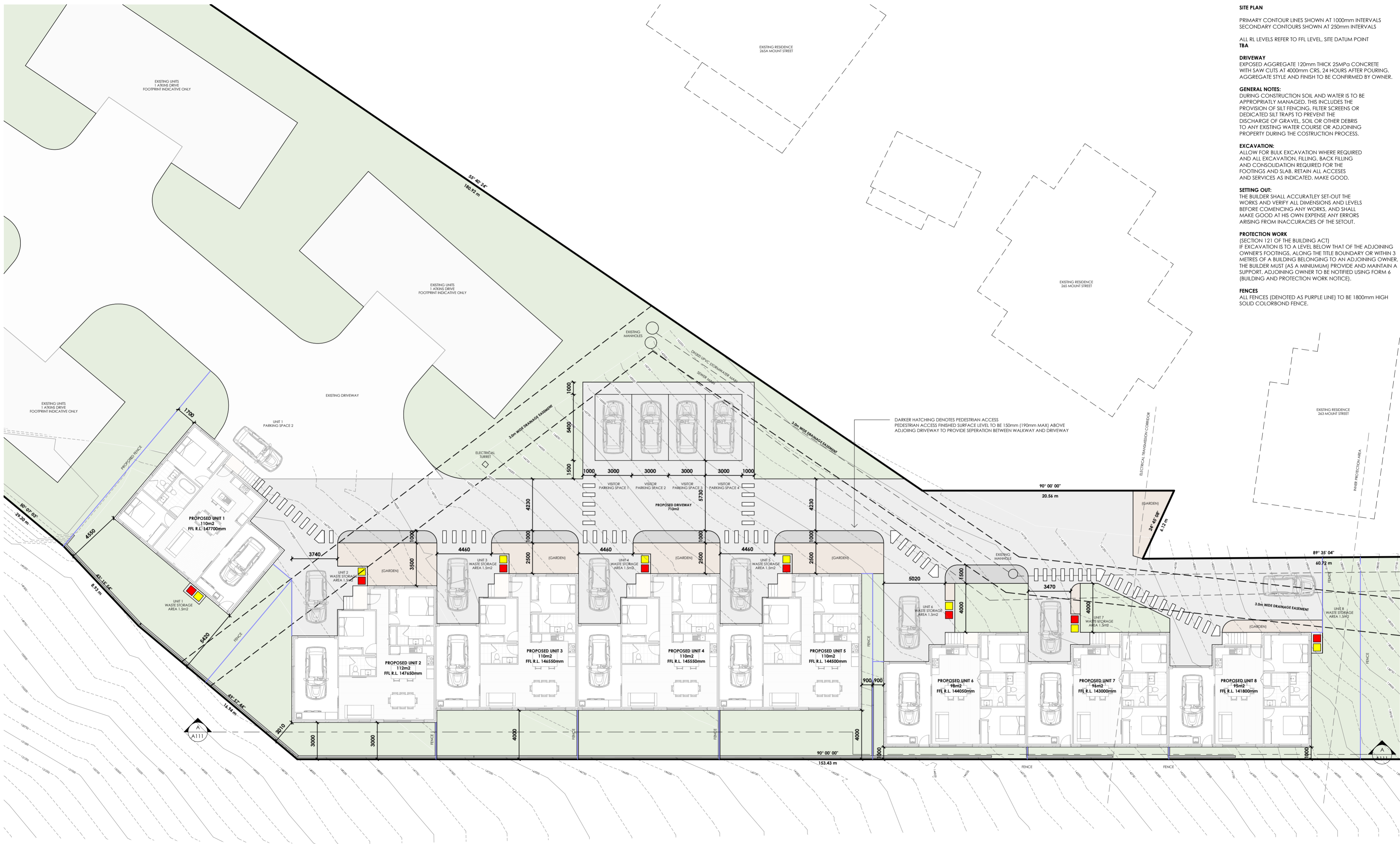
Yours sincerely,



Tony Togany
Senior Associate
SALT
T: 9020 4225
M: 0458 340 274
tony.togany@salt3.com.au

APPENDIX 1 DEVELOPMENT PLANS





SITE PLAN
 PRIMARY CONTOUR LINES SHOWN AT 1000mm INTERVALS
 SECONDARY CONTOURS SHOWN AT 250mm INTERVALS
 ALL RL LEVELS REFER TO FFL LEVEL, SITE DATUM POINT
 TBA

DRIVEWAY
 EXPOSED AGGREGATE 120mm THICK 25MPa CONCRETE
 WITH SAW CUTS AT 4000mm CRS, 24 HOURS AFTER POURING.
 AGGREGATE STYLE AND FINISH TO BE CONFIRMED BY OWNER.

GENERAL NOTES:
 DURING CONSTRUCTION SOIL AND WATER IS TO BE
 APPROPRIATELY MANAGED, THIS INCLUDES THE
 PROVISION OF SILT FENCING, FILTER SCREENS OR
 DEDICATED SILT TRAPS TO PREVENT THE
 DISCHARGE OF GRAVEL, SOIL OR OTHER DEBRIS
 TO ANY EXISTING WATER COURSE OR ADJOINING
 PROPERTY DURING THE CONSTRUCTION PROCESS.

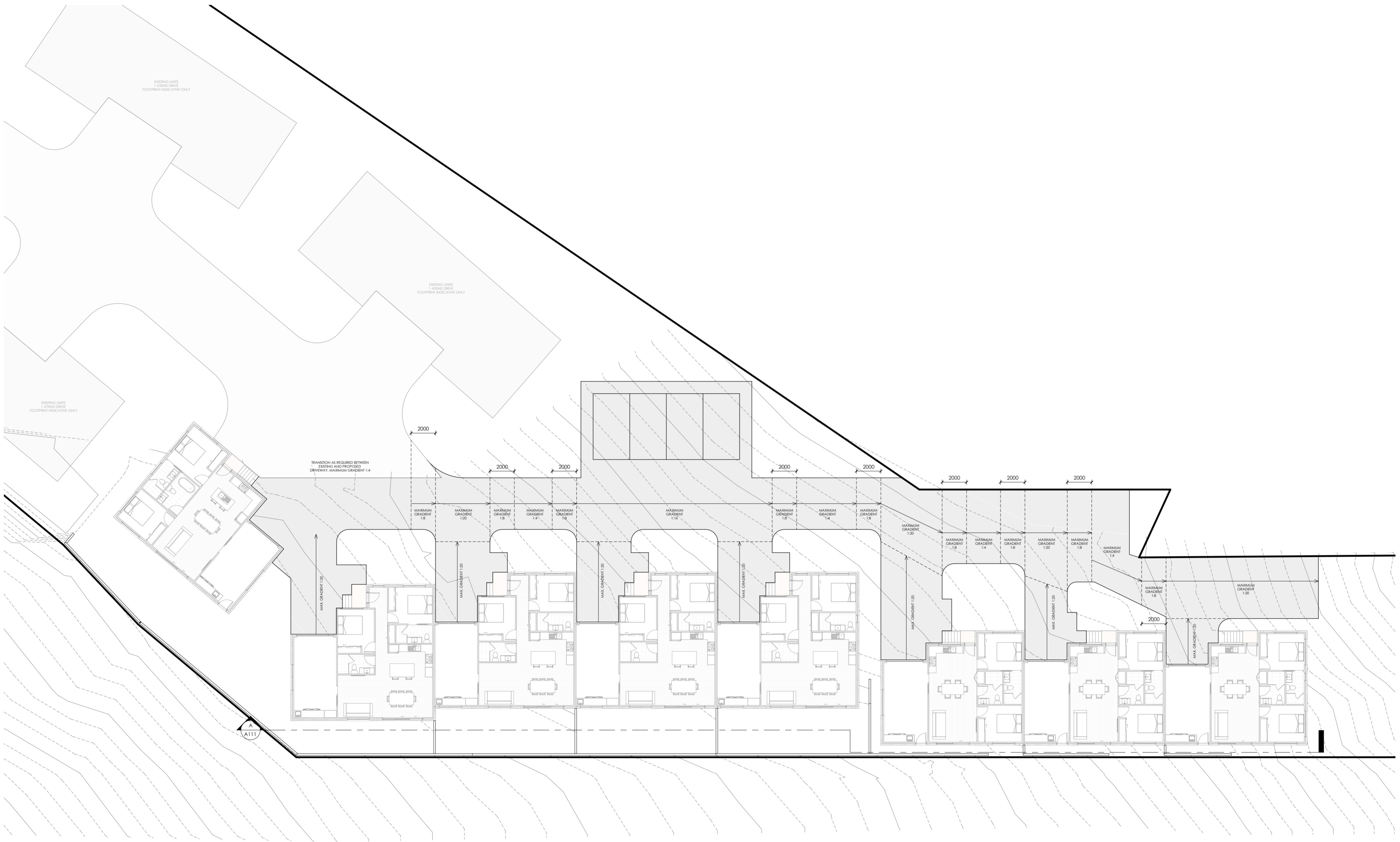
EXCAVATION:
 ALLOW FOR BULK EXCAVATION WHERE REQUIRED
 AND ALL EXCAVATION, FILLING, BACK FILLING
 AND CONSOLIDATION REQUIRED FOR THE
 FOOTINGS AND SLAB. RETAIN ALL ACCESSES
 AND SERVICES AS INDICATED. MAKE GOOD.

SETTING OUT:
 THE BUILDER SHALL ACCURATELY SET-OUT THE
 WORKS AND VERIFY ALL DIMENSIONS AND LEVELS
 BEFORE COMMENCING ANY WORKS, AND SHALL
 MAKE GOOD AT HIS OWN EXPENSE ANY ERRORS
 ARISING FROM INACCURACIES OF THE SETOUT.

PROTECTION WORK
 (SECTION 121 OF THE BUILDING ACT)
 IF EXCAVATION IS TO A LEVEL BELOW THAT OF THE ADJOINING
 OWNER'S FOOTINGS, ALONG THE TITLE BOUNDARY OR WITHIN 3
 METRES OF A BUILDING BELONGING TO AN ADJOINING OWNER,
 THE BUILDER MUST (AS A MINIMUM) PROVIDE AND MAINTAIN A
 SUPPORT, ADJOINING OWNER TO BE NOTIFIED USING FORM 6
 (BUILDING AND PROTECTION WORK NOTICE).

FENCES
 ALL FENCES (DENOTED AS PURPLE LINE) TO BE 1800mm HIGH
 SOLID COLORBOND FENCE.

SITE PLAN
 Scale 1 : 200




DRIVEWAY & PARKING SETOUT PLAN
 Scale 1 : 200



Memo

To N+B Design
From pitt&sherry
Date 20 January 2026
RE **Water Meter Assessment**

This memo evaluates the capacity of existing DN65 water meter connection at 11-16/1 Atkins Drive, Romaine, Tasmania to cater for additional water demand resulting from proposed development. Throughout this report, the existing establishment comprising 16 units is referred to as “Existing Units” and future establishment resulting from addition of 8 more units is referred to as “Future Units”.

1. Basis of Design

- Information on the existing infrastructures has been referenced from TasWater Asset Information Portal and the LISTmap.
- ET Calculations have been based on number of 2-bedroom units (Residential Dwelling: Standard Occupancy) with ET Code RE01 (and ET rate 1.0 for water) as per TasWater Supplement to Water Supply Code of Australia WSA 03 - 2011-3.1 MRWA Edition V2.0.
- Water demands are calculated based upon “TasWater Supplement MRWA V2.0 - 2.3.4.2 Demands General”.
 - To calculate Peak Day Demand (PD), a peaking factor of 2.25 (recommended by TasWater Supplement MRWA V2.0 - 2.3.4.2) has been applied.
 - To calculate Peak Hour Demand (PH), a peaking factor of 2.0 (recommended by TasWater Supplement MRWA V2.0 - 2.3.4.3) been applied.
- Probable simultaneous demand (PSD) has been referred from Table 3.2.3 of AS/NZS 3500.1 (assuming 1 ET is equivalent to 1 Dwelling).

2. Estimated Water Parameters

The outcomes of calculations of Equivalent Tenements (ET's) and design flows for Existing and Future Units are presented in the following table.

Table 1: Summary of ET and Design Flows

	Existing Units	Future Units
ET Calculations		
Number of Units	16	24
ET Code	RE01	RE01
ET Rate	1.0	1.0
Total ET	16	24
Design Flow Calculations		
Average Day Demand (AD) (kL)	10.96	16.44
Peak Day Demand (PD) (kL)	24.66	36.99
Peak Hour Demand (PH) (L/s)	0.57	0.86
Probable Simultaneous Demand as per Table 3.2.3 AS3500.1 (L/s)	2.30	2.95
Flow velocity through existing DN65 water meter (m/s)	0.69	0.89

The meter size and private watermain size have been assessed based on potential simultaneous flow (PSD) as per Table 3.2.3 AS/NZS 3500.1

3. Water Meter Size

The PSDs for Existing Units and Future Units are 2.30 L/s and 2.95 L/s, respectively. This results in the flow velocity of 0.69 m/s for Existing Units and 0.89 m/s for Future Units through the existing DN65 water meter and associated internal pipework.

Maximum allowable velocity in pipework for potable water is 3.0 m/s as per Clause 3.4 of AS/NZS 3500.1 which suggests that the existing DN65 meter and pipework is suitable for existing 16 units as well as for proposed expansion to 24 units.

4. Water Pressure

It is assumed that the water for the property is supplied from Mount Road Basin Reservoir with a Top Water Level (TWL) of RL = 200m AHD. The highest point of the site is RL = 149m AHD resulting in total available static pressure of 51m.

The following head losses were allowed for through the meter arrangement at the nominated flows.

Table 2: Head loss summary and available head a connection

Head Loss Summary	Existing Units	Future Units
Probable Simultaneous Demand (L/s)	2.30	2.95
ValvCheQ DCO3U double check valve (m)	3.0	3.1
Sensus Meistream Plus Water Meter (m)	0.045	0.065
Sensus WP-F Dirt Box	N/A - Negligible	N/A - Negligible
10.0m DN65 Pipe (m)	0.10	0.17
Total Head Loss (m)	3.2	3.3
Approximate Dynamic Pressure After Backflow (m)	47.8	47.7

The dynamic pressure downstream of the water meter and backflow prevention assembly for both existing and future units exceeds the minimum service pressure specified in Table 2.5.3.3 of the TasWater Supplement MRWA V2.0. This provides sufficient allowance for additional pressure losses within the network not accounted for in the above assessment.

5. Conclusion

From the assessment outlined above, the existing DN65 water meter is deemed suitable for catering the total of 24 units (16 existing and 8 new) at 11-16/1 Atkins Drive, Romaine, Tasmania.

Yours sincerely



Bradley Gibbins
Senior Water Engineer



Planning Compliance Report

UNIT 11-16, 1 ATKINS DRIVE, ROMAINE

Multiple Dwelling

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PDA Contributors

Planning	Payal Patel	26/03/2024
Review & Approval		

Revision History

Revision	Description	Date
0	First Issue	26/03/2024
1	Revision 1	15/07/2024
2	Revision 2	02/09/2024

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EXECUTIVE SUMMARY

Council approval is sought for the Multiple Dwellings at 1 Atkins Drive, Romaine. This planning assessment, combined with supplementary documentation has been provided in support of the proposed development.

Development Details:

Property Address	1 Atkins Drive, Romaine TAS 7320
Proposal	Multiple Dwellings
Land Area	3384m ² ±
Land Owner/s	Atkins Drive Pty Ltd
Client	Albert Trinh

PID / CT	3121895	162223/17
Planning Ordinance	Tasmanian Planning Scheme - Burnie	
Land Zoning	8.0 General Residential Zone	
Specific Areas Plans	None	
Code Overlays	C4 Electricity Transmission Infrastructure Protection Code	

1. Introduction/Context

Council approval is sought for Multiple Dwellings at 1 Atkins Drive, Romaine. In support of the proposal the following associated documents have been provided in conjunction with this planning assessment:

- The Title Plan and Folio;
- Design Documentation provided by *NplusB*

1.1. The Land



Figure 1. Existing aerial image of the subject land and surrounds (LISTmap, 2024)

The subject site is a vacant balance lot of strata development 162223/0. The subject site is also connected to reticulated water, stormwater and sewer connections. The title has frontage to Atkins Drive and Mount Street. No Natural Values are identified on the subject land.

2. The Proposal

A Planning Permit for Multiple Dwellings is sought, in accordance with Section 57 of the *Land Use Planning and Approvals Act 1993* and Clause 6.8.1 (b) of the *Tasmanian Planning Scheme - Burnie*



Figure 2. Proposed Multiple Dwelling (Page 5 of design drawing set by iDesign Residential)

The proposal is to develop 8 residential units and associated services as shown in the figure 3. The proposal is intended for strata development and will share services such as water, stormwater and sewer as common property.

3. Planning Assessment

This current proposal has been developed in accordance with *Tasmanian Planning Scheme - Burnie*.

3.1 Zoning

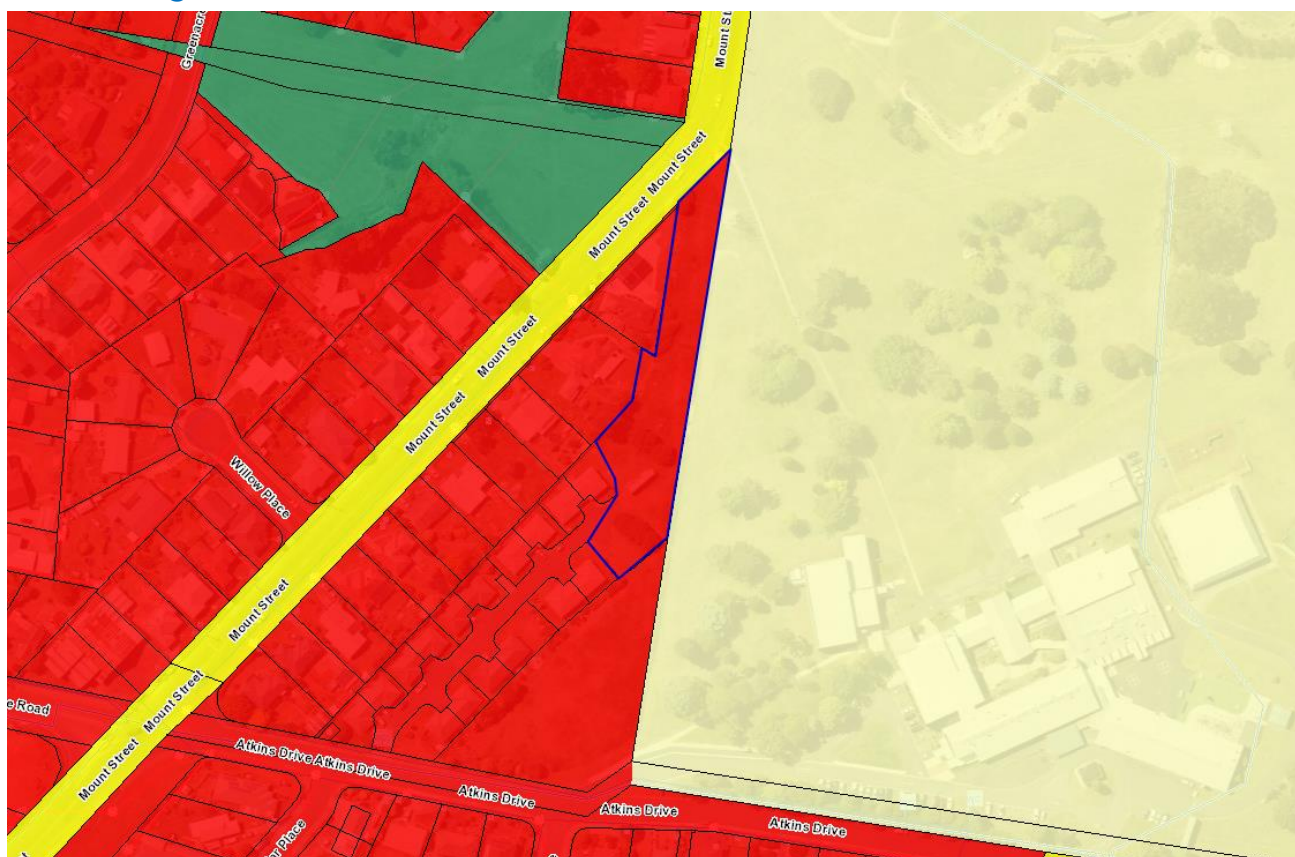


Figure 5. Zoning identification of the subject land and surrounds (LISTmap, 2024)

The subject land is located within the General Residential Zone whilst is also adjacent to the Community Purpose zone (in the east), as shown in Figure 3 above.

3.2 Zone Standards

8.0 General Residential Zone

8.4 Development Standards for Dwellings

8.4.1 Residential density for multiple dwellings

Objective:

That the density of multiple dwellings:
 (a) makes efficient use of land for housing; and
 (b) optimises the use of infrastructure and community services.

Acceptable Solutions

A1
 Multiple dwellings must have a site area per dwelling of not less than 325m².

Performance Criteria

P1

Comment:

A1 is met: as the proposed site has an area of 3384m²± and would contain 8 units. The site area per dwelling would be more than the required minimum of 325m².

8.4.2 Setbacks and building envelope for all dwellings

Objective:

The siting and scale of dwellings:

- (a) provides reasonably consistent separation between dwellings and their frontage within a street;
- (b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings;
- (c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and
- (d) provides reasonable access to sunlight for existing solar energy installations.

Acceptable Solutions

A1

Unless within a building area on a sealed plan, a dwelling, excluding garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is:

- (a) if the frontage is a primary frontage, not less than 4.5m, or, if the setback from the primary frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site;
- (b) if the frontage is not a primary frontage, not less than 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site;
- (c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or
- (d) if located above a non-residential use at ground floor level, not less than the setback from the frontage of the ground floor level.

Performance Criteria

P1

A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.

Comment:

P1 is met:

The proposed dwellings will have frontage setback of more than 4.5m that is consistent with the streetscape.

<p>A2</p> <p>A garage or carport for a dwelling must have a setback from a primary frontage of not less than:</p> <ul style="list-style-type: none"> (a) 5.5m, or alternatively 1m behind the building line; (b) the same as the building line, if a portion of the dwelling gross floor area is located above the garage or carport; or (c) 1m, if the existing ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage. 	<p>P2</p>
--	------------------

Comment:

A2 (a) is met: as all proposed garage are within the dwelling design and will have setback more than 5.5m from the primary frontage. Please see attached design plans by NplusB.

<p>A3</p> <p>A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:</p> <ul style="list-style-type: none"> (a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by: <ul style="list-style-type: none"> (i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and (ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and (b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling: <ul style="list-style-type: none"> (i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or 	<p>P3</p> <p>The siting and scale of a dwelling must:</p> <ul style="list-style-type: none"> a) not cause an unreasonable loss of amenity to adjoining properties, having regard to: <ul style="list-style-type: none"> (i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property; (ii) overshadowing the private open space of a dwelling on an adjoining property; (iii) overshadowing of an adjoining vacant property; and (iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property; (v) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and (vi) not cause an unreasonable reduction in sunlight to an existing solar energy installation on: <ul style="list-style-type: none"> i. an adjoining property; or ii. another dwelling on the same site.
---	---

- (ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser).

Comment:

P3 is met: as proposed dwellings meet applicable setback requirements and are within the acceptable building envelope. Except Lot 6-7-8 which is within 1m from rear boundary and hence assessed against the performance criteria:

- a) No loss of amenity to adjoining properties is foreseen with regards to:
- (i) The adjoining title FR161069/2 is vacant and title FR247185/1 is "Parklands High School" with no buildings anticipated within the immediate 50m offset. Hence no loss or reduction in sunlight is anticipated by the Unit 6-7-8 on adjoining property.
 - (ii) The proposed units are one-storey level buildings as shown on the design plans by NplusB. No overshadowing the private open space is anticipated.
 - (iii) Not applicable as the adjoining title for Unit 6-7-8 is not vacant and is operated as "Parklands High School"
 - (iv) The site is within General Residential Zone and is an infill development within an existing Strata Scheme. The proposed design materials are expected to blend in within surrounding dwelling development.
- b) As mentioned earlier, there is sufficient separation between adjoining buildings regardless of dwelling use or not, which is consistent with other established properties in the area (17-19 Wirilda Drive, Romaine).
- c) No loss of sunlight will be impacted on the adjoining school site (No solar installations are visible through the aerial image)

8.4.3 Site coverage and private open space for all dwellings

Objective:

That dwellings are compatible with the amenity and character of the area and provide:

- (a) for outdoor recreation and the operational needs of the residents;
- (b) opportunities for the planting of gardens and landscaping; and
- (c) private open space that is conveniently located and has access to sunlight.

Acceptable Solutions

A1

Dwellings must have:

- (a) a site coverage of not more than 50% (excluding eaves up to 0.6m wide); and
- (b) for multiple dwellings, a total area of private open space of not less than 60m² associated with each dwelling, unless the dwelling has a finished

Performance Criteria

P1

Dwellings must have:

- (a) site coverage consistent with that existing on established properties in the area;
- (b) private open space that is of a size and with dimensions that are appropriate for the size of the dwelling and is able to accommodate:

floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer).

- (i) outdoor recreational space consistent with the projected requirements of the occupants and, for multiple dwellings, take into account any common open space provided for this purpose within the development; and
- (ii) operational needs, such as clothes drying and storage; and
- (c) reasonable space for the planting of gardens and landscaping.

Comment:

A1 is met:

- (a) Proposed Site covered is 24.6% which is not more than 50%.
- (b) All proposed dwellings are provided with not less than 60m² of private open space, as illustrated on the design documentation.

However, Lot 6 and 7 have 54m² and 35m² of total private open space and is therefore assessed against the performance criteria:

- (a) Not applicable
- (b) The proposed units 6 and 7 do have sufficient open space to undertake outdoor recreational space for the future occupants as shown on design plans
Operation needs will be complied within the proposed private open space to these two units.
- (c) Both units will have sufficient space to undertake small-scale gardening and landscaping.

A2

A dwelling must have private open space that:

- (a) is in one location and is not less than:
 - (i) 24m² or
 - (ii) 12m², if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);
- (b) has a minimum horizontal dimension of not less than:
 - (i) 4m; or

P2

A dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is:

- a) conveniently located in relation to a living area of the dwelling; and
- b) orientated to take advantage of sunlight

- (ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);
- (c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and
- (d) has a gradient not steeper than 1 in 10.

Comment:

P2 is met:

Each multiple dwelling unit is provided with a minimum of 24m² of private open space that is located in relation to the living area. It is designed and located to take advantage of sunlight.

8.4.4 Sunlight to private open space of multiple dwellings

Objective:

That the separation between multiple dwellings provides reasonable opportunity for sunlight to private open space for dwellings on the same site.

Acceptable Solutions

A1

A multiple dwelling, that is to the north of the private open space of another dwelling on the same site, required to satisfy A2 or P2 of clause 8.4.3, must satisfy (a) or (b), unless excluded by (c):

- (a) the multiple dwelling is contained within a line projecting (see Figure 8.4):
 - (i) at a distance of 3m from the northern edge of the private open space; and
 - (ii) vertically to a height of 3m above existing ground level and then at an angle of 45 degrees from the horizontal;
- (b) the multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight between 9.00am and 3.00pm on 21st June; and
- (c) this Acceptable Solution excludes that part of a multiple dwelling consisting of:

Performance Criteria

P1

A multiple dwelling must be designed and sited to not cause an unreasonable loss of amenity by overshadowing the private open space, of another dwelling on the same site, which is required to satisfy A2 or P2 of clause 8.4.3 of this planning scheme.

(i) (ii)	an outbuilding with a building height not more than 2.4m; or protrusions that extend not more than 0.9m horizontally from the multiple dwelling.
-----------------	---

Comment:

P1 is met: as all dwellings are provided with sufficient private open space which meets the requirements of 8.4.3 AP as illustrated on the design documentation provided as well as additional private outdoor areas that receive sufficient solar gain and no unreasonable loss of amenity. Please refer to shadow diagrams provided.

8.4.5 Width of openings for garages and carports for all dwellings

Objective:

To reduce the potential for garage or carport openings to dominate the primary frontage.

Acceptable Solutions

Performance Criteria

A1

A garage or carport for a dwelling within 12m of a primary frontage, whether the garage or carport is free-standing or part of the dwelling, must have a total width of openings facing the primary frontage of not more than 6m or half the width of the frontage (whichever is the lesser).

P1

Comment:

A1 is met: The proposed garage is 3m wide equalling and not more than 6m.

8.4.6 Privacy for all dwellings

Objective:

To provide a reasonable opportunity for privacy for dwellings.

Acceptable Solutions

Performance Criteria

A1

A balcony, deck, roof terrace, parking space, or carport for a dwelling (whether freestanding or part of the dwelling), that has a finished surface or floor level more than 1m above existing ground level must have a permanently fixed screen to a height of not less than 1.7m above the finished surface or floor level, with a uniform transparency of not more than 25%, along the sides facing a:

- (a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 3m from the side boundary;

P1

- (b) rear boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 4m from the rear boundary; and
- (c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is not less than 6m:
 - (i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or
 - (ii) from a balcony, deck, roof terrace or the private open space of the other dwelling on the same site.

¹¹

Comment:

Not Applicable as no balcony, deck roof terrace, parking space or carport above 1m of ground level is proposed.

A2

A window or glazed door to a habitable room of a dwelling, that has a floor level more than 1m above existing ground level, must satisfy (a), unless it satisfies (b):

- (a) the window or glazed door:
 - (i) is to have a setback of not less than 3m from a side boundary;
 - (ii) is to have a setback of not less than 4m from a rear boundary;
 - (iii) if the dwelling is a multiple dwelling, is to be not less than 6m from a window or glazed door, to a habitable room, of another dwelling on the same site; and
 - (iv) if the dwelling is a multiple dwelling, is to be not less than 6m from the private open space of another dwelling on the same site.
- (b) the window or glazed door:
 - (i) is to be offset, in the horizontal plane, not less than 1.5m from the edge of a window or glazed door, to a habitable room of another dwelling;
 - (ii) is to have a sill height of not less than 1.7m above the floor level or have fixed obscure glazing extending to a height of not less than 1.7m above the floor level; or
 - (iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of not less than 1.7m above floor level, with a uniform transparency of not more than 25%.

P2

Comment:

A2 is met: as proposed dwellings will be compliant with required setbacks for windows as per (iii) and (iv).

<p>A3 A shared driveway or parking space (excluding a parking space allocated to that dwelling) must be separated from a window, or glazed door, to a habitable room of a multiple dwelling by a horizontal distance of not less than:</p> <ul style="list-style-type: none"> (a) 2.5m; or (b) 1m if: <ul style="list-style-type: none"> (i) it is separated by a screen of not less than 1.7m in height; or (ii) the window, or glazed door, to a habitable room has a sill height of not less than 1.7m above the shared driveway or parking space, or has fixed obscure glazing extending to a height of not less than 1.7m above the floor level. 	<p>P3 A shared driveway or parking spaces (excluding parking space allocated to that dwelling), must be screened, or otherwise located or designed, to minimise unreasonable impact of vehicle noise or vehicle light intrusion to a habitable room of a multiple dwelling.</p>
--	---

Comment:

A3 is met. All units will have a distance of more than 2.5m from habitable rooms.

8.4.8 Waste storage for multiple dwellings

Objective:

To provide for the storage of waste and recycling bins for multiple dwellings.

Acceptable Solutions	Performance Criteria
<p>A1 A multiple dwelling must have a storage area, for waste and recycling bins, that is not less than 1.5m² per dwelling and is within one of the following locations:</p> <ul style="list-style-type: none"> (a) an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or (b) a common storage area with an impervious surface that: <ul style="list-style-type: none"> (i) has a setback of not less than 4.5m from a frontage; (ii) is not less than 5.5m from any dwelling; and 	<p>P1 A multiple dwelling must have storage for waste and recycling bins that is:</p> <ul style="list-style-type: none"> (a) capable of storing the number of bins required for the site; (b) screened from the frontage and any dwellings; and (c) if the storage area is a common storage area, separated from any dwellings to minimise impacts caused by odours and noise.

(iii)	is screened from the frontage and any dwelling by a wall to a height not less than 1.2m above the finished surface level of the storage area.	
-------	---	--

Comment:

P1 is met: as each dwelling has available area of not less than 1.5m² for waste bin storage located as shown on drawings set. The waste storage area will be effectively screened from the outside view as shown on the plans. Unit 1 – storage area to the rear. All other units have a corner spaces designated with fence or screen that prohibits the view from the other dwellings.

3.3 Codes



Figure 5. Scheme Overlay identification of the subject land and surrounds (LISTmap, 2024)

Code	Comments:
C1.0 Signs Code	N/A
C2.0 Parking and Sustainable Transport Code	Applicable - Please see attached Traffic Impact assessment by SALT.
C3.0 Road and Railway Assets Code	Applicable - Please see attached Traffic Impact assessment by SALT.
C4.0 Electricity Transmission Infrastructure Protection Code	Applicable - The proposed unit 7-8 are within the electricity transmission corridor but not within an inner protection area. Therefore, complies with C4.6.1 A1 (a).
C5.0 Telecommunications Code	N/A
C6.0 Local Historic Heritage Code	N/A
C7.0 Natural Assets Code	N/A
C8.0 Scenic Protection Code	N/A
C9.0 Attenuation Code	N/A
C10.0 Coastal Erosion Hazard Code	N/A
C11.0 Coastal Inundation Hazard Code	N/A

C12.0 Flood-prone Areas Hazard Code	N/A
C13.0 Bushfire-prone Areas Code	N/A
C14.0 Potentially Contaminated Land Code	N/A
C15.0 Landslip Hazard Code	N/A
C16.0 Safeguarding of Airports Code	N/A

4. Conclusion

The planning assessment and supporting documentation demonstrate that the development proposal for Multiple Dwellings at 1 Atkins Drive, Romaine, meets all requirements of the *Tasmanian Planning Scheme - Burnie*. We therefore request that the Council support this application and recommend for approval.

Yours faithfully,



Payal Patel

**On behalf of
PDA Surveyors, Engineers and Planners**

5. Contact

For any enquiries, please contact one of our offices:

HOBART

A: 127 Bathurst Street, Hobart Tasmania 7000

P: (03) 6234 3217

E: pda.hbt@pda.com.au

KINGSTON

A: 6 Freeman Street, Kingston, TAS 7050

P: (03) 6229 2131

E: pda.ktn@pda.com.au

HUONVILLE

A: 8/16 Main Street, Huonville, TAS 7109 - (By appointment)

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E: pda.huon@pda.com.au

EAST COAST

A: 3 Franklin Street, Swansea TAS 7190 - (By appointment)

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E: pda.east@pda.com.au

LAUNCESTON

A: 3/23 Brisbane Street, Launceston, TAS 7250

P: (03) 6331 4099

E: pda.ltn@pda.com.au

BURNIE

A: 6 Queen Street, Burnie, TAS 7320

P: (03) 6431 4400

E: pda.bne@pda.com.au

DEVONPORT

A: 77 Gunn Street, Devonport, TAS 7310

P: (03) 6423 6875

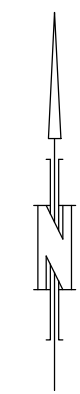
E: pda.dpt@pda.com.au

WALTER SURVEYS

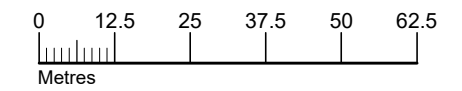
A: 127 Bathurst Street, Hobart, TAS 7000 (Civil Site Surveying and Machine Control)

P: 0419 532 669 (Tom Walter)

E: tom.walter@waltersurveys.com.au



SCALE 1: 1250 (A3)



NOTES:

Date of Survey: 2nd June 2021 & 13th December 2023

Coordinate Datum is planar based on MGA55 (GDA 2020) per coordinate origin SPM7922, with coordinates of

E: 407344.439
 N: 5451673.191
 RL: 143.466 per Surcom

While reasonable effort has been made to locate all visible above ground services, there may be other services which were not located during survey.

Some services have been plotted from DBYD records, and as such are approximate only.

Prior to any demolition, excavation, final design or construction on this site, a comprehensive site investigation should be undertaken to locate all above and below ground service infrastructure.

The boundaries shown on this plan are compiled from recent surveys, as such, are approximate only.

If any works are to be conducted on or near the boundary a remark survey may be required.

Contour Interval 0.25m

Any DTM modeling that is to be done from the accompanying 3D digital file must be done using only the layer TRIANGLE_4 51434-1A_F to ensure that surface matches that verified by PDA Surveyors. No responsibility is taken for the use or interpretation of this data in any other format.


Some feature levels are not shown on this plan for clarity. These can be found turned on in model space or on the OFF Levels layer.

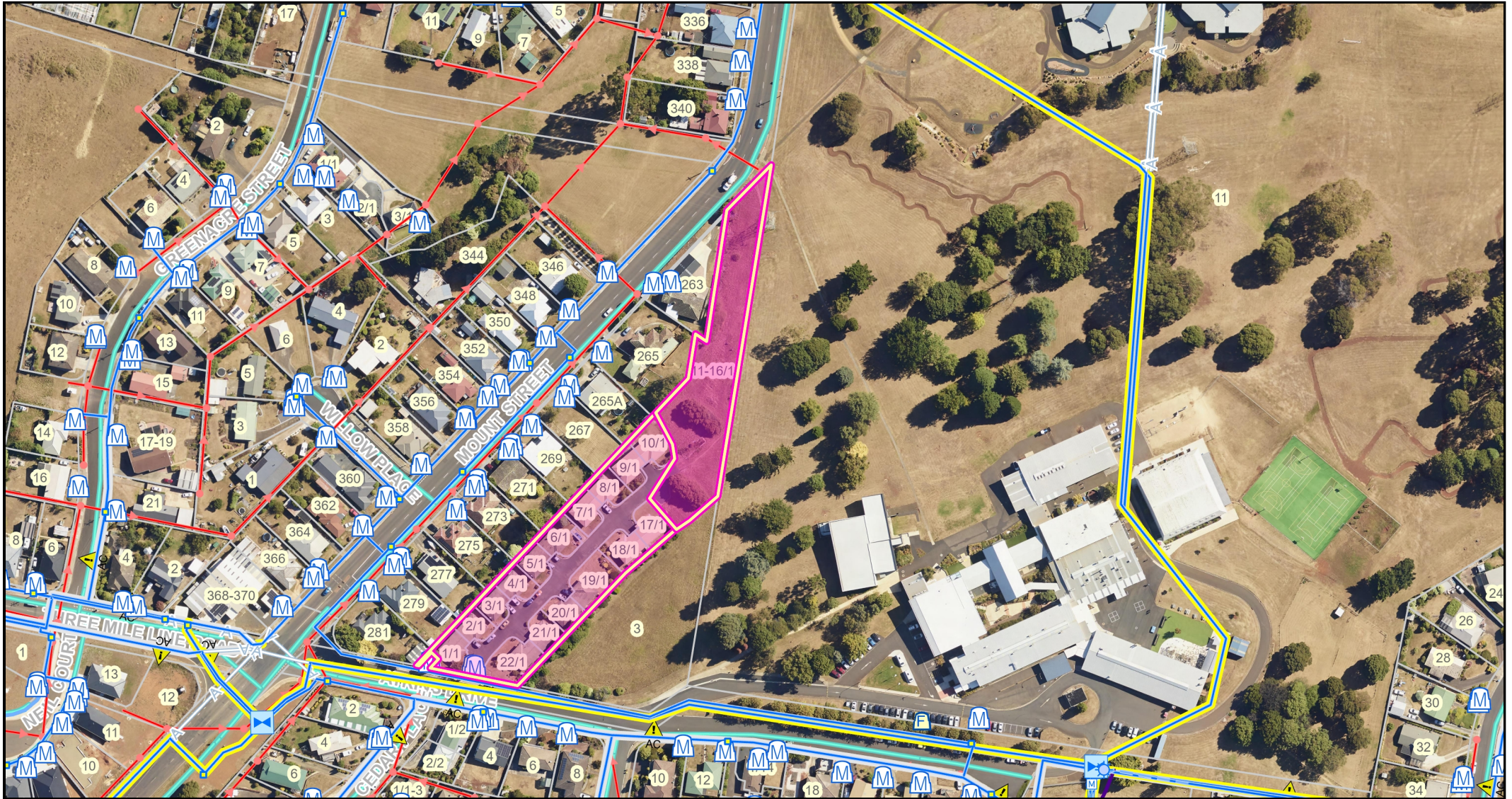
E				
D				
C				
B				
A	Extend detail and plot strata boundaries	AE	14/12/23	AE
REV	AMENDMENTS	DRAWN	DATE	APPR.

NOTES:

SURVEYOR	GEOCID
ML & DJ	47578
DRAWN	CHECKED
CB	AE
DATE	
	13-06-2023

DETAIL SURVEY
1/11 ATKINS DRIVE, BURNIE
FOR TRINH CAPITAL

 PDA SURVEYORS, ENGINEERS & PLANNERS	6 Queen Street Burnie, Tasmania, 7320 PHONE: +61 03 6431 4400 FAX: +61 03 6431 6663 EMAIL: pda.bne@pda.com.au www.pda.com.au	SCALE	PAPER
	Also at: Launceston, Devonport, Hobart & Kingston	1: 1250	(A3)
		JOB NUMBER	DRAWING
		51434	-1A



TasWater Infrastructure

- Water Reticulation Main
- R Stormwater Rising Main
- Stormwater Gravity Reticulation Main
- P Sewer Pressure Reticulation Main
- R Sewer Rising Main
- Sewer Gravity Reticulation Main
- Recycled Water Distribution Main

Private Infrastructure

- P Water Mains - Private
- P Stormwater Gravity Main - Private
- P Sewer Pressurised Main - Private
- P Sewer Gravity Main - Private
- P Recycled Water Main - Private

Abandoned Infrastructure

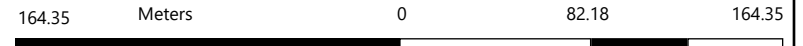
- A Water Abandoned Line
- A Sewer Abandoned Line
- Recycled Water Abandoned Line

NOTE:

The representation of the TasWater assets shown on this map was derived from data supplied by TasWater. TasWater makes no representation as to the accuracy or completeness of the assets shown on this map.



1: 3,235



GENERAL INFORMATION

ACCREDITED DESIGNER:
ACCREDITATION NUMBER:
LAND TITLE REFERENCE NUMBER:
ENERGY ASSESSMENT:
COUNCIL ZONE:
COUNCIL:

NICHOLAS BRANDSEMA
047538582
PID3121895, TITLE REF 162223/17
TBA
GENERAL RESIDENTIAL
BURNIE COUNCIL

SITE INFORMATION

SITE AREA:
DESIGN WIND SPEED:
SOIL CLASSIFICATION:
ALPINE AREA:
CORROSION ENVIRONMENT:
BUSHFIRE ATTACK LEVEL:
CLIMATE ZONE:

8587m²
TBA
TBA
N/A
TBA
TBA
7

SITE PLANS:

A100 COVER PAGE
A101 LOCATION PLAN
A102 SITE PLAN
A103 SEWER DRAINAGE PLAN
A104 STORMWATER DRAINAGE PLAN
A105 WATER LAYOUT PLAN
A106 SITE SETOUT PLAN
A107 STRATA BOUNDARY AREA
A108 DRIVEWAY & PARKING SETOUT PLAN
A109 PRIVATE OPEN SPACE PLAN
A110 VEHICLE TURNING CIRCLES
A111 SECTION A - LONG SECTION
A112 EXCAVATION & RETAINING WALL PLAN

PROPOSED UNIT 1:

U100 ELEVATIONS
U101 FLOOR PLAN
U102 SETOUT PLAN
U103 WALL FRAMING PLAN
U104 ELECTRICAL PLAN
U105 REFLECTED CEILING PLAN
U106 ROOF PLAN
U107 SECTION A-A
U108 DETAILS
U109 WALL TYPES
U110 WATERPROOFING DETAILS
U111 WINDOW & DOOR SCHEDULE
U112 LIGHTING CALCULATOR

PROPOSED UNIT 2-5:

U200 ELEVATIONS
U201 FLOOR PLAN
U202 SETOUT PLAN
U203 WALL FRAMING PLAN
U204 ELECTRICAL PLAN
U205 REFLECTED CEILING PLAN
U206 ROOF PLAN
U207 SECTION A-A
U208 DETAILS
U209 WALL TYPES
U210 WATERPROOFING DETAILS
U211 WINDOW & DOOR SCHEDULE
U212 LIGHTING CALCULATOR

PROPOSED UNIT 2-5:

U200 ELEVATIONS
U201 FLOOR PLAN
U202 SETOUT PLAN
U203 WALL FRAMING PLAN
U204 ELECTRICAL PLAN
U205 REFLECTED CEILING PLAN
U206 ROOF PLAN
U207 SECTION A-A
U208 DETAILS
U209 WALL TYPES
U210 WATERPROOFING DETAILS
U211 WINDOW & DOOR SCHEDULE
U212 LIGHTING CALCULATOR

ATKINS DRIVE

MOUNT STREET



LOCATION PLAN
Scale 1 : 500

n+b

22 Fieldings Way
Ulverstone, Tasmania
Australia
7315

m 0417 134 369 e nick@nplusb.com.au
License No. 047538582 ABN 946 222 219 16

Issued As
PRELIMINARY

Scale A2
1 : 500

Revision No.	Date	Description
A	12/11/24	Issued as PRELIMINARY

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do not scale off plans
all dimensions are in millimeters
confirm all dimensions on site
all work relevant NCC & AS

Project
PROPOSED UNIT DEVELOPMENT
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

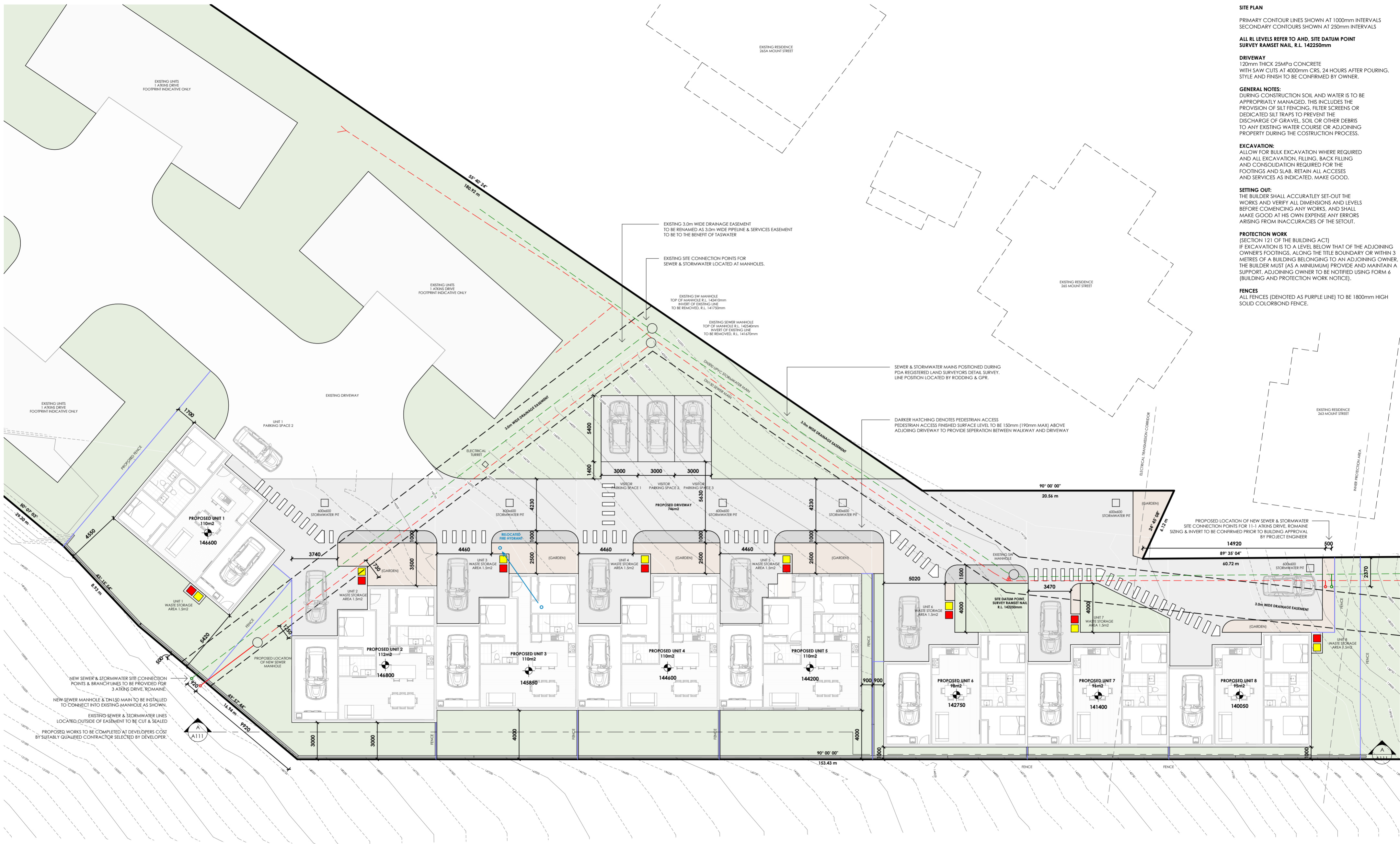
Sheet Title
LOCATION PLAN

Drawn	Issue Date	Project No.	Revision
NJB	30/01/26	P12456	A

Sheet Number

A101

/A112



SITE PLAN
 PRIMARY CONTOUR LINES SHOWN AT 1000mm INTERVALS
 SECONDARY CONTOURS SHOWN AT 250mm INTERVALS

ALL RL LEVELS REFER TO AHD, SITE DATUM POINT SURVEY RAMSET NAIL, R.L. 142250mm

DRIVEWAY
 120mm THICK 25MPa CONCRETE
 WITH SAW CUTS AT 4000mm CRS, 24 HOURS AFTER POURING.
 STYLE AND FINISH TO BE CONFIRMED BY OWNER.

GENERAL NOTES:
 DURING CONSTRUCTION SOIL AND WATER IS TO BE APPROPRIATELY MANAGED, THIS INCLUDES THE PROVISION OF SILT FENCING, FILTER SCREENS OR DEDICATED SILT TRAPS TO PREVENT THE DISCHARGE OF GRAVEL, SOIL OR OTHER DEBRIS TO ANY EXISTING WATER COURSE OR ADJOINING PROPERTY DURING THE CONSTRUCTION PROCESS.

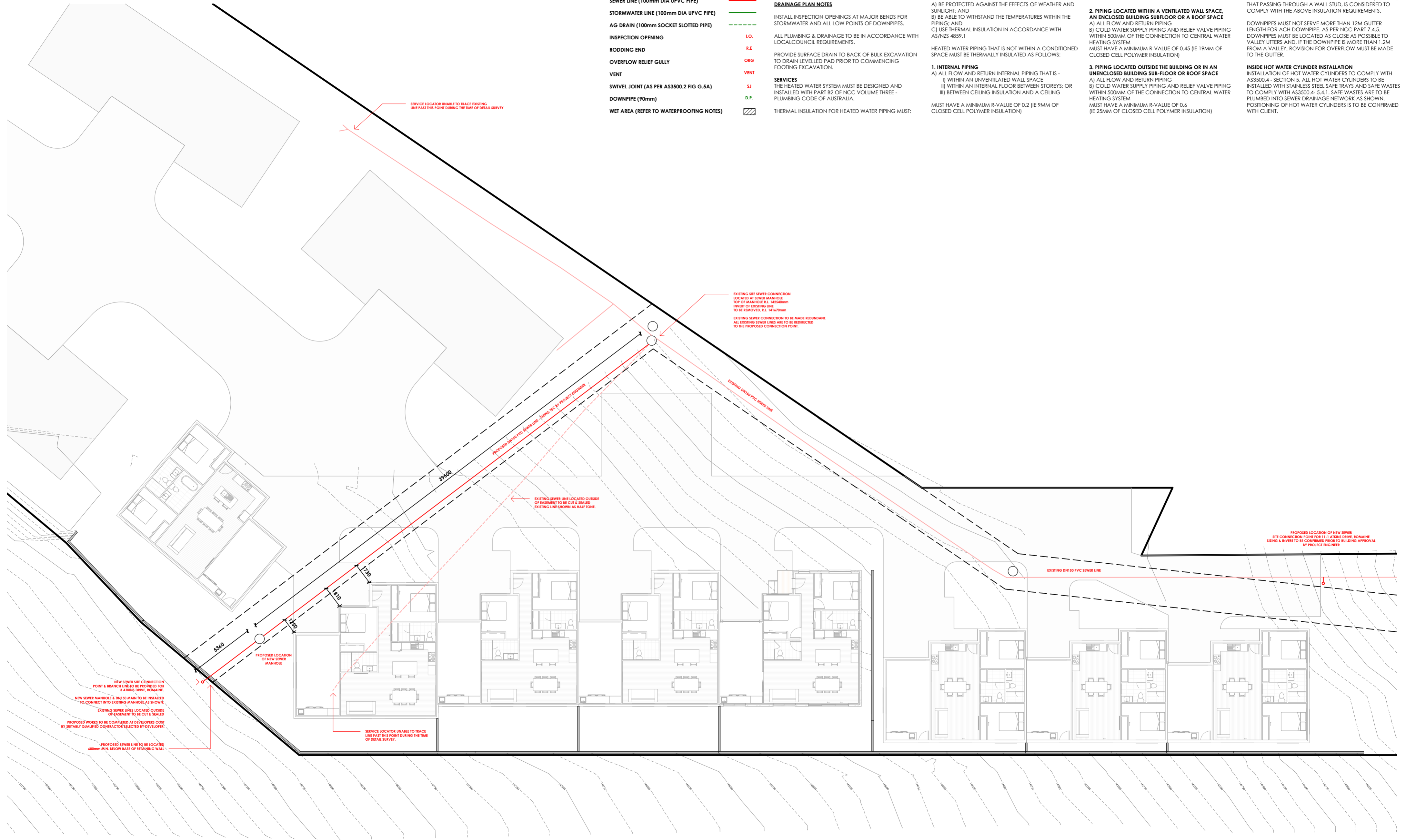
EXCAVATION:
 ALLOW FOR BULK EXCAVATION WHERE REQUIRED AND ALL EXCAVATION, FILLING, BACK FILLING AND CONSOLIDATION REQUIRED FOR THE FOOTINGS AND SLAB. RETAIN ALL ACCESSES AND SERVICES AS INDICATED. MAKE GOOD.

SETTING OUT:
 THE BUILDER SHALL ACCURATELY SET-OUT THE WORKS AND VERIFY ALL DIMENSIONS AND LEVELS BEFORE COMMENCING ANY WORKS, AND SHALL MAKE GOOD AT HIS OWN EXPENSE ANY ERRORS ARISING FROM INACCURACIES OF THE SETOUT.

PROTECTION WORK
 (SECTION 121 OF THE BUILDING ACT)
 IF EXCAVATION IS TO A LEVEL BELOW THAT OF THE ADJOINING OWNER'S FOOTINGS, ALONG THE TITLE BOUNDARY OR WITHIN 3 METRES OF A BUILDING BELONGING TO AN ADJOINING OWNER, THE BUILDER MUST (AS A MINIMUM) PROVIDE AND MAINTAIN A SUPPORT, ADJOINING OWNER TO BE NOTIFIED USING FORM 6 (BUILDING AND PROTECTION WORK NOTICE).

FENCES
 ALL FENCES (DENOTED AS PURPLE LINE) TO BE 1800mm HIGH SOLID COLORBOND FENCE.

SITE PLAN
 Scale 1 : 200



DRAINAGE PLAN LEGEND

SEWER LINE (100mm DIA UPVC PIPE)	—
STORMWATER LINE (100mm DIA UPVC PIPE)	---
AG DRAIN (100mm SOCKET SLOTTED PIPE)	---
INSPECTION OPENING	I.O.
RODDING END	R.E.
OVERFLOW RELIEF GULLY	O.R.G.
VENT	VENT
SWIVEL JOINT (AS PER AS3500.2 FIG G.5A)	S.J.
DOWNPIPE (90mm)	D.P.
WET AREA (REFER TO WATERPROOFING NOTES)	▨

DRAINAGE PLAN NOTES

INSTALL INSPECTION OPENINGS AT MAJOR BENDS FOR STORMWATER AND ALL LOW POINTS OF DOWNPIPES.

ALL PLUMBING & DRAINAGE TO BE IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS.

PROVIDE SURFACE DRAIN TO BACK OF BULK EXCAVATION TO DRAIN LEVELLED PAD PRIOR TO COMMENCING FOOTING EXCAVATION.

SERVICES
THE HEATED WATER SYSTEM MUST BE DESIGNED AND INSTALLED WITH PART B2 OF NCC VOLUME THREE - PLUMBING CODE OF AUSTRALIA.

THERMAL INSULATION FOR HEATED WATER PIPING MUST:

A) BE PROTECTED AGAINST THE EFFECTS OF WEATHER AND SUNLIGHT; AND
B) BE ABLE TO WITHSTAND THE TEMPERATURES WITHIN THE PIPING; AND
C) USE THERMAL INSULATION IN ACCORDANCE WITH AS/NZS 4859.1

HEATED WATER PIPING THAT IS NOT WITHIN A CONDITIONED SPACE MUST BE THERMALLY INSULATED AS FOLLOWS:

1. INTERNAL PIPING
A) ALL FLOW AND RETURN INTERNAL PIPING THAT IS -
I) WITHIN AN UNVENTILATED WALL SPACE
II) WITHIN AN INTERNAL FLOOR BETWEEN STOREYS; OR
III) BETWEEN CEILING INSULATION AND A CEILING
MUST HAVE A MINIMUM R-VALUE OF 0.2 (IE 9MM OF CLOSED CELL POLYMER INSULATION)

2. PIPING LOCATED WITHIN A VENTILATED WALL SPACE, AN ENCLOSED BUILDING SUBFLOOR OR A ROOF SPACE
A) ALL FLOW AND RETURN PIPING
B) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500MM OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM
MUST HAVE A MINIMUM R-VALUE OF 0.45 (IE 19MM OF CLOSED CELL POLYMER INSULATION)

3. PIPING LOCATED OUTSIDE THE BUILDING OR IN AN UNENCLOSED BUILDING SUB-FLOOR OR ROOF SPACE
A) ALL FLOW AND RETURN PIPING
B) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500MM OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM
MUST HAVE A MINIMUM R-VALUE OF 0.6 (IE 25MM OF CLOSED CELL POLYMER INSULATION)

PIPING WITHIN AN INSULATED TIMBER FRAMED WALL, SUCH AS THAT PASSING THROUGH A WALL STUD, IS CONSIDERED TO COMPLY WITH THE ABOVE INSULATION REQUIREMENTS.

DOWNPIPES MUST NOT SERVE MORE THAN 12M GUTTER LENGTH FOR EACH DOWNPIPE. AS PER NCC PART 7.4.5. DOWNPIPES MUST BE LOCATED AS CLOSE AS POSSIBLE TO VALLEY UTTERS AND, IF THE DOWNPIPE IS MORE THAN 1.2M FROM A VALLEY, ROVISION FOR OVERFLOW MUST BE MADE TO THE GUTTER.

INSIDE HOT WATER CYLINDER INSTALLATION
INSTALLATION OF HOT WATER CYLINDERS TO COMPLY WITH AS3500.4 - SECTION 5. ALL HOT WATER CYLINDERS TO BE INSTALLED WITH STAINLESS STEEL SAFE TRAYS AND SAFE WASTES TO COMPLY WITH AS3500.4 - 5.4.1. SAFE WASTES ARE TO BE PLUMBING INTO SEWER DRAINAGE NETWORK AS SHOWN. POSITIONING OF HOT WATER CYLINDERS IS TO BE CONFIRMED WITH CLIENT.

NEW SEWER SITE CONNECTION POINT & BRANCH LINE TO BE PROVIDED FOR 11-1 ATKINS DRIVE, ROMAINE.
NEW SEWER MANHOLE & DN150 MAIN TO BE INSTALLED TO CONNECT INTO EXISTING MANHOLE AS SHOWN.
EXISTING SEWER LINES LOCATED OUTSIDE OF EASEMENT TO BE CUT & SEALED.
PROPOSED WORKS TO BE COMPLETED AT DEVELOPER'S COST BY SUITABLY QUALIFIED CONTRACTOR SELECTED BY DEVELOPER.
PROPOSED SEWER LINE TO BE LOCATED 400mm MIN. BELOW BASE OF RETAINING WALL.

PROPOSED LOCATION OF NEW SEWER MANHOLE

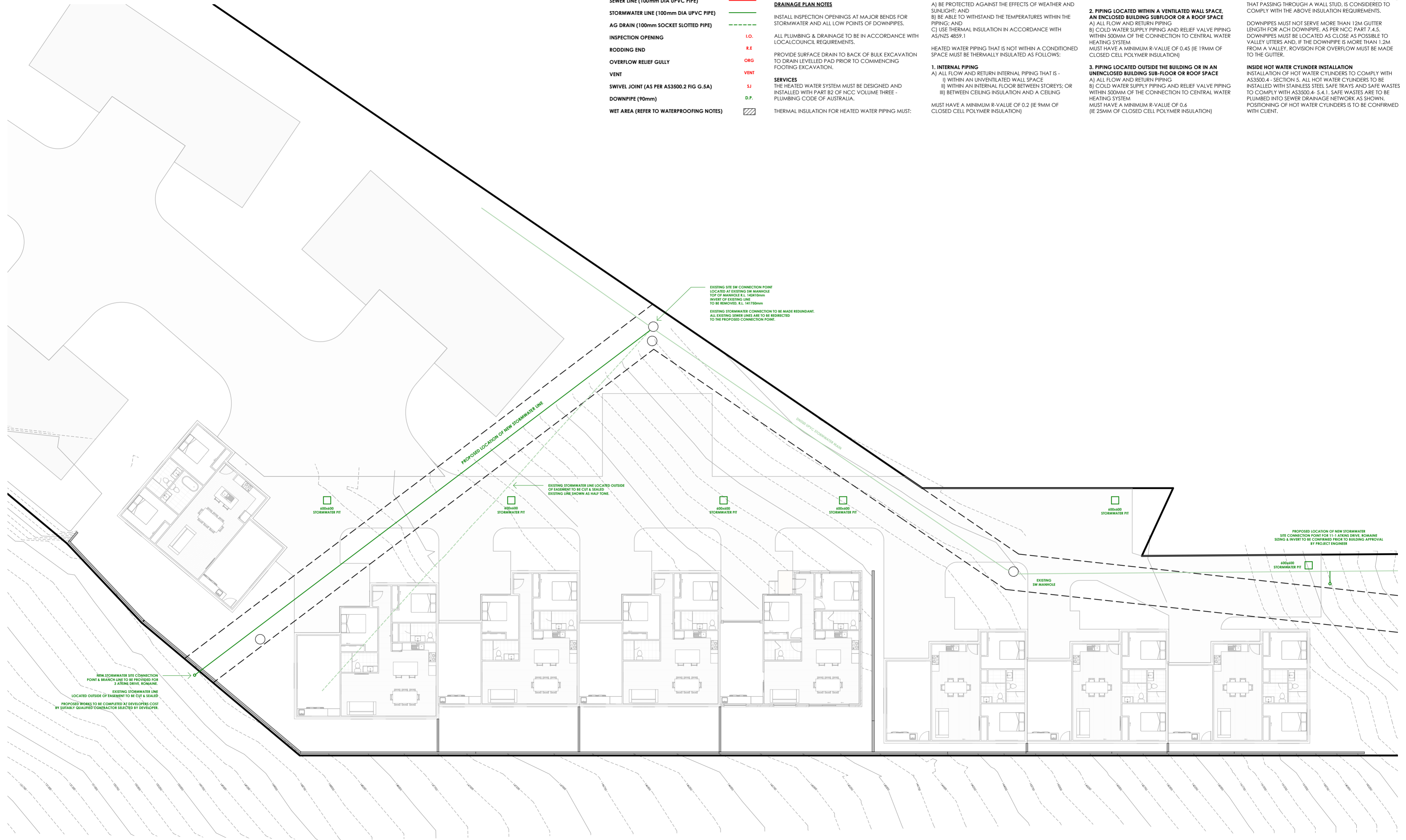
SERVICE LOCATOR UNABLE TO TRACE LINE PAST THIS POINT DURING THE TIME OF DETAIL SURVEY.

EXISTING SEWER LINE LOCATED OUTSIDE OF EASEMENT TO BE CUT & SEALED. EXISTING LINE SHOWN AS HALF TONE.

EXISTING SITE SEWER CONNECTION LOCATED AT SEWER MANHOLE TOP OF MANHOLE R.L. 142.45m. INVERT OF EXISTING LINE TO BE REMOVED. R.L. 141.70m.
EXISTING SEWER CONNECTION TO BE MADE REDUNDANT. ALL EXISTING SEWER LINES ARE TO BE REDIRECTED TO THE PROPOSED CONNECTION POINT.

PROPOSED LOCATION OF NEW SEWER SITE CONNECTION POINT FOR 11-1 ATKINS DRIVE, ROMAINE. SIGN & INVERT TO BE CONFIRMED PRIOR TO BUILDING APPROVAL BY PROJECT ENGINEER.

SEWER DRAINAGE PLAN
Scale 1:200



- DRAINAGE PLAN LEGEND**
- SEWER LINE (100mm DIA UPVC PIPE) ———
 - STORMWATER LINE (100mm DIA UPVC PIPE) ———
 - AG DRAIN (100mm SOCKET SLOTTED PIPE) - - - - -
 - INSPECTION OPENING I.O.
 - RODDING END R.E.
 - OVERFLOW RELIEF GULLY O.R.G.
 - VENT VENT
 - SWIVEL JOINT (AS PER AS3500.2 FIG G.5A) S.J.
 - DOWNPIPE (90mm) D.P.
 - WET AREA (REFER TO WATERPROOFING NOTES) [Hatched Box]

- DRAINAGE PLAN NOTES**
- INSTALL INSPECTION OPENINGS AT MAJOR BENDS FOR STORMWATER AND ALL LOW POINTS OF DOWNPIPES.
 - ALL PLUMBING & DRAINAGE TO BE IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS.
 - PROVIDE SURFACE DRAIN TO BACK OF BULK EXCAVATION TO DRAIN LEVELLED PAD PRIOR TO COMMENCING FOOTING EXCAVATION.
 - SERVICES**
THE HEATED WATER SYSTEM MUST BE DESIGNED AND INSTALLED WITH PART B2 OF NCC VOLUME THREE - PLUMBING CODE OF AUSTRALIA.
 - THERMAL INSULATION FOR HEATED WATER PIPING MUST:

- A) BE PROTECTED AGAINST THE EFFECTS OF WEATHER AND SUNLIGHT; AND
 - B) BE ABLE TO WITHSTAND THE TEMPERATURES WITHIN THE PIPING; AND
 - C) USE THERMAL INSULATION IN ACCORDANCE WITH AS/NZS 4859.1
- HEATED WATER PIPING THAT IS NOT WITHIN A CONDITIONED SPACE MUST BE THERMALLY INSULATED AS FOLLOWS:
- 1. INTERNAL PIPING**
- A) ALL FLOW AND RETURN INTERNAL PIPING THAT IS -
 - I) WITHIN AN UNVENTILATED WALL SPACE
 - II) WITHIN AN INTERNAL FLOOR BETWEEN STOREYS; OR
 - III) BETWEEN CEILING INSULATION AND A CEILING
- MUST HAVE A MINIMUM R-VALUE OF 0.2 (IE 9MM OF CLOSED CELL POLYMER INSULATION)

- 2. PIPING LOCATED WITHIN A VENTILATED WALL SPACE, AN ENCLOSED BUILDING SUBFLOOR OR A ROOF SPACE**
- A) ALL FLOW AND RETURN PIPING
 - B) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500MM OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM MUST HAVE A MINIMUM R-VALUE OF 0.45 (IE 19MM OF CLOSED CELL POLYMER INSULATION)
- 3. PIPING LOCATED OUTSIDE THE BUILDING OR IN AN UNENCLOSED BUILDING SUB-FLOOR OR ROOF SPACE**
- A) ALL FLOW AND RETURN PIPING
 - B) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500MM OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM MUST HAVE A MINIMUM R-VALUE OF 0.6 (IE 25MM OF CLOSED CELL POLYMER INSULATION)

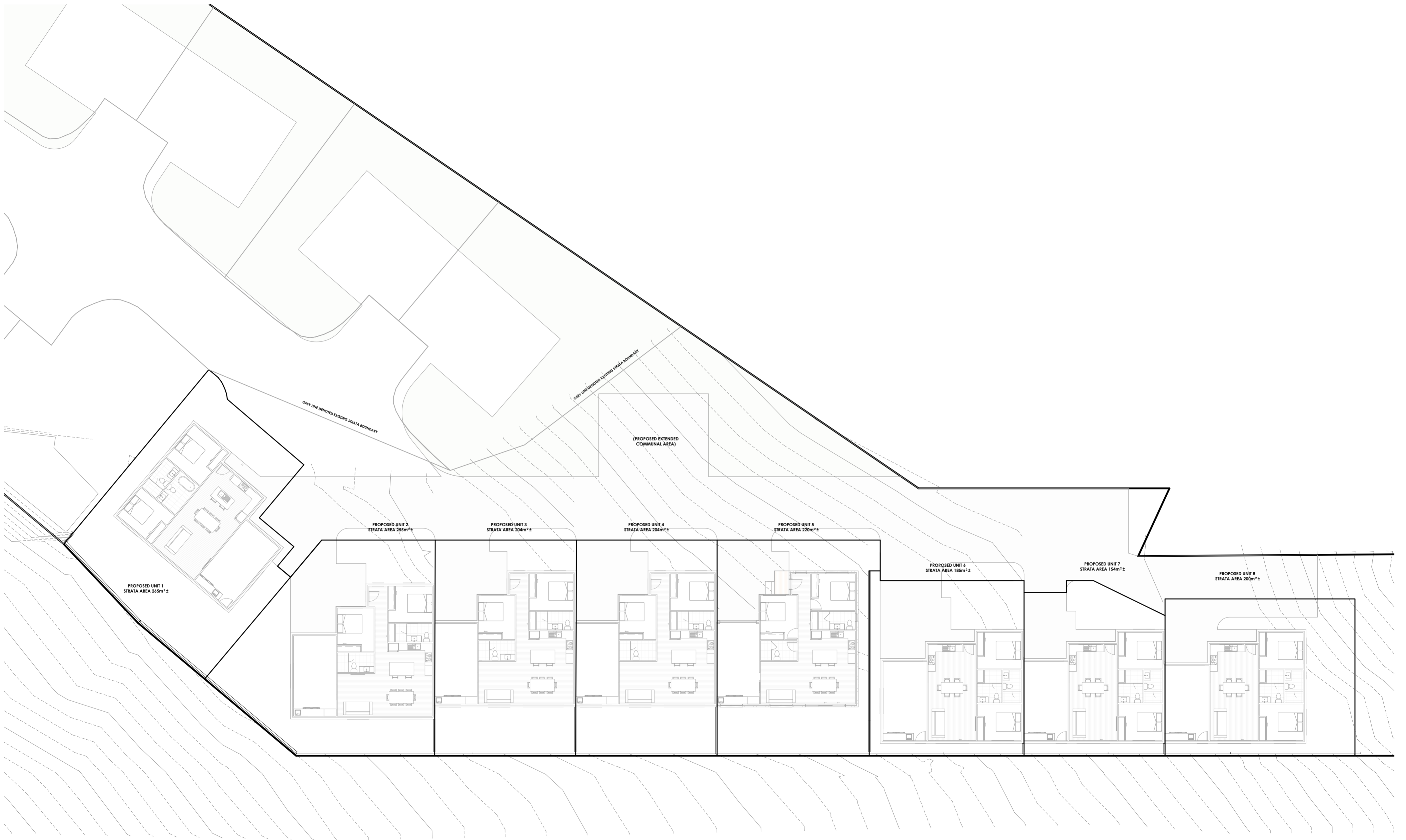
PIPING WITHIN AN INSULATED TIMBER FRAMED WALL, SUCH AS THAT PASSING THROUGH A WALL STUD, IS CONSIDERED TO COMPLY WITH THE ABOVE INSULATION REQUIREMENTS.

DOWNPIPES MUST NOT SERVE MORE THAN 12M GUTTER LENGTH FOR EACH DOWNPIPE. AS PER NCC PART 7.4.5. DOWNPIPES MUST BE LOCATED AS CLOSE AS POSSIBLE TO VALLEY UTTERS AND, IF THE DOWNPIPE IS MORE THAN 1.2M FROM A VALLEY, PROVISION FOR OVERFLOW MUST BE MADE TO THE GUTTER.

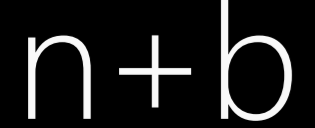
INSIDE HOT WATER CYLINDER INSTALLATION

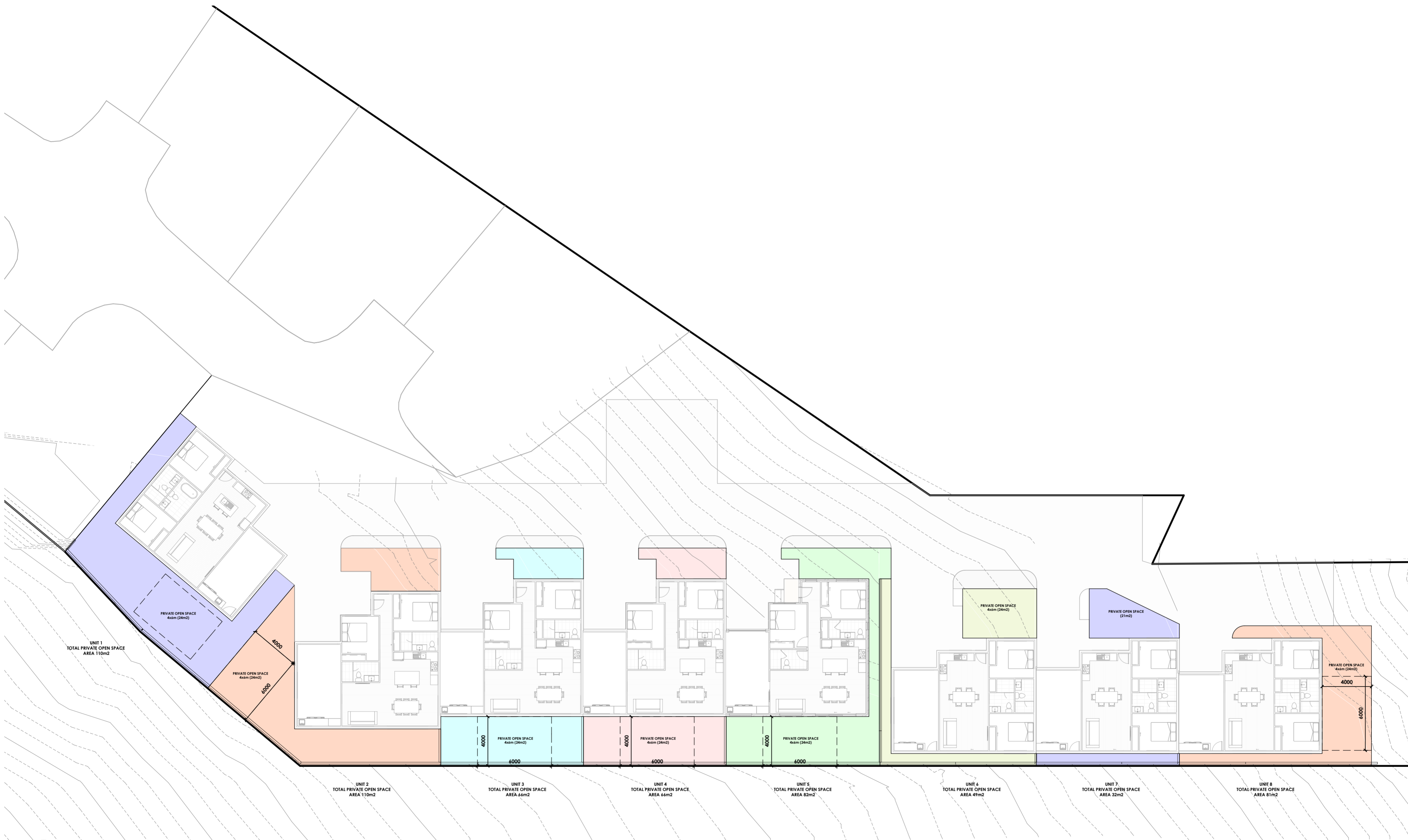
INSTALLATION OF HOT WATER CYLINDERS TO COMPLY WITH AS3500.4 - SECTION 5. ALL HOT WATER CYLINDERS TO BE INSTALLED WITH STAINLESS STEEL SAFE TRAYS AND SAFE WASTES TO COMPLY WITH AS3500.4 - 5.4.1. SAFE WASTES ARE TO BE PLUMBING INTO SEWER DRAINAGE NETWORK AS SHOWN. POSITIONING OF HOT WATER CYLINDERS IS TO BE CONFIRMED WITH CLIENT.

STORMWATER DRAINAGE PLAN
Scale 1 : 200




PROPOSED STRATA PLAN
 Scale 1 : 200

	22 Fieldings Way Ulverstone, Tasmania Australia 7315	Issued As PRELIMINARY	Scale A2 1 : 200	Revision No. A Date 12/11/24 Description Issued as PRELIMINARY	Project PROPOSED UNIT DEVELOPMENT Location 11-1 ATKINS DRIVE, ROMAINE Client ATKINS DRIVE PTY LTD	Sheet Title STRATA BOUNDARY AREA	Drawn NJB Issue Date 30/01/26 Project No. P12456 Revision A	Sheet Number A107 /A112
	<small>©COPYRIGHT These drawings and designs and the copyright there of are the property of nplusb and must not be used, retained or copied without the written permission of nplusb. ABN 946 222 219 16</small>		<small>do not scale off plans all dimensions are in millimeters confirm all dimensions on site all work relevant NCC & AS</small>					



PRIVATE OPEN SPACE PLAN
Scale 1 : 200



22 Fieldings Way
Ulverstone, Tasmania
Australia
7315
m 0417 134 369 e nick@nplusb.com.au
License No. 047538582 ABN 946 222 219 16

Issued As
PRELIMINARY

Scale A2
1 : 200

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Revision No.	Date	Description
A	12/11/24	Issued as PRELIMINARY

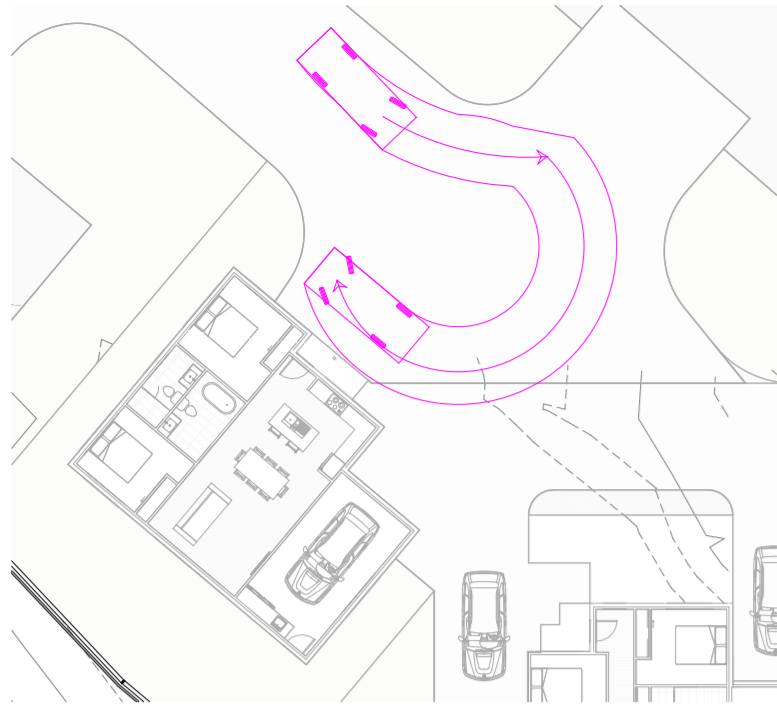
do not scale off plans
all dimensions are in millimeters
confirm all dimensions on site
all work relevant NCC & AS

Project
PROPOSED UNIT DEVELOPMENT
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

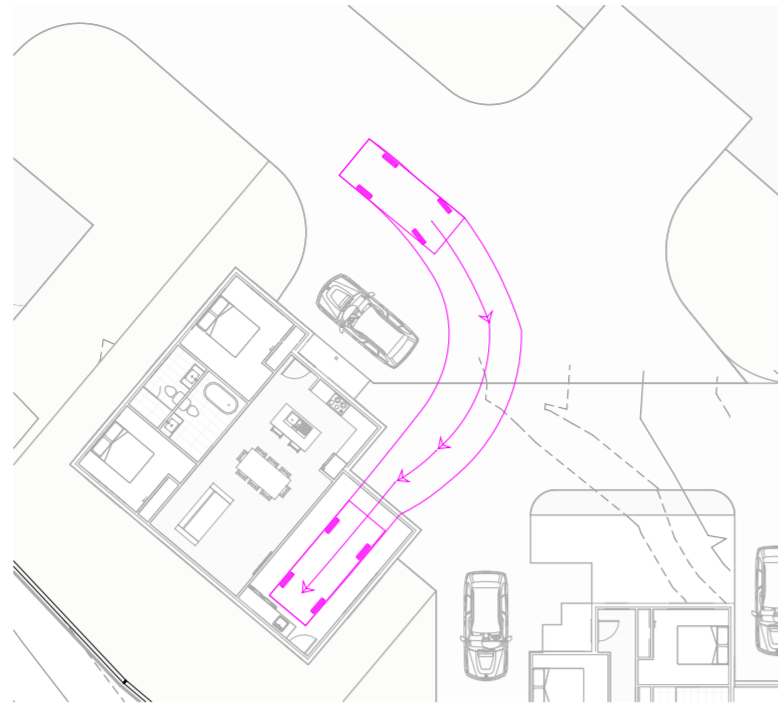
Drawn	Issue Date	Project No.	Revision
NJB	30/01/26	P12456	A

Sheet Title
PRIVATE OPEN SPACE PLAN

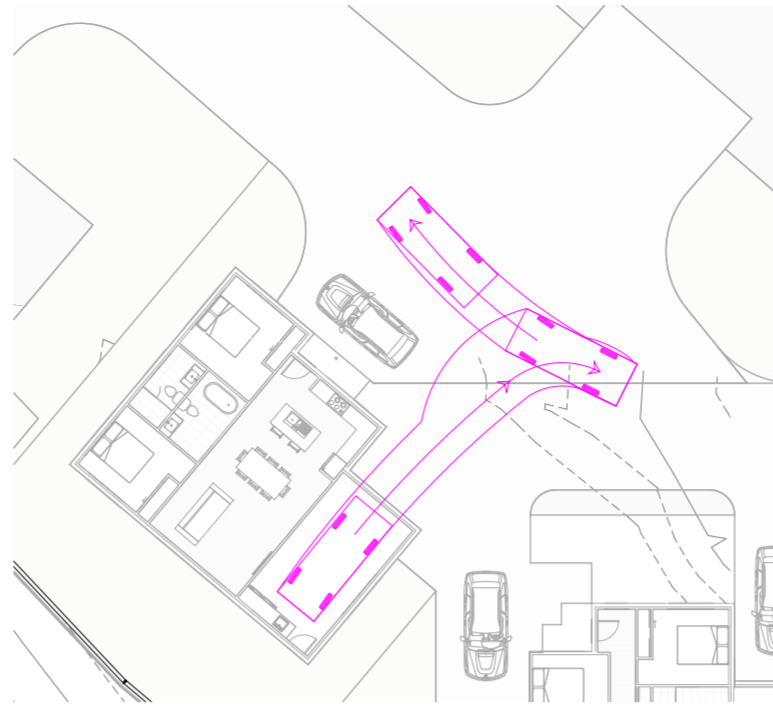
Sheet Number
A109
/A112



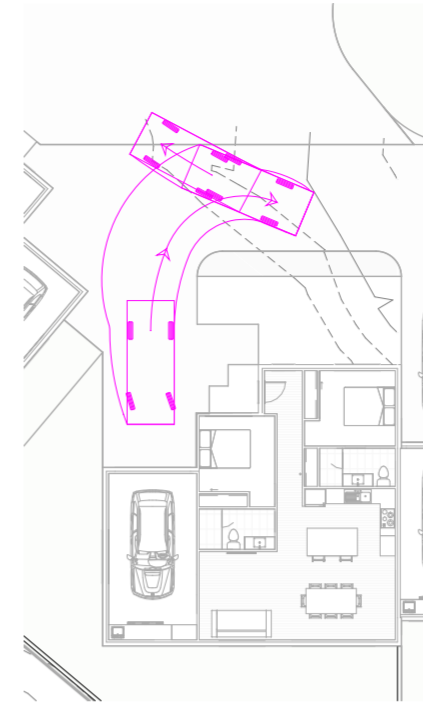
UNIT 1 - PARKING MANEUVER 1
Scale 1 : 300



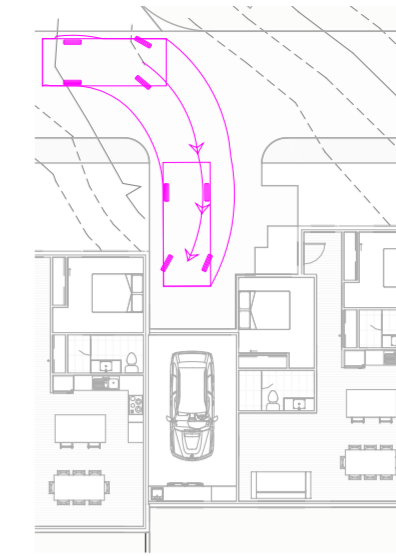
UNIT 1 - PARKING MANEUVER 2
Scale 1 : 300



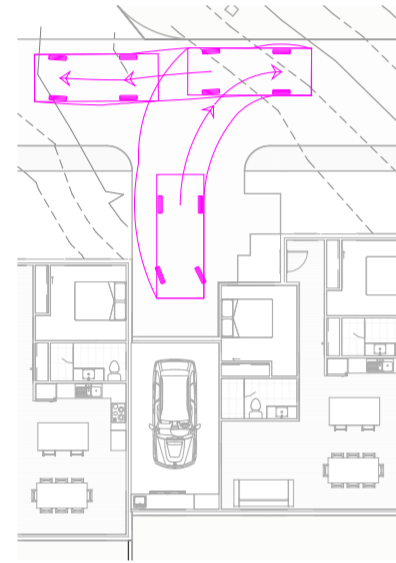
UNIT 1 - PARKING MANEUVER 3
Scale 1 : 300



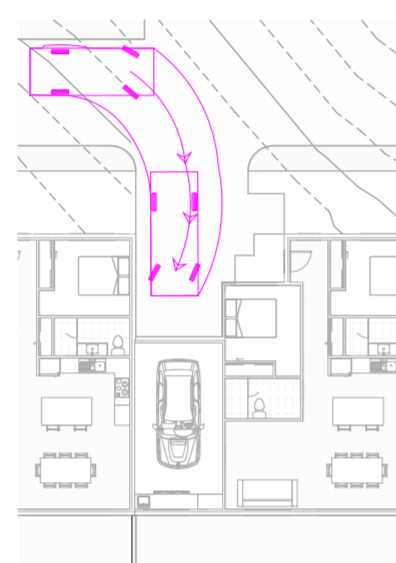
UNIT 2 - PARKING MANEUVER 1
Scale 1 : 300



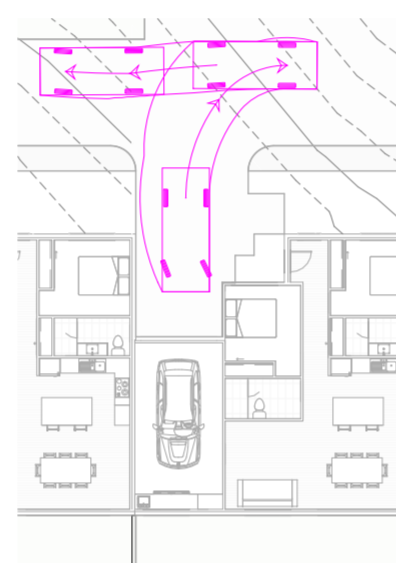
UNIT 3 - PARKING MANEUVER 1
Scale 1 : 300



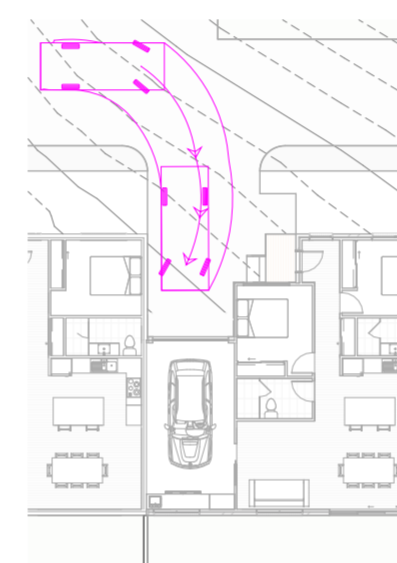
UNIT 3 - PARKING MANEUVER 2
Scale 1 : 300



UNIT 4 - PARKING MANEUVER 1
Scale 1 : 300



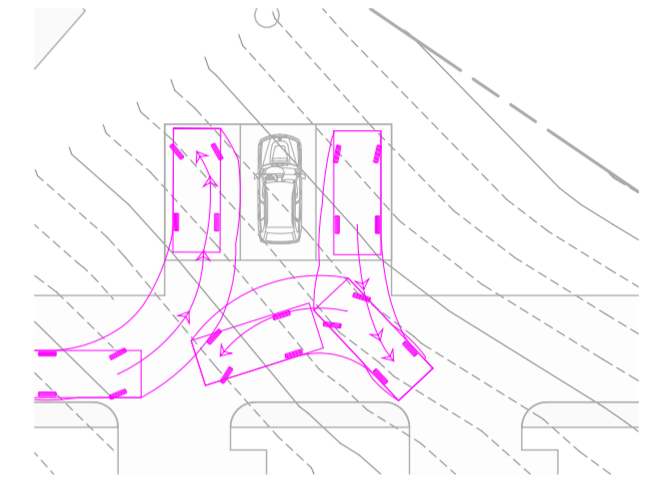
UNIT 4 - PARKING MANEUVER 2
Scale 1 : 300



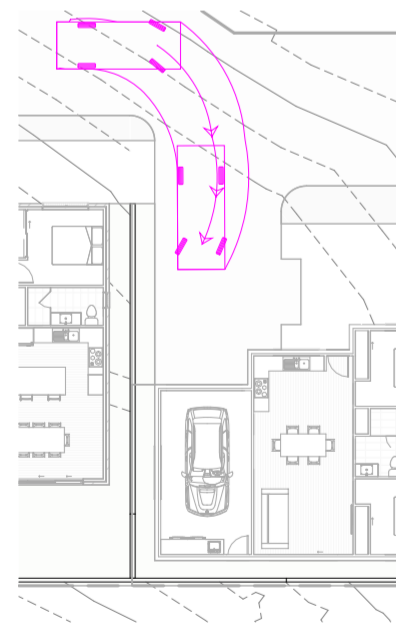
UNIT 5 - PARKING MANEUVER 1
Scale 1 : 300



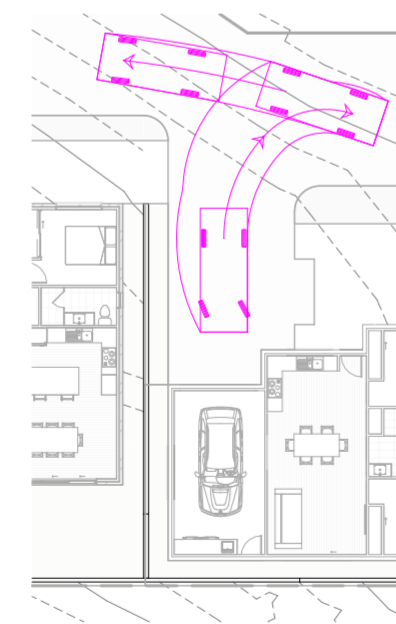
UNIT 5 - PARKING MANEUVER 2
Scale 1 : 300



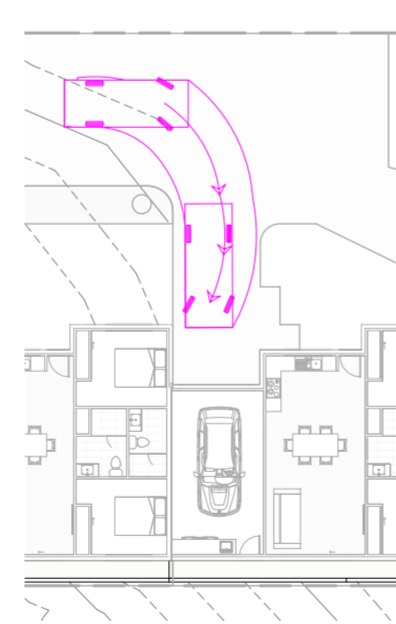
VISITOR PARKING MANEUVER
Scale 1 : 300



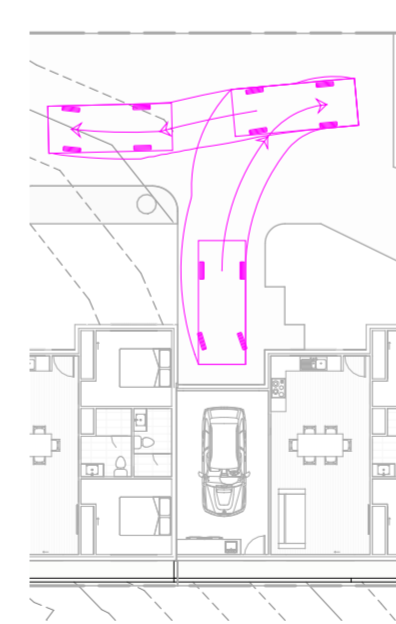
UNIT 6 - PARKING MANEUVER 1
Scale 1 : 300



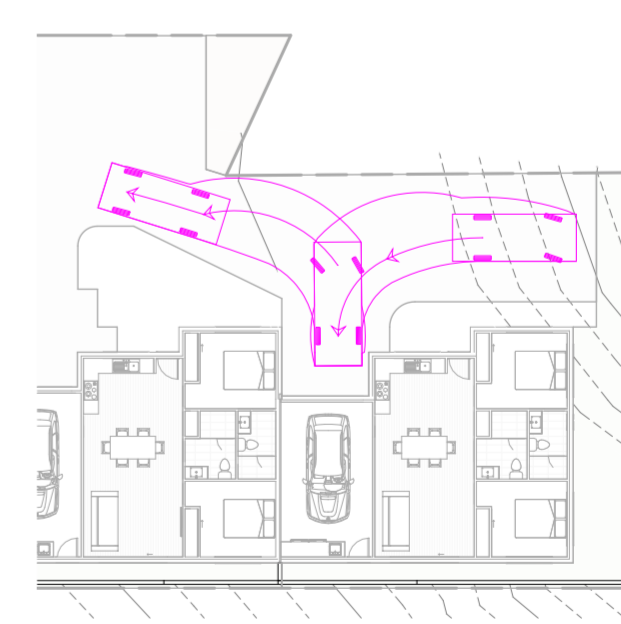
UNIT 6 - PARKING MANEUVER 2
Scale 1 : 300



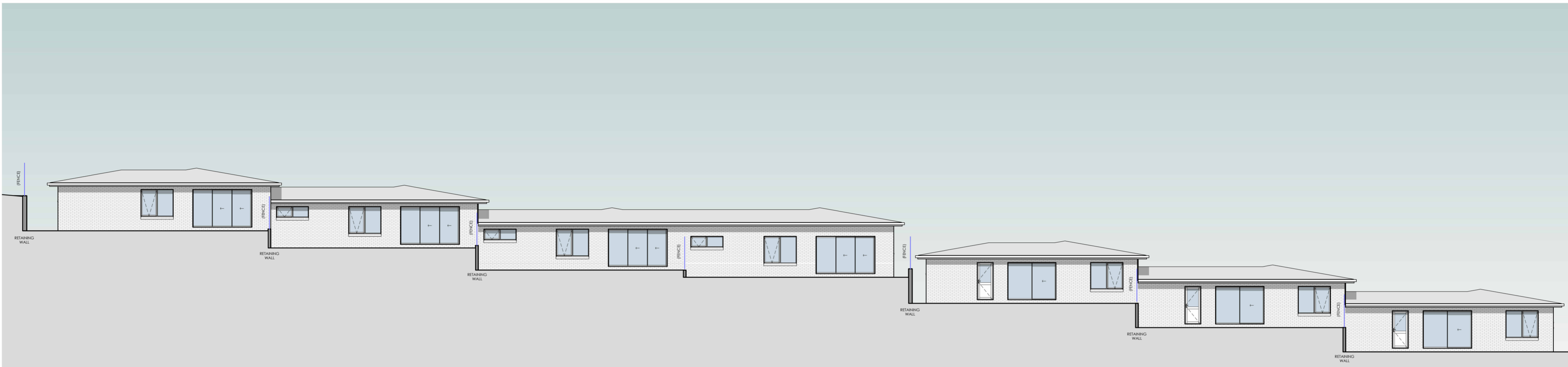
UNIT 7 - PARKING MANEUVER 1
Scale 1 : 300



UNIT 7 - PARKING MANEUVER 2
Scale 1 : 300

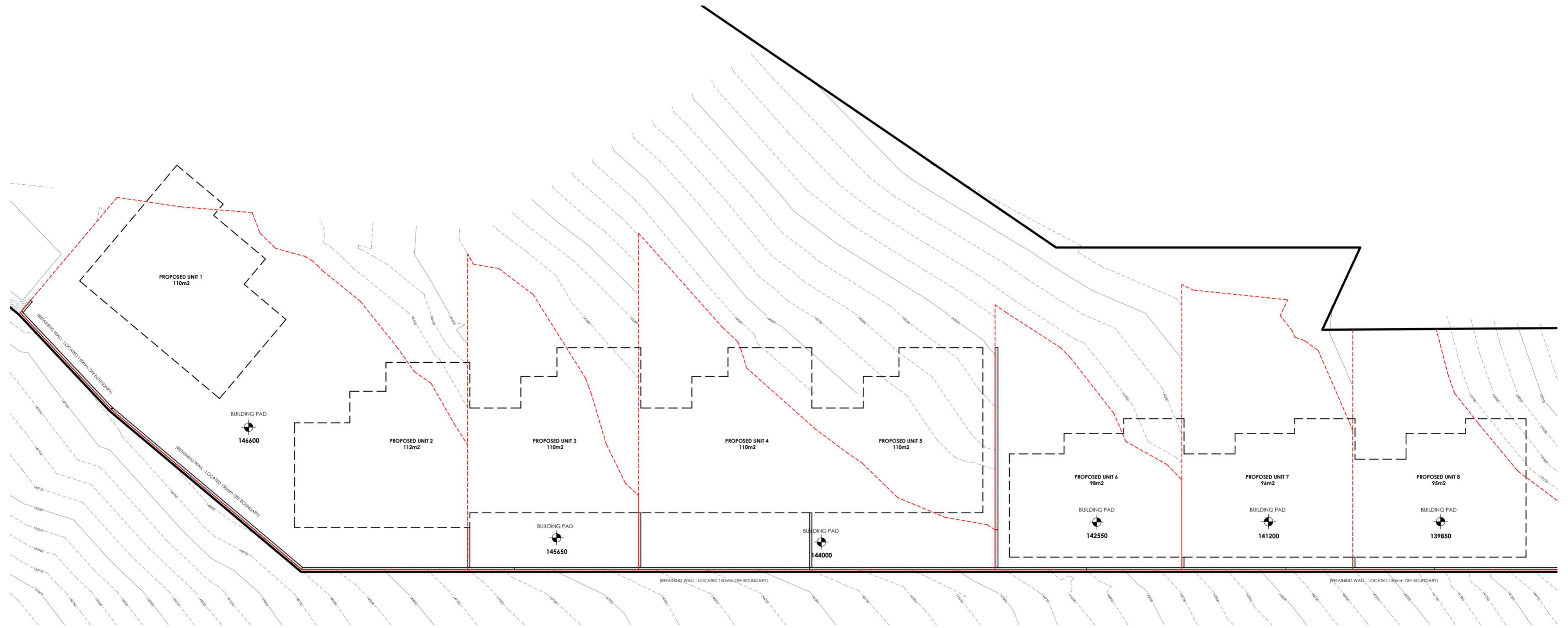


UNIT 8 - PARKING MANEUVER 1
Scale 1 : 300

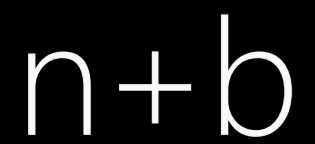


SECTION A - LONG SECTION

Scale 1 : 150



EXCAVATION & RETAINING WALL PLAN
Scale 1 : 200



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Ulverstone, Tasmania
Australia
7315
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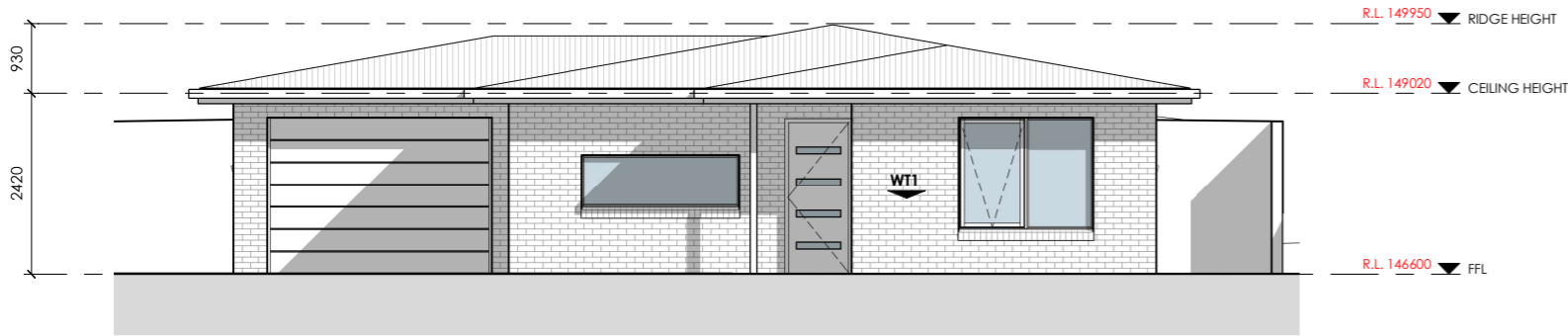
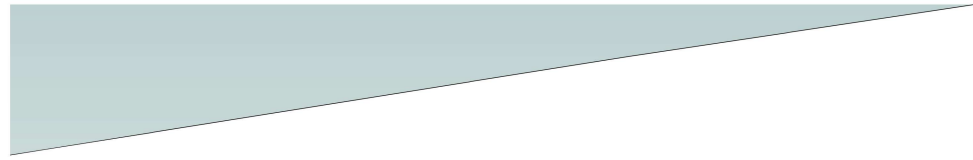
Issued As
PRELIMINARY
Scale A2
1 : 200
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Revision
No. B Date 30/01/26 Description Issued as PRELIMINARY
do not scale off plans all dimensions are in millimeters confirm all dimensions on site all work relevant NCC & AS

Project
PROPOSED UNIT DEVELOPMENT
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

Sheet Title
EXCAVATION & RETAINING WALL PLAN
Drawn **NJB** Issue Date **30/01/26** Project No. **P12456** Revision **B**

Sheet Number
A112
/A112



NORTH ELEVATION

Scale 1 : 100

WALL | FACADE MATERIALS & FINISHES

WT-1 BRICK VENEER, COLOUR & STYE BY OWNER.



EAVE CONSTRUCTION NCC VOLUME 2 PART 3.5.3.5
EAVE WIDTH OVERHANG - 600mm

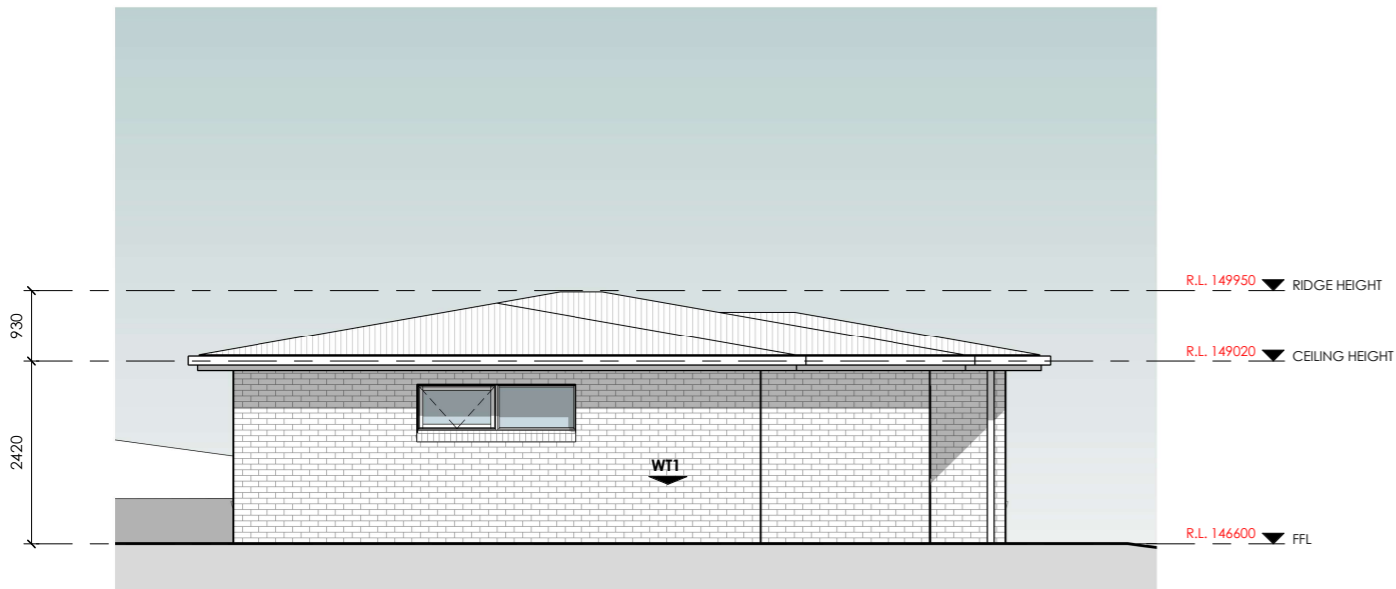
EAVES LINED WITH 'HARDIFLEX' CEMENT SHEET TRIMMERS LOCATED WITHIN 1200mm OF EXTERNAL CORNERS TO BE SPACED @ 500mm CENTERS. REMAINDER OF SHEET - 700mm CENTERS

FASTENER / FIXINGS WITHIN 1200mm OF EXTERNAL CORNERS @ 200mm CENTERS, REMAINDER OF SHEET - 300mm CENTERS

COLORBOND CUSTOM ORB ROOF CLADDING
INSTALLED AS PER MANUFACTURERS SPECIFICATIONS & AS1562 COLOUR BY OWNER.

SELECTED ALUMINIUM FRAMED WINDOWS & DOORS
NCC VOLUME 2 PART 3.6 POWDER COATED ALUMINIUM WINDOW & DOOR FRAMES, UNLESS OTHERWISE NOTED. REVEALS AS SELECTED. ALL FLASHING & FIXINGS TO MANUFACTURERS SPECIFICATIONS

GLAZING & FRAME CONSTRUCTION TO AS2047 & AS1288
ALL FIXINGS & FLASHINGS TO MANUFACTURERS REQUIREMENTS



EAST ELEVATION

Scale 1 : 100

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Scale A3
1 : 100

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Revision		
No.	Date	Description
B	30/01/26	Issued as PRELIMINARY

do not scale off plans
all dimensions are in millimeters
confirm all dimensions on site
all work relevant NCC & AS

Project
PROPOSED UNIT 1
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

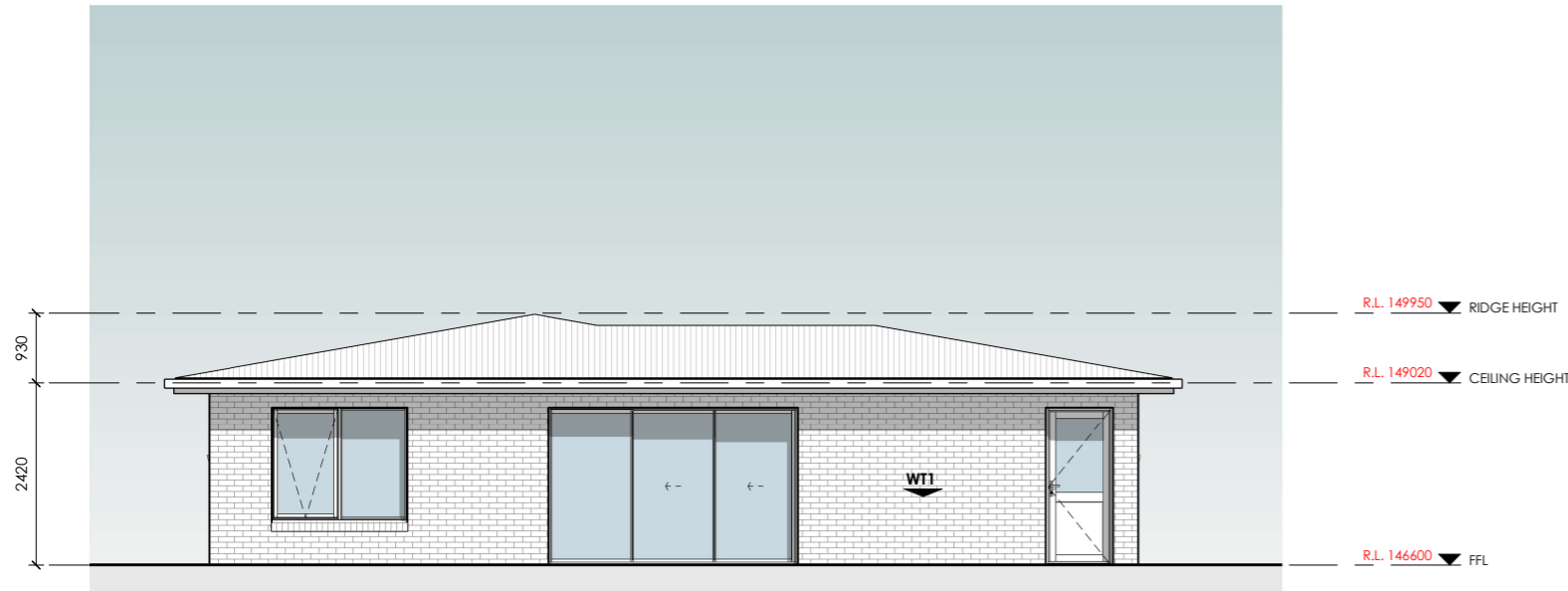
Sheet Title
ELEVATIONS 1 OF 2

Drawn	Issue Date	Project No.	Revision
NJB	30/01/26	P12456	B

Sheet Number

U100

/U120



SOUTH ELEVATION
Scale 1 : 100

WALL | FACADE MATERIALS & FINISHES

WT-1 BRICK VENEER, COLOUR & STYE BY OWNER.



EAVE CONSTRUCTION NCC VOLUME 2 PART 3.5.3.5
EAVE WIDTH OVERHANG - 600mm

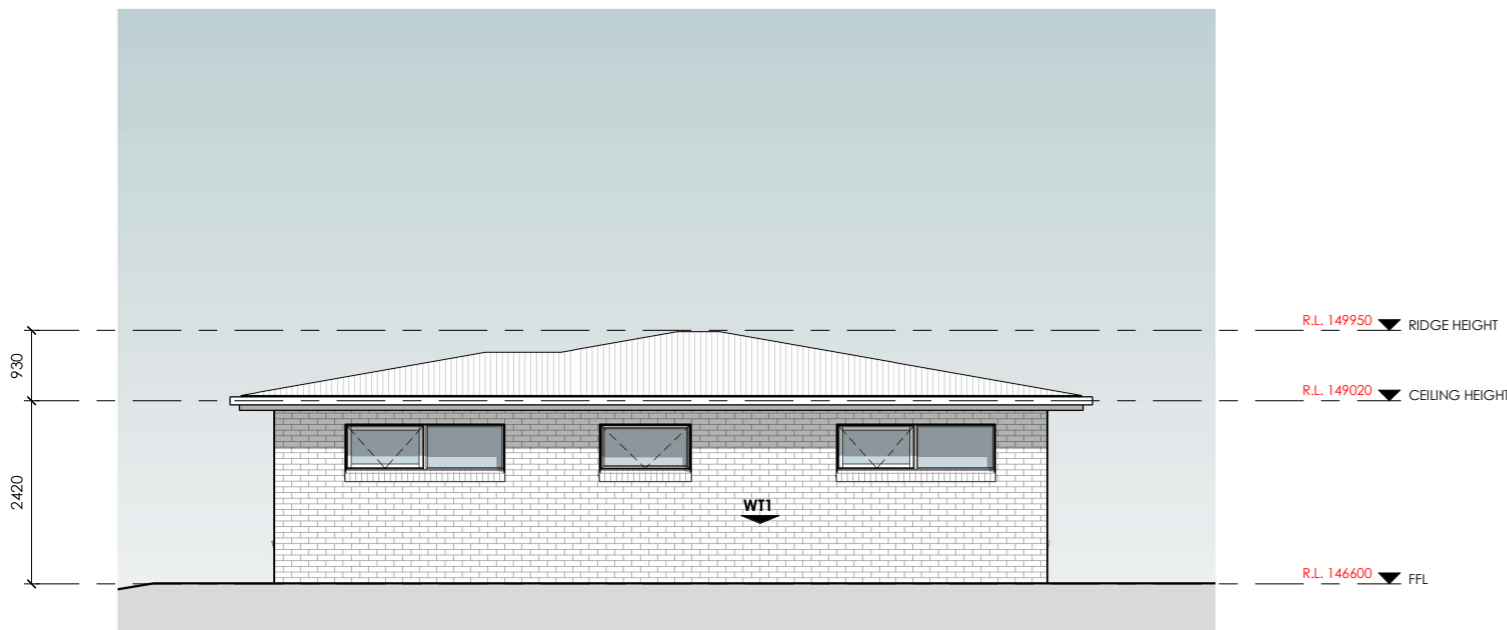
EAVES LINED WITH 'HARDIFLEX' CEMENT SHEET TRIMMERS LOCATED WITHIN 1200mm OF EXTERNAL CORNERS TO BE SPACED @ 500mm CENTERS. REMAINDER OF SHEET - 700mm CENTERS

FASTENER / FIXINGS WITHIN 1200mm OF EXTERNAL CORNERS @ 200mm CENTERS, REMAINDER OF SHEET - 300mm CENTERS

COLORBOND CUSTOM ORB ROOF CLADDING
INSTALLED AS PER MANUFACTURERS SPECIFICATIONS & AS1562 COLOUR BY OWNER.

SELECTED ALUMINIUM FRAMED WINDOWS & DOORS
NCC VOLUME 2 PART 3.6 POWDER COATED ALUMINIUM WINDOW & DOOR FRAMES, UNLESS OTHERWISE NOTED. REVEALS AS SELECTED. ALL FLASHING & FIXINGS TO MANUFACTURERS SPECIFICATIONS

GLAZING & FRAME CONSTRUCTION TO AS2047 & AS1288
ALL FIXINGS & FLASHINGS TO MANUFACTURERS REQUIREMENTS



WEST ELEVATION
Scale 1 : 100

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Scale A3
1 : 100

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Revision
No. Date Description
B 30/01/26 Issued as PRELIMINARY

do not scale off plans
all dimensions are in millimeters
confirm all dimensions on site
all work relevant NCC & AS

Project
PROPOSED UNIT 1
Location
11-1 ATKINS DRIVE, ROMAINE
Client
ATKINS DRIVE PTY LTD

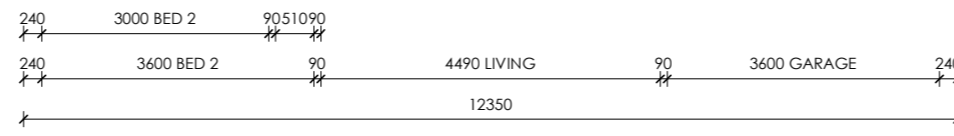
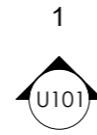
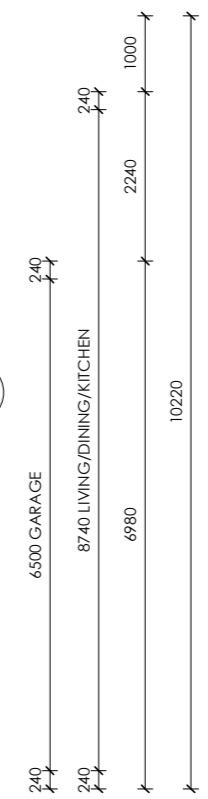
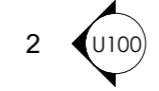
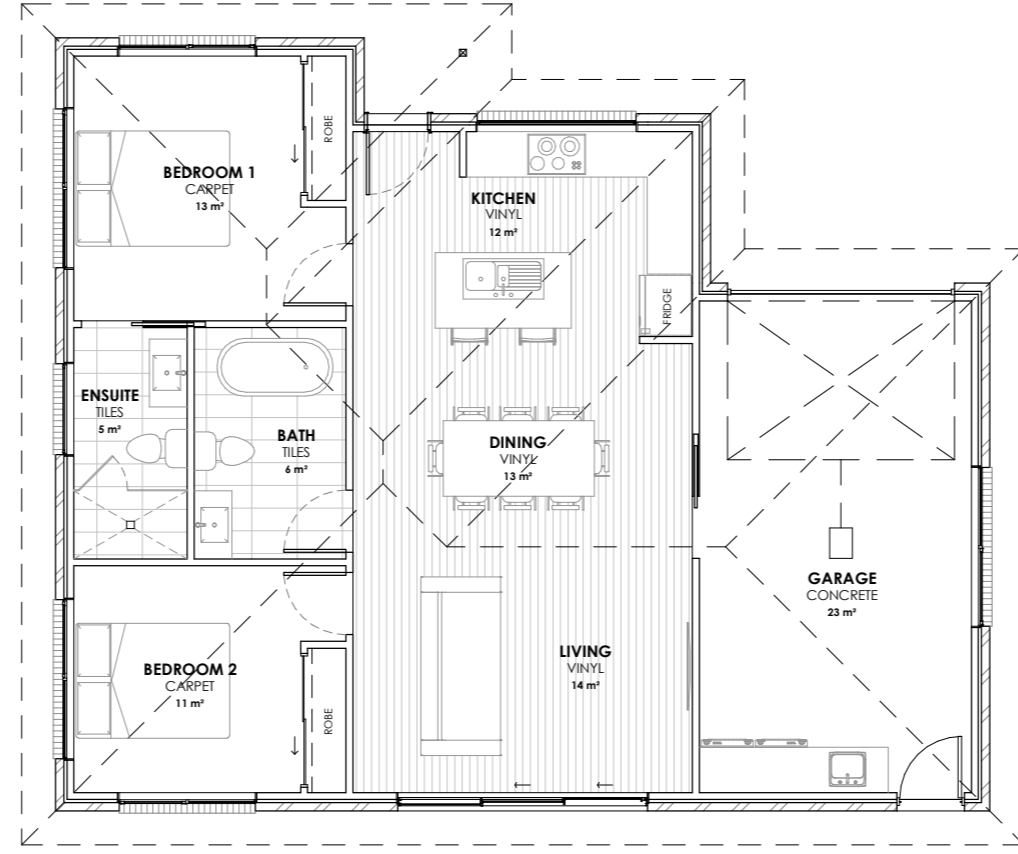
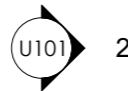
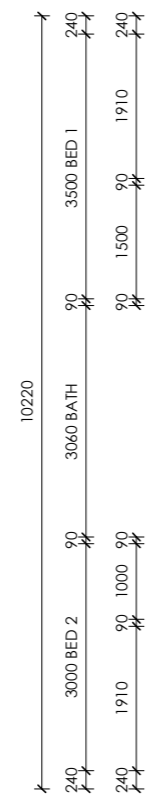
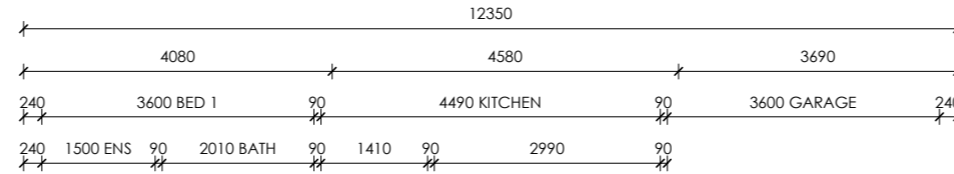
Sheet Title
ELEVATIONS 2 OF 2

Drawn	Issue Date	Project No.	Revision
NJB	30/01/26	P12456	B

Sheet Number

U101

/U120



FLOOR AREAS & FINISHES

FLOOR AREA - 110m2

VINYL
FLOOR AREA APPROX - 39m2
 5mm SELECTED VINYL PLANK FLOORING

CARPET
FLOOR AREA APPROX - 24m2
 SELECTED CARPET AND UNDERLAY


TILES
FLOOR AREA APPROX - 11m2
 SELECTED TILES, GROUT, SEALANT, TRIMS
 SEALED PRIOR WITH A WATERPROOF
 MEMBRANE SYSTEM

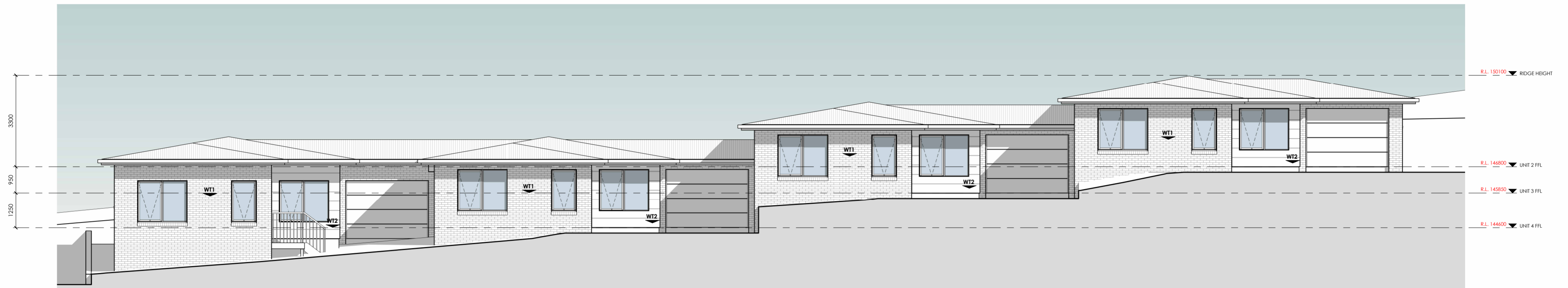
CONCRETE
FLOOR AREA APPROX - 23m2
 NO SURFACE FINISH REQUIRED.

NOTES
 ALL WINDOW DIMENSIONS TO BE CONFIRMED WITH
 CLIENT PRIOR TO CONSTRUCTION BEGINNING.

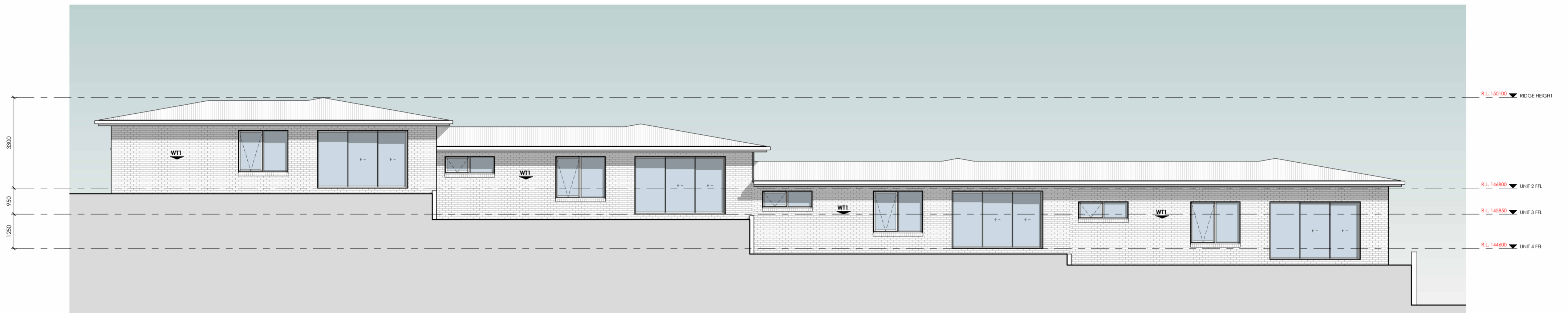
ALL GLAZING TO COMPLY WITH WITH NCC 3.6 & AS1288 & AS2047
 ALL WET AREAS TO COMPLY WITH NCC 3.8.1 & AS3740
 ALL TIMBER FRAMING TO COMPLY WITH NCC 3.4.3 & AS1684
 ALL WORKS TO BE IN COMPLIANCE WITH NCC 3.12 & ENERGY EFFICIENCY

FLOOR PLAN
 Scale 1 : 100

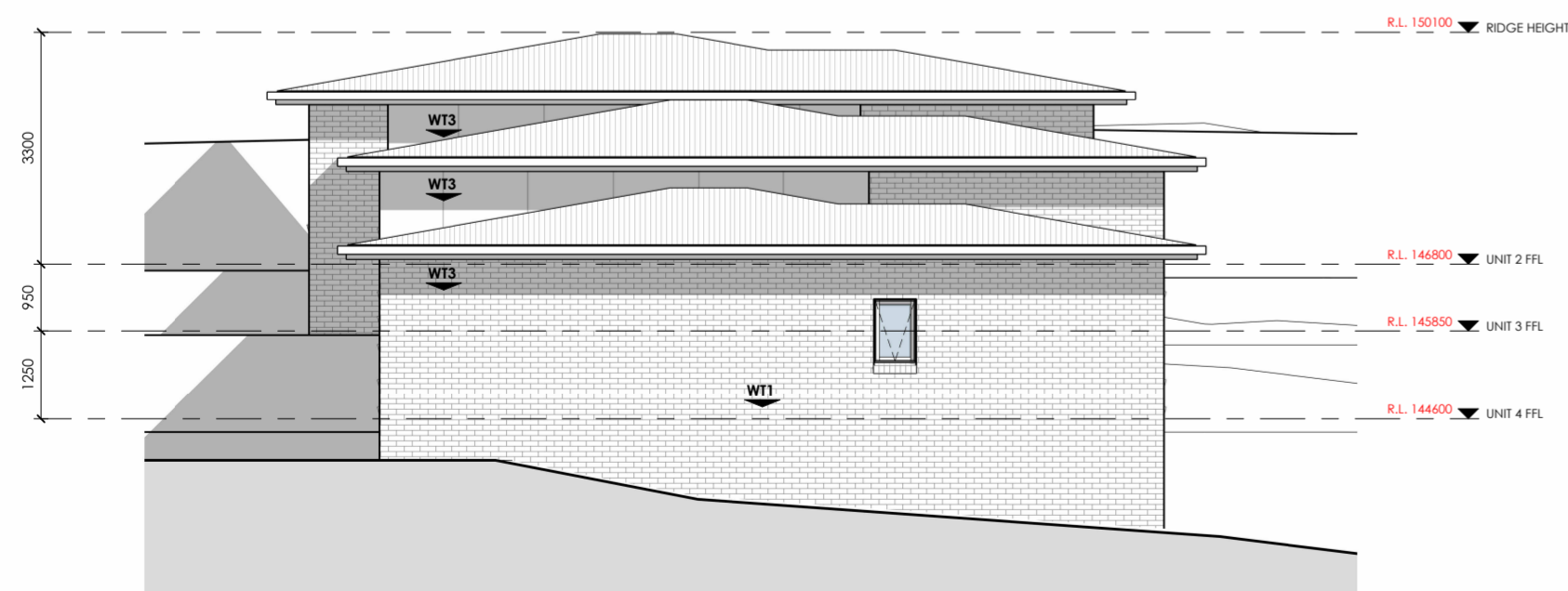
 <p>22 Fieldings Way Ulverstone, Tasmania Australia 7315</p> <p>m 0417 134 369 e nick@nplusb.com.au License No. 047538582 ABN 946 222 219 16</p>	<p>Issued As PRELIMINARY</p> <p>Scale A3 1 : 100</p>	<p>Revision</p> <table border="1"> <thead> <tr> <th>No.</th> <th>Date</th> <th>Description</th> </tr> </thead> <tbody> <tr> <td>B</td> <td>30/01/26</td> <td>Issued as PRELIMINARY</td> </tr> </tbody> </table> <p>do not scale off plans all dimensions are in millimeters confirm all dimensions on site all work relevant NCC & AS</p>	No.	Date	Description	B	30/01/26	Issued as PRELIMINARY	<p>Project PROPOSED UNIT 1</p> <p>Location 11-1 ATKINS DRIVE, ROMAINE</p> <p>Client ATKINS DRIVE PTY LTD</p>	<p>Sheet Title FLOOR PLAN</p> <table border="1"> <thead> <tr> <th>Drawn</th> <th>Issue Date</th> <th>Project No.</th> <th>Revision</th> </tr> </thead> <tbody> <tr> <td>NJB</td> <td>30/01/26</td> <td>P12456</td> <td>B</td> </tr> </tbody> </table>	Drawn	Issue Date	Project No.	Revision	NJB	30/01/26	P12456	B	<p>Sheet Number U102 /U120</p>
	No.	Date	Description																
B	30/01/26	Issued as PRELIMINARY																	
Drawn	Issue Date	Project No.	Revision																
NJB	30/01/26	P12456	B																



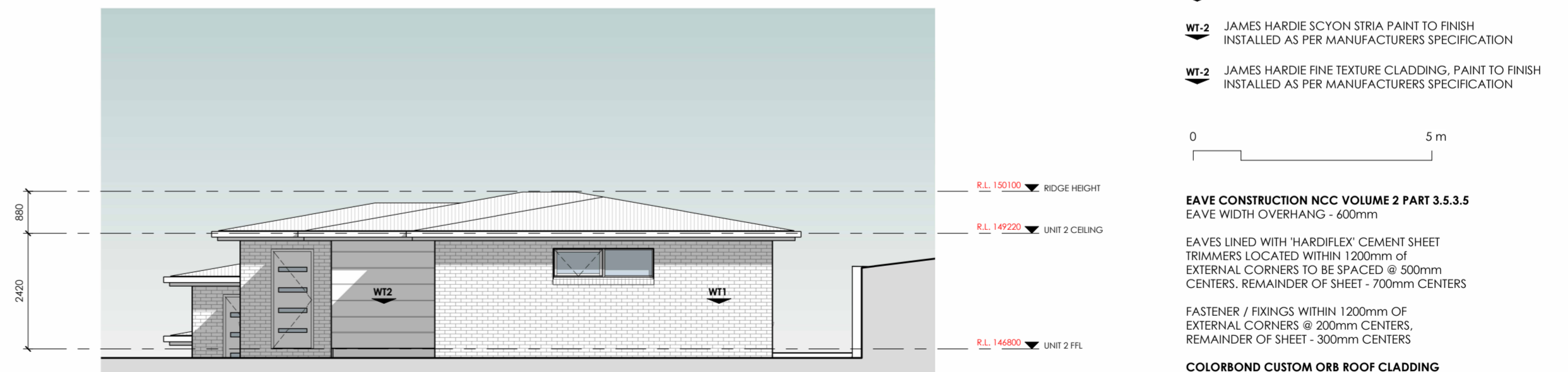
WEST ELEVATION
Scale 1 : 100



EAST ELEVATION
Scale 1 : 100



NORTH ELEVATION
Scale 1 : 100



SOUTH ELEVATION
Scale 1 : 100

WALL | FACADE MATERIALS & FINISHES

- WT-1** BRICK VENEER, COLOUR & STYE BY OWNER.
- WT-2** JAMES HARDIE SCYON STRIA PAINT TO FINISH
INSTALLED AS PER MANUFACTURERS SPECIFICATION
- WT-2** JAMES HARDIE FINE TEXTURE CLADDING, PAINT TO FINISH
INSTALLED AS PER MANUFACTURERS SPECIFICATION

EAVE CONSTRUCTION NCC VOLUME 2 PART 3.5.3.5
EAVE WIDTH OVERHANG - 600mm

EAVES LINED WITH 'HARDIFLEX' CEMENT SHEET TRIMMERS LOCATED WITHIN 1200mm OF EXTERNAL CORNERS TO BE SPACED @ 500mm CENTERS. REMAINDER OF SHEET - 700mm CENTERS

FASTENER / FIXINGS WITHIN 1200mm OF EXTERNAL CORNERS @ 200mm CENTERS. REMAINDER OF SHEET - 300mm CENTERS

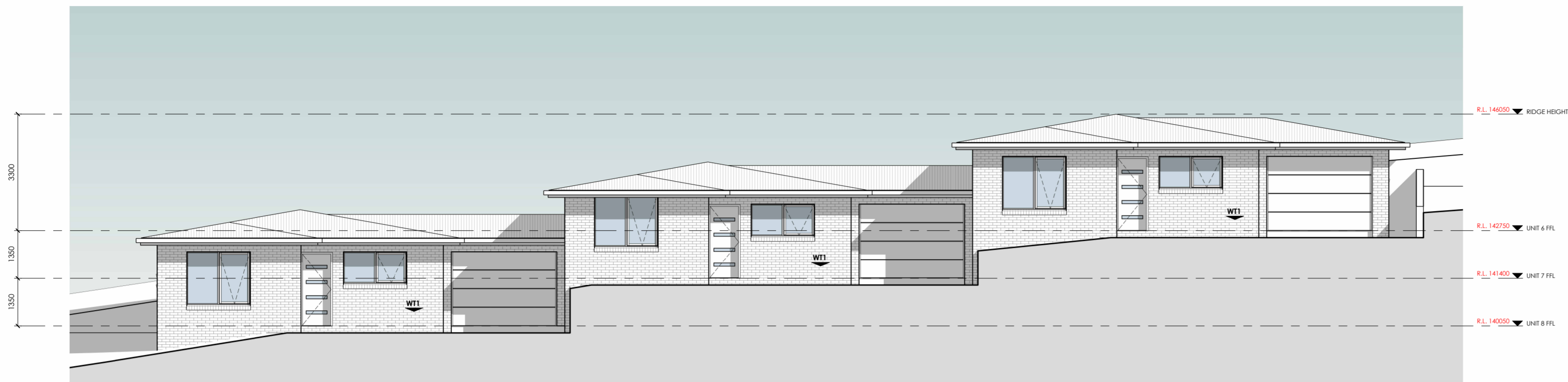
COLORBOND CUSTOM ORB ROOF CLADDING

INSTALLED AS PER MANUFACTURERS SPECIFICATIONS & AS1562 COLOUR BY OWNER.

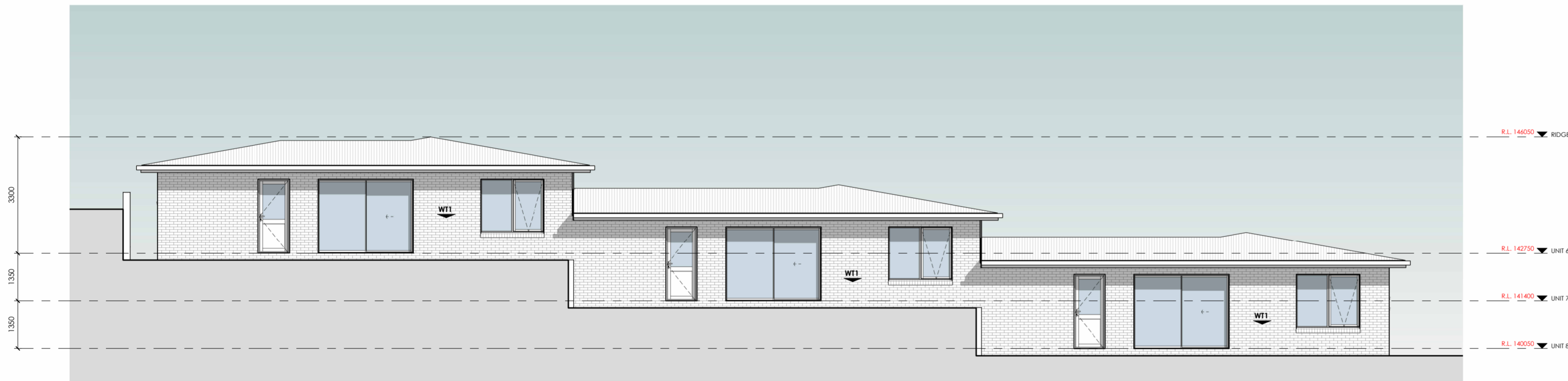
SELECTED ALUMINIUM FRAMED WINDOWS & DOORS

NCC VOLUME 2 PART 3.6 POWDER COATED ALUMINIUM WINDOW & DOOR FRAMES, UNLESS OTHERWISE NOTED. REVEALS AS SELECTED. ALL FLASHING & FIXINGS TO MANUFACTURERS SPECIFICATIONS

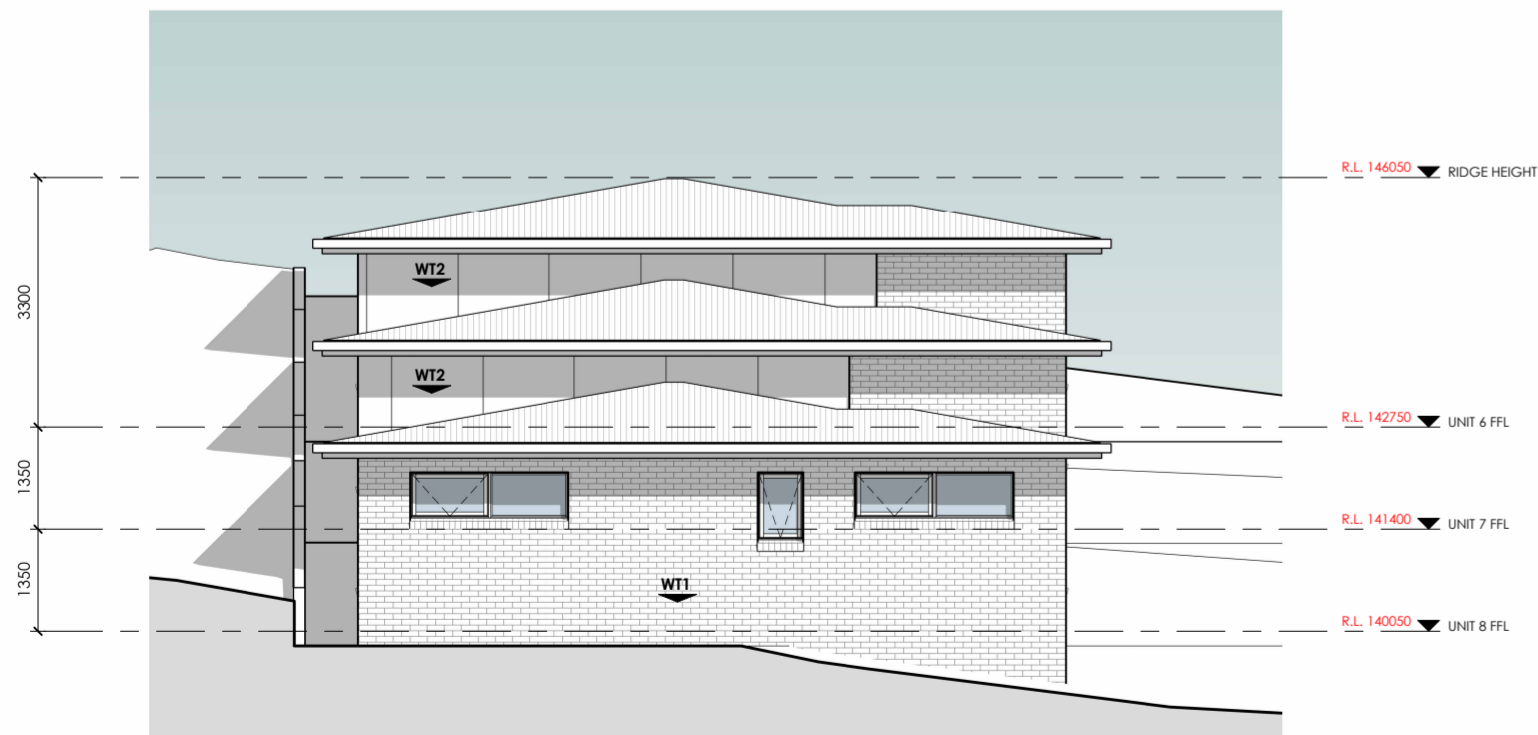
GLAZING & FRAME CONSTRUCTION TO AS2047 & AS1288 ALL FIXINGS & FLASHINGS TO MANUFACTURERS REQUIREMENTS



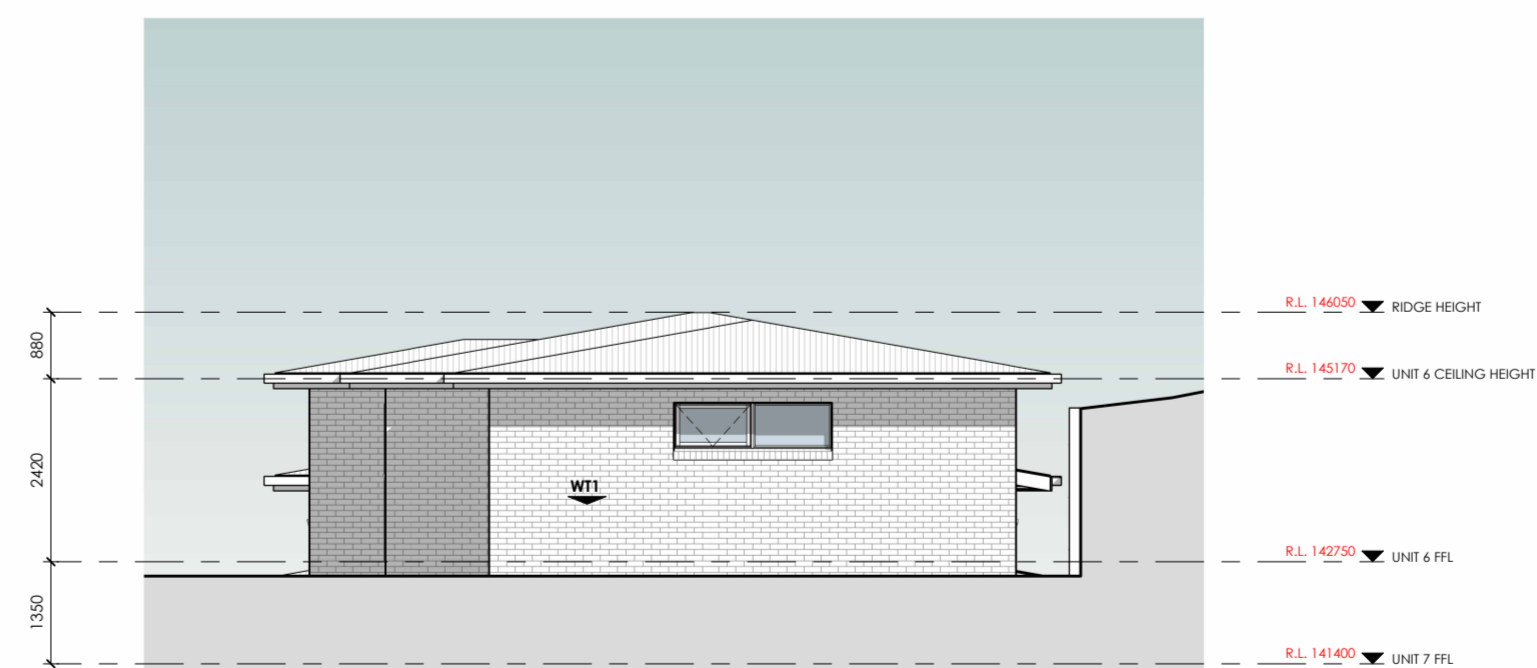
WEST ELEVATION
Scale 1 : 100



EAST ELEVATION
Scale 1 : 100



NORTH ELEVATION
Scale 1 : 100



SOUTH ELEVATION
Scale 1 : 100

WALL | FACADE MATERIALS & FINISHES

WT-1 BRICK VENEER, COLOUR & STYE BY OWNER.

WT-2 JAMES HARDIE FINE TEXTURE CLADDING, PAINT TO FINISH
INSTALLED AS PER MANUFACTURERS SPECIFICATION



EAVE CONSTRUCTION NCC VOLUME 2 PART 3.5.3.5
EAVE WIDTH OVERHANG - 600mm

EAVES LINED WITH 'HARDIFLEX' CEMENT SHEET
TRIMMERS LOCATED WITHIN 1200mm OF
EXTERNAL CORNERS TO BE SPACED @ 500mm
CENTERS, REMAINDER OF SHEET - 700mm CENTERS

FASTENER / FIXINGS WITHIN 1200mm OF
EXTERNAL CORNERS @ 200mm CENTERS,
REMAINDER OF SHEET - 300mm CENTERS

COLORBOND CUSTOM ORB ROOF CLADDING

INSTALLED AS PER MANUFACTURERS SPECIFICATIONS & AS1562
COLOUR BY OWNER.

SELECTED ALUMINIUM FRAMED WINDOWS & DOORS

NCC VOLUME 2 PART 3.6 POWDER COATED ALUMINIUM WINDOW &
DOOR FRAMES, UNLESS OTHERWISE NOTED, REVEALS AS SELECTED.
ALL FLASHING & FIXINGS TO MANUFACTURERS SPECIFICATIONS

GLAZING & FRAME CONSTRUCTION TO AS2047 & AS1288
ALL FIXINGS & FLASHINGS TO MANUFACTURERS REQUIREMENTS



FLOOR AREAS & FINISHES

UNIT 6 FLOOR AREA - 98m²
UNIT 7 FLOOR AREA - 96m²
UNIT 8 FLOOR AREA - 95m²

VINYL
FLOOR AREA APPROX - 96m²
 5mm SELECTED VINYL PLANK FLOORING

CARPET
FLOOR AREA APPROX - 66m²
 SELECTED CARPET AND UNDERLAY

TILES
FLOOR AREA APPROX - 27m²
 SELECTED TILES, GROUT, SEALANT, TRIMS
 SEALED PRIOR WITH A WATERPROOF
 MEMBRANE SYSTEM

CONCRETE
FLOOR AREA APPROX - 69m²
 NO SURFACE FINISH REQUIRED.

SKIRTING 66x18 PRE PRIMED BEVELLED
 SKIRTINGBOARD, PAINT TO FINISH.

NOTES
 ALL WINDOW DIMENSIONS TO BE CONFIRMED WITH
 CLIENT PRIOR TO CONSTRUCTION BEGINNING.

ALL GLAZING TO COMPLY WITH WITH NCC 3.6 & AS1288 & AS2047
 ALL WET AREAS TO COMPLY WITH NCC 3.8.1 & AS3740
 ALL TIMBER FRAMING TO COMPLY WITH NCC 3.4.3 & AS1684
 ALL WORKS TO BE IN COMPLIANCE WITH NCC 3.12 & ENERGY EFFICIENCY

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