

PLEASE QUOTE

Your Ref:

Our Ref: DA 2025/48

Enquiries: Planning Department

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.au

We value your feedback on our service.
Tell us about it at www.burnie.tas.gov.au/feedback



NOTICE OF APPLICATION FOR LAND USE PERMIT

(Section 57(3) Land Use Planning and Approvals Act 1993)

Advice to Adjoining Land Owner or Occupier

Application No: - DA 2025/48
Development Site: - 2-10 Massy-Greene Drive SOUTH BURNIE
CT: 165351/7
Proposal: - Transport Depot

Notice of the above application is served on you as an adjoining land owner or occupier.

The application may be viewed at -

Burnie City Council Customer Services Counter
Ground Floor, City Offices,
80 Wilson Street, Burnie

Between the hours of 8.45 am – 4.45 pm Monday to Friday inclusive (excluding public holidays) or on Council's website at www.burnie.tas.gov.au/permits

You are entitled to make representation in writing on any aspect of the proposal addressed to: -

General Manager,
Burnie City Council,
PO Box 973, Burnie 7320

or burnie@burnie.tas.gov.au by no later than 5.00 pm on **29 September 2025**. Council must have regard to any written representation received during the exhibition period when considering its decision on the application.

All persons who make representation will be notified within seven (7) days of the Council's decision. Any persons who made representation and is not satisfied with the Council decision may, under Section 61(5) of the *Land Use Planning and Approvals Act 1993*, lodge an appeal against that decision within fourteen (14) days of the date of that notice to: -

The Tasmanian Civil and Administrative Tribunal,
GPO Box 1311,
HOBART TAS 7001.

Should you have any enquiries regarding this development proposal, please do not hesitate to contact the Planning Department on (03) 6430 5700.

Troy McCarthy

PRINCIPAL PLANNER

Date of Notice: - **13 September 2025**

BURNIE CITY COUNCIL
PO Box 973, BURNIE, TASMANIA 7320.
Ph : (03) 6430 5700
Email : burnie@burnie.tas.gov.au



Land Use Planning and Approvals Act 1993

Tasmanian Planning Scheme

PERMIT APPLICATION

Office use only

Application No _____

Date Received _____

Permit Pathway - *Permitted/Discretionary*

Use or Development Site:

Street Address

2-10 Massy Greene Drive, Burnie, Tas

Certificate of
Title Reference

Volume 165351 Folio 7 Edition 3

Applicant

First Name

Brendan

Second
Name

Surname

Stanborough

Postal Address:

Rare Innovation
22-24 Patterson Street
Launceston
Tasmania
7250

Phone No:

0363889200

Mobile:

Email Address:

I/we consent for all giving of information and the serving of notices in relation to this application to be delivered electronically to the above email address?

YES



NO



Applicants Signature: Bstanborough

Owner (note – if more than one owner, all names must be indicated)

First Name

David

Second Name

Surname

Stubbs

Postal Address:

Phone No:

Instruction for making a permit application

a) *Use or development?*

The application must provide a full description of the proposed use and/or development and of the manner in which the use and/or development is to operate.

“Use” is the purpose or manner for which land is utilised. “Development” is any site works (including any change in natural condition or topography of land and the clearing or conversion of vegetation), and the construction, alteration, or removal of buildings, structures and signs, required in order to prepare a site for use or to change existing conditions within a site. Subdivision is development.

Clause 6.2 Tasmanian Planning Scheme provides the use classes by which all use or development must be described. Development must be categorised by reference to the use class it is to serve.

b) *Required Information*

Adequate statements, plans and specifications must be included within the permit application to address and demonstrate compliance with all applicable requirements of the planning scheme, including any site analysis, impact report and recommendation, and advice, consent or determination required from a State agency or utility entity.

The application must clearly identify the documents relied upon for determination.

Section 51(1AC) *Land Use Planning and Approvals Act 1993* provides that a permit application is not valid unless it includes all of the information required by a planning scheme. Clause 6.1 Tasmanian Planning Scheme prescribes the minimum information that is necessary in order to complete a valid permit application.

S54 *Land Use Planning and Approvals Act 1993* provides that the planning authority may require the applicant to supply further information before it considers a permit application. If the planning authority requires further information to more particularly address one or more of the applicable requirements of the Tasmanian Planning Scheme, the statutory period for determination of a permit application does not run until that information is answered to the satisfaction of the planning authority

c) *Applicable Provisions and Standards*

The permit application must be assessed against the applicable provisions and standards of the Tasmanian Planning Scheme. The application is to identify by reference the clauses it relies upon to demonstrate compliance. (eg *clause 8.4.3 (A1 – A4, and P5)*)

d) *Discretionary Permits*

If a permit is discretionary the permit application must be notified for a period of 14 days to allow opportunity for any interested person to consider the proposed use and/or development and to provide comment on the discretionary matter.

If a permit application relies on performance criteria to satisfy an applicable standard or is discretionary under another provision of the interim planning scheme, the permit is discretionary only with respect to that standard.

The Council must have regard to all representations received during the notification period on a discretionary matter when determining whether to grant or refuse a permit.

e) *If the applicant is not the landowner*

If the applicant is not the owner of the land in the use or development site, the applicant is required to notify all of the owners either prior to or within 7 days from the date of making the permit application.

The permit application must identify all of the landowners; and the applicant must sign the application form to acknowledge the obligation to advise such landowners that the permit application has been made.

If the site includes land owned or administered by the Burnie City Council or by a State government agency, the consent in writing from the Council or the Minister responsible for Crown land must be provided at the time of making the application.

f) *Applicant declaration*

It is an offence for a person to do any act that is contrary to a compliance requirement created under the section 63 *Land Use Planning and Approvals Act 1993*. The applicant is required to complete a declaration that the information given in the permit application is true and correct.

g) *Payment of Fees*

The Council is not required to take any action on the permit application until all the relevant fees have been paid.

Permit Information

(NB If insufficient space, please attach separate document)

Proposed Use:**Use Class** General Industrial - Container Depot**Documents included with the permit application to describe the Use**

- Cover Letter

Proposed Development**Use class to which the development applies** General Industrial - Container Depot**Documents included with the permit application to describe the Development**

- Cover Letter (LET-001)
- Title Folio
- Letter of Authority
- Schedule of Easements
- Yard and Warehouse Architectural Drawings
- Office Architectural Drawings
- Civil Drawings
- Trade Waste Evaluation (MEM-001)
- Surface Evaluation (MEM-002)
- Trade Water Risk Categorisation (MEM-003)

Provisions and Standards relied upon for grant of a Permit

Value of use and/or development

Notification of Landowner/s

If land is not in applicant's ownership

I, **Brendan Stanborough**, declare that the owner/each of the owners of the land has been notified of the intention to make this permit application.

Signature of Applicant

Bstanborough

Date **28/07/2025**

If the permit application involves land owned or administered by the BURNIE CITY COUNCIL

Burnie City Council consents to the making of this permit application.

General Manager (Signature)

Date

If the permit application involves land owned or administered by the CROWN

I, the Minister responsible for the land, consent to the making of this permit application.

Minister (Signature)

Date

Applicant Declaration

I, **Brendan Stanborough** declare that the information I have given in this permit application to be true and correct to the best of my knowledge.

Signature of Applicant

Bstanborough

Date **28/07/25**

Office use only

SEARCH OF TORRENS TITLE

VOLUME 165351	FOLIO 7
EDITION 3	DATE OF ISSUE 21-Jun-2013

SEARCH DATE : 12-Jul-2023

SEARCH TIME : 08.40 AM

DESCRIPTION OF LAND

City of BURNIE
 Lot 7 on Sealed Plan 165351
 Derivation : Part of 50,000 Acres, Gtd. to The Van Diemen's
 Land Company
 Prior CTs 122874/1 and 122875/1

SCHEDULE 1

M411171 TRANSFER to STUBBS & LICHT INVESTMENTS PTY LTD
 Registered 04-Apr-2013 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP165351 EASEMENTS in Schedule of Easements
 SP165351 FENCING PROVISION in Schedule of Easements
 D121726 BURDENING ELECTRICITY INFRASTRUCTURE EASEMENT with
 the benefit of a restriction as to user of land in
 favour of Aurora Energy Pty Ltd over the Electricity
 Infrastructure Easement shown on Sealed Plan 165351
 (Subject to Provisions) Registered 09-Oct-2014 at
 noon
 19/7698 CONVEYANCE Made Subject to Exceptions And
 Reservations in favour of The V.D.L. Co.
 19/9147 CONVEYANCE Made Subject to Conditions
 D94182 MORTGAGE to Stubbs Constructions Pty Ltd Registered
 21-Jun-2013 at noon

UNREGISTERED DEALINGS AND NOTATIONS

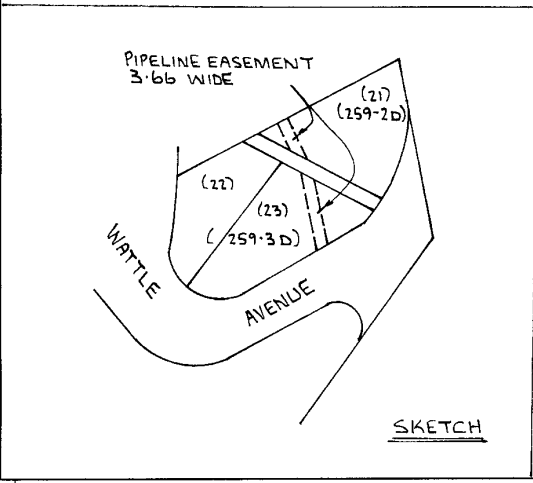
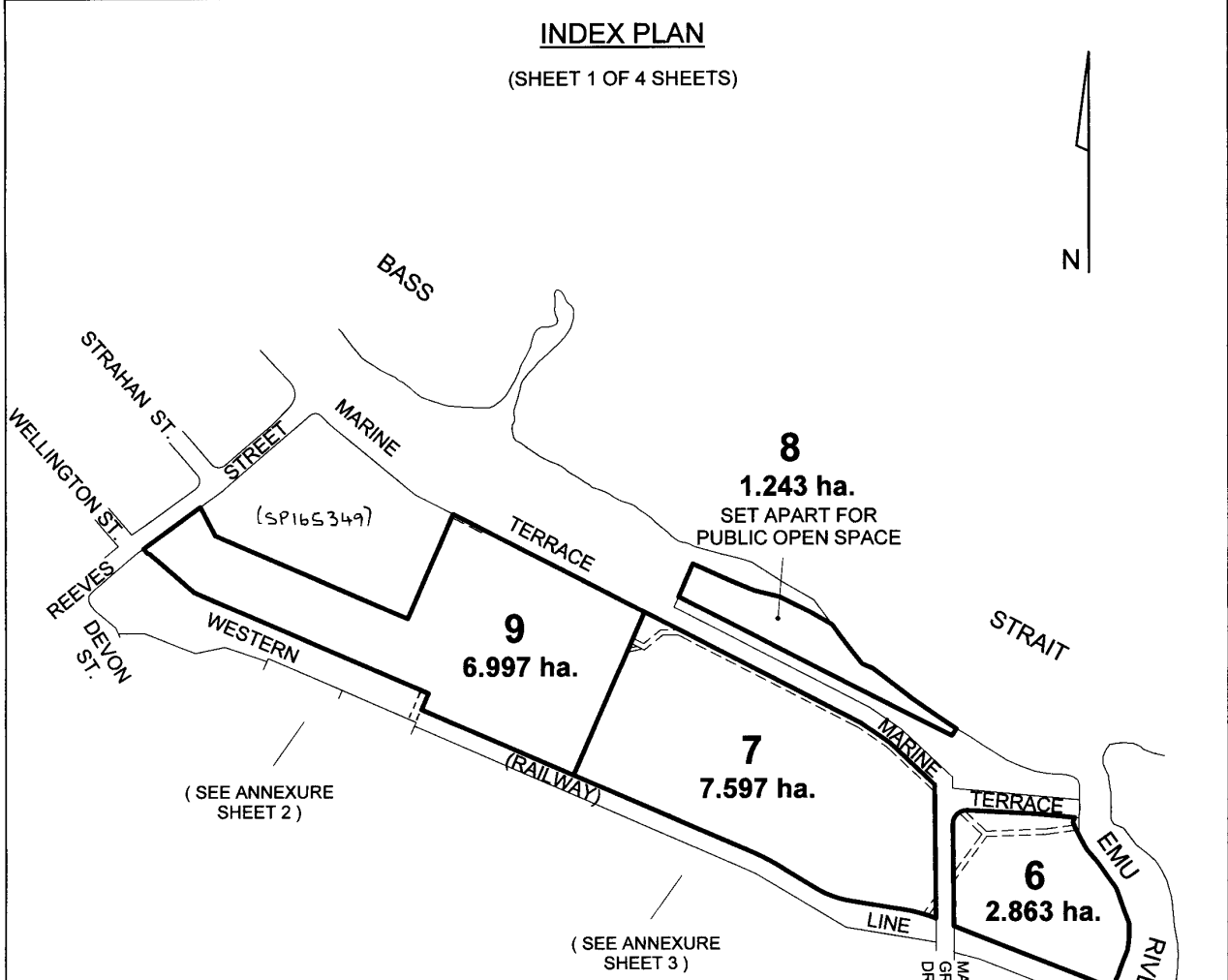
No unregistered dealings or other notations

NSWAL BAK P/L J.R. & C.J. SALT MARSH OWNER: TAS PAPER PTY LTD 122875 FOLIO REFERENCE: 122875-3, 122874-1, 122874-1, 236139-1, 198316-1 GRANTEE: PART OF 50,000 ACRES GTD TO THE VAN DIEMENS LAND COMPANY, WHOLE OF 1a-2r-15 9/10ths p. & WHOLE OF LOT 36393, 2.529ha, BOTH GTD TO ASSOCIATED PULP & PAPER MILLS LTD.	PLAN OF SURVEY		REGISTERED NUMBER SP165351
	BY SURVEYOR: MICHAEL WARD of LAND & SEA SURVEYS, EUGENANA LOCATION: CITY OF BURNIE		APPROVED EFFECTIVE FROM 22 FEB 2013 <i>Alice Kawa</i> Recorder of Titles
MAPSHEET MUNICIPAL CODE No. 103 (4045-43)		LAST UPI No. KHC15 KFG 09 KHZ36 FBL54 KHZ55	LAST PLAN No. P236139, P122874, P198316, P122875

ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN

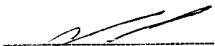
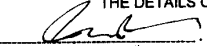

INDEX PLAN

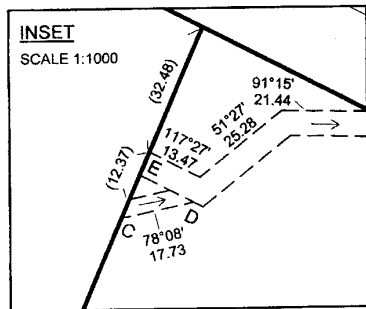
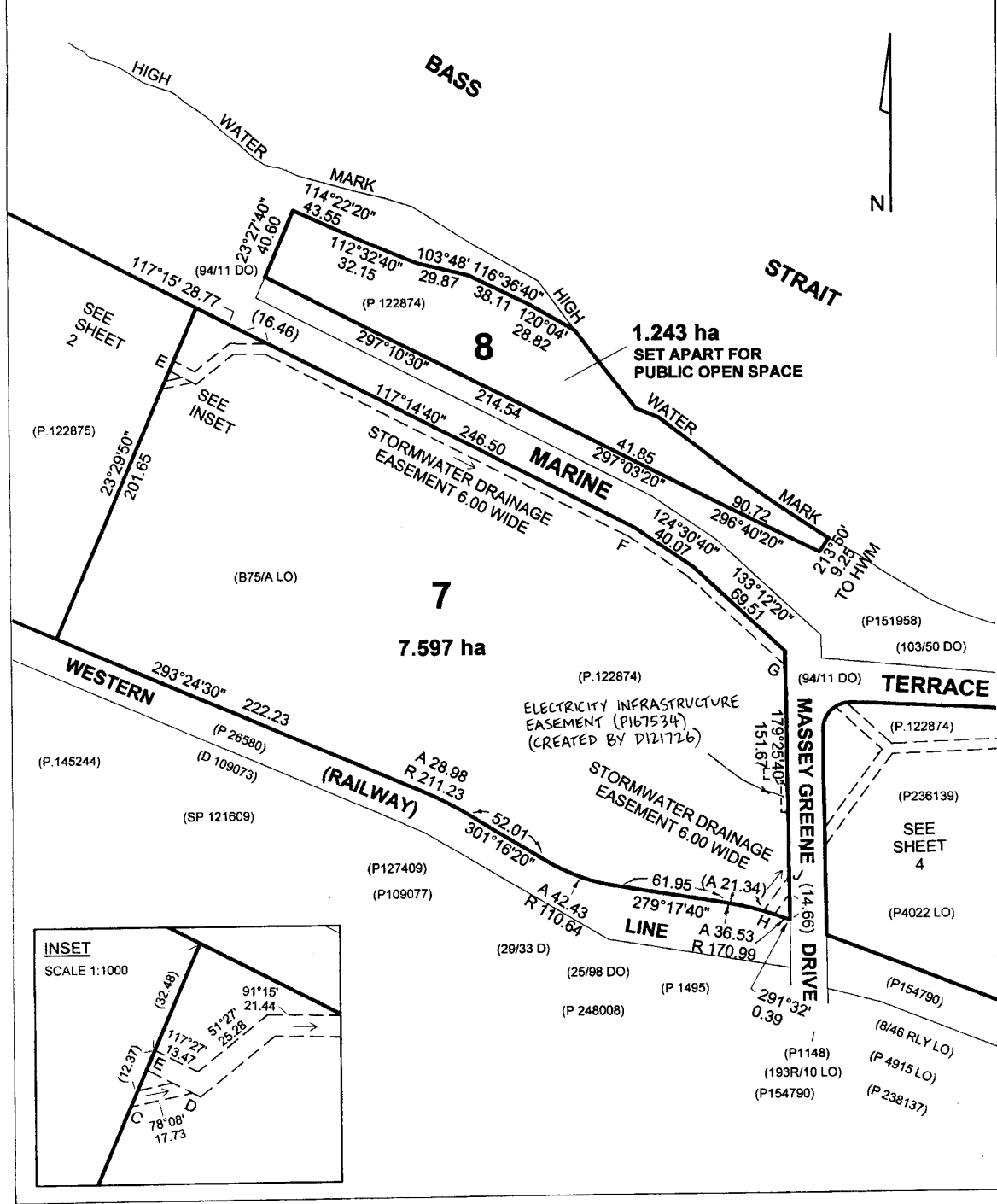
(SHEET 1 OF 4 SHEETS)



COUNCIL DELEGATE

17-9-12
 DATE

<p>PLAN OF SURVEY ANNEXURE SHEET SHEET 3 OF 4 SHEETS</p>	<p>OWNER: TAS PAPER PTY LTD FOLIO REFERENCE: 122875-3, 198316-1, 122874-1, 236139-1, SCALE 1: 2000 LENGTHS IN METRES</p>	<p>Registered Number SP165351</p>
<p>SIGNED FOR IDENTIFICATION PURPOSES  Council Delegate Date 17/9/12</p>	<p>THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. THE SURVEYORS CERTIFICATE EXTENDS TO THE DETAILS ON THIS SHEET  Registered Land Surveyor Date 18/8/12</p>	<p>APPROVED EFFECTIVE FROM 22 FEB 2013  Recorder of Titles</p>



SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 165351

PAGE 1 OF 4 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

1. Easements

1.1 Lot 9 on the plan is:

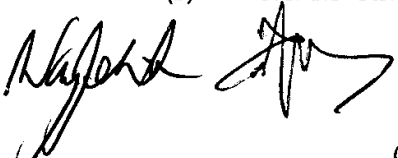
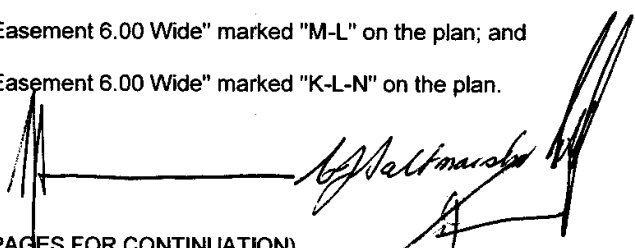
- (a) subject to a right of drainage in favour of Burnie City Council over the "Stormwater Drainage Easement 4.00 Wide" marked "A-B" on the plan; and
- (b) together with a right of drainage over that part of Lot 7 marked "Stormwater Drainage Easement C-D 4.00 Wide" on the plan.

1.2 Lot 7 on the plan is subject to a right of drainage over:


- (a) the "Stormwater Drainage Easement 6.00 Wide" marked "E-F-G" on the plan in favour of Burnie City Council;
- (b) the "Stormwater Drainage Easement 6.00 Wide" marked "H-J" on the plan in favour of Burnie City Council; and
- (c) the "Stormwater Drainage Easement C-D 4.00 Wide" appurtenant to Lot 9 on the plan.

1.3 Lot 6 on the plan is subject to rights of drainage in favour of Burnie City Council:

- (a) over the "Stormwater Drainage Easement 6.00 Wide" marked "M-L" on the plan; and
- (b) over the "Stormwater Drainage Easement 6.00 Wide" marked "K-L-N" on the plan.


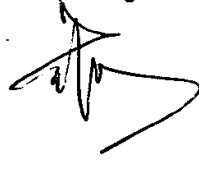
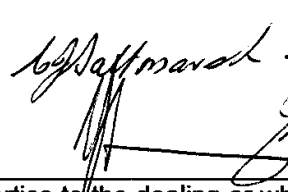

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: TAS PAPER PTY LTD, NOSWAL BAK PTY LTD, JOHN RODNEY SALTMARSH and CAROL JOAN SALTMARSH FOLIO REF: 122875-1, 122875-3, 122874-1, 236139-1, 198316-1 SOLICITOR & REFERENCE: Hunt & Hunt (refer BP/2002545)	PLAN SEALED BY: CITY OF BURNIE CITY COUNCIL DATE: <u>25.1.2013</u> REF NO. <u>2963046</u>  Council Delegate
<p>NOTE: The Council Delegate must sign the Certificate for the purposes of identification.</p>	

ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 2 OF 4 PAGES	Registered Number SP 165351
SUBDIVIDER: Tas Paper Pty Ltd, Noswal Bak Pty Ltd, John Rodney Saltmarsh and Carol Joan Saltmarsh FOLIO REFERENCE: 122875-1, 122875-3, 122874-1, 236139-1, 198316-1	

1.4 That part of Lot 9 on the plan formerly comprising Lot 1 on Plan 198316 is:

- (a) together with a right of way for the registered proprietor successors and assigns and its and their tenants and servants visitors and licensees in common with all other persons having a like right at all times hereafter by day or by night with or without horses cattle or other animals carts carriages or other means of transport of any description to pass and repass along the land marked WXYZ on the Plan ~~land marked A.B.C.D. on Plan No. 198316~~ for all purposes connected with the lawful use of the land comprised therein but so the exercise or use of such right of easement shall not interfere with the passage at any time of the railway rolling stock over the railway lines of the Tasmanian Government Railways from time to time or any part of the land marked ~~A.B.C.D.~~ WXYZ on the Plan ~~on Plan No. 198316~~ and to erect and maintain subject as aforesaid wires for the conduct of electrical energy between Lot 1 and Lot 2 on Plan No. 198316 over and across the land marked WXYZ on the Plan ~~marked A.B.C.D. on Plan No. 198316~~ or lay and maintain subject as aforesaid such wires beneath the surface of the land marked ~~A.B.C.D. on Plan No. 198316~~; WXYZ on the Plan .
- (b) together with the full and free right and liberty at all times hereafter to lay and install water mains of pipes with such valves cocks and inspection boxes as may be necessary for carrying and conducting water in or under (but at a depth of not less than 30 inches from the surface of the land) the land marked "Pipe Line Easement ~~12 feet wide" on Plan 198316; and~~ 3.66 wide on the Plan .
- (c) together with the full and free right and liberty for the said registered proprietor its successors or assigns it or their servants and workmen of entering upon Lot 21 on Diagram No. 259/2 and Lot 23 on Diagram No. 259/3 from time to time and at all times hereafter for the purposes of inspecting maintaining cleansing repairing renewing and replacing the said mains pipes valves cocks and inspection boxes provided that the said registered proprietor its successors or assigns as the case may be in the exercise of the said right and liberty hereby excepted and reserved shall cause as little disturbance of or damage to the said Lots 21 and 23 or anything growing or being thereon as shall be reasonably possible and will with all reasonable speed restore any disturbance and make good any damage which may be so caused and provided further that the said registered proprietor its successors or assigns as the case may be will at all times hereafter keep the said mains pipes valves cocks and inspection boxes in sound condition and efficient working order and so as to prevent any leakage or escape of water therefrom and provided that the said registered proprietor its successors or assigns as the case may be will indemnify and keep indemnified Bryn Samuel Woolaway and Leonard Edward Griffiths their respective successors or assigns against any loss or damage caused by any leakage or escape of water from the said mains pipes valves cocks and inspection boxes however caused except by the act neglect or default of the said Bryn Samuel Woolaway and Leonard Edward Griffiths their respective successors or assigns or his or their servants or agents.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 3 OF 4 PAGES	Registered Number SP 165351
SUBDIVIDER: Tas Paper Pty Ltd, Noswal Bak Pty Ltd, John Rodney Saltmarsh and Carol Joan Saltmarsh FOLIO REFERENCE: 122875-1, 122875-3, 122874-1, 236139-1, 198316-1	

~~2.~~ **Reservations**

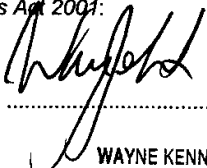
~~2.1~~ These parts of Lot 7 and 9 on the plan formerly comprised in Lot 1 on Plan 122875 are affected by these exceptions and reservations in favour of The Van Diemen's Land Company set out in Conveyance 10/7698.

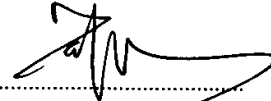
~~2.2~~ That part of Lot 9 on the plan formerly comprised in Lot 3 on Plan 122874 is affected by these exceptions and reservations in favour of The Van Diemen's Land Company set out in Conveyance 23/2491.

~~3.~~ **Fencing Provision**

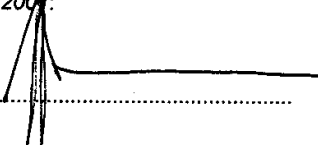
~~3.1~~ In respect of each lot on the plan the subdivider, Tas Paper Pty Ltd, shall not be required to fence.

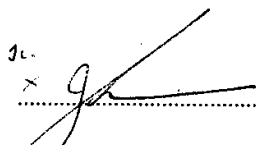
EXECUTED by Tas Paper Pty Ltd ACN 135 318
 204 in accordance with section 127 of the
 Corporations Act 2001:

Signature: 
 Name: WAYNE KENNETH JOHNSTON
 PLEASE PRINT
 Director

Signature: 
 Name: YEW FUNG WONG
 PLEASE PRINT
 Director/Secretary

EXECUTED by Noswal Bak Pty Ltd ACN 085
 575 766 in accordance with section 127 of the
 Corporations Act 2001:

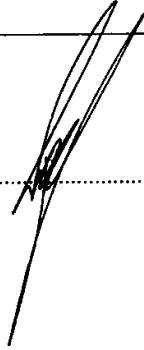
Signature: 
 Name: ANDREW LAWSON
 PLEASE PRINT
 Director

Signature: 
 Name: JACKI LAWSON
 PLEASE PRINT
 Director/Secretary

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body, be signed by the persons who have attested the affixing of the seal of that body to the dealing.

ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 4 OF 4 PAGES	Registered Number SP 165351
SUBDIVIDER: Tas Paper Pty Ltd, Noswal Bak Pty Ltd, John Rodney Saltmarsh and Carol Joan Saltmarsh FOLIO REFERENCE: 122875-1, 122875-3, 122874-1, 236139-1, 198316-1	

SIGNED SEALED AND DELIVERED by John Rodney Saltmarsh in the presence of:

Signature: 

Signature: K Pieterse

Name: KATHLEEN PIETERSE
PLEASE PRINT

Witness 122 JAMES STREET,
DEVONPORT TAS 7310

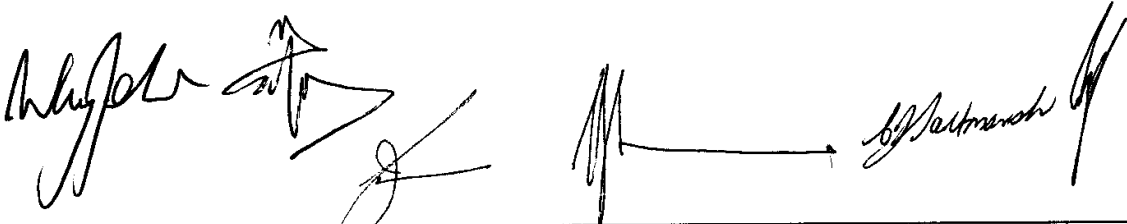
SIGNED SEALED AND DELIVERED by Carol Joan Saltmarsh in the presence of:

Signature: 

Signature: K Pieterse

Name: KATHLEEN PIETERSE
PLEASE PRINT


Witness



NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

CLIENT:
STUBBS & LICHT INVESTMENTS PTY LTD.
 PROJECT:
STRAITLINK - BURNIE TERMINAL
 ADDRESS:
MASSY GREENE DRIVE, SOUTH BURNIE
 PROJECT NO :
245019
 STATUS :
CONTROLLED DOCUMENT
 ISSUED FOR/DESCRIPTION :
DEVELOPMENT APPLICATION
 DRAWINGS :

SHEET LIST					
SHEET NUMBER	SHEET NAME	CURRENT REVISION	DESCRIPTION	DATE	DRAWN BY
A000	COVER SHEET	D	RE-ISSUED FOR DEVELOPMENT APPLICATION	27-08-25	BT
A000a	ARCHITECTURAL NOTES	C	DEVELOPMENT APPLICATION	28-07-25	BT
A01	3D ISOMETRIC VIEW	C	DEVELOPMENT APPLICATION	28-07-25	BT
A10	BUILDING PLAN	C	DEVELOPMENT APPLICATION	28-07-25	BT
A11	SIGNAGE & LIGHTING - PLAN	D	RE-ISSUED FOR DEVELOPMENT APPLICATION	27-08-25	BT
A12	SITE PLAN	D	RE-ISSUED FOR DEVELOPMENT APPLICATION	27-08-25	BT
A21	GENERAL ARRANGEMENT - PLAN	C	DEVELOPMENT APPLICATION	28-07-25	BT
A26	ROOF - SHEETING PLAN	C	DEVELOPMENT APPLICATION	28-07-25	BT
A27	ROOF FRAMING - AWNING PLAN - SHT 1 OF 2	C	DEVELOPMENT APPLICATION	28-07-25	BT
A30	TYPICAL SECTIONS - SHEET 1 OF 4	C	DEVELOPMENT APPLICATION	28-07-25	BT
A31	TYPICAL SECTIONS - SHEET 2 OF 4	C	DEVELOPMENT APPLICATION	28-07-25	BT
A32	TYPICAL SECTIONS - SHEET 3 OF 4	C	DEVELOPMENT APPLICATION	28-07-25	BT
A33	TYPICAL SECTIONS - SHEET 4 OF 4	C	DEVELOPMENT APPLICATION	28-07-25	BT
A36	SIGNAGE DETAILS	D	RE-ISSUED FOR DEVELOPMENT APPLICATION	27-08-25	BT
A40	WASH DOWN BAY - GENERAL ARRANGEMENT	C	DEVELOPMENT APPLICATION	28-07-25	BT
A52	3D VIEW - BUILDING	C	DEVELOPMENT APPLICATION	28-07-25	BT
A53	3D VIEW - WASHDOWN BAY	C	DEVELOPMENT APPLICATION	28-07-25	BT

STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH DESIGN CHK: BS DRAWN BY: BT DRAFT CHK: RN	 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LICHT INVESTMENTS PTY LTD. PROJECT: STRAITLINK - BURNIE TERMINAL ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	TITLE: COVER SHEET SCALE: SHEET SIZE: A1 DWG IN SET: PROJECT No: 245019 DWG No: A000 REV: D
D RE-ISSUED FOR DEVELOPMENT APPLICATION C DEVELOPMENT APPLICATION B REVIEW/INFORMATION A REVIEW/INFORMATION REV: DESCRIPTION:	BT 27-08-25 BT 28-07-25 BT 03-06-25 BT 28-05-25 BY: DATE:	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 588 257 APPROVED: B.STANBOROUGH ACRED. No: - DATE: 20-05-25		22-24 Paterson Street Launceston, TAS 7250	

GENERAL NOTES

1. GENERAL

- A. THESE DRAWINGS AND NOTES SHALL BE READ IN CONJUNCTION WITH ARCHITECTURAL, CIVIL, BUILDING SERVICES AND OTHER DISCIPLINES DRAWINGS AND SPECIFICATIONS AND WITH ANY WRITTEN ENGINEERS INSTRUCTIONS ISSUED DURING THE CONTRACT.
- B. ANY DISCREPANCIES ARE TO BE REPORTED TO THE PROJECT MANAGER BEFORE PROCEEDING WITH THE WORK.
- C. THESE GENERAL NOTES DO NOT HAVE PRECEDENCE OVER THE SPECIFICATION OR DRAWING NOTES.
- D. ALL SET OUT DIMENSIONS ON THE DRAWINGS ARE TO BE VERIFIED BY THE CONTRACTOR ON SITE BEFORE COMMENCING WORK.
- E. DO NOT SCALE FOR DIMENSIONS OFF THESE DRAWINGS.
- F. UNLESS NOTED OTHERWISE, ALL DIMENSIONS SHOWN ARE IN MILLIMETRES WITH THE EXCEPTION OF SURVEY LEVELS, WHICH ARE IN METRES.
- G. THE CONTRACTOR IS TO ENSURE THAT ANY PROFESSIONALS, TRADESMEN OR SUPPLIERS ENGAGED THROUGHOUT THE DURATION OF THE CONTRACT ARE ACCREDITED AND QUALIFIED FOR THEIR DUTY OF WORK AND CARRY ALL NECESSARY PERMITS REQUIRED BY ANY STATUTORY AUTHORITY.
- 2. DESIGN LOADING**
- A. THE STRUCTURE HAS BEEN DESIGNED AND DETAILED IN ACCORDANCE WITH ALL RELEVANT AUSTRALIAN STANDARDS AND THE CURRENT BUILDING CODE OF AUSTRALIA.
- B. THE STRUCTURE HAS BEEN DESIGNED FOR THE FOLLOWING FLOOR LIVE LOADS IN ACCORDANCE WITH AS1170.1-2002 AND THE PROPOSED FLOOR USE:
- HEAVY DUTY STORAGE : 125kPa + TRAFFIC
WARE HOUSE : 65kPa + TRAFFIC
OFFICE : 3.0 kPa
STORE : 5.0 kPa
- C. THE STRUCTURE HAS BEEN DESIGNED FOR ALL SELF WEIGHT AND APPLIED DEAD LOADS.
- D. THE STRUCTURE HAS BEEN DESIGNED FOR WIND LOADS IN ACCORDANCE WITH AS1170.2-2021
- 3. CONSTRUCTION / TEMPORARY WORKS**
- A. DURING CONSTRUCTION IT IS THE CONTRACTORS RESPONSIBILITY TO ENSURE THAT THE STRUCTURE IS MAINTAINED IN A SAFE AND STABLE CONDITION AND NO ELEMENT IS OVERLOADED DUE TO CONSTRUCTION ACTIVITIES.
- B. TEMPORARY BRACING IS TO BE DESIGNED ACCORDINGLY AND INSTALLED BY THE CONTRACTOR SO THAT ALL BUILDING AND EXCAVATION WORK IS STABLE AT ALL TIMES.
- C. IF ANY STRUCTURAL COMPONENT DETAILED IN THESE DRAWINGS PRESENTS A PROBLEM WITH BUILDABILITY OR SAFETY, THE CONTRACTOR IS TO REPORT THE PROBLEM TO THE SUPERINTENDENT FOR RESOLUTION PRIOR TO PROCEEDING WITH THE WORK.

LEGENDS

1. ABBREVIATIONS

ADD. ADDENDUM	HOZ	HORIZONTAL
AFFL	HS	HIGH STRENGTH
AG	HT	HIGH-TENSILE STEEL
AGG.	HW	HARDWOOD
AMDT	ID	INTERNAL DIAMETER
APPROX.	INT.	INTERNAL
ARCH.	JUNC.	JUNCTION
AS	LEV.	LEVEL
AVG	LG	LENGTH / LONG
A/W	LH	LEFT HAND
(B)	LIGS	LIGATURES
BK	LONG.	LONGITUDINAL
BLDG	M/A	MASONRY ANCHOR
BMT	MAN.	MANUFACTURER
BPL	MAX.	MAXIMUM
BTM	MIN.	MINIMUM
BWK	MISC.	MISCELLANEOUS
C.A.	MSRY	MASONRY
CANT.	N	NORTH
CEM.	NEG.	NEGATIVE
CFW	NF	NEAR FACE
CHAM.	No.	NUMBER
CHEM.	NOM	NOMINAL
CHS	NS	NOMINAL SIZE
CIRC	NTS	NOT TO SCALE
CL or ϕ	OA	OVERALL
CNR	OD	OUTSIDE DIAMETER
COL.	OH	OVERHEAD
CONC.	OP	OPPOSITE
CONT.	PAR.	PARALLEL
CRS	PC	PRECAST
CSK	PD	PLAN DETAIL
CSK HD	PFC	PARALLEL FLANGE CHANNEL
CYL.	PL	PLATE
db	PLY	PLYWOOD
DET.	PT	POST TENSIONED
DIA. or ϕ	PREFAB.	PREFABRICATED
DIAG.	PRELIM.	PRELIMINARY
DIM.	QTY	QUANTITY
DIST.	RAD.	RADIUS
DL	RD	ROUND
DN	RECT.	RECTANGLE / RECTANGULAR
DPC	REF.	REFERENCE
DWG	REINF.	REINFORCEMENT
e	REO	REINFORCEMENT
EA	RH	RIGHT HAND
ELEC.	RHS	RECTANGULAR HOLLOW
ELEV.	R.L.	REDUCED LEVEL
ESL	RSA	ROLLED-STEEL ANGLE
EST.	RSC	ROLLED-STEEL CHANNEL
EXP.	RSJ	ROLLED-STEEL JOIST
EXT.	SEC.	SECTION
EXTG	S	SEWER
EW	SHS	SQUARE HOLLOW SECTION
(F)	SL	SURFACE LEVEL
FF	SPEC.	SPECIFICATION
FFL	STD	STANDARD
FL	SQ	SQUARE
FLG	SW	STORMWATER
FPW	(T)	TOP
FS	T.B.A.	TO BE ADVISED
FSBW	T.B.C.	TO BE CONCLUDED
FSL	TS	TENSILE STRENGTH
FW	TYP.	TYPICAL
GA	(U)	UNDER
GALV.	UA	UNEQUAL ANGLE
GR	UB	UNIVERSAL BEAM
H	UC	UNIVERSAL COLUMN
HD	UG	UNDERGROUND
H.D.	U.N.O.	UNLESS NOTED OTHERWISE
VERT.	U/S	UNDERSIDE
	V	VERTICAL

2. NOTATION EXAMPLES

N12 -300 BTM	D500N12 BARS AT 300 MAX. CENTRES IN THE BOTTOM OF MEMBER.
2-N12	TWO No. D500N12 BARS
SL82 TOP	D500SL82 SQUARE MESH IN THE TOP OF THE MEMBER.
SL818 TOP	D500SL818 RECTANGULAR MESH IN THE TOP OF THE MEMBER.
3-1.12M TOP & BTM	3 No. BAR SL12TM TRENCH MESH IN TOP AND BOTTOM OF MEMBER.
R10 LIGS -400	250R ϕ 10 PLAIN BAR LIGATURES AT 400 MAX. CENTRES IN THE MEMBER.

LEGENDS - CONTINUED

3. NOTATION MARKS

B	BEAM / BEARER
BAJ	BRICK ARTICULATION JOINT
BB	BOND BEAM
BCJ	BLOCK CONTROL JOINT
BoI	BOLLARD
Br	BRACING
Bt	BATTEN
C	COLUMN
CB	CONC. BEAM
CJ	SLAB CONTROL / CONSTRUCTION JOINT
DF	SLAB FALL THROUGH DOORWAY
DH	DOOR HEAD
DJ	DOOR JAMB
DP	MASS CONC. DRILLED PIER
DS	DOUBLE STUD
EJ	SLAB EXPANSION JOINT
Fa	FASCIA TRUSS
FB	FLY BRACE
F	F RAKE
FT	FLOOR TRUSS
G	GIRT
GD	GRATED DRAIN
GP	GRATED PIT
J	JOIST
L	LINTEL
LBW	LOAD BEARING WALL
Le	LEDGER
Lt	TIMBER LINTEL - REFER SCHEDULE
M	MULLION
OR	OUTRIGGER
P	PURLIN
Pe	CONC. PEDESTAL
PF	MASS CONC. PAD FOOTING
PI	PILE
Pl	CONC. PLINTH
R	RAFTER
Rk	RAKER
RT	TIMBER ROOF TRUSS
RW	RETAINING WALL FOOTING
SCJ	SLAB SAWCUT JOINT (1/4 DEPTH OF SLAB)
	SAWCUT WITHIN 24 HOURS OF POUR
SF	STRIP FOOTING
SR	SLAB REBATE
St	STRUT / STUB
ST	SLAB THICKENING
T	TIE MEMBER
Tr	STEEL TRUSS
UP	MASS CONC. UNDERPIN
WP	CAST IN WELD PLATE
WPJ	SLAB WEAKENED PLANE JOINT

4. SYMBOLS

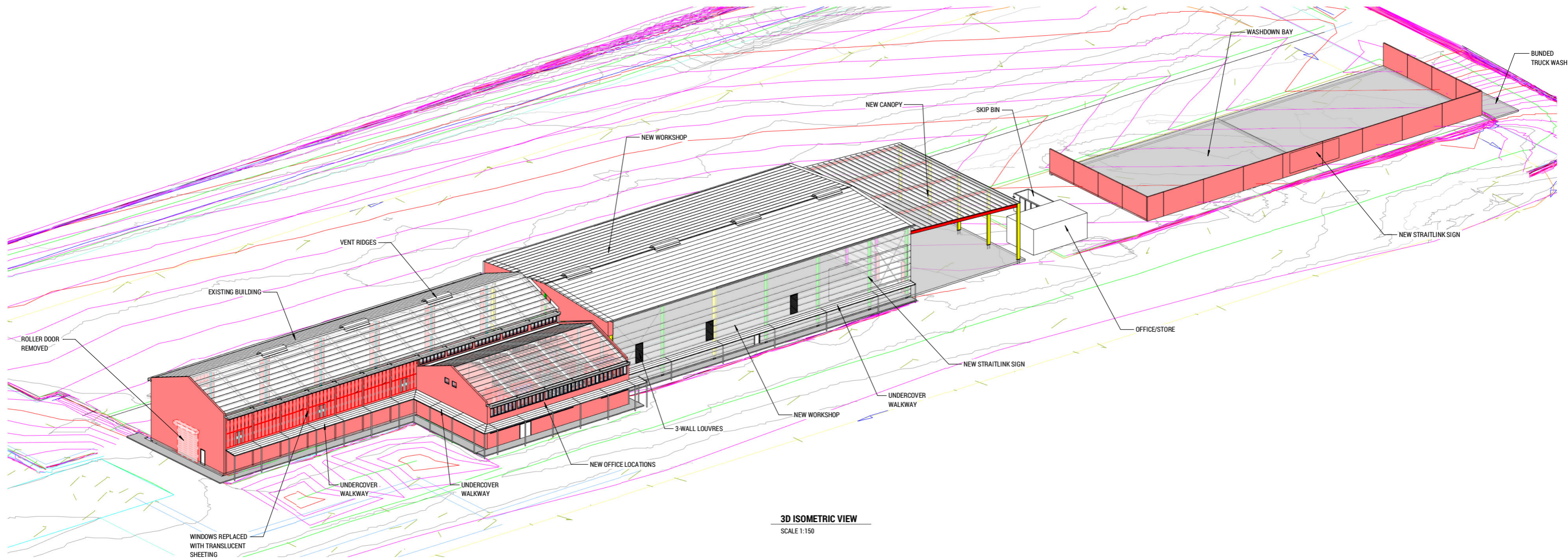
	TOP OF FOOTING R.L.
	SLAB MARK, REFER SCHEDULE FOR SLAB DEPTH, REINF., FORM ETC.
	DIRECTION OF SPAN
	BLOCKWORK
	BLOCK PIER
	LOAD BEARING MASONRY UNDER
	NON-LOAD BEARING MASONRY UNDER
	CONC. PRECAST WALL PANEL
	LOAD BEARING CONC. PRECAST WALL PANEL UNDER SLAB
	CONC. PRECAST COLUMN
	CONC. PRECAST COLUMN UNDER SLAB
	110mm FACE BRICKWORK
	110mm FACE BRICKWORK PIER
	190 BLOCKWORK
	FLOOR SLAB SETDOWN REFER ARCH. DRAWINGS
	AG - AGRICULTURAL PIPE DRAIN
	eAG - EXTG AGRICULTURAL PIPE DRAIN
	S - SEWER LINE
	eS - EXTG SEWER LINE
	SW - STORMWATER LINE
	eSW - EXTG STORMWATER LINE
	W - WATER LINE
	eW - EXTG WATER LINE

DISCREPANCIES & ALLOWANCES

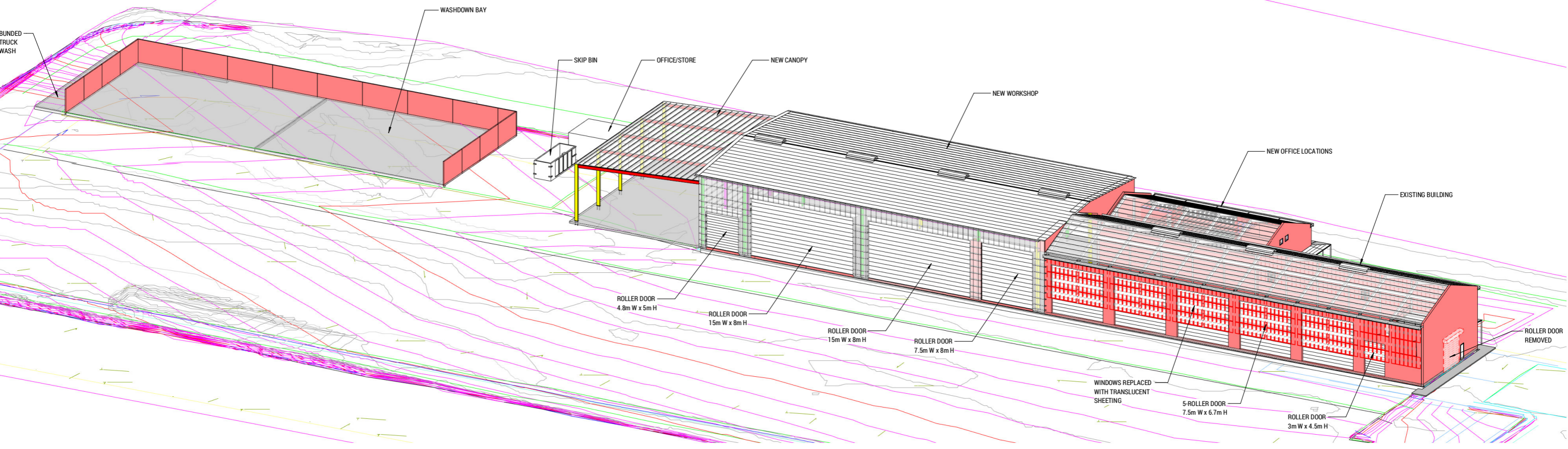
1. GENERAL


STRUCTURAL ENGINEERING DRAWINGS ARE TO BE READ IN CONJUNCTION WITH THE ARCHITECTURAL DRAWINGS. ANY DISCREPANCIES ARE TO BE REFERRED TO THE DESIGNER FOR CLARIFICATION, AND FOR TENDERING PURPOSES TENDERERS ARE TO ALLOW FOR THE HIGHER COST ITEM. REFER ARCH. DRAWINGS FOR ADDITIONAL STEELWORK / METALWORK NOT SHOWN ON STRUCTURAL DRAWINGS.

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B	REVIEW/INFORMATION	BT	03-06-25	DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A000a REV: C
A	REVIEW/INFORMATION	BT	28-05-25	DRAFT CHK: RN			
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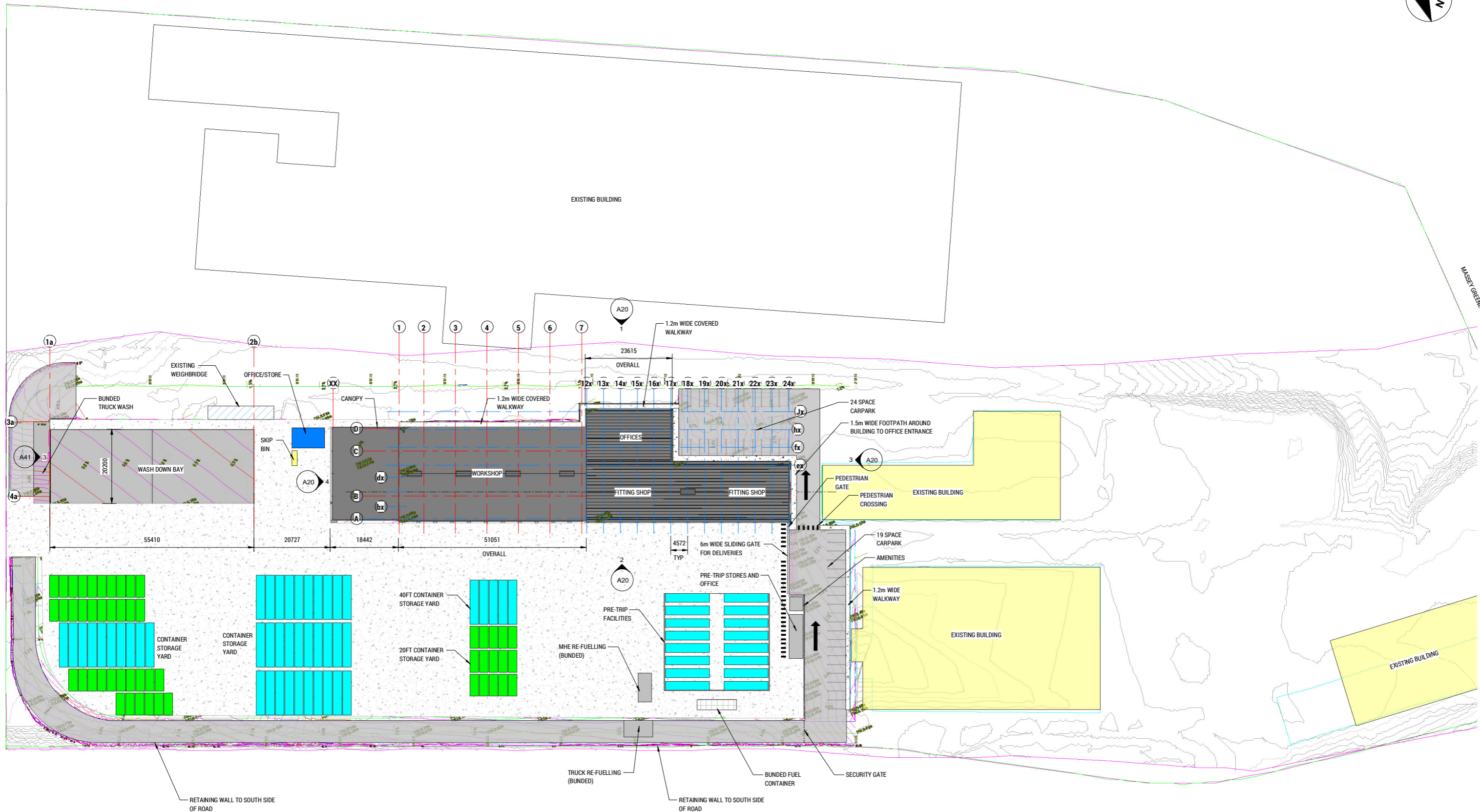
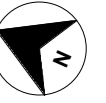


3D ISOMETRIC VIEW
SCALE 1:150




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A	REVIEW/INFORMATION	BT	28-05-25			DATE: 20-05-25			
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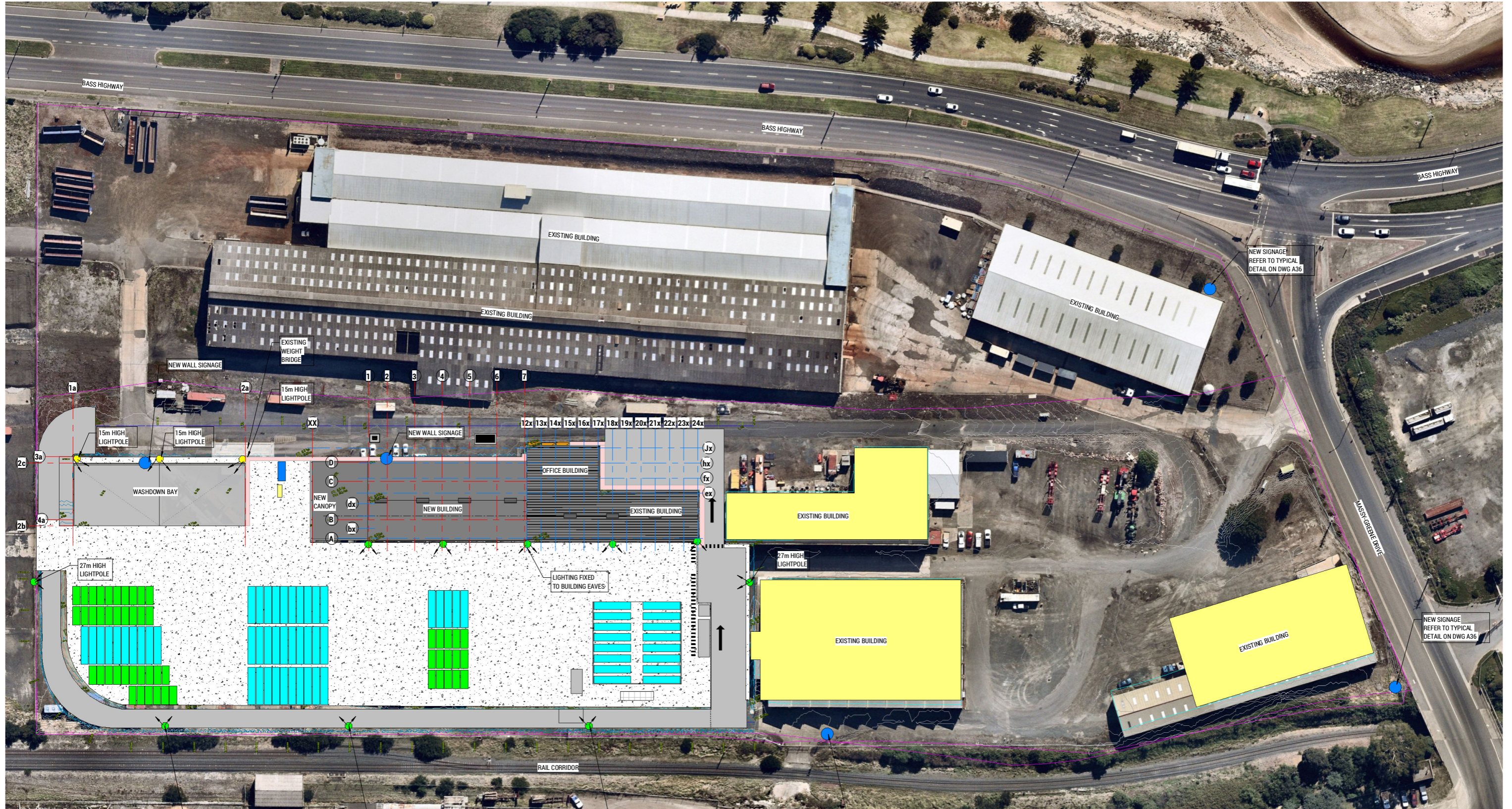
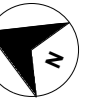
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BUILDING PLAN
SCALE 1:500

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B	REVIEW/INFORMATION	BT	03-06-25	DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A10 REV: C
A	REVIEW/INFORMATION	BT	28-05-25	DRAFT CHK: RN			
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1 SIGNAGE & LIGHTING - PLAN
A13 1:600

REV:	DESCRIPTION:	BY:	DATE:
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C	DEVELOPMENT APPLICATION	BT	28-07-25
B	REVIEW/INFORMATION	BT	03-06-25
A	REVIEW/INFORMATION	BT	28-05-25

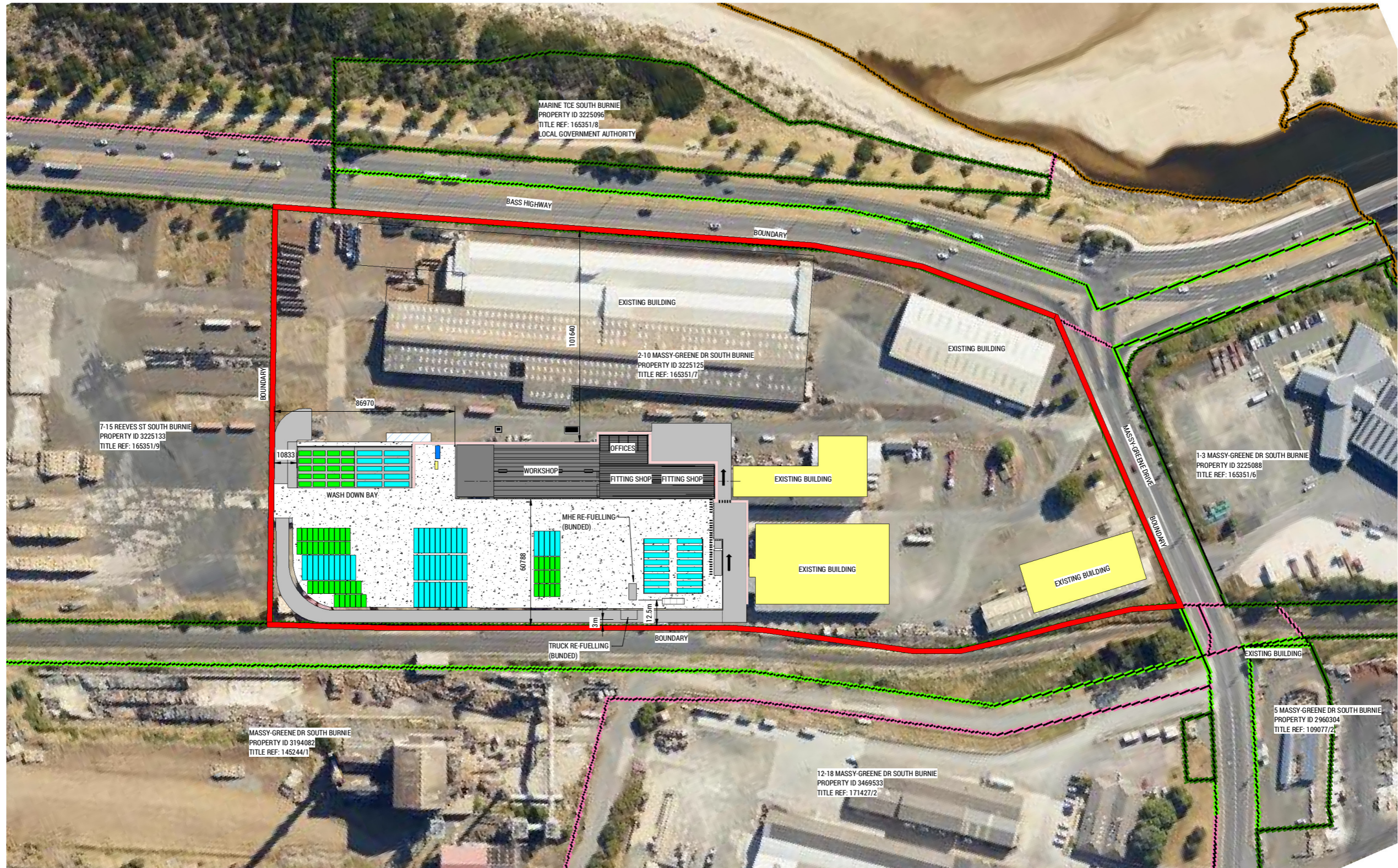
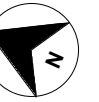
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CLIENT:	STUBBS & LIGHT INVESTMENTS PTY LTD.
PROJECT:	STRAITLINK - BURNIE TERMINAL
ADDRESS:	MASSEY GREENE DRIVE SOUTH BURNIE

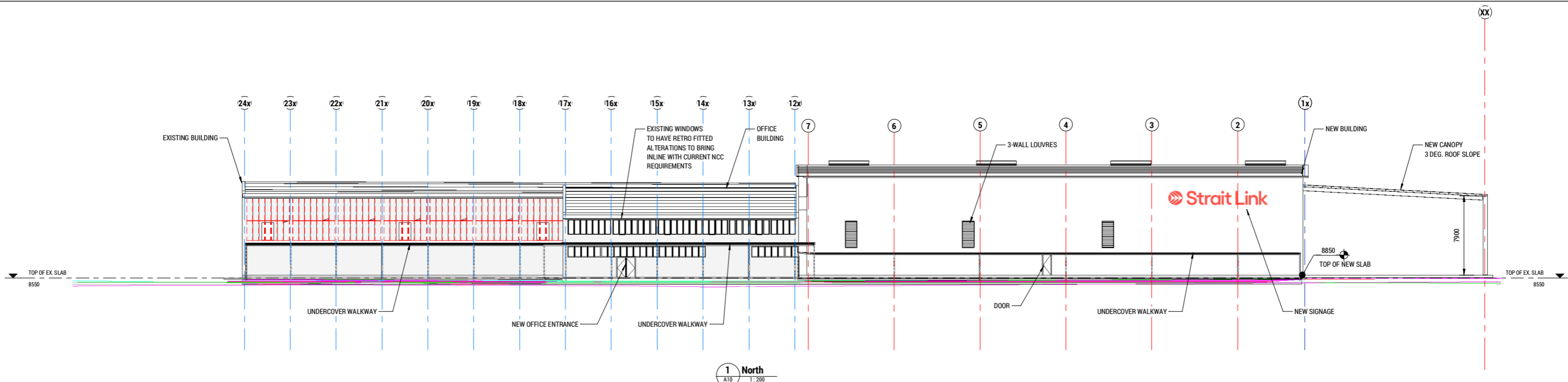
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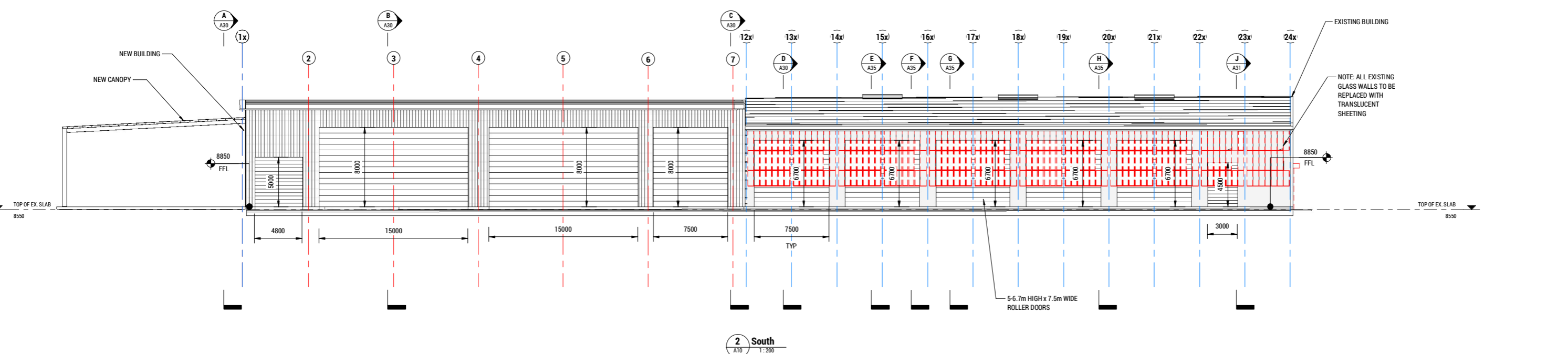


1 SITE - PLAN
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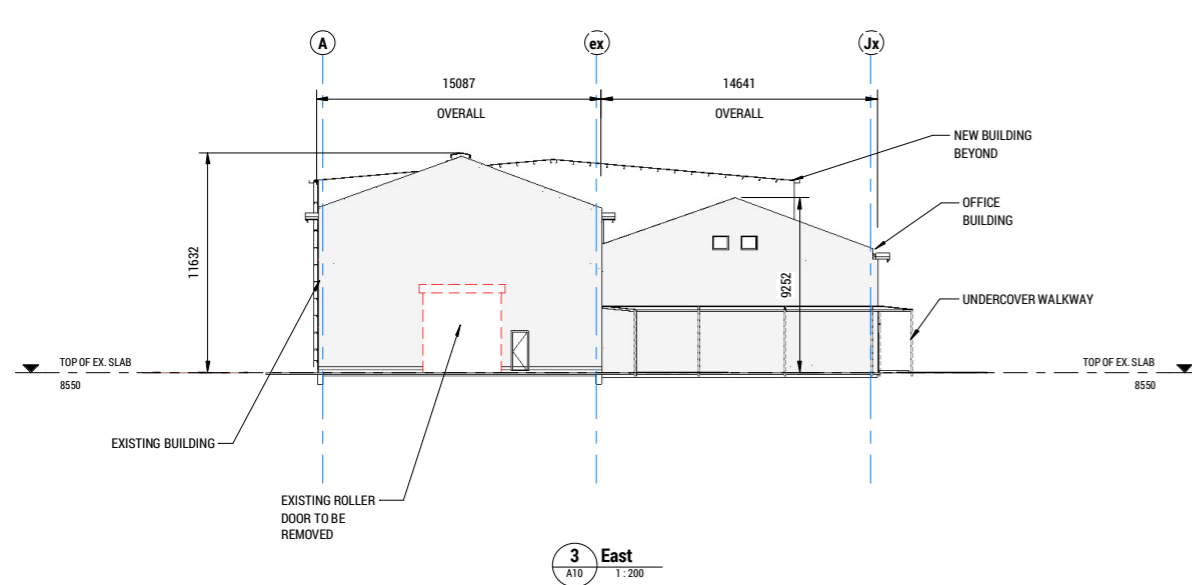
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A	REVIEW/INFORMATION	BT	28-05-25				
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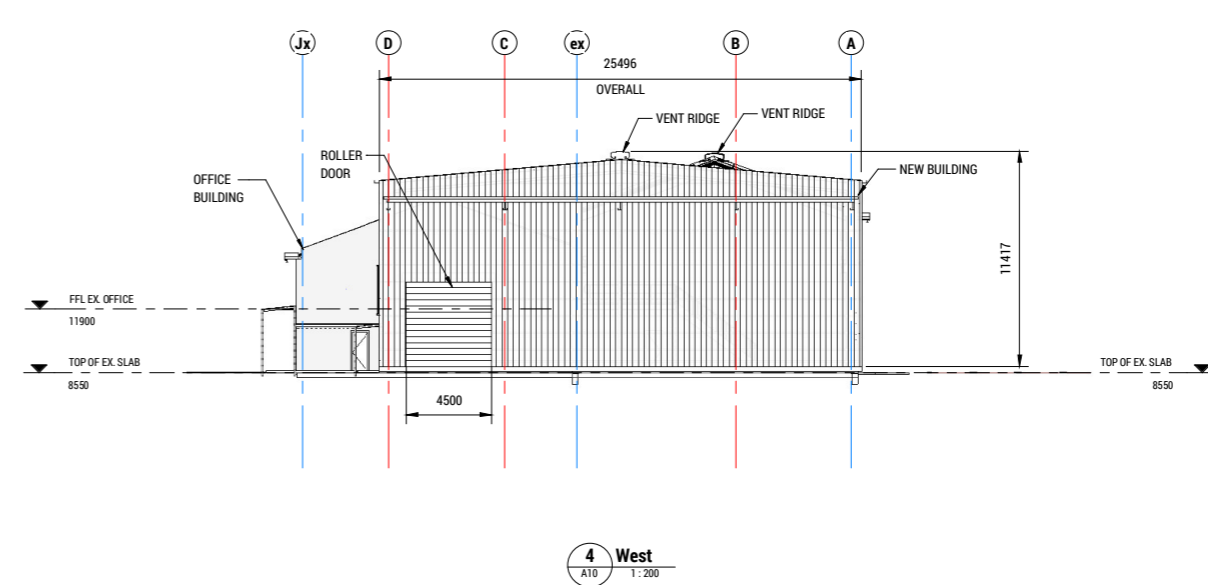
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A10 1:200




2 South
A10 1:200



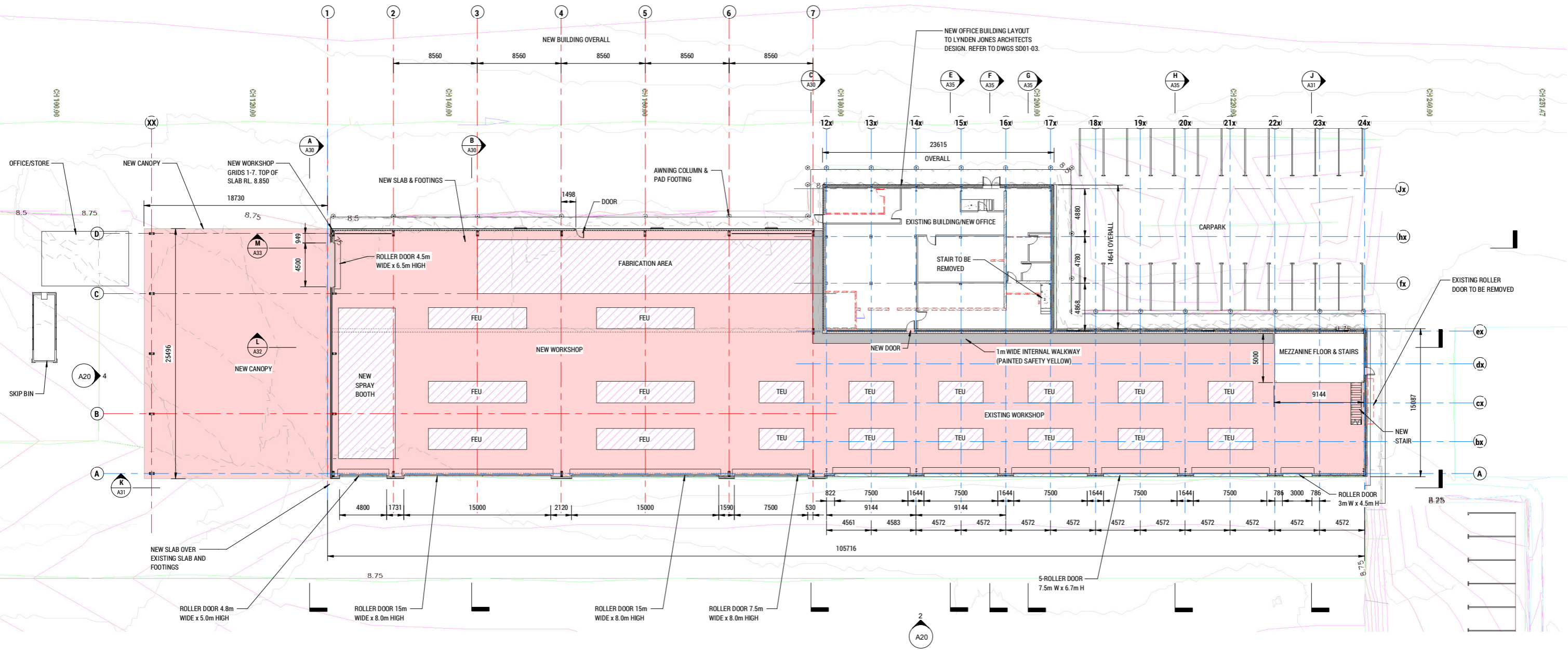
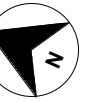
3 East
A10 1:200




4 West
A10 1:200

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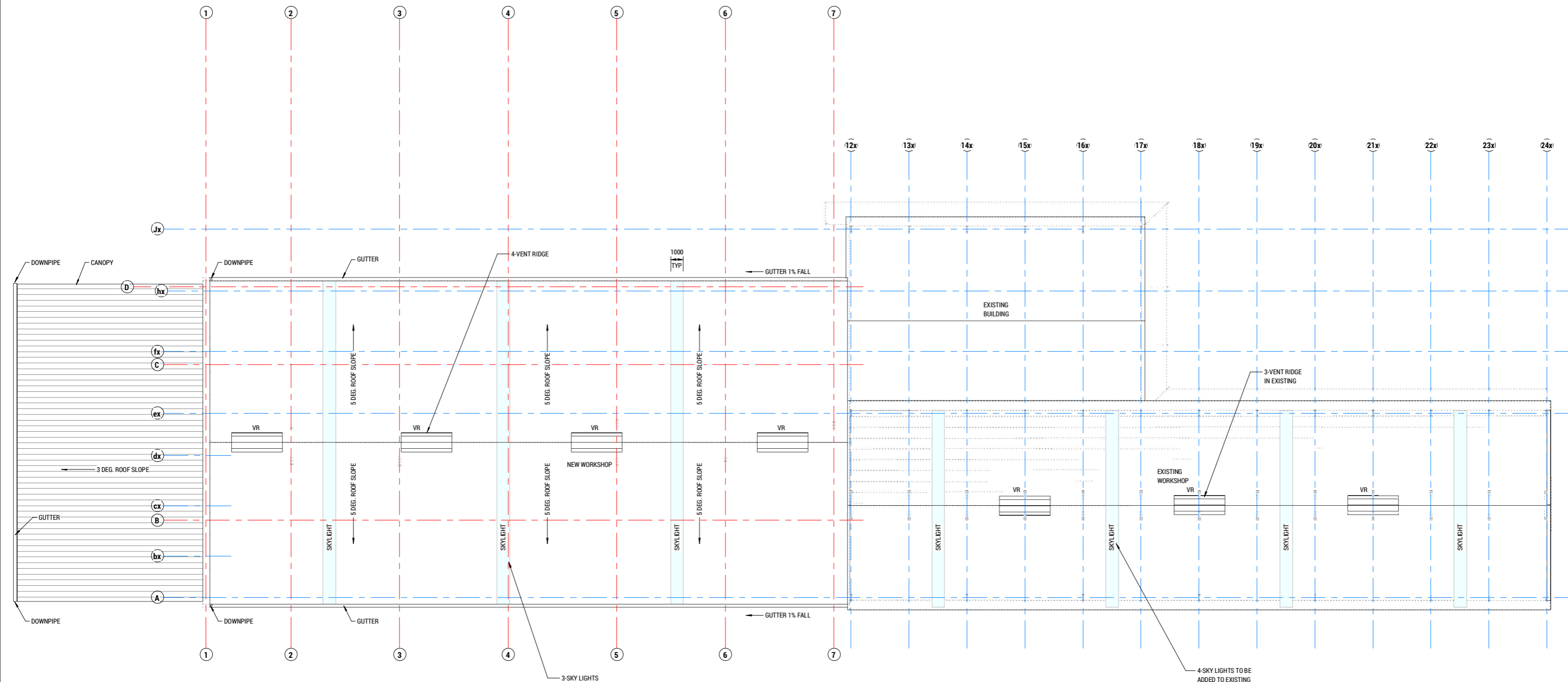
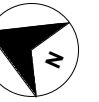
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GENERAL ARRANGEMENT - PLAN
SCALE 1:200


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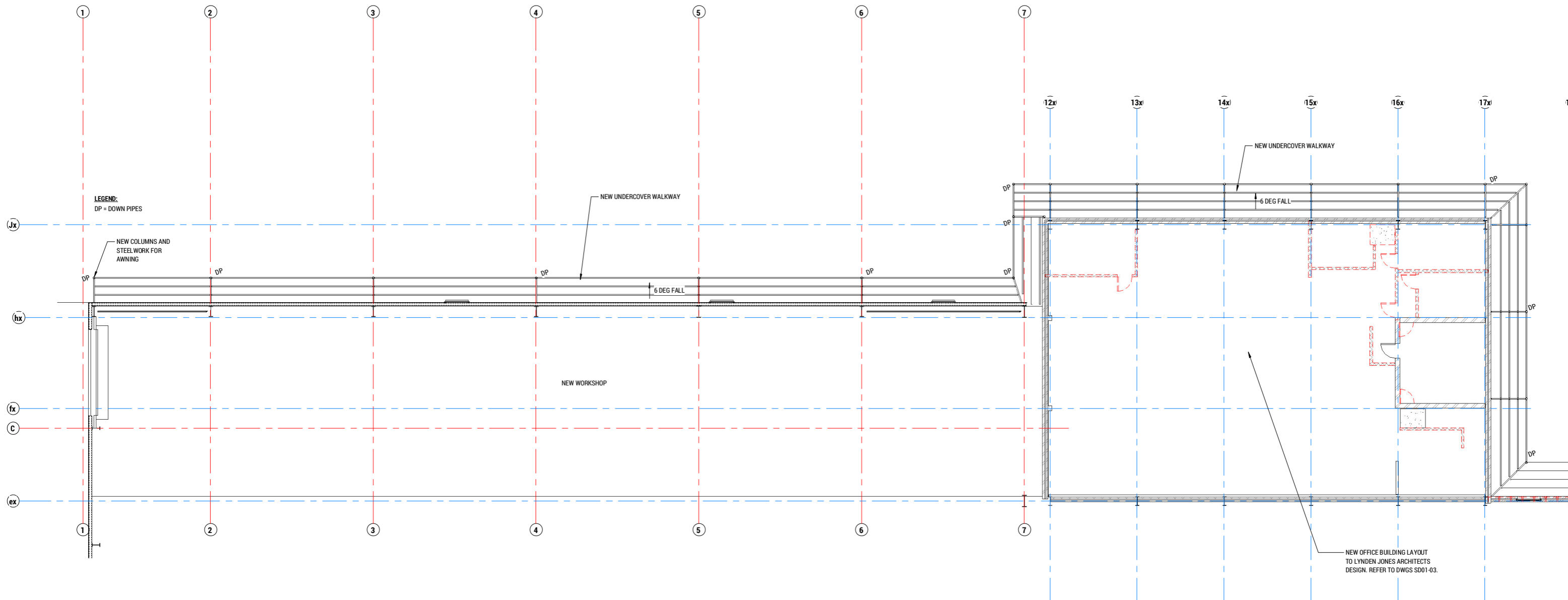
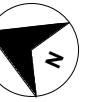
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ROOF FRAMING - PLAN
SCALE 1:150

4-SKY LIGHTS TO BE ADDED TO EXISTING BUILDING

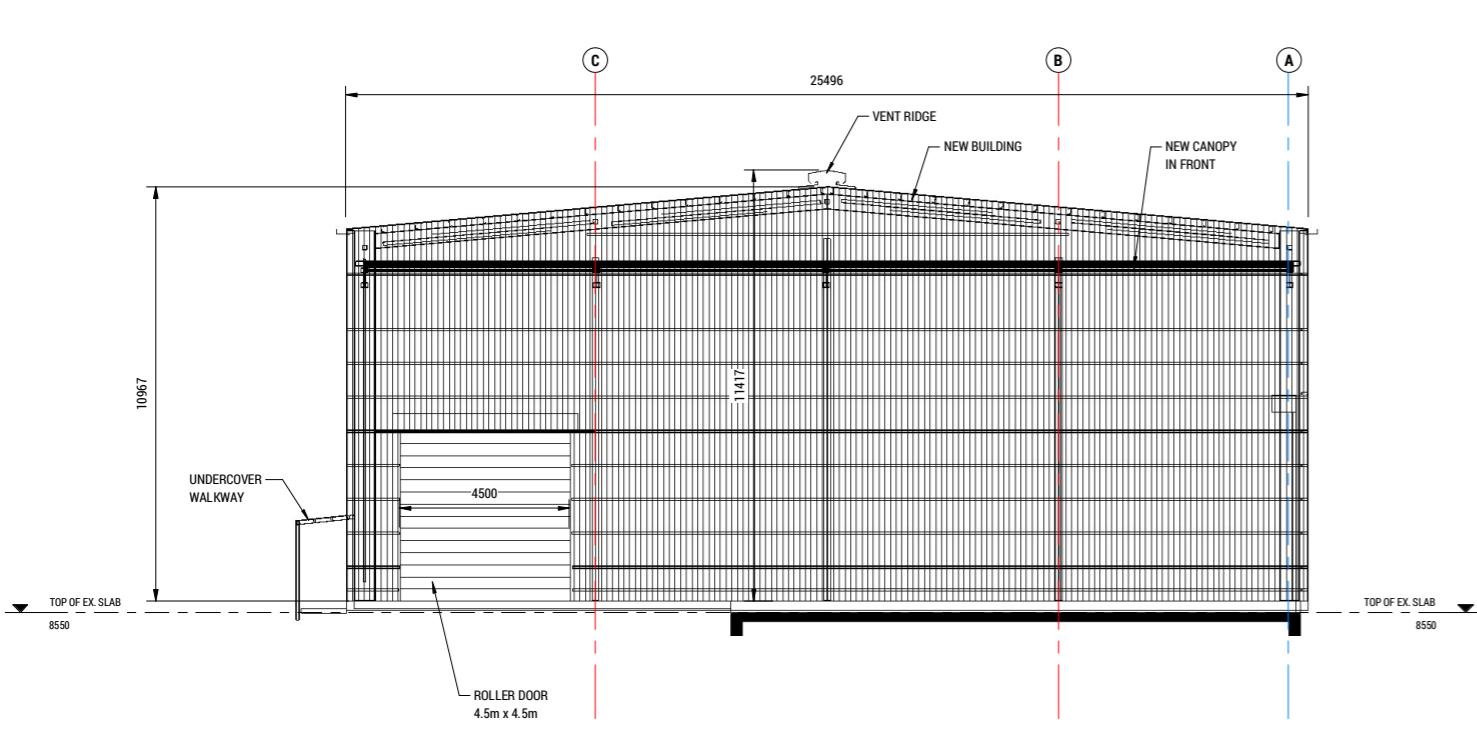
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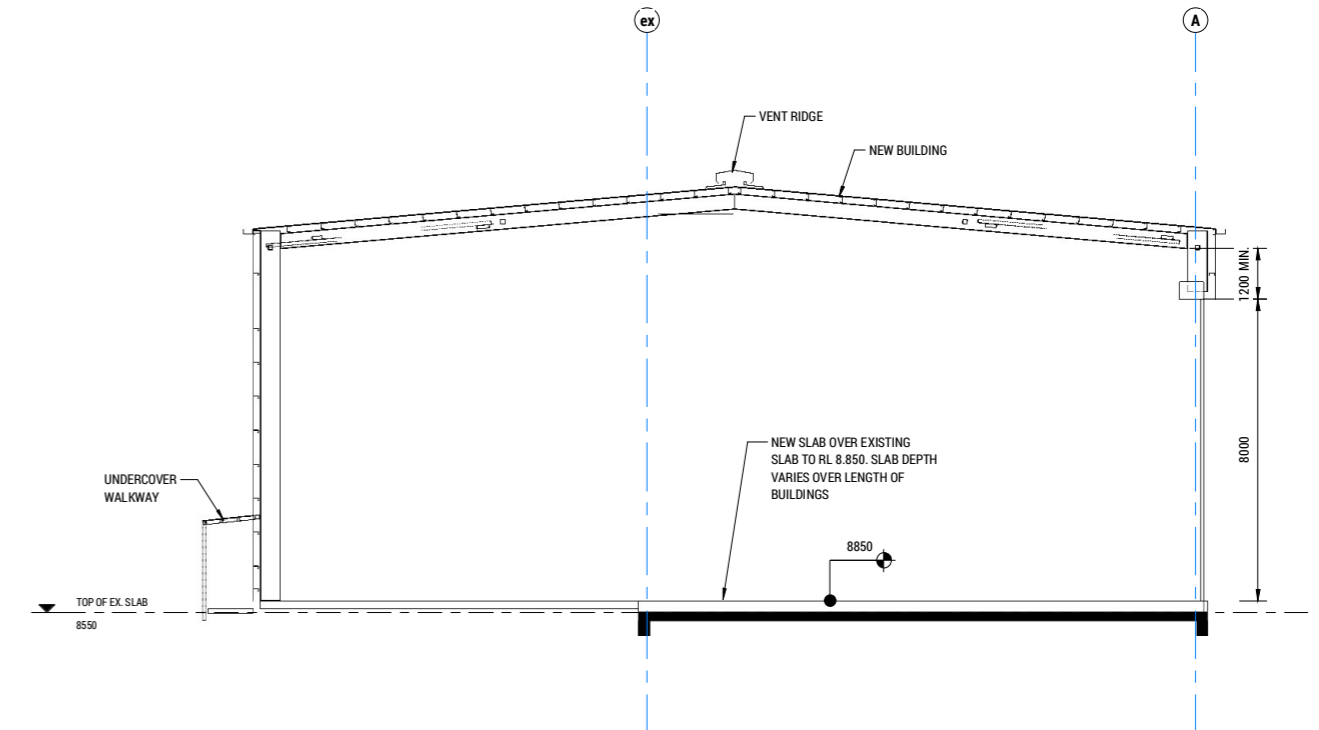
AWNING FRAMING - PLAN
SCALE 1:100

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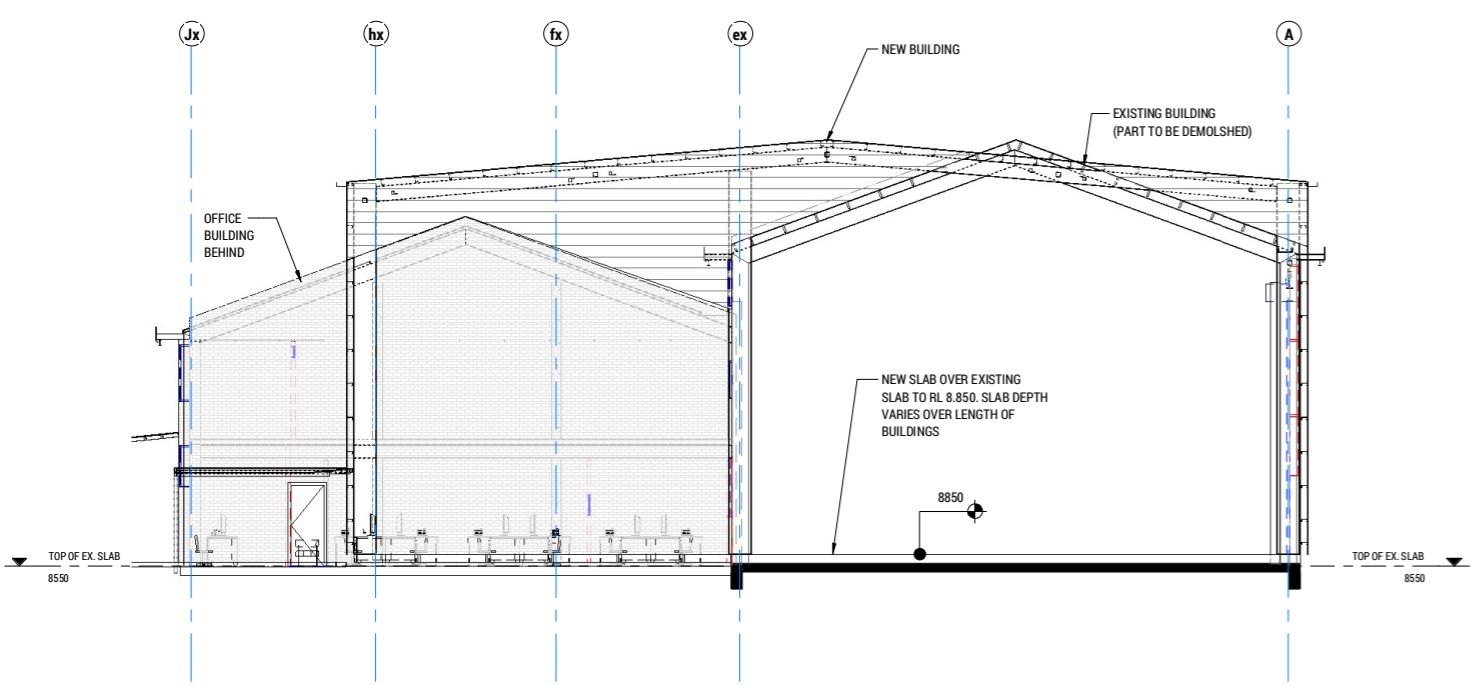
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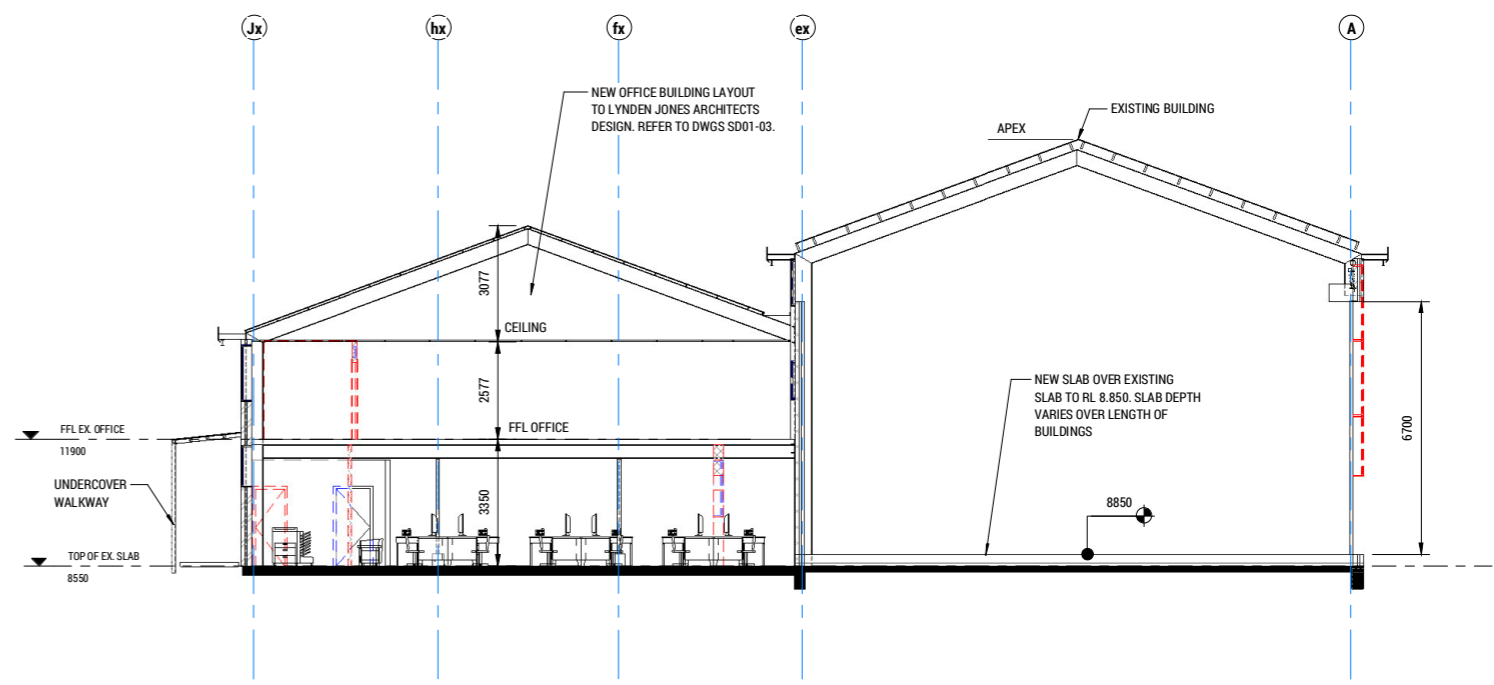
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B SECTION B
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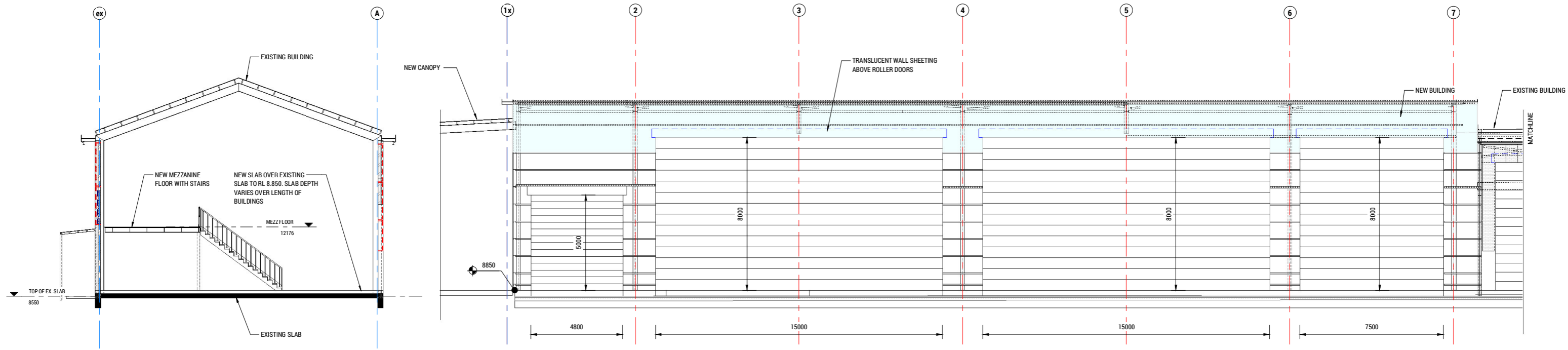
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D SECTION D
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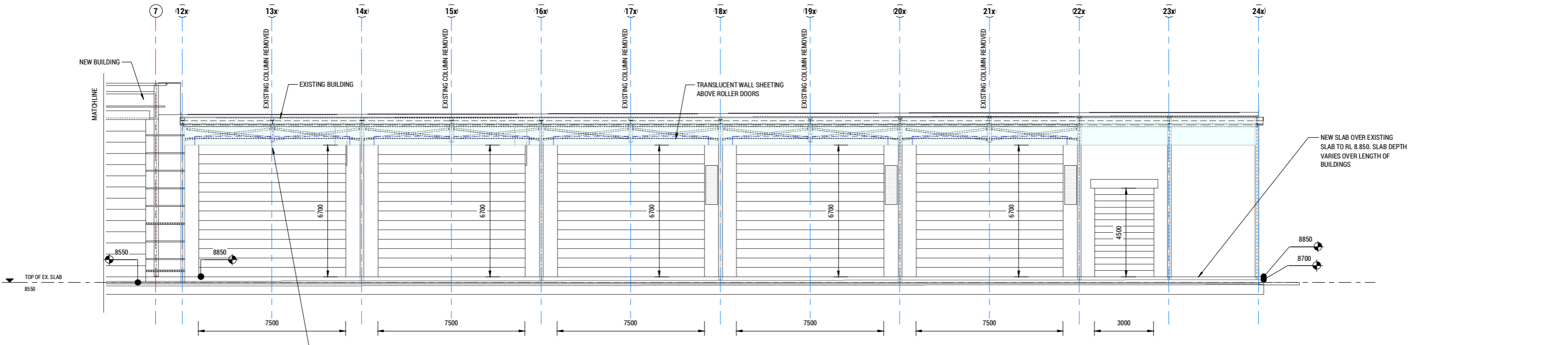
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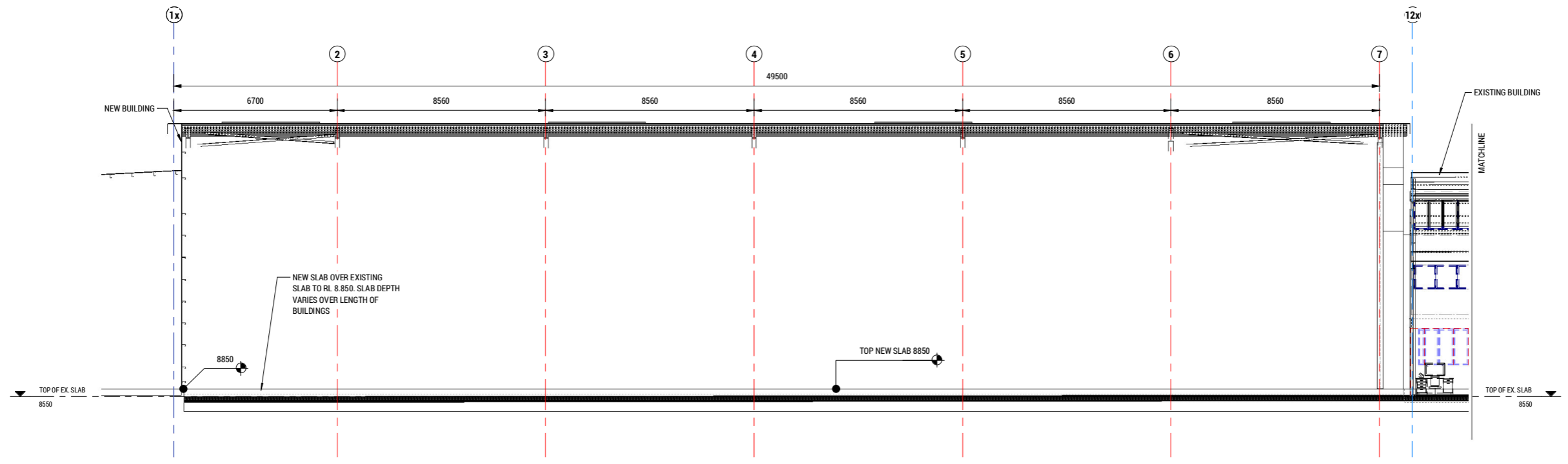
K SECTION K
A21 1:100



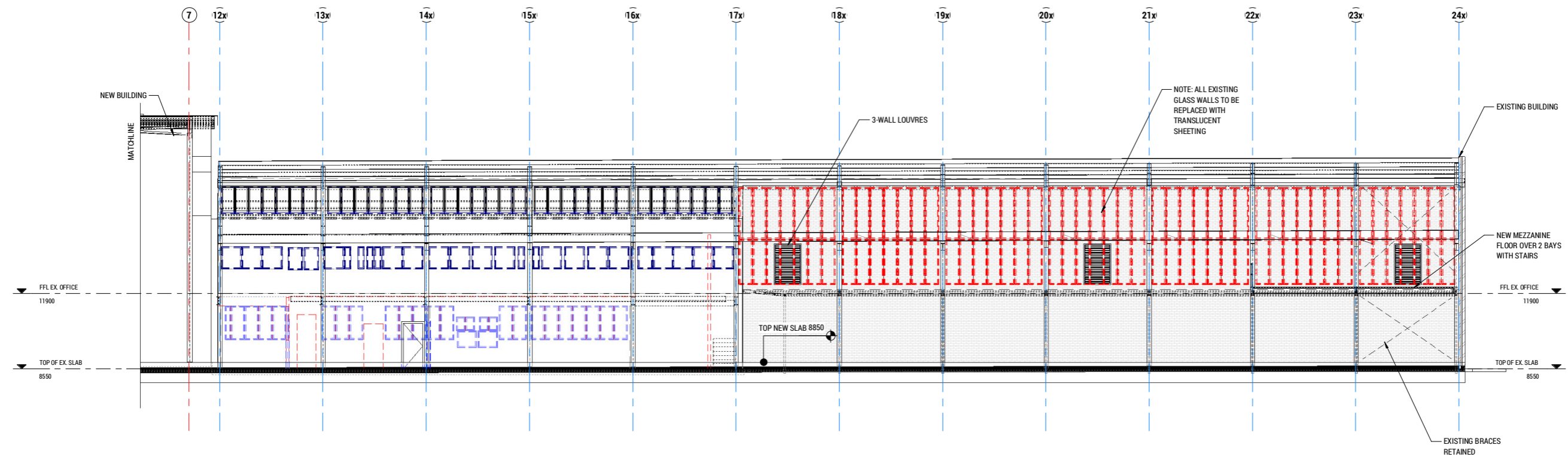
Ka SECTION Ka
1:100

GRID A ONLY:
EXISTING COLUMNS
TO BE REMOVED AND
NEW BEAMS AND
BRACING INSTALLED
TO STRUCTURAL
ENGINEERS DETAILS


				STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH	 22-24 Paterson Street Launceston, TAS 7250 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: TYPICAL SECTIONS - SHEET 2 OF 4
C	DEVELOPMENT APPLICATION	BT	28-07-25	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:100 SHEET SIZE: A1 DWGS IN SET:
B	REVIEW/INFORMATION	BT	03-06-25			DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A31 REV: C
A	REVIEW/INFORMATION	BT	28-05-25			DRAFT CHK: RN			
REV:	DESCRIPTION:	BY:	DATE:	APPROVED: B.STANBOROUGH	ACRED. No: -	DATE: 20-05-25			

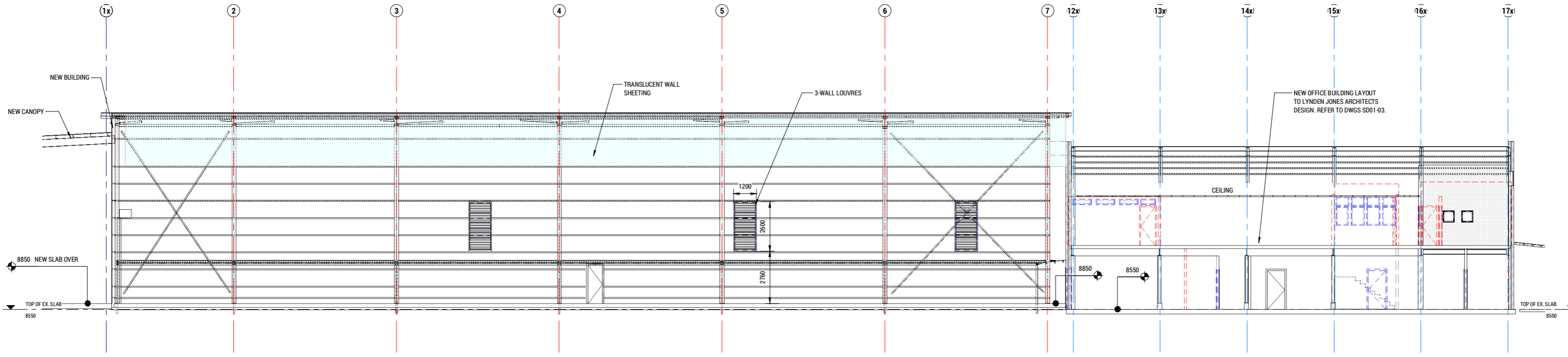


L SECTION L
A21
1:100




La SECTION La
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C	DEVELOPMENT APPLICATION	BT	28-07-25	DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:100 SHEET SIZE: A1 DWGs IN SET:
B	REVIEW/INFORMATION	BT	03-06-25	DRAFT CHK: RN		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A32 REV: C
A	REVIEW/INFORMATION	BT	28-05-25	BY: DATE:		APPROVED: B.STANBOROUGH ACRED. No: -	DATE PRINTED: 28/07/2025 1:57:46 PM

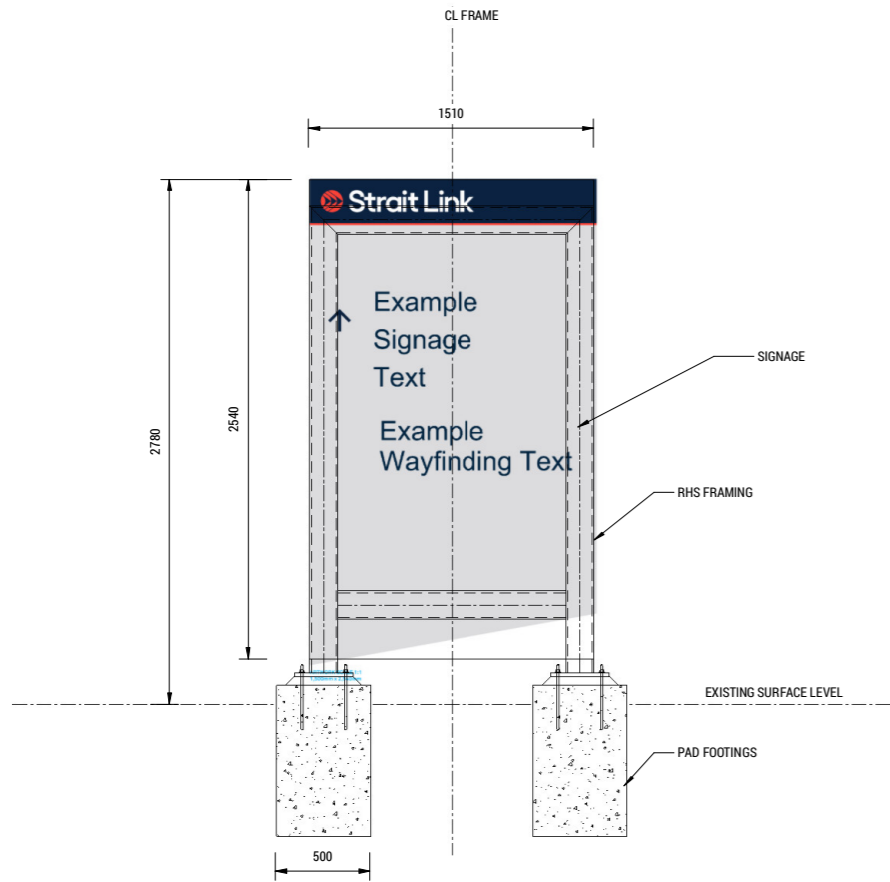


M SECTION M
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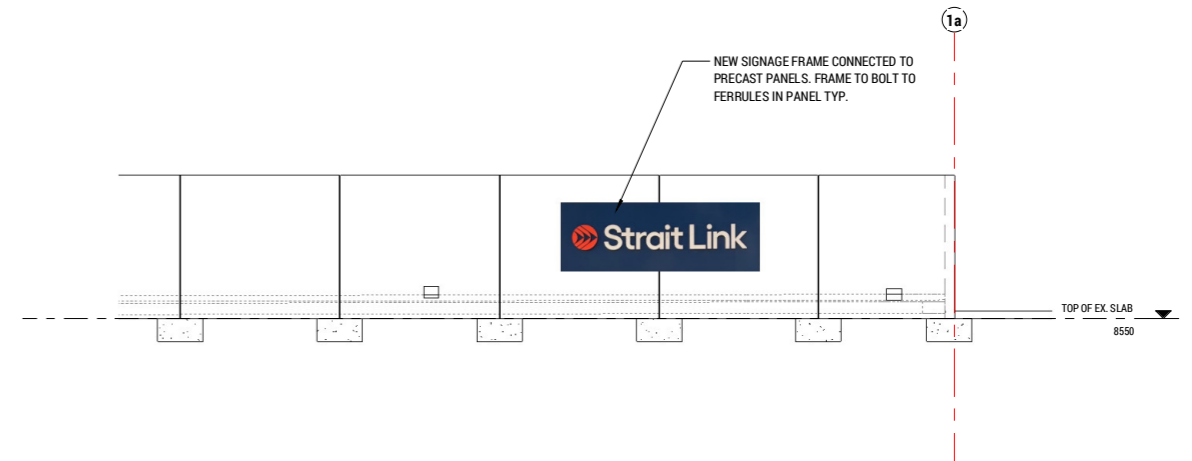
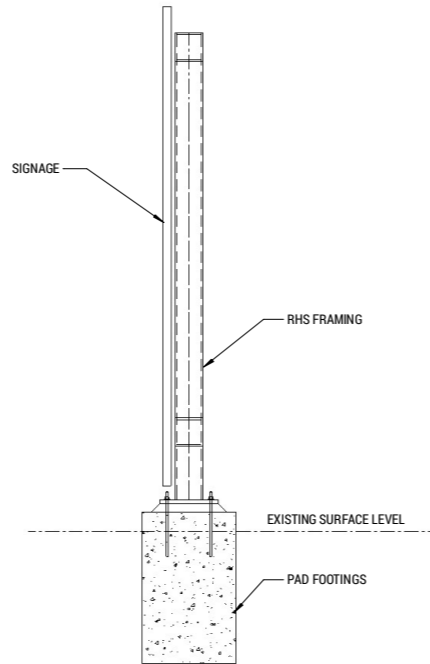
				STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH	 22-24 Paterson Street Launceston, TAS 7250 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LICHT INVESTMENTS PTY LTD.	TITLE: TYPICAL SECTIONS - SHEET 4 OF 4
C	DEVELOPMENT APPLICATION	BT	28-07-25	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 588 257		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:100 SHEET SIZE: A1 DWGS IN SET:
B	REVIEW/INFORMATION	BT	03-06-25			DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A33 REV: C
A	REVIEW/INFORMATION	BT	28-05-25			DRAFT CHK: RN			
REV:	DESCRIPTION:	BY:	DATE:	APPROVED: B.STANBOROUGH	ACRED. No: -	DATE: 20-05-25			

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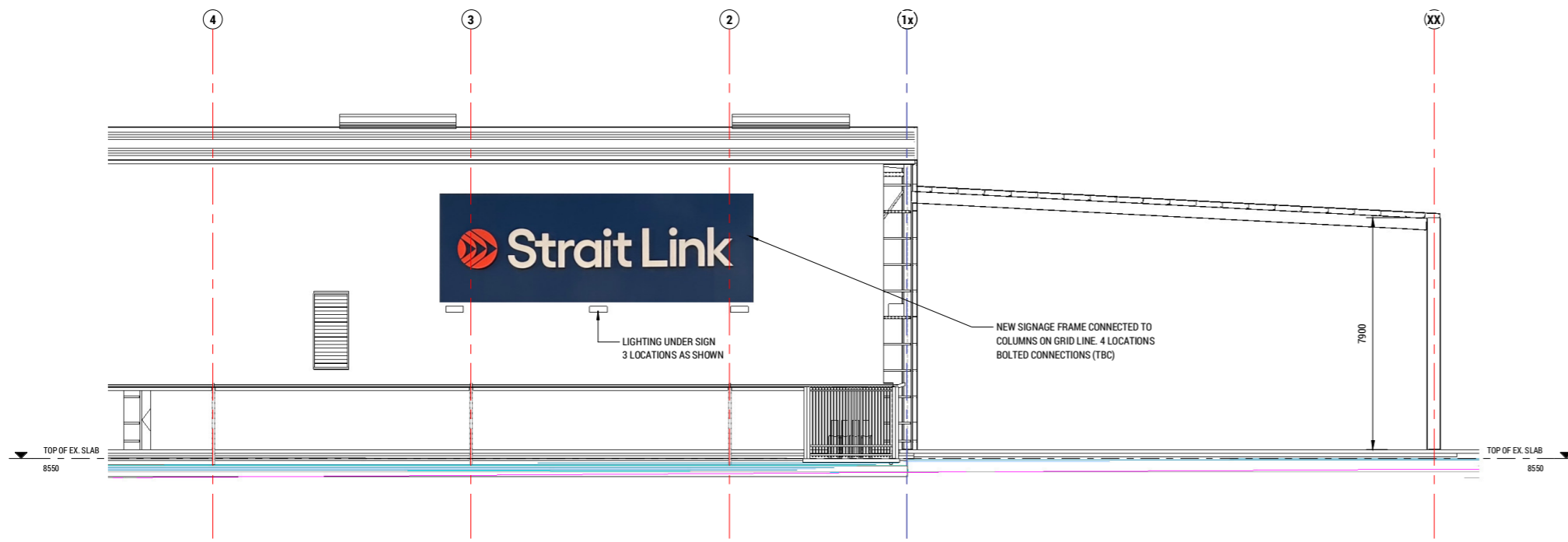
NOTE: FRAMING SIZES TO BE CONFIRMED BY STRUCTURAL ENGINEER ONCE LOADING DETAILS KNOWN



TYPICAL SIGNAGE DETAIL



B WASHDOWN BAY - SIGNAGE
A40 1:100



A BUILDING SIGNAGE - ELEVATION
1:100

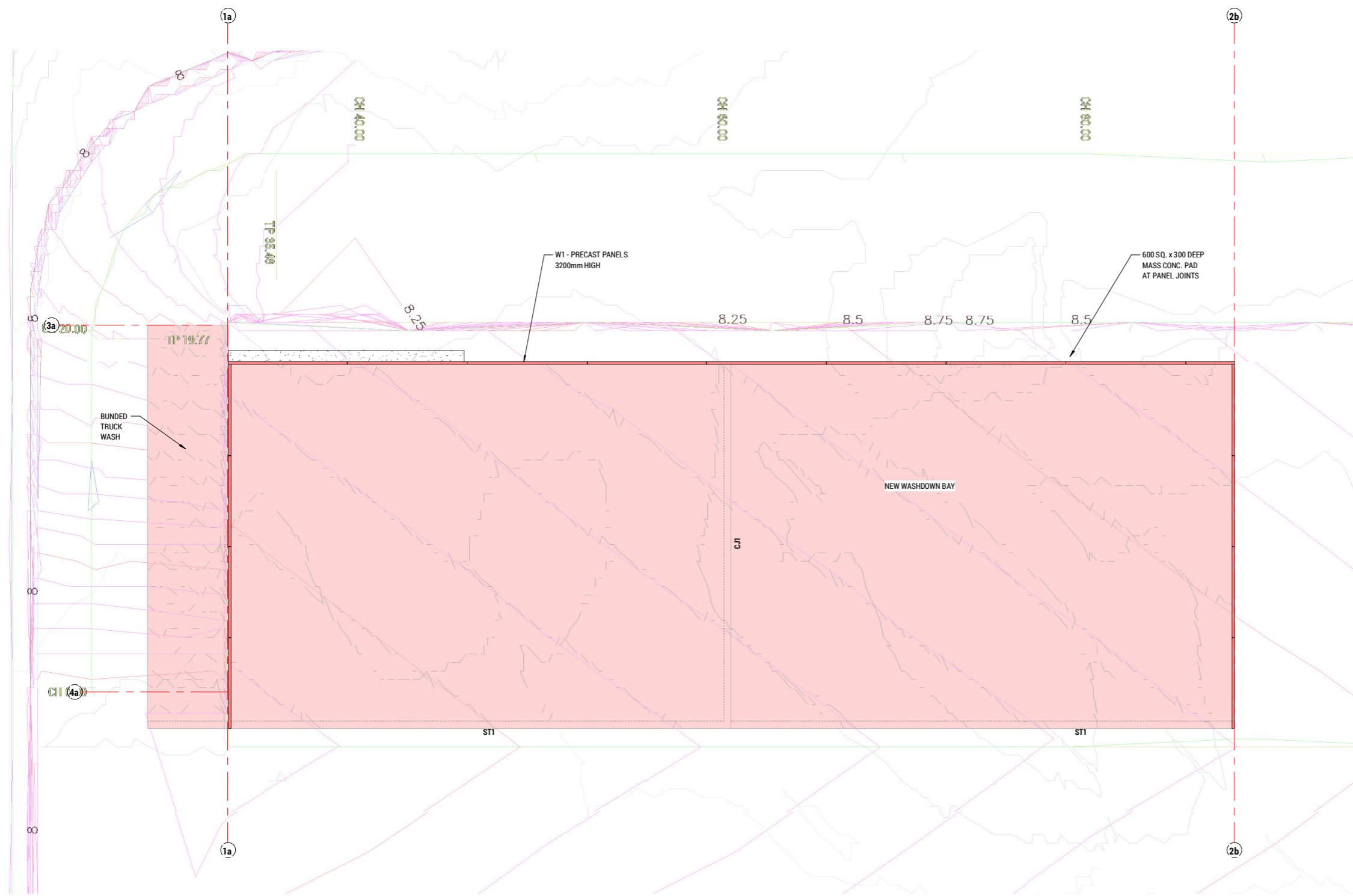
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D	RE-ISSUED FOR DEVELOPMENT APPLICATION	BT	27-08-25
C	DEVELOPMENT APPLICATION	BT	28-07-25
B	REVIEW/INFORMATION	BT	03-06-25
A	REVIEW/INFORMATION	BT	28-05-25

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		DRAFT CHK: RN
		DATE: 20-05-25


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rarein.com.au
P. 03 6388 9200

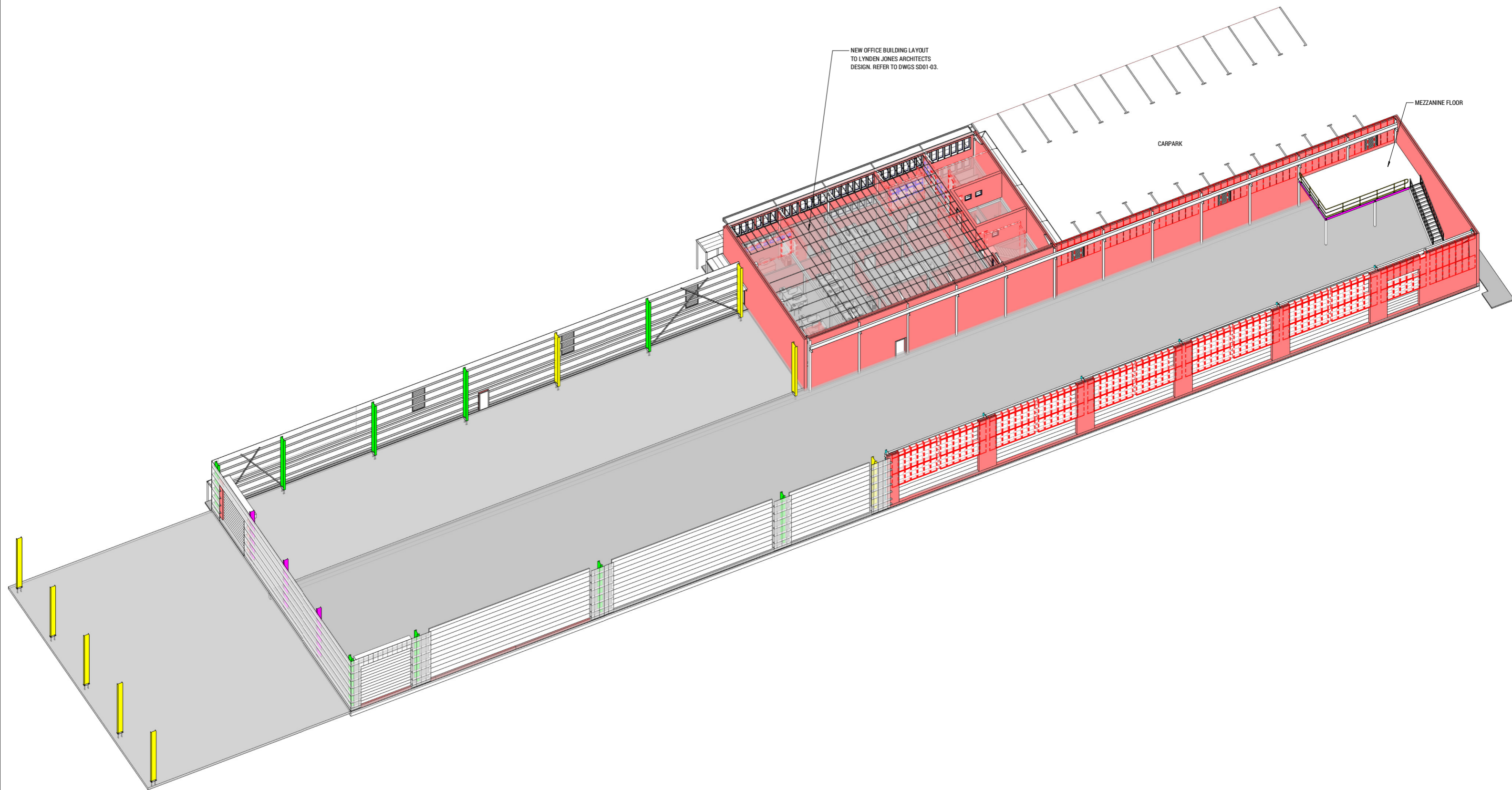
CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.
PROJECT: STRAITLINK - BURNIE TERMINAL
ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE

TITLE: SIGNAGE DETAILS
SCALE: As indicated SHEET SIZE: A1 DWGs IN SET:
PROJECT No: 245019 DWG No: A36 REV: D



WASHDOWN BAY - GENERAL ARRANGEMENT - PLAN
SCALE 1:200

		STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH	 22-24 Paterson Street Launceston, TAS 7250 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: WASH DOWN BAY - GENERAL ARRANGEMENT
C	DEVELOPMENT APPLICATION	BT	28-07-25	DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:125 SHEET SIZE: A1 DWGs IN SET:
B	REVIEW/INFORMATION	BT	03-06-25	DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A40 REV: C
A	REVIEW/INFORMATION	BT	28-05-25	DRAFT CHK: RN			
REV:	DESCRIPTION:	BY:	DATE:	APPROVED: B.STANBOROUGH	ACRED. No: -	DATE: 20-05-25	




NEW OFFICE BUILDING LAYOUT
TO LYNDEN JONES ARCHITECTS
DESIGN. REFER TO DWGS SD01-03.

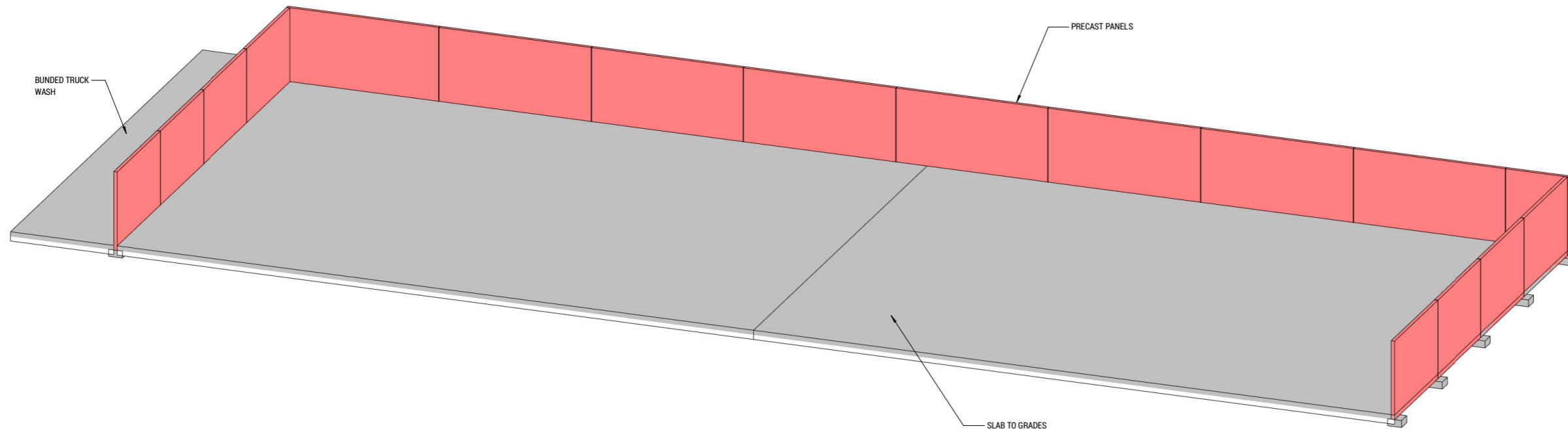
CARPARK

MEZZANINE FLOOR


3D ISOMETRIC VIEW - BUILDING
SCALE 1:150

				STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH	 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LICHT INVESTMENTS PTY LTD.	TITLE: 3D VIEW - BUILDING
C	DEVELOPMENT APPLICATION	BT	28-07-25	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 588 257		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: SHEET SIZE: A1 DWGS IN SET:
B	REVIEW/INFORMATION	BT	03-06-25			DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A52 REV: C
A	REVIEW/INFORMATION	BT	28-05-25			DRAFT CHK: RN			
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3D ISOMETRIC VIEW - WASHDOWN BAY
SCALE 1:100

				STATUS: CONTROLLED DOCUMENT		DESIGN BY: SH	 22-24 Paterson Street Launceston, TAS 7250 rarein.com.au P. 03 6388 9200	CLIENT: STUBBS & LICHT INVESTMENTS PTY LTD.	TITLE: 3D VIEW - WASHDOWN BAY
C	DEVELOPMENT APPLICATION	BT	28-07-25	DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 588 257		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: SHEET SIZE: A1 DWGs IN SET:
B	REVIEW/INFORMATION	BT	03-06-25			DRAWN BY: BT		ADDRESS: MASSEY GREENE DRIVE SOUTH BURNIE	PROJECT No: 245019 DWG No: A53 REV: C
A	REVIEW/INFORMATION	BT	28-05-25			DRAFT CHK: RN			
REV:	DESCRIPTION:	BY:	DATE:	APPROVED: B.STANBOROUGH	ACRED. No: -	DATE: 20-05-25			

CLIENT:
STUBBS & LIGHT INVESTMENTS PTY LTD.

PROJECT:
STRAITLINK - BURNIE TERMINAL

ADDRESS:
MASSY-GREENE DRIVE, SOUTH BURNIE


PROJECT No:
245019

STATUS:
PRELIMINARY / INFORMATION

ISSUED FOR / DESCRIPTION:
REVIEW / INFORMATION

DRAWINGS:

- COV - COVER SHEET
- C001 - CIVIL NOTES
- C101 - EXISTING SITE / DEMOLITION PLAN
- C201 - BULK EARTHWORKS PLAN
- C401 - CIVIL WORKS PLAN
- C402 - CIVIL LEVELS PLAN
- C411 - CIVIL WORKS LONG SECTIONS
- C421 - CIVIL WORKS CROSS SECTIONS - SHEET 1
- C422 - CIVIL WORKS CROSS SECTIONS - SHEET 2
- C423 - CIVIL WORKS CROSS SECTIONS - SHEET 3
- C501 - DRAINAGE PLAN
- C601 - WATER RETICULATION PLAN
- C701 - SECTIONS & DETAILS

				STATUS:	DESIGN BY: JQ	 rare. 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: COVER SHEET
E	REVIEW / INFORMATION	JQ	25-07-25	PRELIMINARY / INFORMATION	DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: - SHEET SIZE: A1 DWGs IN SET: -
D	REVIEW / INFORMATION	JQ	10-07-25	DO NOT SCALE - IF IN DOUBT, ASK	DRAWN BY: JQ		ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No: 245019 DWG No: COV REV: E
C	REVIEW / INFORMATION	JQ	17-06-25	THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257	DRAFT CHK: JS			
B	REVIEW / INFORMATION	JQ	06-06-25					
A	REVIEW / INFORMATION	JQ	28-05-25					
REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:	APPROVED: R.NEVILLE	ACRED. No: CC2080J	DATE: 25-07-25		

GENERAL

1. NOTICE TO TENDERER

THE CONTRACTOR / TENDERER IS TO MAKE THEMSELVES AWARE OF THE LOCAL COUNCIL, TASWATER AND THE DEPARTMENT OF STATE GROWTH (D.S.G.) STANDARDS FOR CIVIL WORKS. CONSTRUCTION IS TO BE CARRIED OUT TO THESE STANDARDS. TENDERER IS TO ALLOW FOR THESE STANDARDS DURING PRICING. COPIES OF THE STANDARDS ARE AVAILABLE FOR INSPECTION UPON REQUEST FROM THE LOCAL COUNCIL OR D.S.G.'s WEB SITE.

2. NOTIFICATION

THE CONTRACTOR IS TO NOTIFY ALL RELEVANT STATUTORY AUTHORITIES PRIOR TO COMMENCING ANY WORK FOR THE POSSIBLE LOCATION OF ANY EXISTING SERVICES NOT SHOWN ON THESE PLANS, AND IS TO NOTIFY THE SUPERINTENDENT OF THE SAME. ALL EXISTING SERVICES ARE TO BE PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO EXISTING SERVICES IS TO BE MADE GOOD AT THE CONTRACTOR'S EXPENSE.

4. COMMON TRENCHING

WHERE ANY COMMON TRENCHING IS REQUIRED, THE FOLLOWING CLEARANCE DISTANCES (BARREL TO BARREL) MUST BE MAINTAINED FROM EXISTING OR PROPOSED SERVICES:

HORIZONTALLY:
 - 300mm ALONG A LENGTH GREATER THAN 2 METRES.
 - 500mm MINIMUM FROM ANY MAIN GREATER THAN 200mm DIA.
 - 150mm MINIMUM ALONG A LENGTH LESS THAN 2 METRES.

VERTICALLY:
 - 150mm MINIMUM
 - 300mm MINIMUM FROM ANY MAIN GREATER THAN 200mm DIA.
 ELECTRICAL CABLES SHOULD BE LOCATED ON THE OPPOSITE SIDE OF THE STREET, WHERE THIS IS NOT POSSIBLE A 400mm MINIMUM DISTANCE MUST BE OBSERVED OF WHICH 300mm SHOULD BE IN NATURAL AND UNDISTURBED MATERIAL.

5. TASNETWORKS TRENCHING

THE CONTRACTOR IS TO ALLOW FOR EXCAVATION AND BACKFILLING OF ALL TRENCHES FOR THE INSTALLATION OF TASNETWORKS CABLES. CONTRACTOR IS TO LIAISE WITH THE TASNETWORKS FOR THE EXTENT OF CABLE TRENCHING, CONDUITS & PITS.

6. COMMUNICATION TRENCHING

THE CONTRACTOR IS TO ALLOW FOR EXCAVATION AND BACKFILLING OF ALL TRENCHES FOR THE INSTALLATION OF COMMUNICATIONS CABLES. CONTRACTOR IS TO LIAISE WITH COMMUNICATION AUTHORITY FOR THE EXTENT OF CABLE TRENCHING.

7. EXISTING SERVICES

LOCATE EXISTING SERVICES PRIOR TO COMMENCING DEMOLITION AND SITE WORKS. THE CONTRACTOR IS TO ARRANGE AND PAY FOR THE ON SITE MARKING AND CONFIRMATION OF DEPTH OF SERVICE LOCATIONS FOR ALL UNDERGROUND SERVICES INCLUDING COMMUNICATIONS, TASNETWORKS, TASWATER (WATER & SEWER) AND COUNCIL SERVICES (ie. STORMWATER) IN THE AREA OF NEW WORKS. LOCATION TO BE CONFIRMED USING CABLE LOCATORS AND HAND DIGGING METHODS. PRIOR TO ANY WORKS ON SITE, ANY CLASHES WITH DESIGNED SERVICES ON FOLLOWING DRAWINGS ARE TO BE REPORTED TO DESIGN ENGINEER FOR DIRECTION.

8. COUNCIL & AUTHORITIES APPROVALS

ALL WORKS ARE TO BE IN ACCORDANCE WITH THE FOLLOWING APPROVALS:
 - TWBA/2024/0017-BCC

9. SIGNAGE

ALL SIGN WORKS AND INSTALLATION TO BE IN ACCORDANCE WITH CURRENT VERSION OF MUTCD & AUSTRADROADS FOR SIGNAGE DETAILS.

10. SCOPE OF WORKS

THE SCOPE OF WORKS ARE SHOWN IN THESE DOCUMENTS AND THE SPECIFICATION. IT IS EXPECTED THE CONTRACTOR WILL RESOLVE ALL ISSUES UNCOVERED ON SITE THAT ARE NOT DETAILED IN CONJUNCTION WITH THE SUPERINTENDENT.

GENERAL CONT.

11. LINE TYPE LEGEND

	DN100 AGE PIPE OR MEGAFLOW DRAIN AS NOTED @ 1:100 FALL TO STORM WATER SYSTEM
	DENOTES EXISTING STORM WATER MAIN (CONFIRM EXACT LOCATION)
	DENOTES PROPOSED STORM WATER MAIN
	DENOTES EXISTING SEWER MAIN (CONFIRM EXACT LOCATION)
	DENOTES PROPOSED SEWER MAIN
	DENOTES EXISTING WATER MAIN (CONFIRM EXACT LOCATION)
	DENOTES PROPOSED WATER MAIN
	DENOTES EXISTING GAS MAIN (CONFIRM EXACT LOCATION)
	DENOTES PROPOSED GAS MAIN
	DENOTES EXISTING UNDERGROUND TELECOM / FIBRE OPTIC LINE (CONFIRM EXACT LOCATION)
	DEMOLITION

12. SITE WORKS SYMBOLS LEGEND

	PEDESTRIAN RAMP
	BARRIER KERB
	KERB AND CHANNEL
	KERB AND CHANNEL - SMALL
	MOUNTABLE KERB AND CHANNEL
	VEHICULAR CROSSING
	BOLLARD, REFER DETAIL
	HUDSON CIVIL PRECAST CONCRETE WHEEL STOP (2000 LONG x 100 HIGH)
	WS1

13. BUILDING SERVICES SYMBOLS LEGEND

	TELECOMMUNICATION PIT
--	-----------------------

14. SURVEY SYMBOLS LEGEND

	EXISTING
	SPOT LEVEL WITH DESCRIPTION
	EXISTING SPOT LEVEL

15. DRAINAGE SYMBOLS LEGEND

	STORMWATER MANHOLE
	SEWER MANHOLE
	GRATED/GULLY PIT - STORM WATER
	GRATED DRAIN - STORM WATER
	SIDE ENTRY PIT - STORM WATER
	UNPLASTICIZED POLYVINYL CHLORIDE
	REINFORCED CONCRETE PIPE (OR FCR) CLASS 4 (Z)
	NOMINAL DIAMETER
	COVER LEVEL
	INVERT LEVEL
	DOWN PIPE
	INSPECTION OPENING
	INSPECTION OPENING TO SURFACE
	GRATED PIT

16. WATER RETICULATION SYMBOLS LEGEND

	METER
	CHECK METER
	FIRE PLUG
	ISOLATION VALVE
	CHECK VALVE
	STRAINER
	MONITORED VALVE
	BALANCE VALVE
	STOP VALVE
	DN100 REFLEX VALVE
	BACK FLOW PREVENTION DEVICE
	PRESSURE REDUCING VALVE
	HOSE BIB COCK
	FIRE HYDRANT
	DUAL HEAD FIRE HYDRANT
	FIRE HOSE REEL

EARTHWORKS

1. GENERAL

GENERAL EARTHWORKS, MATERIAL AND WORKMANSHIP SHALL COMPLY WITH THIS SPECIFICATION AND THE CURRENT EDITION OF THE S.A. CODE FOR EARTHWORKS AS 3798 TOGETHER WITH ANY CODES, STANDARDS OR REGULATIONS REFERRED TO THEREIN.

2. INSPECTIONS

THE CONTRACTOR IS TO ENGAGE AN APPROVED GEOTECHNICAL ENGINEER TO CARRY OUT LEVEL 2 TESTING OF ALL EARTH WORKS TO AS 3798, INCLUDING

- SUBGRADE
- FILLS
- PAVEMENTS
- BACKFILLING OF SERVICE TRENCHES

CERTIFICATION OF THESE ELEMENTS IS TO BE PROVIDED PRIOR TO PRACTICAL COMPLETION

3. AREAS OF FILL

- REMOVE TOP SOIL AND ORGANIC MATERIAL
- PROOF ROLL SUBGRADE IN ACCORDANCE WITH AS1289 TO:
 - 98% STANDARD DRY DENSITY UNDER BUILDING
 - 98% STANDARD DRY DENSITY UNDER ROADS AND CARPARKS
 - REMOVE ANY SOFT SPOTS AND COMPACT WITH 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE
- PLACE FILL AS SPECIFIED AND COMPACT WITHIN 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE
- SUB-GRADE IMPROVEMENT MATERIAL TO BE PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL.

4. AREAS OF CUT

- REMOVE TOP SOIL AND ORGANIC MATERIAL
- PROOF ROLL SUBGRADE IN ACCORDANCE WITH AS1289 TO:
 - 98% STANDARD DRY DENSITY UNDER BUILDINGS
 - 98% STANDARD DRY DENSITY UNDER ROADS AND CAR PARKS
 - REMOVE ANY SOFT SPOTS AND COMPACT WITH 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE

SOIL & WATER MANAGEMENT

1. GENERAL

ALL WORKS ARE TO BE CARRIED OUT IN ACCORDANCE WITH SOIL & WATER MANAGEMENT ON BUILDING & CONSTRUCTION SITES' GUIDELINES AVAILABLE FROM NORTHERN RESOURCE MANAGEMENT (NRM).

2. SOIL EROSION CONTROL

- SOIL EROSION CONTROL IN ACCORDANCE WITH NRM GUIDELINES. CONTRACTOR TO ALLOW TO:
- LIMIT DISTURBANCE WHEN EXACTING BY PRESERVING VEGETATED AREAS AS MUCH AS POSSIBLE
 - DIVERT UP-SLOPE WATER WHERE PRACTICAL
 - INSTALL SEDIMENT FENCES DOWN SLOPE OF ALL DISTURBED LANDS TO FILTER LARGE PARTICLES PRIOR TO STORM WATER SYSTEM
 - WASH EQUIPMENT IN DESIGNATED AREA THAT DOES NOT DRAIN TO STORM WATER SYSTEM
 - PLACE STOCK PILES AWAY FROM ON-SITE DRAINAGE & UP-SLOPE FROM SEDIMENT FENCES
 - LEAVE & MAINTAIN VEGETATED FOOT PATH
 - STORE ALL HARD WASTE & LITTER IN A DESIGNATED AREA THAT WILL PREVENT IT FROM BEING BLOWN AWAY & WASHED INTO THE STORM WATER SYSTEM
 - RESTRICT VEHICLE MOVEMENT TO A STABILISED ACCESS

3. NRM GUIDELINES

- CONTRACTOR TO COMPLETE ALL WORKS IN ACCORDANCE WITH NRM SOIL & WATER MANAGEMENT ON BUILDING & CONSTRUCTION SITE USING THE FACT SHEETS:
- FACT SHEET 1: SOIL & WATER MANAGEMENT ON LARGE BUILDING & CONSTRUCTION SITES
 - FACT SHEET 2: SOIL & WATER MANAGEMENT ON STANDARD BUILDING & CONSTRUCTION SITES
 - FACT SHEET 3: SOIL & WATER MANAGEMENT PLANS
 - FACT SHEET 4: DISPERSIVE SOILS - HIGH RISK OF TUNNEL EROSION
 - FACT SHEET 5: MINIMISE SOIL DISTURBANCE
 - FACT SHEET 6: PRESERVE VEGETATION
 - FACT SHEET 7: DIVERT UP-SLOPE WATER
 - FACT SHEET 8: EROSION CONTROL MATS & BLANKETS
 - FACT SHEET 9: PROTECT SERVICE TRENCHES & STOCKPILES
 - FACT SHEET 10: EARLY ROOF DRAINAGE CONNECTION
 - FACT SHEET 11: SCOUR PROTECTION - STORM WATER PIPE OUTFALLS & CHECK DAMS
 - FACT SHEET 12: STABILISED SITE ACCESS
 - FACT SHEET 13: WHEEL WASH
 - FACT SHEET 14: SEDIMENT FENCES & FIBRE ROLLS
 - FACT SHEET 15: PROTECTION OF STORM WATER PITS
 - FACT SHEET 16: MANAGE CONCRETE, BRICK & TILE CUTTING
 - FACT SHEET 17: SEDIMENT BASINS
 - FACT SHEET 18: DUST CONTROL
 - FACT SHEET 19: SITE RE-VEGETATION

ROAD WORKS

1. GENERAL

ALL WORKS ARE TO BE CARRIED OUT TO THE LOCAL COUNCIL AND D.S.G. STANDARDS. ANY DEPARTURES FROM THESE STANDARDS REQUIRES THE PRIOR APPROVAL OF THE SUPERINTENDENT AND THE LOCAL COUNCIL WORKS SUPERVISOR.

2. INSPECTIONS

THE CONTRACTOR IS RESPONSIBLE FOR ORGANISING THE FOLLOWING INSPECTIONS WITH THE SUPERINTENDENT. 48 HOURS NOTICE IS REQUIRED TO BE GIVEN TO THE SUPERINTENDENT PRIOR TO THE INSPECTION.

- SUBGRADE PREPARATION
- SUB-BASE FOR ROADS, CARPARKS AND KERBS
- BASE COURSE
- FINAL TRIM PRIOR TO PLACING KERBS
- FINAL TRIM PRIOR TO SEALING

3. TESTING

THE CONTRACTOR IS TO BE RESPONSIBLE FOR ORGANISING AND PAYING ALL COSTS ASSOCIATED WITH TESTING IN ACCORDANCE WITH D.S.G. SPEC SECTION 173-EXAMINATION AND TESTING OF MATERIALS AND WORK (ROADWORKS).

4. BASE COURSE LAYERS

- SUB-BASE TYPE 3 MATERIAL TO BE PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR SUB-BASE CLASS 3 MATERIAL
 - BASE CLASS 2 MATERIAL TO BE PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR BASE CLASS 2 MATERIAL

5. HOTMIX

ALL HOTMIX IS TO BE BLACK IN COLOUR AND IS TO MEET AND BE PLACED IN ACCORDANCE WITH D.S.G. SPEC SECTION 407-HOT MIX ASPHALT.

6. KERBS

ALL KERBS ARE TO BE AS SHOWN ON THE DRAWINGS AND BE IN ACCORDANCE WITH IPWEA LGAT STANDARD DRAWINGS.

7. ROAD RESERVE WORKS

ALL WORKS IN (OR REQUIRING OCCUPATION) IN THE ROAD RESERVE MUST BE UNDERTAKEN BY CONTRACTOR REGISTERED WITH COUNCIL'S (REGISTERED CONTRACTOR).

8. FOOTPATHS

CONSTRUCT FOOTPATHS INCLUDING EXPANSION / CONTROL / WEAKENED PLANE JOINTS IN ACCORDANCE WITH IPWEA STD DWG TSD-R11 -v3

9. LANDSCAPE / STREET FURNITURE

- BOLLARDS, REFER DETAILS / SUPERINTENDENTS SPEC.
- LANDSCAPING & STREET FURNITURE BY CONTRACTOR - U.N.O

STORMWATER

1. GENERAL

ALL WORKS ARE TO BE CARRIED OUT TO THE LOCAL COUNCIL AND DSG STANDARDS. ANY DEPARTURES FROM THESE STANDARDS REQUIRES THE PRIOR APPROVAL OF THE SUPERINTENDENT AND THE LOCAL COUNCIL WORKS SUPERVISOR. ALL STORM WATER PLUMBING & DRAINAGE TO COMPLY WITH A.S.3508.3:2003 STORM WATER DRAINAGE.

2. TESTING

ALL DRAINAGE WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES HAVING JURISDICTION OVER THE VARIOUS SERVICES. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND PROPERLY INSTALLED AT THE CONTRACTOR'S EXPENSE.

3. MANHOLES

MANHOLES ARE TO BE 1050 I.D. U.N.O PRECAST CONCRETE INSTALLED TO LOCAL COUNCIL STANDARDS. ALL MANHOLES IN TRAFFICED AREAS ARE TO BE FITTED WITH HEAVY DUTY GATIC COVERS AND SURROUNDS. ALL MANHOLES ARE TO HAVE A 5 METRE LENGTH OF 75mm AG-PIPE CONNECTED TO THEM AND LAID IN THE UPSTREAM PIPE TRENCH IMMEDIATELY ADJACENT TO AND AT THE INVERT OF THE LOWEST PIPE WORK.

4. SIDE ENTRY PIT (SEP)

- PIT INVERT DEPTHS VARY, REFER SITE PLAN.
- BENCH OUT IN A NEAT AND TIDY MANNER TO ENGINEERS APPROVAL.
- GRATED PIT - GULLY HINGED OR OTHER TYPE APPROVED
- CONCRETE KERB LINTEL - STEEL KERB LINTEL AND 1200 LONG GALV BAR

5. TRENCHING AND BACKFILL

ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THE DRAWINGS AND THE LOCAL COUNCIL STANDARDS.

6. INSPECTIONS

THE CONTRACTOR IS RESPONSIBLE FOR ORGANISING THE FOLLOWING INSPECTIONS WITH THE SUPERINTENDENT. 48 HOURS NOTICE IS REQUIRED TO BE GIVEN TO THE SUPERINTENDENT PRIOR TO THE INSPECTION.

- PIPEWORK BEDDING
- INSTALLED PIPE PRIOR TO BACKFILLING
- BACKFILLING

7. AS CONSTRUCTED DRAWINGS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PRODUCING 'AS CONSTRUCTED' DRAWINGS TO THE STANDARD REQUIRED BY THE LOCAL COUNCIL. THE DRAWINGS SHALL BE CERTIFIED AS BEING CORRECT BY EITHER A CHARTERED CIVIL ENGINEER OR A REGISTERED SURVEYOR. RARE CAN PROVIDE THIS SERVICE, HOWEVER THE CONTRACTOR WILL BE CHARGED FOR THIS SERVICE AND SHOULD BE AWARE OF THIS WHEN PRICING.

8. TESTING

CONTRACTOR SHALL CAMERA TEST ALL PIPES AND SUBMIT FOOTAGE TO LOCAL COUNCIL FOR APPROVAL.

9. REDUNDANT PIPE WORK

FILL REDUNDANT SECTION OF PIPEWORK WITH 'LIQUIFILL' (GRADE PC.1 - 0.5-2.0 MPa)

SEWERAGE

1. GENERAL

ALL SEWER WORKS TO BE IN ACCORDANCE WITH THE WSA SEWER CODE (WSA 02-2014-3.1 MRWA) AND AS AMENDED BY THE TASWATER SUPPLEMENT. TASWATER APPROVED PRODUCTS ARE OBTAINED ON THE CITY WEST WATER WEBSITE: HTTP://WWW.MRWA.COM.AU/PAGES/PRODUCTS.ASPX ANY DEPARTURES FROM THESE STANDARDS REQUIRES THE PRIOR APPROVAL OF THE SUPERINTENDENT AND TASWATER FIELD SERVICES OFFICER.

2. TESTING

ALL DRAINAGE WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES HAVING JURISDICTION OVER THE VARIOUS SERVICES. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND PROPERLY INSTALLED AT THE CONTRACTOR'S EXPENSE.

3. SEWER MAIN CONNECTIONS

ALL NEW 'LIVE' CONNECTIONS TO EXISTING TASWATER SEWER INFRASTRUCTURE INCLUDING BUT NOT LIMITED TO SEWER MAINS / MANHOLES TO BE COMPLETED BY TASWATER (UNLESS PRIOR WRITTEN APPROVAL) AT OWNERS COST. INSTALL PROPERTY SEWER CONNECTIONS (STANDARD OR SLOPED) WITH SURFACE 1.0. NOMINALLY 110m WITHIN EACH NEW LOT IN ACCORDANCE WITH SECTION 5 OF WSA 02-2014-3.1.

4. MANHOLES

MANHOLES ARE TO BE 1050 I.D. PRECAST CONCRETE INSTALLED TO WSA STANDARDS. CONSTRUCT ALL MANHOLES (MH) AND MANHOLE COVERS IN ACCORDANCE WITH THE SEWERAGE CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES INTEGRATED CODE - WSA 02-2014-3.1 MRWA VERSION 2.0 AND TASWATER'S SUPPLEMENT TO THIS CODE. ALL MANHOLES IN TRAFFICABLE AREAS ARE TO BE FITTED WITH HEAVY DUTY CLASS 8 GATIC COVERS AND SURROUNDS. ALL MANHOLES IN NON-TRAFFICABLE AREAS ARE TO BE FITTED WITH MEDIUM DUTY CLASS 8 GATIC COVERS AND SURROUNDS. BENCHING TO BE FULL DEPTH OF PIPE DIAMETER AS PER DETAILS IN WSA 02-2014-3.1 MRWA VERSION 2.0

5. TRENCHING AND BACKFILL

ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THE DRAWINGS AND TASWATER STANDARDS INCLUDING ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON METALLIC PIPE TRENCHES.

CEMENT STABILISED EMBEDMENT:

FOR SEWER MAINS THE FOLLOWING CHANGES SHOULD BE APPLIED TO THE MRWA SEWERAGE STANDARDS DRAWINGS MRWA-S-202 AND MRWA-S-205 MRWA-S-202 THE REQUIREMENT IDENTIFIED IN THE THIRD DOT POINT FOR TYPE B IN THE NOTES REGARDING TABLE 202-A SHALL BE AMENDED TO READ "WHERE SEWER AT GRADE > 1 IN 10 MRWA-S-205

NOTE C REMAINS VALID "WHEN SOCKETS MAINS ARE LAID AT > 1 IN 20 SLOPE IN AREAS THAT ARE LIKELY TO HAVE HIGH GROUND WATER, CEMENT STABILIZED EMBEDMENT SHALL BE USED AS PER MRWA-S-202"

6. INSPECTIONS

THE CONTRACTOR IS RESPONSIBLE FOR ORGANISING THE FOLLOWING INSPECTIONS WITH THE SUPERINTENDENT (LIAS WITH TASWATER). 48 HOURS NOTICE IS REQUIRED TO BE GIVEN TO THE SUPERINTENDENT PRIOR TO THE INSPECTION.

- PIPEWORK BEDDING
- INSTALLED PIPE PRIOR TO BACKFILLING
- BACKFILLING

7. AS CONSTRUCTED DRAWINGS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PRODUCING 'AS INSTALLED' DRAWINGS TO THE STANDARD REQUIRED BY TASWATER. THE DRAWINGS SHALL BE CERTIFIED AS BEING CORRECT BY EITHER A CHARTERED CIVIL ENGINEER OR A REGISTERED SURVEYOR. RARE CAN PROVIDE THIS SERVICE, HOWEVER THE CONTRACTOR WILL BE CHARGED FOR THIS SERVICE AND SHOULD BE AWARE OF THIS WHEN PRICING.

8. TESTING

CONTRACTOR SHALL CCTV ALL PIPES AND SUBMIT FOOTAGE TO TASWATER FOR APPROVAL.

9. REDUNDANT PIPE WORK

FILL REDUNDANT SECTION OF PIPEWORK WITH 'LIQUIFILL' (GRADE PC.1 - 0.5-2.0 MPa)

WATER RETICULATION

1. GENERAL

ALL WATER SUPPLY CONSTRUCTION TO:
 • WATER SUPPLY CODE OF AUSTRALIA (WSA 03-2011-3.1 VERSION MRWA EDITION V2.0) - PART 2: CONSTRUCTION
 • WATER SERVICES ASSOCIATION OF AUSTRALIA - TASWATER SUPPLEMENT
 • TASWATER'S STANDARD DRAWINGS TWS-W-0002 SERIES
 • WATER METERING POLICY/METERING GUIDELINES
 • TASWATER'S STANDARD DRAWINGS TWS-W-0003 - FOR PROPERTY SERVICE CONNECTIONS - CASE FOR WATER METER ASSEMBLY
 • BOUNDARY BACKFLOW CONTAINMENT REQUIREMENTS AND ASS200.1:2003

ANY DEPARTURES FROM THESE STANDARDS REQUIRES THE PRIOR APPROVAL OF THE SUPERINTENDENT AND THE LOCAL WATER AUTHORITY WORKS SUPERVISOR.

2. TESTING

ALL WATER RETICULATION WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES HAVING JURISDICTION OVER THE VARIOUS SERVICES. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND PROPERLY INSTALLED AT THE CONTRACTOR'S EXPENSE.

3. FIRE HYDRANTS

FIRE HYDRANTS ARE TO BE AS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS TO ALLOW TO PLACE STANDARD MARKERS AS REQUIRED BY THE LOCAL AUTHORITY.

4. THRUST AND ANCHOR BLOCKS

THRUST AND ANCHOR BLOCKS ARE TO BE PROVIDED AT BENDS, VALVES, HYDRANTS AND LINE ENDS IN ACCORDANCE WITH TASWATER STANDARDS.

5. TRENCHING AND BACKFILL

ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THE DRAWINGS AND TASWATER STANDARDS INCLUDING ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON METALLIC PIPE TRENCHES.

CEMENT STABILISED EMBEDMENT:

THE LATEST VERSION OF DRAWING MRWA-W-208 (REV 3) INCLUDES TABLE 208-A WITH NOTE G INDICATING THAT WHEN TRENCHSTOPS OR BULKHEADS ARE USED (GRADES GREATER THAN 5%) CEMENT STABILISED EMBEDMENT MUST BE USED. THIS IS NOT TASWATER'S PREFERRED STANDARD.

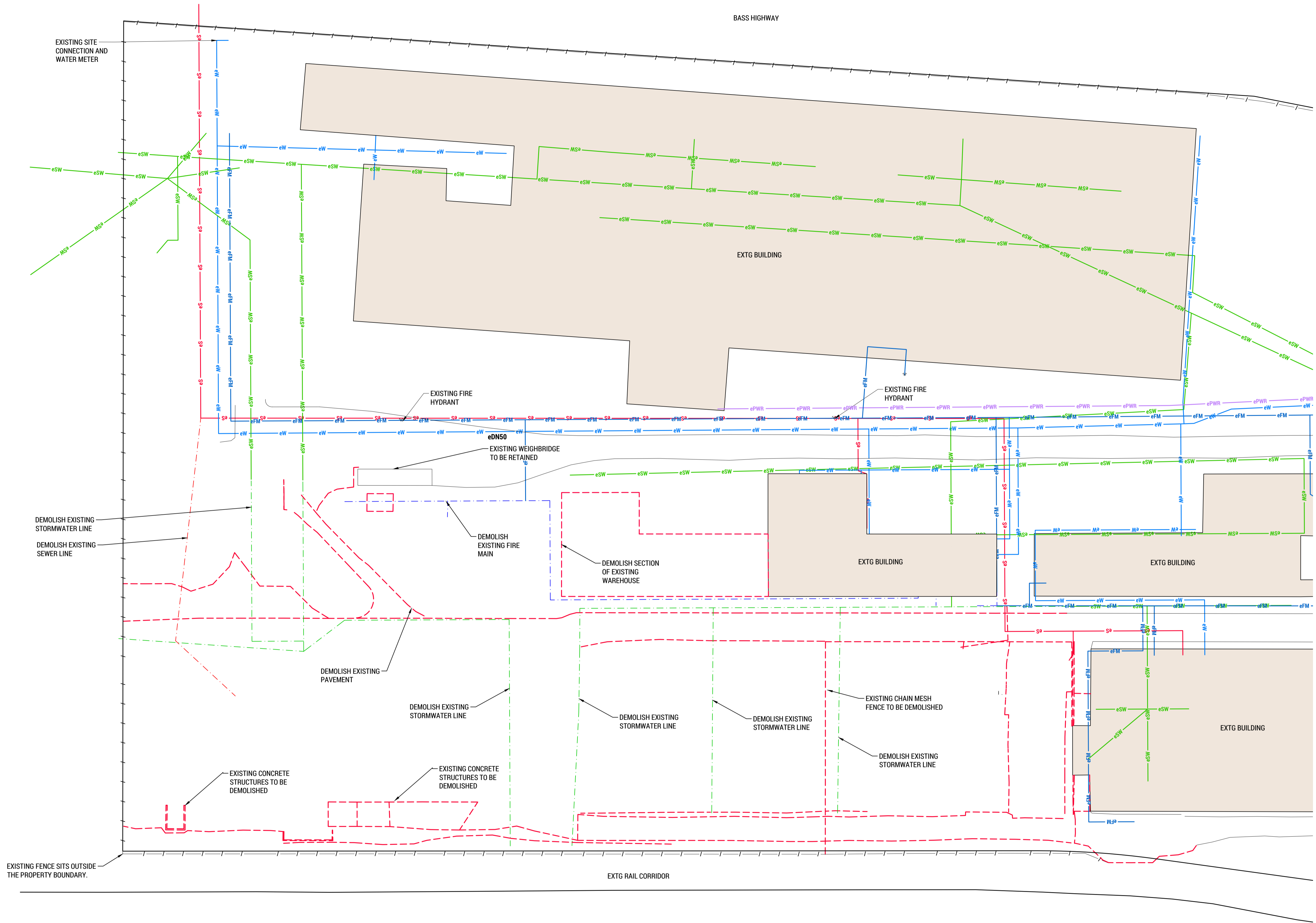
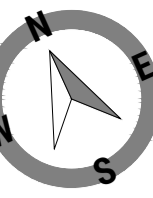
FOR PIPES UP TO 10% GRADE TASWATER WILL ACCEPT THE PREVIOUS REVISION OF MRWA (REV 2), IE. PIPES UP TO 10% GRADE DO NOT REQUIRE CEMENT STABILISED EMBEDMENT UNLESS THE CONDITIONS OF NOTE H APPLY. "WHEN SOCKETS MAINS ARE LAID AT >5% SLOPE IN AREAS THAT ARE LIKELY TO HAVE HIGH GROUND WATER, CEMENT STABILISED EMBEDMENT SHALL BE USED." FOR PIPES AT GRADE GREATER THAN 10% MRWA-W-208 REV 3 REMAINS VALID.

THE LATEST VERSION OF MRWA-W-203 (REV 2) EMBEDMENT SHALL BE ADOPTED NOTING THAT THE REQUIREMENT IDENTIFIED IN THE THIRD DOT POINT FOR TYPE B IN THE NOTES REGARDING TABLE 203-A SHALL BE AMENDED TO READ "WHERE WATER MAIN GRADE > 10%".

FURTHER TO THIS IT SHOULD BE NOTED THAT MOST WATER MAINS ARE LIKELY TO REQUIRE A TYPE A EMBEDMENT SYSTEM. THE VARIOUS MATERIALS AVAILABLE FOR THIS SYSTEM ARE IDENTIFIED IN TABLE 203-B

6. INSPECTIONS


THE CONTRACTOR IS RESPONSIBLE FOR ORGANISING THE FOLLOWING INSPECTIONS WITH THE SUPERINTENDENT. 48 HOURS NOTICE IS REQUIRED TO BE GIVEN TO THE SUPERINTENDENT PRIOR TO THE INSPECTION.

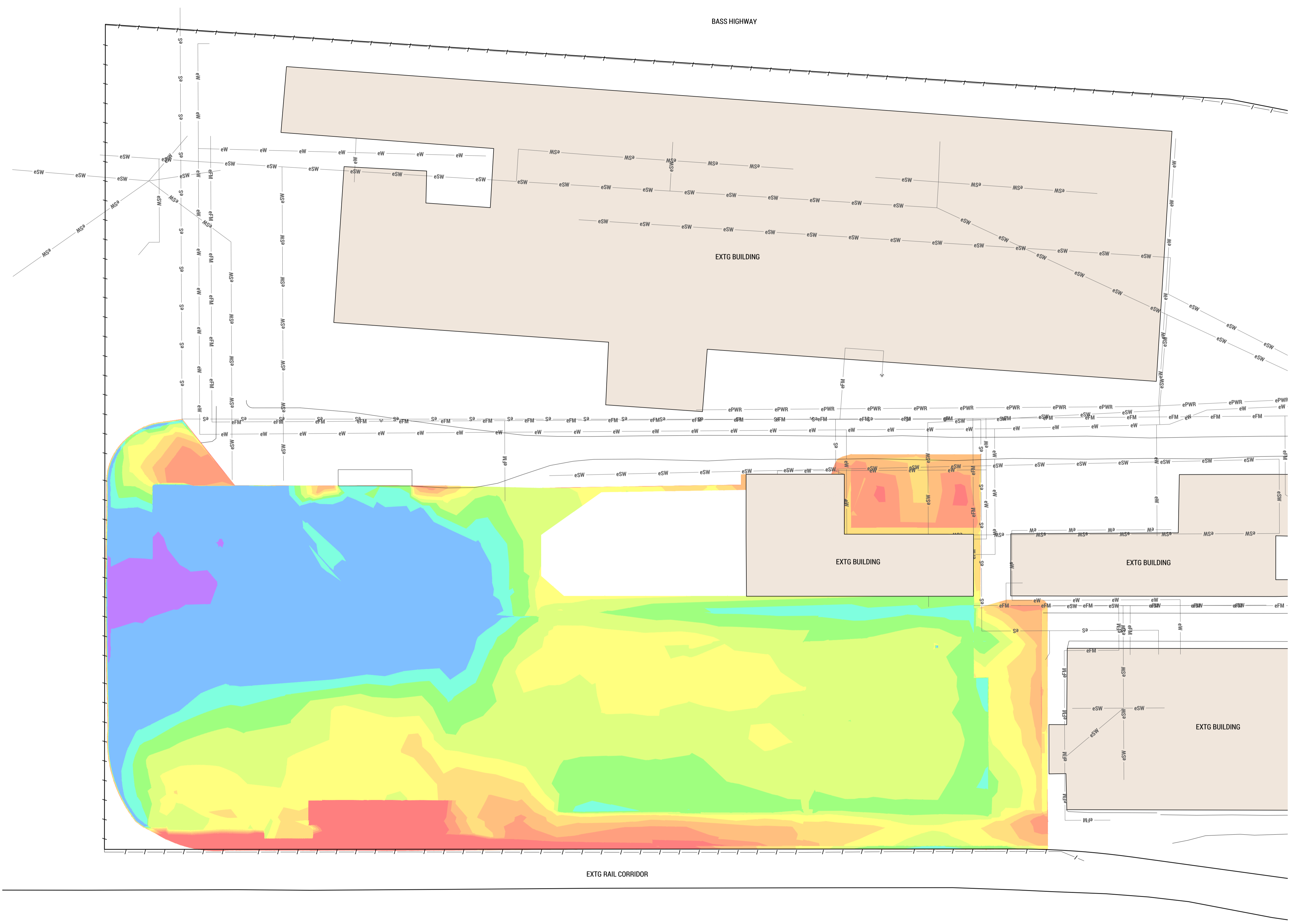
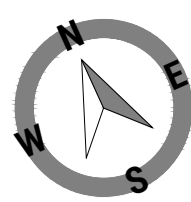


- DEMOLITION NOTES**
1. PRIOR TO COMMENCING DEMOLITION AND SITE WORKS, THE CONTRACTOR IS TO ARRANGE AND PAY FOR THE ON SITE MARKING AND CONFIRMATION OF DEPTH, OF SERVICE LOCATIONS FOR ALL UNDERGROUND SERVICES INCLUDING COMMUNICATIONS, TASNETWORKS, POWERCO AND COUNCIL SERVICES (ie: WATER, STORMWATER AND SEWER) IN THE AREA OF NEW WORKS. LOCATION TO BE CONFIRMED USING CABLE LOCATORS AND HAND DIGGING METHODS. PRIOR TO ANY WORKS ON SITE, ANY CLASHES WITH DESIGNED SERVICES ON FOLLOWING DRAWINGS ARE TO BE REPORTED TO DESIGN ENGINEER FOR DIRECTION.
 2. REFER DRAWINGS FOR SET OUT DIMENSIONS & COORDINATE ALL LEVELS, CONTRACTOR TO REFER ENGINEER FOR ANY DISCREPANCIES / CLASHES.
 3. CAP & TERMINATE & REMOVE REDUNDANT DISUSED DRAINAGE SERVICES TO SATISFACTION OF ENGINEER & LOCAL AUTHORITIES
 4. INSTALL SILT FENCES & TRAPS TO PREVENT SEDIMENTS & POLLUTANTS ENTERING STORM WATER SYSTEM OR NATURAL DRAINAGE LINES
 5. STOCK PILING OF SOILS OR MATERIALS AFFECTED BY WATER TO BE STORED CLEAR OF ANY DRAINAGE PATH
 6. CLEAN SITE VEHICLES BEFORE EXITING SITE
 7. DISPOSE OF EXCAVATED MATERIAL TO LICENSED WASTE FACILITY OR APPROVED LAND FILL SITE
 8. TRENCHES WHERE SERVICES ARE REMOVED ARE TO BE FILLED WITH AN APPROVED COMPACTED MATERIAL & TO ENGINEERS COMPACTION SPECIFICATIONS. MATCH & MAKE GOOD EXISTING SURFACES TO MATCH EXISTING SURROUNDINGS.
- STRIPPING NOTES**
1. CONTRACTOR TO ALLOW TO EXCAVATE TO 200mm NOM. BELOW EXISTING SURFACE LEVEL OR TO DESIGN SURFACE SUB-GRADE LEVEL - WHICH EVER IS LOWER. NOTE NO VARIATION WILL BE PAID FOR UNDER / OVER EXCAVATION, UNLESS PRIOR APPROVAL FROM ENGINEER.

- LEGEND**
- eSW EXISTING STORM WATER MAIN
 - eS EXISTING SEWER MAIN
 - eW EXISTING WATER MAIN
 - eCOM EXISTING COMMUNICATIONS LINE
 - - - EXISTING SURFACE/STRUCTURE TO BE DEMOLISHED
 - - - EXISTING SERVICE LINE TO BE DEMOLISHED

EXISTING SITE/DEMOLITION PLAN
SCALE 1:500

E REVIEW / INFORMATION		JQ	25-07-25	STATUS: PRELIMINARY / INFORMATION DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257	DESIGN BY: JQ	 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: EXISTING SITE / DEMOLITION PLAN
D REVIEW / INFORMATION		JQ	10-07-25		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:500 SHEET SIZE: A1 DWGS IN SET: -
C REVIEW / INFORMATION		JQ	17-06-25		DRAWN BY: JQ		ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No: 245019 DWG No: C101 REV: E
B REVIEW / INFORMATION		JQ	06-06-25		DRAFT CHK: JS			
A REVIEW / INFORMATION		JQ	28-05-25					
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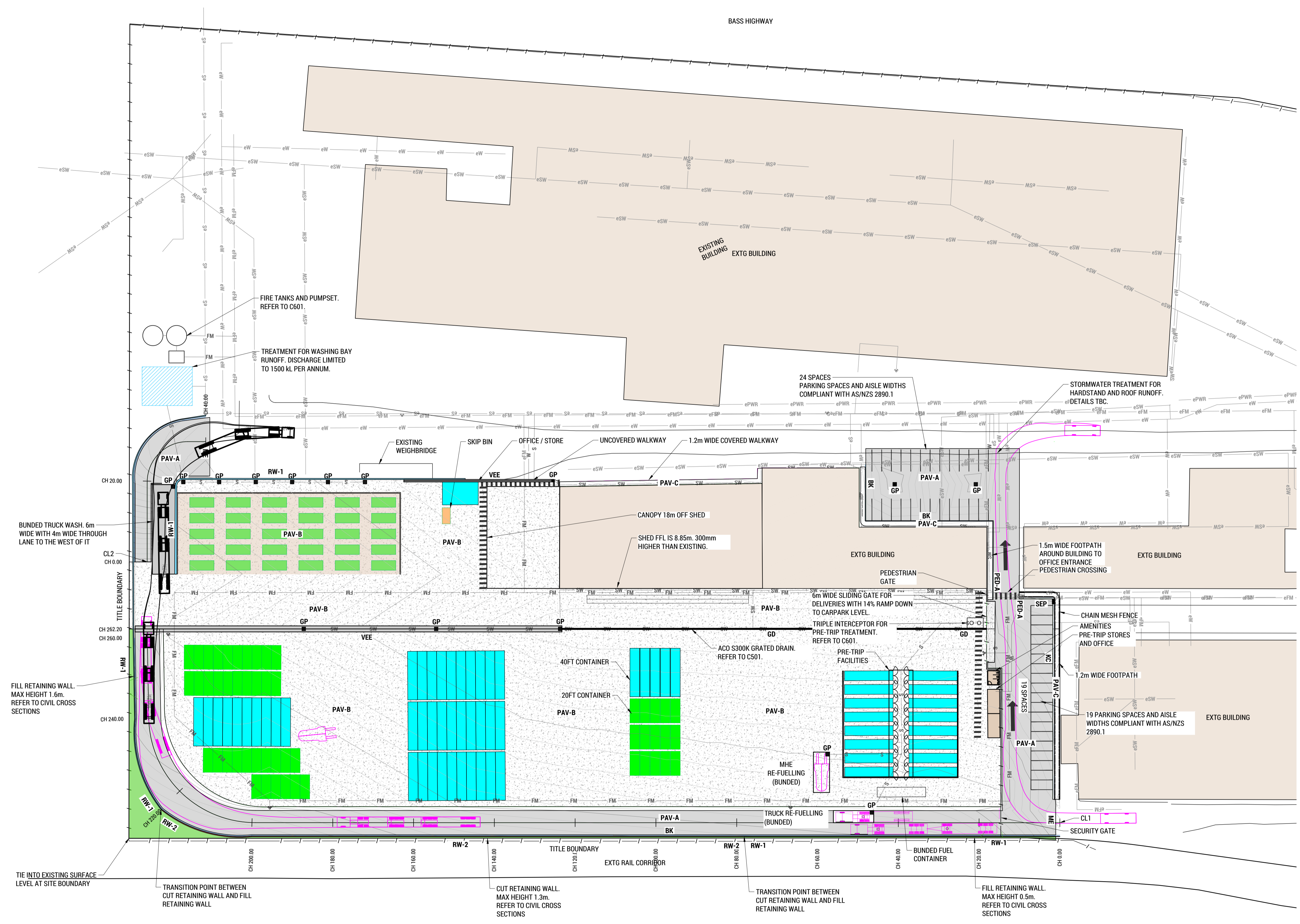
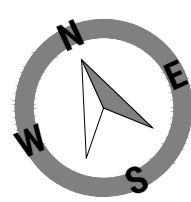
- BULK EARTHWORKS LEGEND**
- TYPE PAV-A - HOTMIX - CIRCULATION ROAD**
 - STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL TO SUBGRADE LEVEL 335mm NOMINAL BELOW FINISHED SURFACE LEVEL
 - PROOF ROLL EXPOSED SUB-GRADE AND CARRY OUT SUB-GRADE IMPROVEMENT WITH AN APPROVED EMBANKMENT MATERIAL IMPORTED OR STRIPPED FROM SITE AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - CUT AND/OR FILL TO 335mm BELOW FINISHED SURFACE LEVELS SHOWN ON THESE DRAWINGS WITH IMPORTED EMBANKMENT MATERIAL APPROVED BY ENGINEER AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - FILL OVER EXPOSED SUBGRADE TO 135mm BELOW FINISHED SURFACE LEVEL WITH 200mm SUB-BASE CLASS 3 MATERIAL PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR SUB-BASE CLASS 3 MATERIAL
 - FILL OVER SUB-BASE TO 35mm BELOW FINISHED SURFACE LEVEL WITH 100mm BASE CLASS 2 MATERIAL PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR BASE CLASS 2 MATERIAL
 - TYPE PAV-B - CONCRETE HARDSTAND**
 - STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL
 - PROOF ROLL EXPOSED SUB-GRADE AND CARRY OUT SUB-GRADE IMPROVEMENT WITH AN APPROVED EMBANKMENT MATERIAL IMPORTED OR STRIPPED FROM SITE AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - CUT AND/OR FILL TO 300mm BELOW FINISHED SURFACE LEVELS SHOWN ON THESE DRAWINGS WITH IMPORTED EMBANKMENT MATERIAL APPROVED BY ENGINEER AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - FILL OVER EXPOSED SUB-GRADE TO 100mm BELOW FINISHED SURFACE LEVEL WITH 200mm BASE CLASS 2 MATERIAL PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR BASE CLASS 2 MATERIAL
 - TYPE PAV-C - CONCRETE FOOTPATHS**
 - STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL
 - PROOF ROLL EXPOSED SUB-GRADE AND CARRY OUT SUB-GRADE IMPROVEMENT WITH AN APPROVED EMBANKMENT MATERIAL IMPORTED OR STRIPPED FROM SITE AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - CUT AND/OR FILL TO 200mm BELOW FINISHED SURFACE LEVELS SHOWN ON THESE DRAWINGS WITH IMPORTED EMBANKMENT MATERIAL APPROVED BY ENGINEER AND PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 204 FOR EMBANKMENT MATERIAL
 - FILL OVER EXPOSED SUB-GRADE TO 100mm BELOW FINISHED SURFACE LEVEL WITH 100mm BASE CLASS 2 MATERIAL PLACED AND TESTED IN ACCORDANCE WITH DSG SPEC SECTION 304 FOR BASE CLASS 2 MATERIAL
 - TYPE SURF - LANDSCAPING**
 - STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL. PLACE 200mm TOPSOIL IN BED
 - CONTRACTOR TO ALLOW TO EXCAVATE TO 200mm NOM. BELOW EXISTING SURFACE LEVEL OR TO DESIGN SURFACE SUB-GRADE LEVEL - WHICH EVER IS LOWER. NOTE NO VARIATION WILL BE PAID FOR UNDER / OVER EXCAVATION, UNLESS PRIOR APPROVAL FROM ENGINEER.

Height Table for VOL-Subgrade (FSL-0.3) to NS

Elevation	Colour
-2.64 to -1.00	Red
-1.00 to -0.50	Orange
-0.50 to -0.40	Yellow-Orange
-0.40 to -0.30	Yellow
-0.30 to -0.20	Light Green
-0.20 to -0.10	Green
-0.10 to 0.00	Light Blue
0.00 to 0.10	Blue
0.10 to 0.20	Light Blue
0.20 to 0.30	Light Green
0.30 to 1.00	Light Blue
>1.00	Light Green


BULK EARTHWORKS PLAN
SCALE 1:500

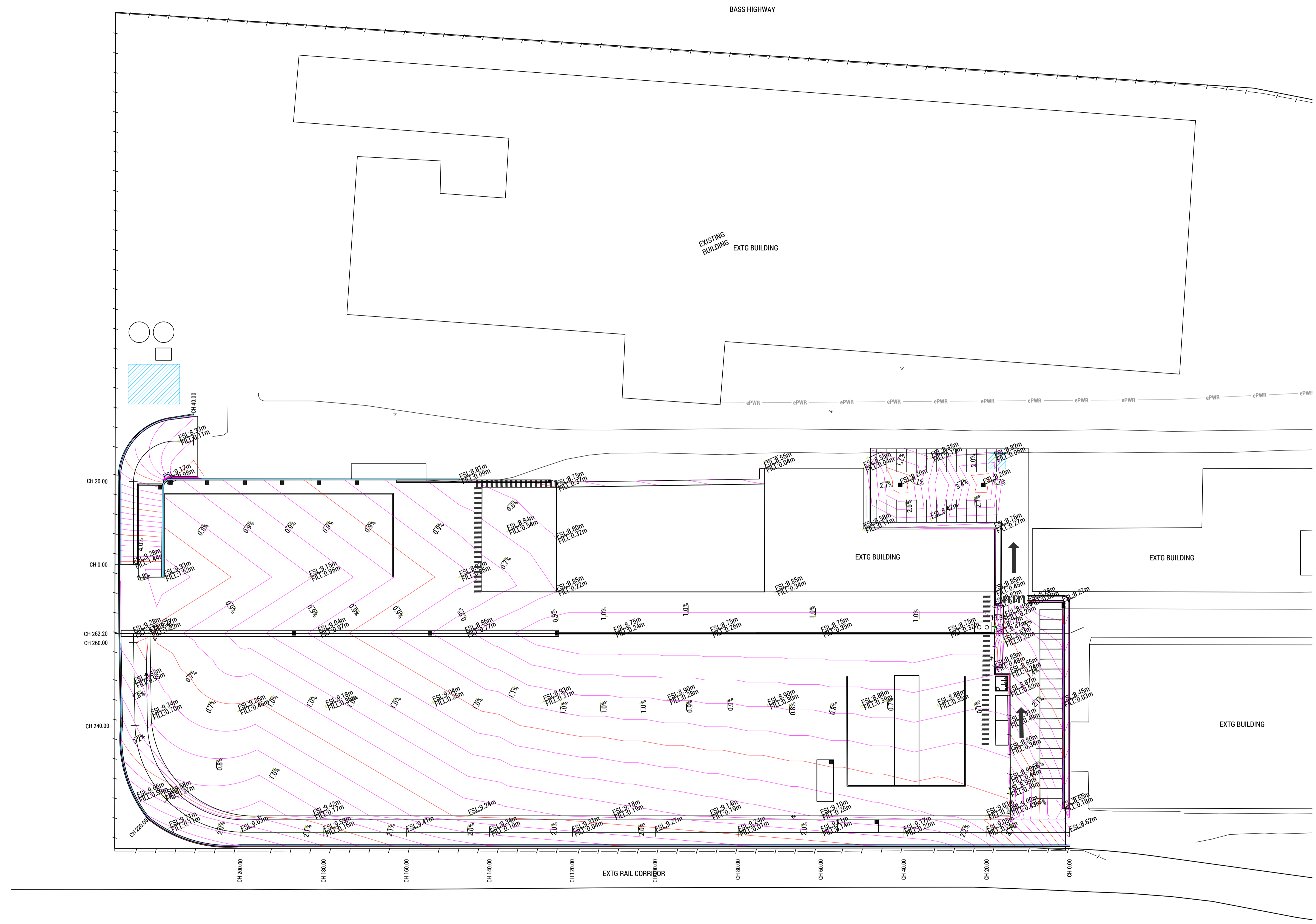
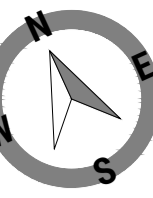
<table border="1"> <tr> <td>REV:</td> <td>ISSUED FOR / DESCRIPTION:</td> <td>BY:</td> <td>DATE:</td> </tr> <tr> <td>A</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>28-05-25</td> </tr> <tr> <td>B</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>06-06-25</td> </tr> <tr> <td>C</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>17-06-25</td> </tr> <tr> <td>D</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>10-07-25</td> </tr> <tr> <td>E</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>25-07-25</td> </tr> </table>	REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:	A	REVIEW / INFORMATION	JQ	28-05-25	B	REVIEW / INFORMATION	JQ	06-06-25	C	REVIEW / INFORMATION	JQ	17-06-25	D	REVIEW / INFORMATION	JQ	10-07-25	E	REVIEW / INFORMATION	JQ	25-07-25	<p>STATUS: PRELIMINARY / INFORMATION</p> <p>DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257</p> <p>APPROVED: R.NEVILLE ACRED. No: CC2080J DATE: 25-07-25</p>	<p>DESIGN BY: JQ DESIGN CHK: BS DRAWN BY: JQ DRAFT CHK: JS</p> <p>rare. 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200</p>	<p>CLIENT: STUBBS & LICHT INVESTMENTS PTY LTD. PROJECT: STRAITLINK - BURNIE TERMINAL ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE</p>	<p>TITLE: BULK EARTHWORKS PLAN SCALE: 1:500 SHEET SIZE: A1 DWGS IN SET: - PROJECT No: 245019 DWG No: C201 REV: E</p>
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LEGEND	
PAV-A	HOTMIX - TRAFFICABLE
PAV-B	CONCRETE - TRAFFICABLE
PAV-C	CONCRETE - PEDESTRIAN
SURF-A	GRASSED / TURFED / LANDSCAPED AREA 200mm MIN GOOD QUALITY TOP SOIL
KC	KERB & CHANNEL
KCV	KERB & CHANNEL VEHICULAR
BK	BARRIER KERB
PED-A	PEDESTRIAN ACCESS RAMP TYPE A - REFER LGAT STD DWG TSD-R18-v3
Bol	BOLLARD
SEP	SIDE ENTRY PIT
GP	GRATED PIT
GD	ACO S300K GRATED DRAIN - CLASS G GRATE
VEE	VEE DRAIN
ME	MATCH EXISTING
SAW	SAW CUT
RW-1	FILL RETAINING WALL
RW-2	CUT RETAINING WALL

CIVIL WORKS PLAN
SCALE 1:500

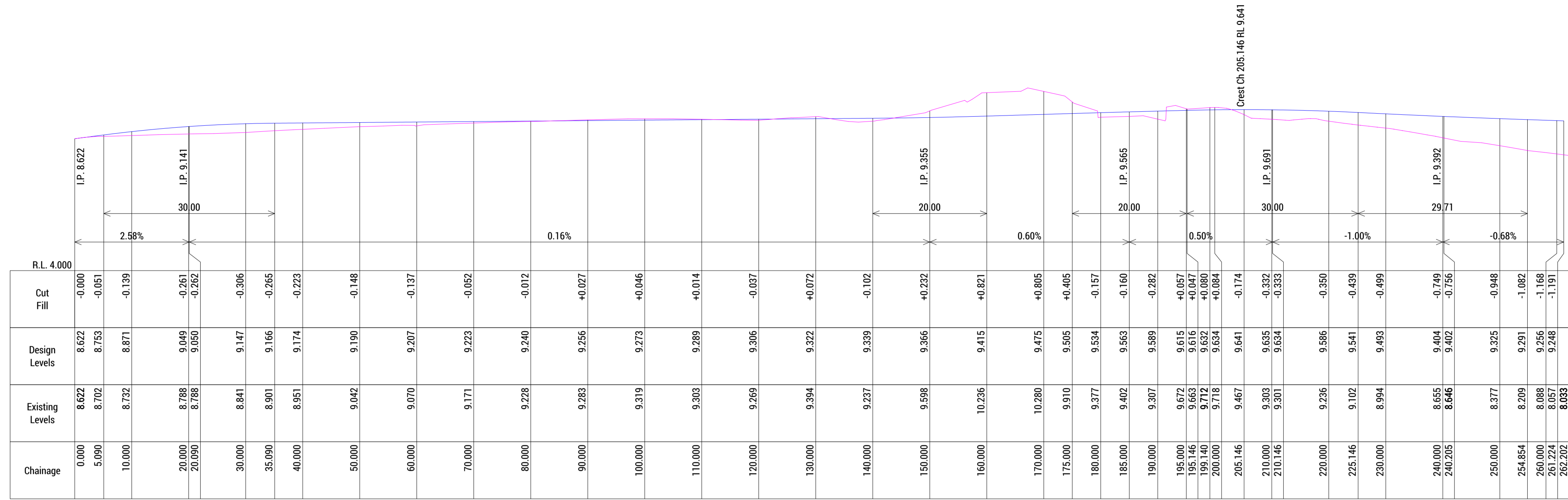
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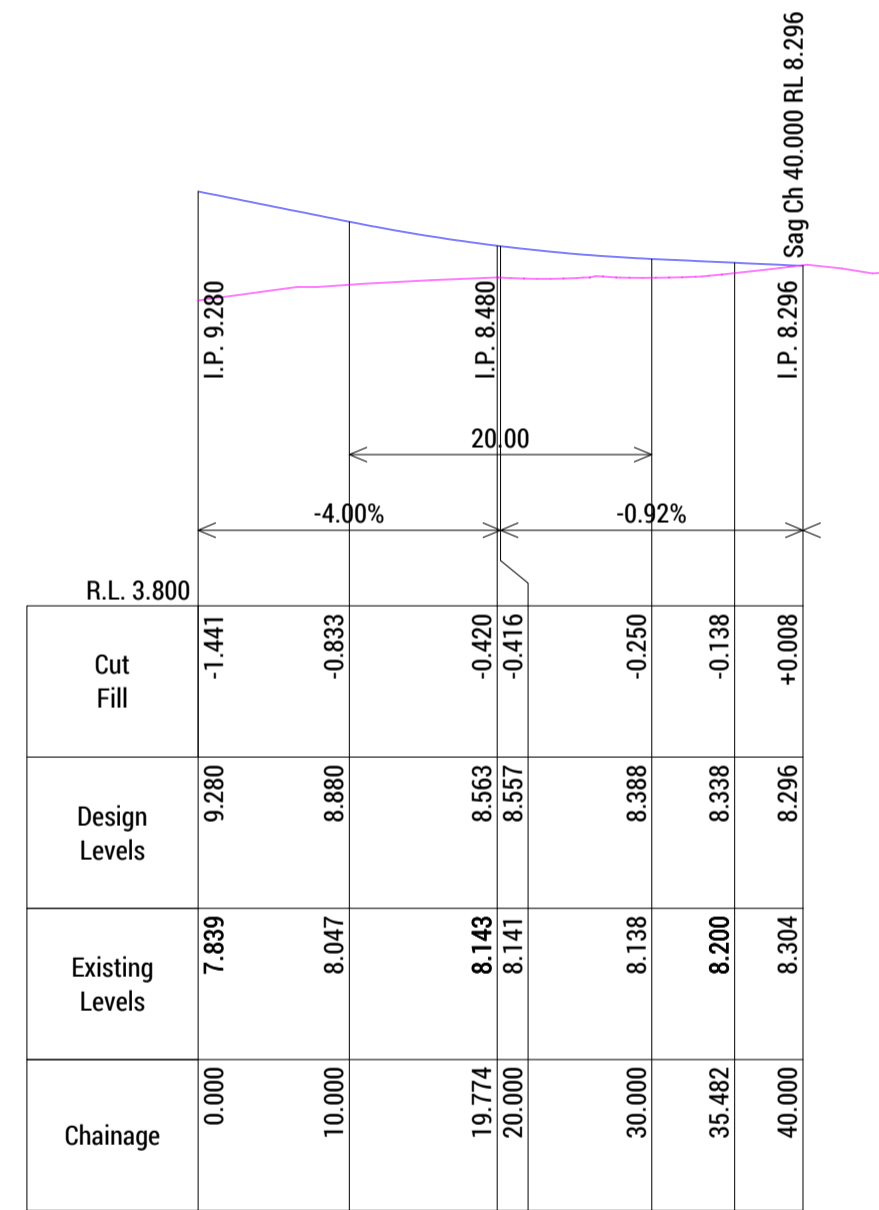
LEGEND	
	HOTMIX - TRAFFICABLE
	CONCRETE - TRAFFICABLE
	CONCRETE - PEDESTRIAN
	GRASSED / TURFED / LANDSCAPED AREA 200mm MIN GOOD QUALITY TOP SOIL
KC	KERB & CHANNEL
KCV	KERB & CHANNEL VEHICULAR
BK	BARRIER KERB
PED-A	PEDESTRIAN ACCESS RAMP TYPE A - REFER LGAT STD DWG TSD-R18-v3
Bol	BOLLARD
SEP	SIDE ENTRY PIT
GP	GRATED PIT
GD	ACO S300K GRATED DRAIN - CLASS G GRATE
VEE	VEE DRAIN
ME	MATCH EXISTING
SAW	SAW CUT
RW-1	FILL RETAINING WALL
RW-2	CUT RETAINING WALL

CIVIL LEVELS PLAN
SCALE 1:500

<table border="1"> <tr> <td>E</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>25-07-25</td> </tr> <tr> <td>D</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>10-07-25</td> </tr> <tr> <td>C</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>17-06-25</td> </tr> <tr> <td>B</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>06-06-25</td> </tr> <tr> <td>A</td> <td>REVIEW / INFORMATION</td> <td>JQ</td> <td>28-05-25</td> </tr> <tr> <td>REV:</td> <td>ISSUED FOR / DESCRIPTION:</td> <td>BY:</td> <td>DATE:</td> </tr> </table>		E	REVIEW / INFORMATION	JQ	25-07-25	D	REVIEW / INFORMATION	JQ	10-07-25	C	REVIEW / INFORMATION	JQ	17-06-25	B	REVIEW / INFORMATION	JQ	06-06-25	A	REVIEW / INFORMATION	JQ	28-05-25	REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:	<table border="1"> <tr> <td colspan="2">STATUS:</td> <td>DESIGN BY:</td> <td>JQ</td> </tr> <tr> <td colspan="2">PRELIMINARY / INFORMATION</td> <td>DESIGN CHK:</td> <td>BS</td> </tr> <tr> <td colspan="4">DO NOT SCALE - IF IN DOUBT, ASK</td> </tr> <tr> <td colspan="4">THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257</td> </tr> <tr> <td colspan="2">APPROVED: R.NEVILLE</td> <td>ACRED. No:</td> <td>CC2080J</td> </tr> <tr> <td colspan="2">DATE:</td> <td>DATE:</td> <td>25-07-25</td> </tr> </table>		STATUS:		DESIGN BY:	JQ	PRELIMINARY / INFORMATION		DESIGN CHK:	BS	DO NOT SCALE - IF IN DOUBT, ASK				THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257				APPROVED: R.NEVILLE		ACRED. No:	CC2080J	DATE:		DATE:	25-07-25	<p>22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200</p>		<table border="1"> <tr> <td>CLIENT:</td> <td>STUBBS & LIGHT INVESTMENTS PTY LTD.</td> <td>TITLE:</td> <td>CIVIL LEVELS PLAN</td> </tr> <tr> <td>PROJECT:</td> <td>STRAITLINK - BURNIE TERMINAL</td> <td>SCALE:</td> <td>1:500</td> </tr> <tr> <td>ADDRESS:</td> <td>MASSY GREENE DRIVE, SOUTH BURNIE</td> <td>SHEET SIZE:</td> <td>A1 DWGS IN SET: -</td> </tr> <tr> <td>PROJECT No:</td> <td>245019</td> <td>DWG No:</td> <td>C402</td> </tr> <tr> <td>REV:</td> <td>E</td> <td></td> <td></td> </tr> </table>		CLIENT:	STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE:	CIVIL LEVELS PLAN	PROJECT:	STRAITLINK - BURNIE TERMINAL	SCALE:	1:500	ADDRESS:	MASSY GREENE DRIVE, SOUTH BURNIE	SHEET SIZE:	A1 DWGS IN SET: -	PROJECT No:	245019	DWG No:	C402	REV:	E		
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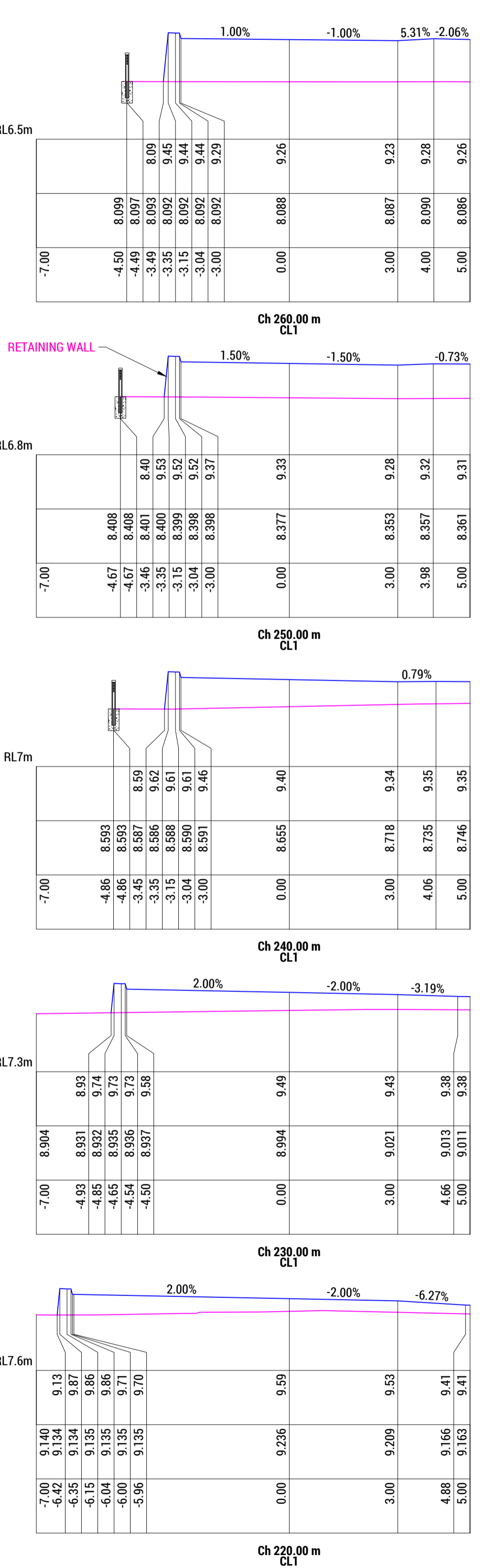
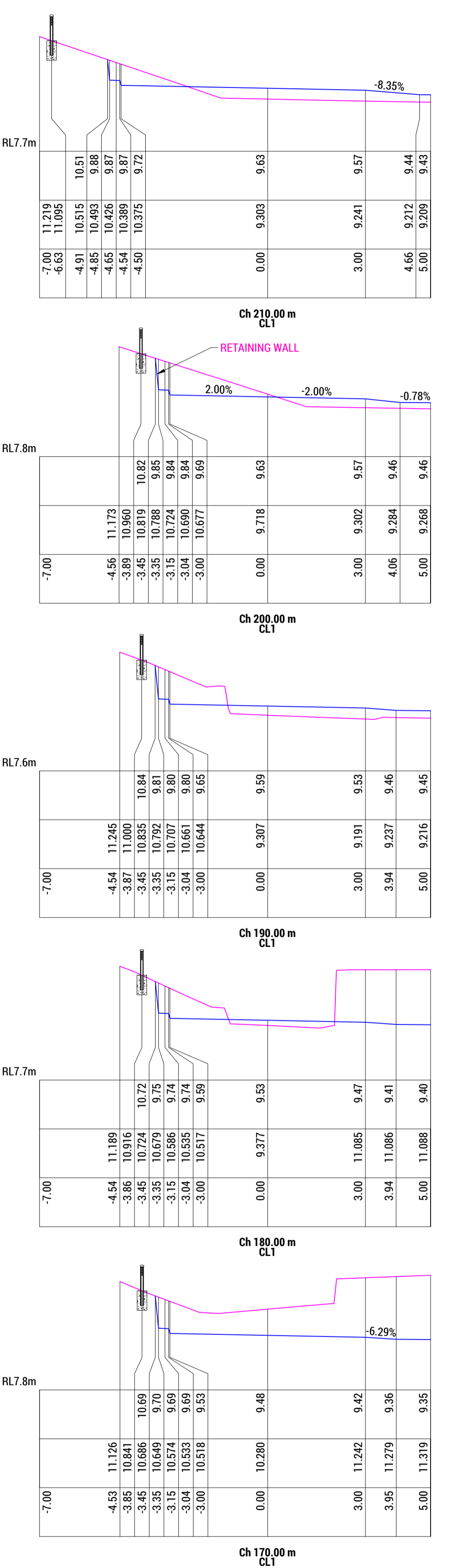
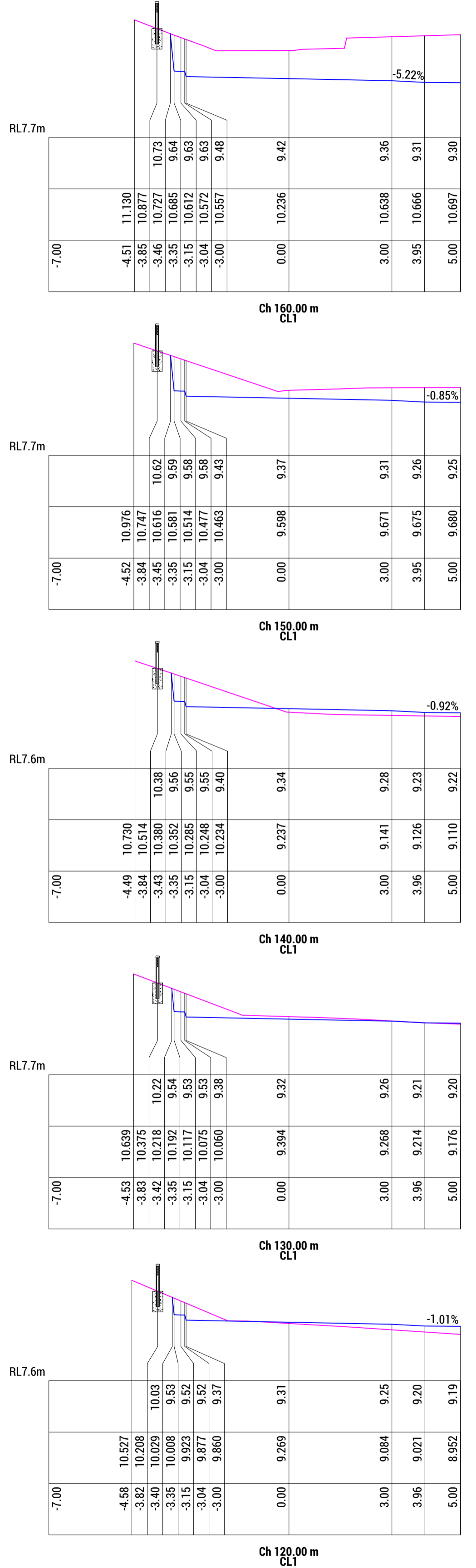
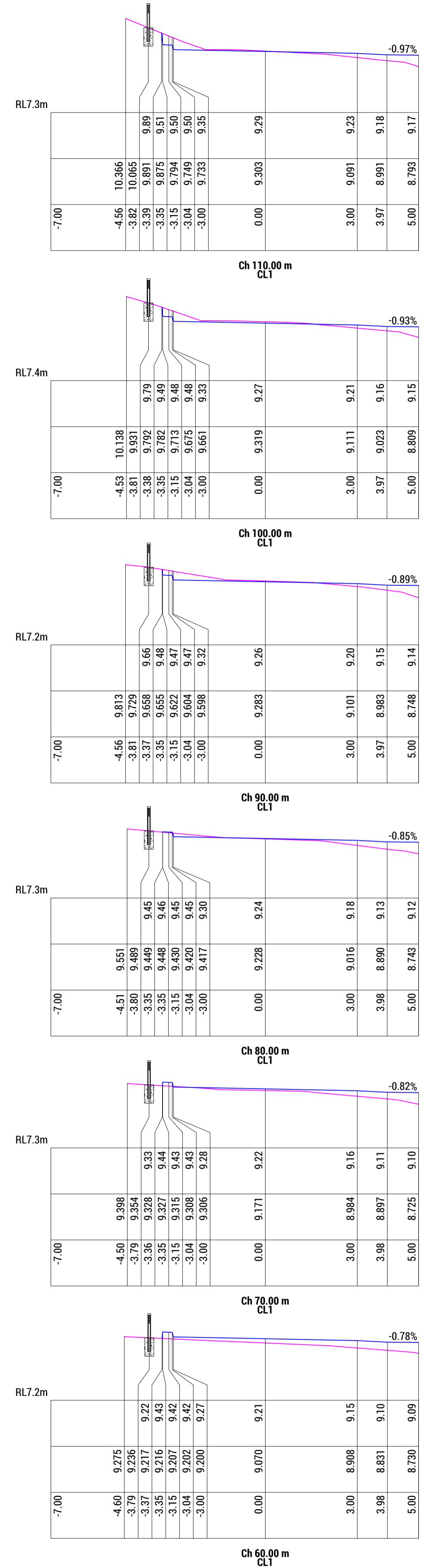
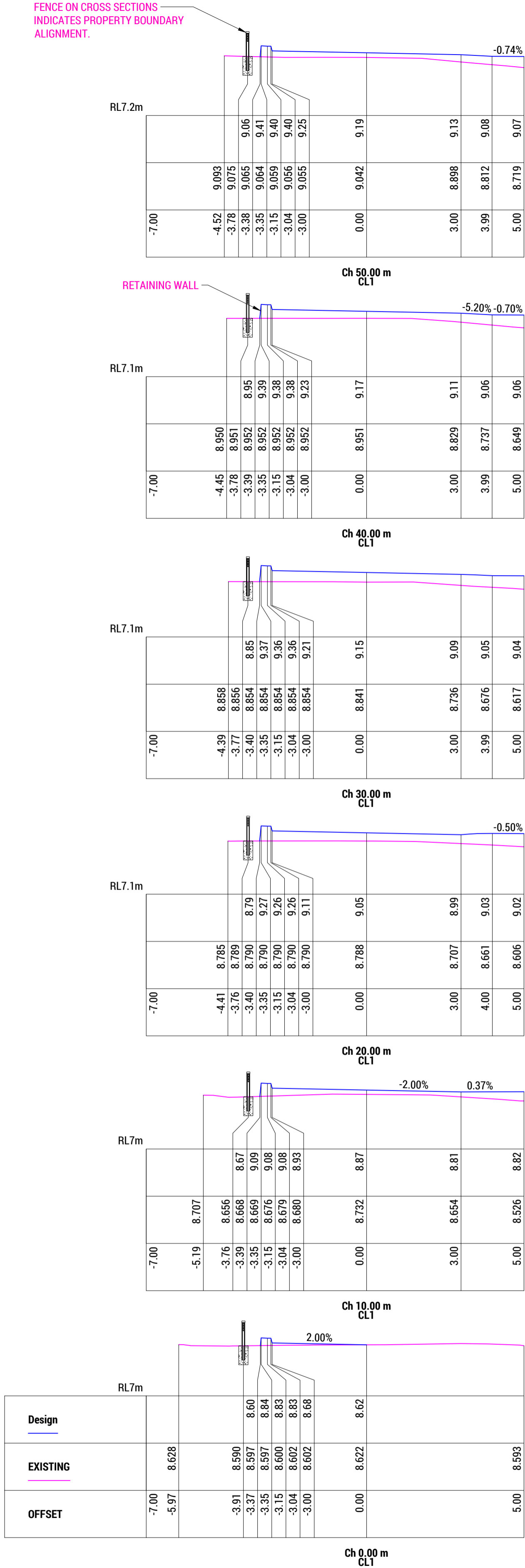


LONGITUDINAL SECTION
Scales: H 1:500 V 1:100



LONGITUDINAL SECTION
Scales: H 1:500 V 1:100

REV: ISSUED FOR / DESCRIPTION:	BY: DATE:	APPROVED: R.NEVILLE	ACRED. No: CC2080J	DATE: 25-07-25	STATUS: PRELIMINARY / INFORMATION	DESIGN BY: JQ	DESIGN CHK: BS	DRAWN BY: JQ	DRAFT CHK: JS	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: CIVIL WORKS LONG SECTIONS
					DO NOT SCALE - IF IN DOUBT, ASK					PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: AS NOTED SHEET SIZE: A1 DWGS IN SET: -
					THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257					ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No: 245019 DWG No: C411 REV: E
								22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200			



Design	EXISTING	OFFSET
7.00	8.628	
-5.19	8.707	
-3.76	8.656	8.67
-3.39	8.668	8.67
-3.35	8.669	8.67
-3.15	8.676	8.67
-3.04	8.679	8.68
-3.00	8.680	8.68
0.00	8.732	8.87
3.00	8.654	8.81
5.00	8.526	8.82

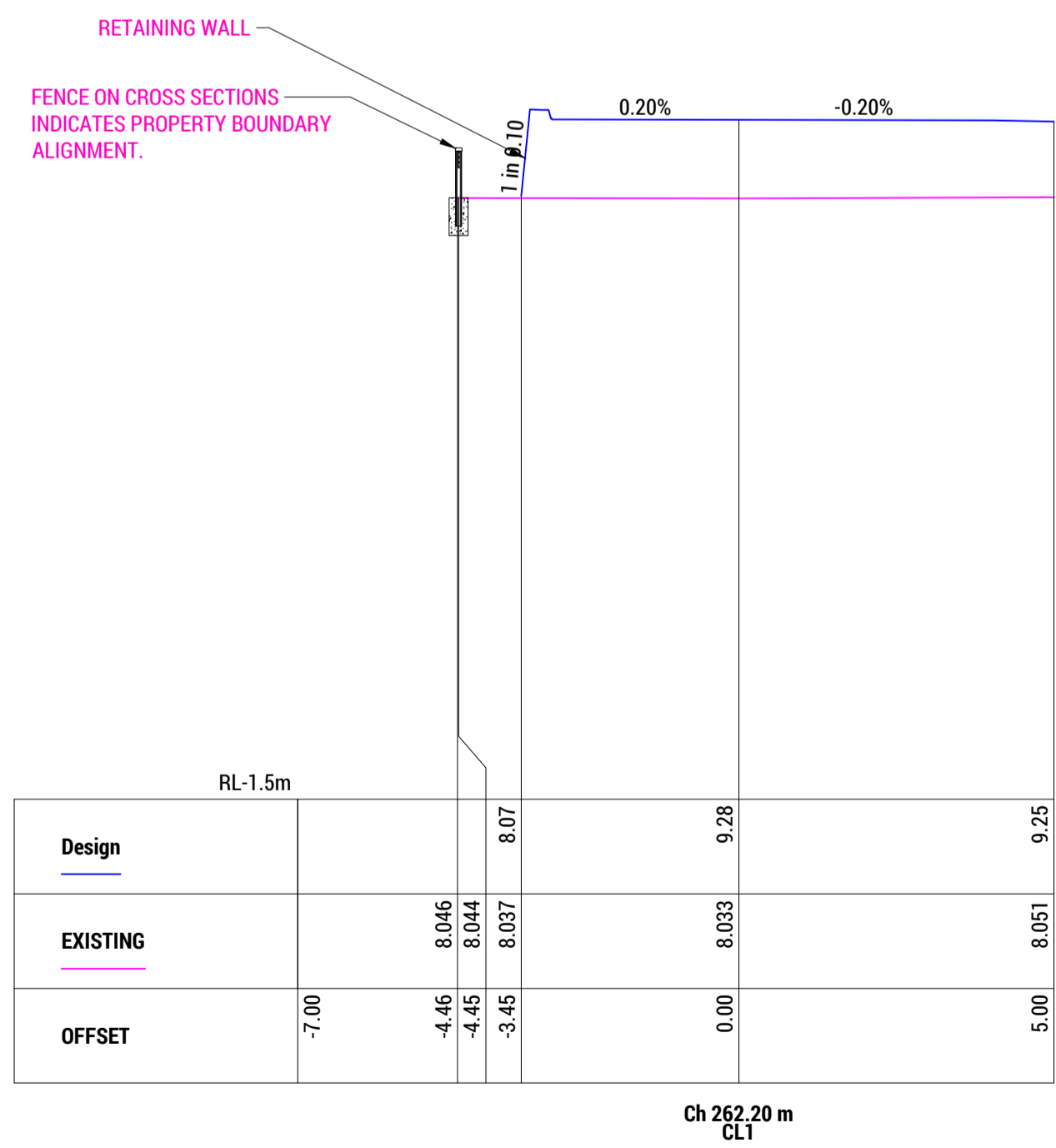
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-4.59	10.627	
-3.79	10.298	9.22
-3.32	10.218	9.33
-3.35	10.029	9.43
-3.15	9.207	9.42
-3.04	9.202	9.42
-3.00	9.200	9.27
0.00	9.070	9.21
3.00	8.908	9.15
3.98	8.831	9.10
5.00	8.730	9.09



7.00	11.126	
-4.53	10.941	10.69
-3.45	10.666	10.69
-3.35	10.649	9.70
-3.15	10.874	9.69
-3.04	10.553	9.69
-3.00	10.518	9.33
0.00	10.280	9.48
3.00	11.242	9.42
3.95	11.279	9.36
5.00	11.319	9.35

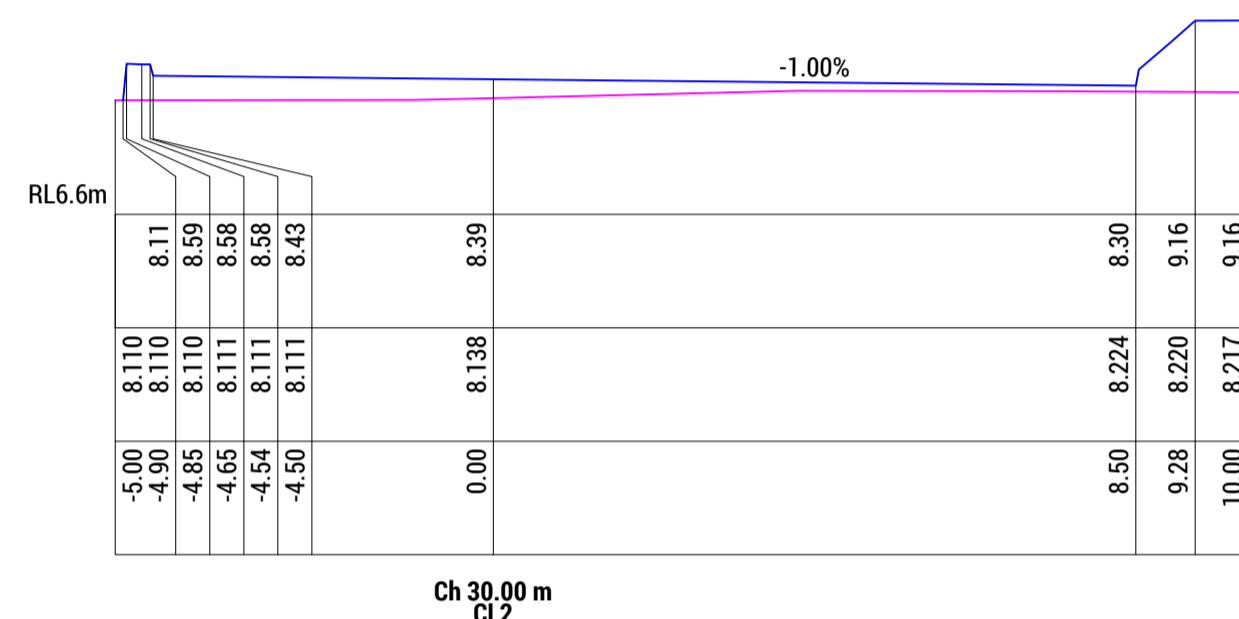
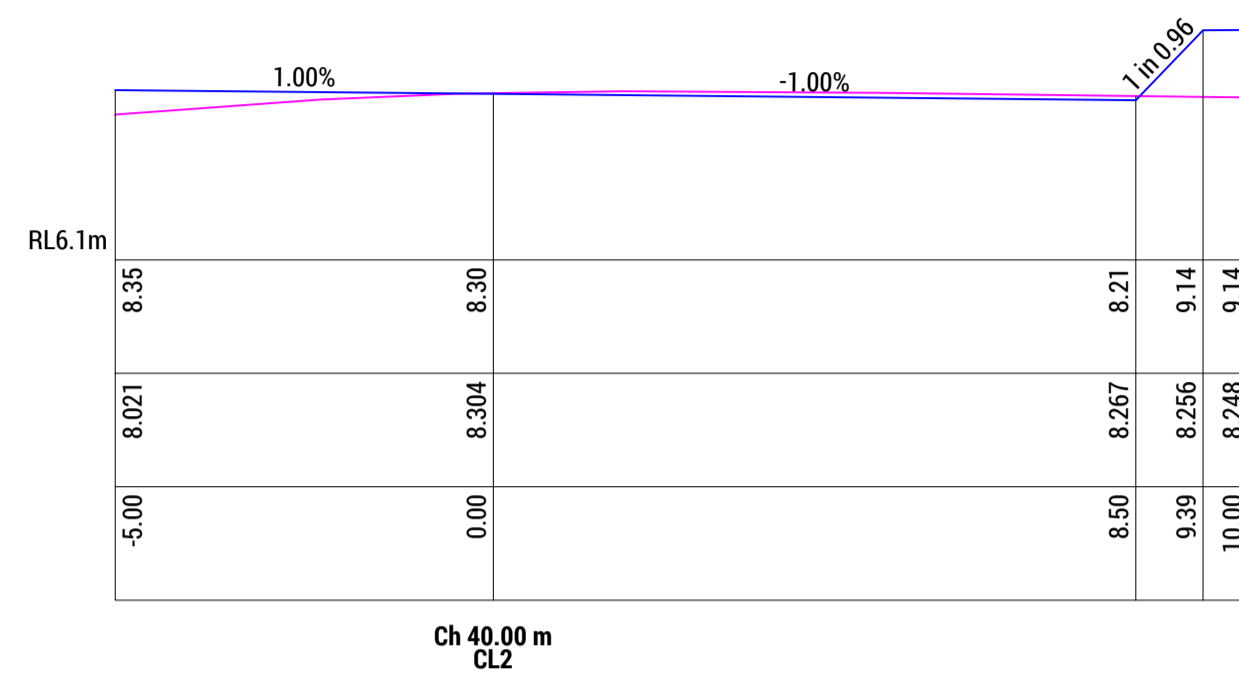
7.00	8.904	
-4.93	8.931	8.93
-6.15	9.135	9.86
-6.04	9.135	9.86
-6.00	9.135	9.71
-5.96	9.135	9.70
0.00	8.994	9.49
3.00	9.021	9.43
4.66	9.013	9.38
5.00	9.011	9.38

7.00	9.140	9.13
-6.42	9.134	9.87
-6.15	9.135	9.86
-6.04	9.135	9.86
-6.00	9.135	9.71
-5.96	9.135	9.70
0.00	9.226	9.59
3.00	9.209	9.53
4.88	9.166	9.41
5.00	9.163	9.41

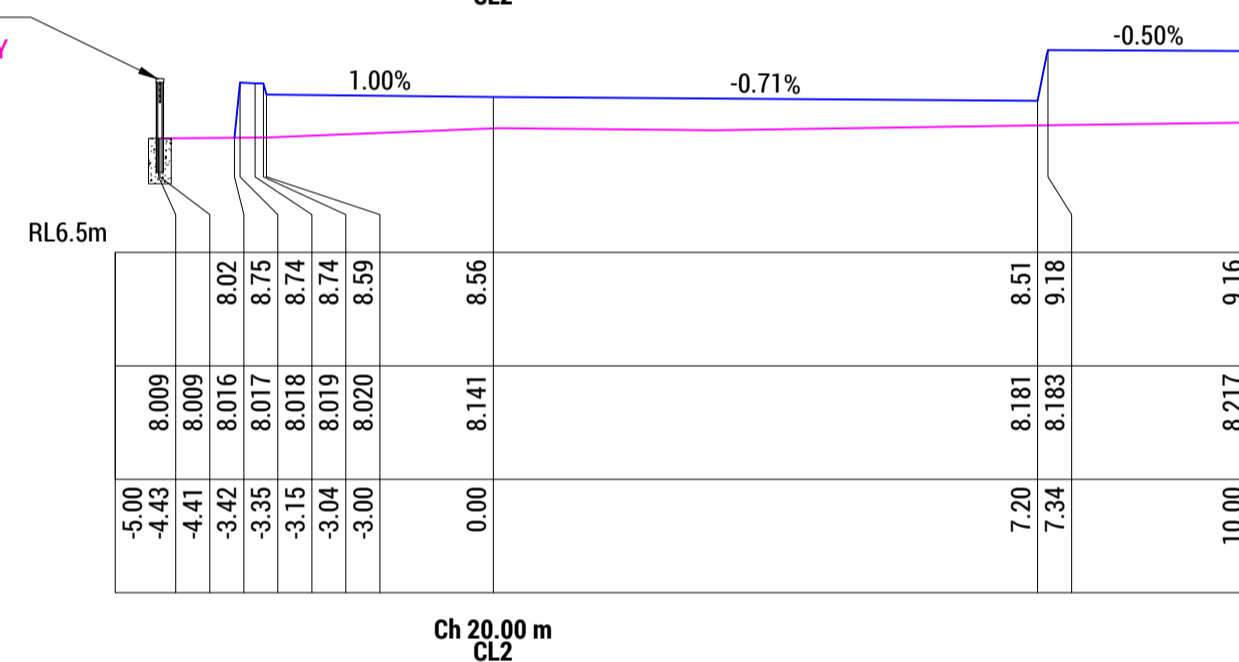
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D	REVIEW / INFORMATION	JQ	10-07-25		DESIGN CHK:	BS		PROJECT:	STRAITLINK - BURNIE TERMINAL	SCALE:	1:100	SHEET SIZE:	A1 DWGS IN SET: -
C	REVIEW / INFORMATION	JQ	17-06-25		DRAWN BY:	JQ		ADDRESS:	MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No:	245019	DWG No:	C421
B	REVIEW / INFORMATION	JQ	06-06-25		DRAFT CHK:	BN		DATE:	25-07-25	REV:	E		
A	REVIEW / INFORMATION	JQ	28-05-25		APPROVED:	R.NEVILLE		ACRED. No:	CC2080J				



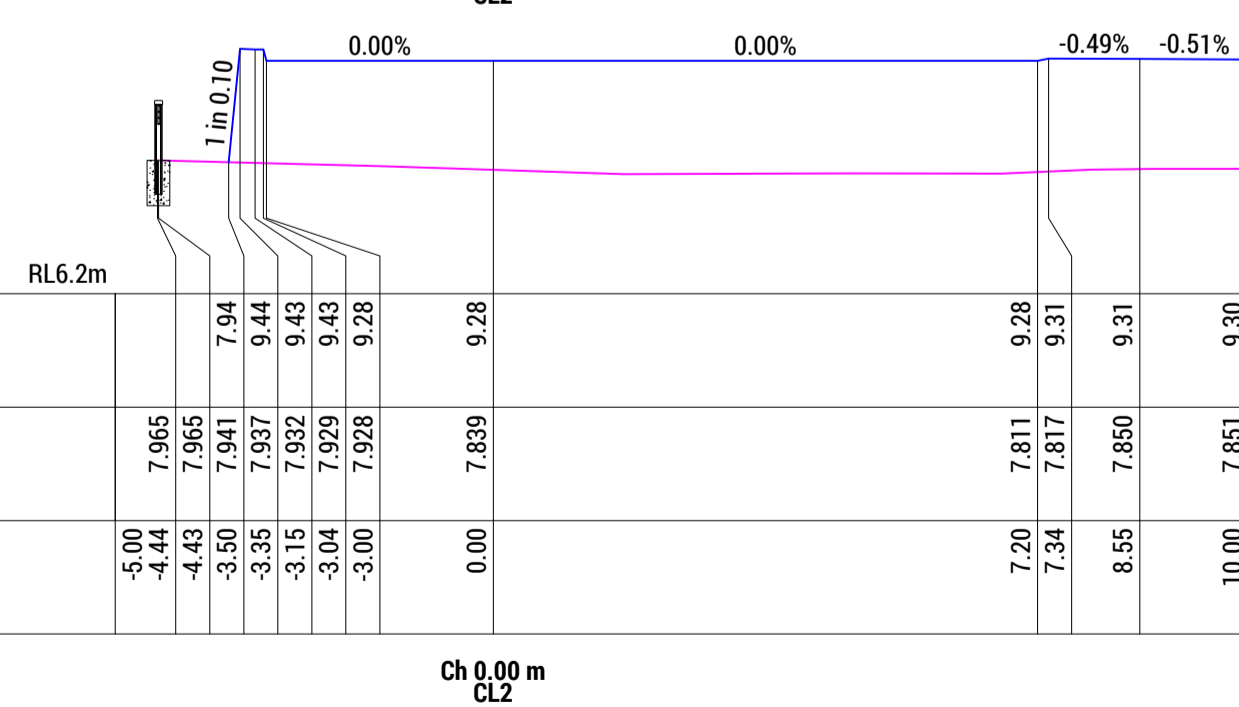
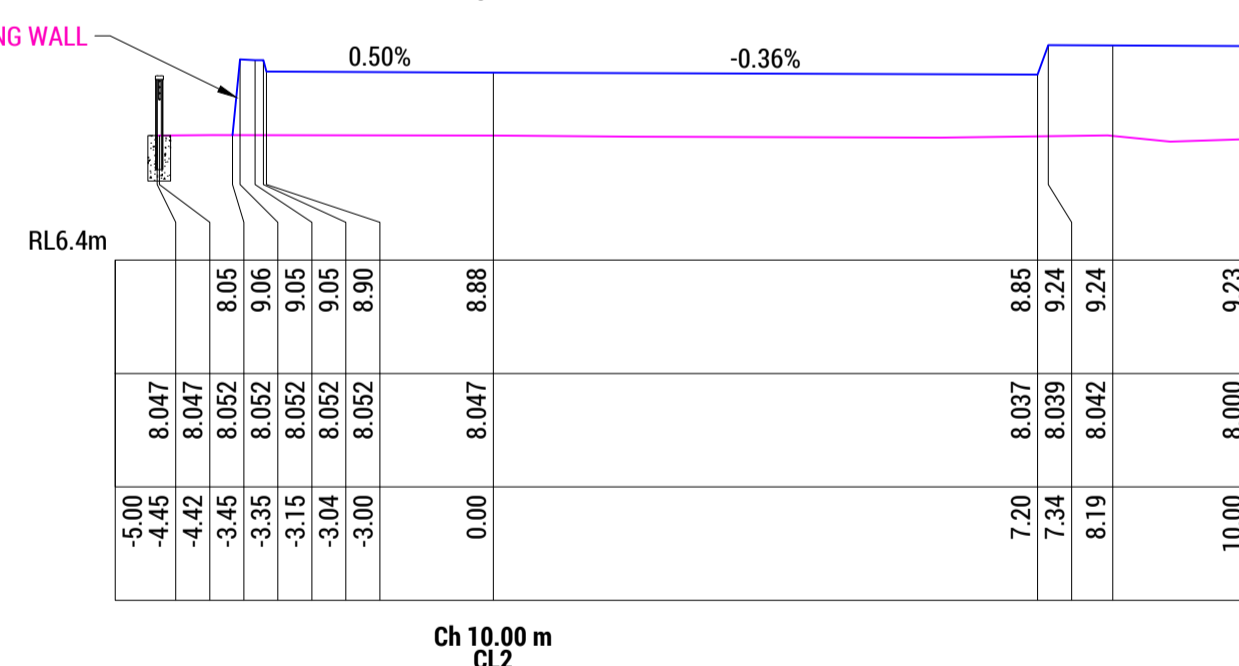
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D		REVIEW / INFORMATION	JQ	10-07-25		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:100 SHEET SIZE: A1 DWGs IN SET: -
C		REVIEW / INFORMATION	JQ	17-06-25	DO NOT SCALE - IF IN DOUBT, ASK	DRAWN BY: JQ		ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No: 245019 DWG No: C422 REV: E
B		REVIEW / INFORMATION	JQ	06-06-25	THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257	DRAFT CHK: JS			
A		REVIEW / INFORMATION	JQ	28-05-25	APPROVED: R.NEVILLE	ACRED. No: CC2080J	DATE: 25-07-25	22-24 Paterson Street Launceston TAS 7250	
REV:		ISSUED FOR / DESCRIPTION:	BY:	DATE:					




FENCE ON CROSS SECTIONS INDICATES PROPERTY BOUNDARY ALIGNMENT.

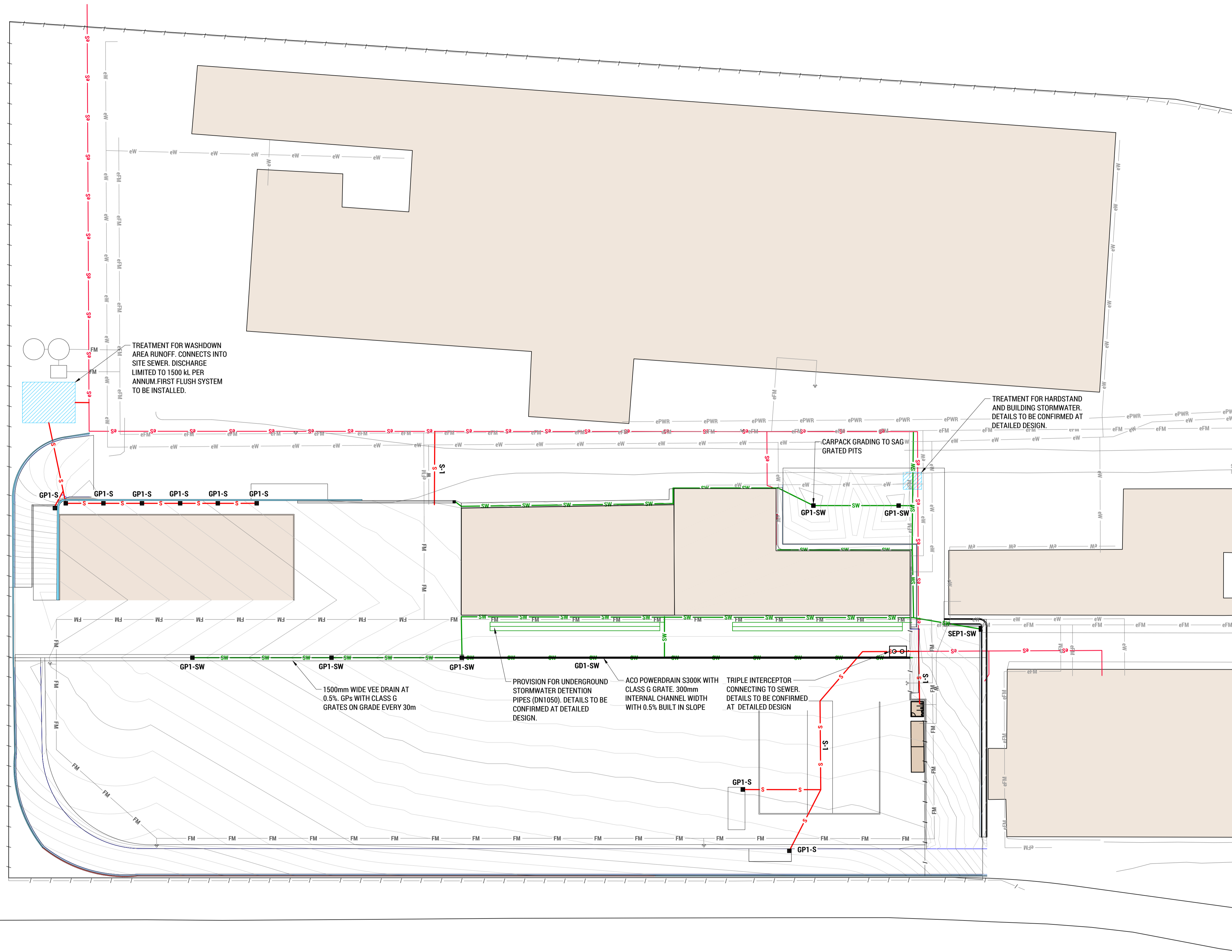
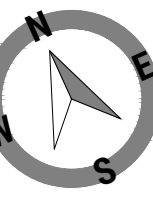


RETAINING WALL



Design	Blue line
EXISTING	Pink line
OFFSET	Values from diagram

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D REVIEW / INFORMATION		JQ	10-07-25	DO NOT SCALE - IF IN DOUBT, ASK		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:100 SHEET SIZE: A1 DWGS IN SET: -
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B REVIEW / INFORMATION		JQ	06-06-25			DRAFT CHK: JS			
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LEGEND

eSW	EXISTING STORM WATER MAIN
SW	PROPOSED STORM WATER MAIN
eS	EXISTING SEWER MAIN
S	PROPOSED SEWER MAIN
AG	PROPOSED AG DRAIN
---	PROPOSED OPEN / SWALE / VEE DRAIN
MH-S	SEWER MANHOLE
MH-SW	STORMWATER MANHOLE
SEP-SW	SIDE ENTRY PIT
GPx-SW	GRADED PIT
GDx-SW	GRADED DRAIN

STORMWATER PIPE SCHEDULE

MARK	PIPE SIZE	TYPE	CLASS	GRADE
SW-1	Ø300	BLACKMAX	SN8	MIN. 0.5% U.N.O.
SW-2	Ø375	BLACKMAX	SN8	MIN. 0.5% U.N.O.
SW-3	Ø450	BLACKMAX	SN8	MIN. 0.5% U.N.O.
SW-4	Ø300	RCP	CLASS 4	MIN. 0.5% U.N.O.

STORMWATER PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
GP1-SW	900x900	PRECAST. CONC.	CLASS G TRAFFICABLE GRATED LID
SEP1-SW	1200	TYPE 1	REFER LGAT STD DWG
GD1-SW	300 WIDE	ACO S300K	CLASS G TRAFFICABLE GRATED LID

SEWER PIPE SCHEDULE

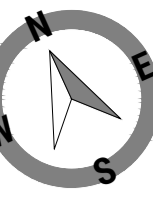
MARK	PIPE SIZE	TYPE	CLASS	GRADE
S-1	Ø150	uPVC	SN8	MIN. 1.67% U.N.O.

SEWER PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
MH1-S	Ø1050	PRECAST. CONC.	CLASS B 'S' MARKED GATIC LID
GP1-S	900x900	PRECAST CONC.	CLASS G TRAFFICABLE GRATED LID

DRAINAGE PLAN
SCALE 1:500

E REVIEW / INFORMATION D REVIEW / INFORMATION C REVIEW / INFORMATION B REVIEW / INFORMATION A REVIEW / INFORMATION REV: ISSUED FOR / DESCRIPTION:	JQ	25-07-25	STATUS: PRELIMINARY / INFORMATION DO NOT SCALE - IF IN DOUBT, ASK THIS DOCUMENT MAY ONLY BE USED FOR THE PURPOSE FOR WHICH IT WAS PREPARED. © RARE INNOVATION PTY LTD. ABN 51 619 598 257	DESIGN BY: JQ	 22-24 Paterson Street Launceston TAS 7250 rarein.com.au P.03 6388 9200	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: DRAINAGE PLAN
	JQ	10-07-25		DESIGN CHK: BS		PROJECT: STRAITLINK - BURNIE TERMINAL	SCALE: 1:500 SHEET SIZE: A1 DWGS IN SET: -
	JQ	17-06-25		DRAWN BY: JQ		ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE	PROJECT No: 245019 DWG No: C501 REV: E
	JQ	06-06-25		DRAFT CHK: JS			
	JQ	28-05-25					
	BY: R.NEVILLE	DATE: 25-07-25	APPROVED: R.NEVILLE	ACRED. No: CC2080J			



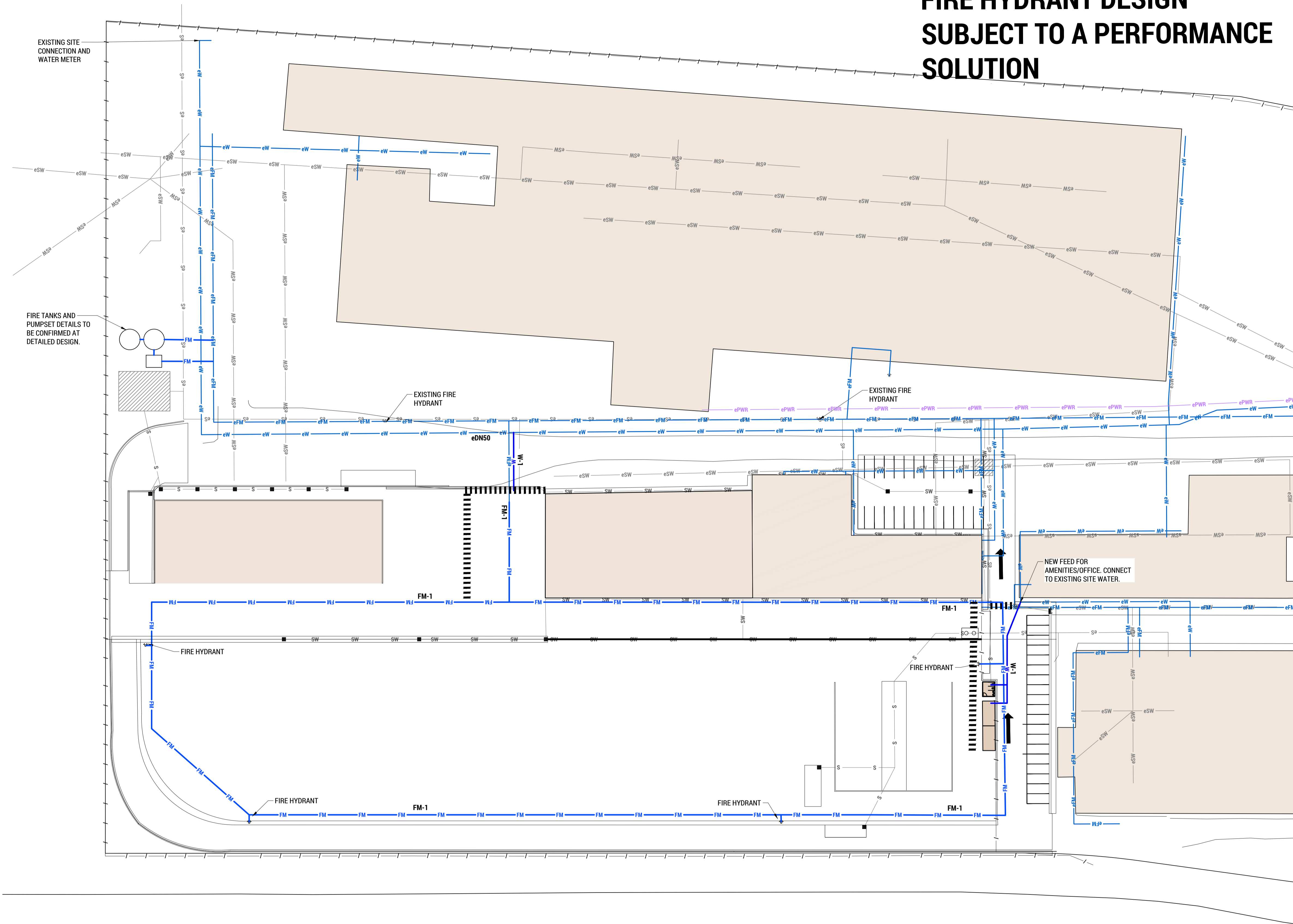
FIRE HYDRANT DESIGN SUBJECT TO A PERFORMANCE SOLUTION

LEGEND

	eW	EXISTING WATER MAIN
	W	PROPOSED WATER MAIN
	FP	EXISTING FIRE PLUG
	SV	EXISTING STOP VALVE
	M	EXISTING WATER METER

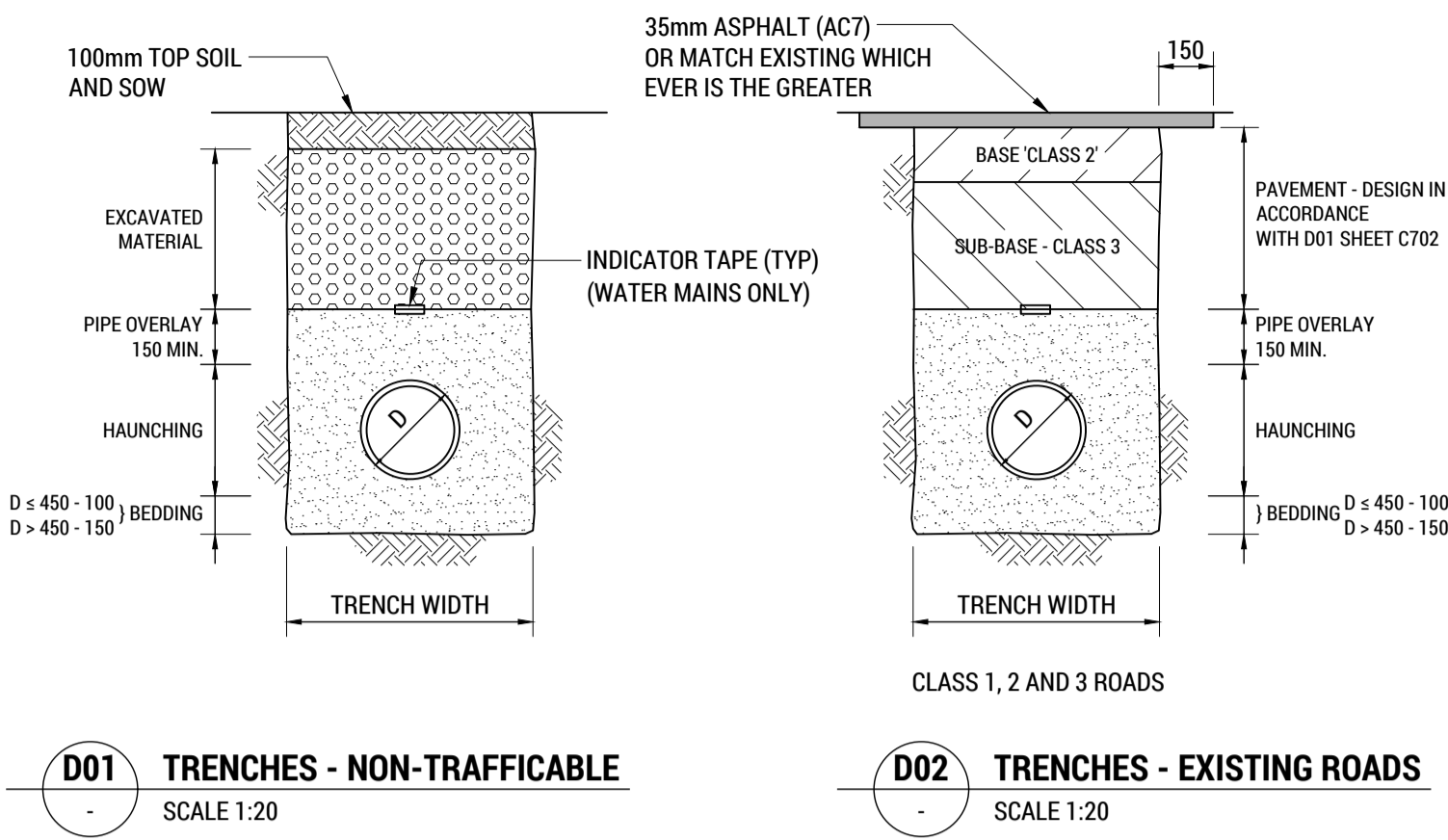
WATER MAIN SCHEDULE

MARK	PIPE SIZE	TYPE
W-1	DN40 (ID32)	HDPE PE100 SDR11 PN16
FM-1	DN100	PVC-O SERIES 2 PN16 RRJ

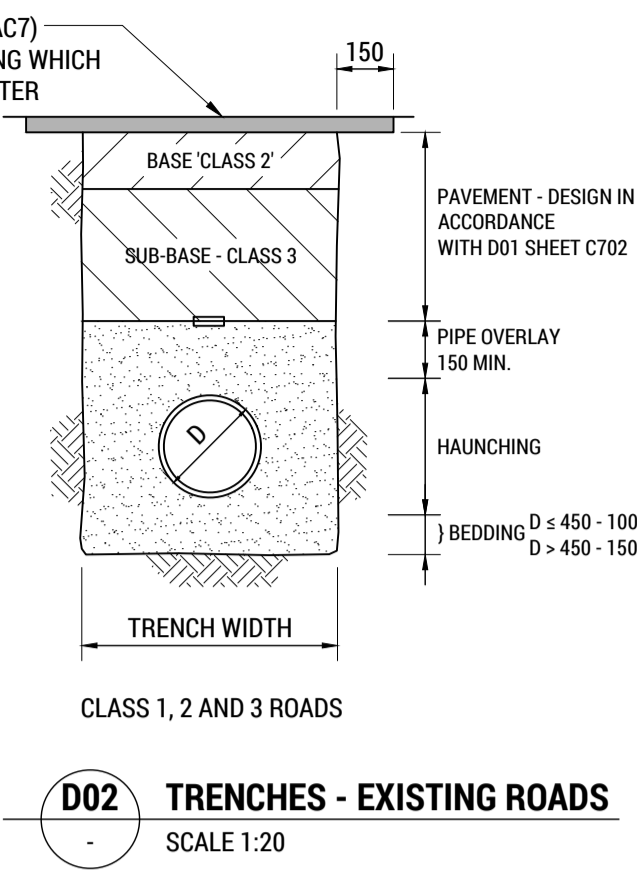


WATER RETICULATION PLAN
SCALE 1:500

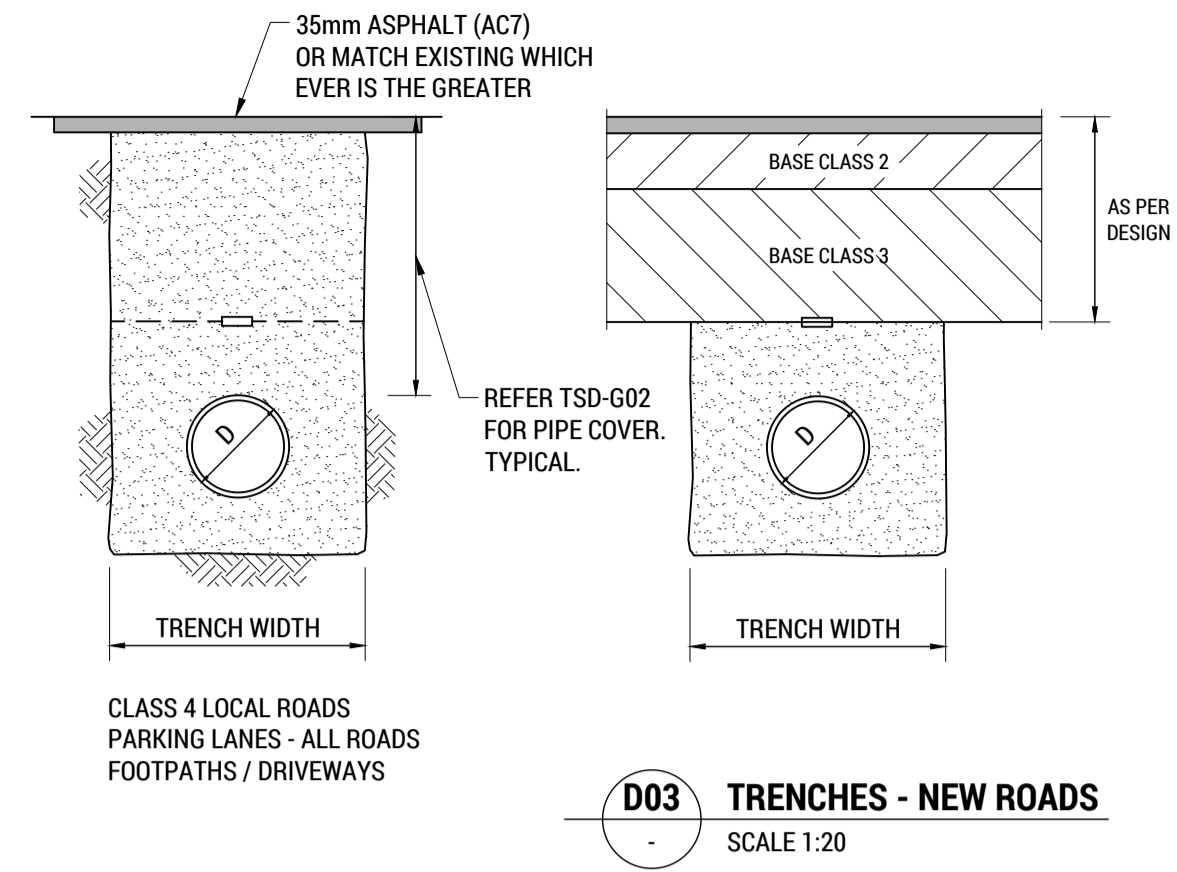
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	E	REVIEW / INFORMATION	JQ	25-07-25																																
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DRAWN BY:	JQ																																			
DRAFT CHK:	JS																																			



D01 TRENCHES - NON-TRAFFICABLE
SCALE 1:20



D02 TRENCHES - EXISTING ROADS
SCALE 1:20



D03 TRENCHES - NEW ROADS
SCALE 1:20

TRENCH WIDTH		
PIPE TYPE	NOM. DIA (D)	TRENCH WIDTH
CONCRETE	≤ 1500	D + 300
	> 1500	DESIGN REQ.
OTHER PIPES	100	300
	150	450
	225-300	600
	450	750
	450-1500	D + 600
	> 1500	DESIGN REQ.

MINIMUM TRENCH WIDTHS MAY BE VARIED ABOVE THE PIPE OVERLAY ZONE TO MEET WORKPLACE STANDARDS REQUIREMENTS ie EXCAVATIONS OVER 1.5m MAY REQUIRE RISK ASSESSMENT.

COMPACTION OF BEDDING, HAUNCHING & OVERLAY REFER TO AS 1289.5.5

CONCRETE PIPES = MIN. DENSITY INDEX = 60% (8% STD. COMPACTION)

uPVC PIPES = DENSITY INDEX = 65% (90% STD. COMPACTION)

DICL PIPES = DENSITY INDEX = 65% (90% STD. COMPACTION)

BEDDING, HAUNCHING AND OVERLAY MATERIAL SHALL CONTAIN NO DELETERIOUS MATERIAL OR CLAY LUMPS AND SHALL COMPLY WITH THE FOLLOWING GRADINGS:

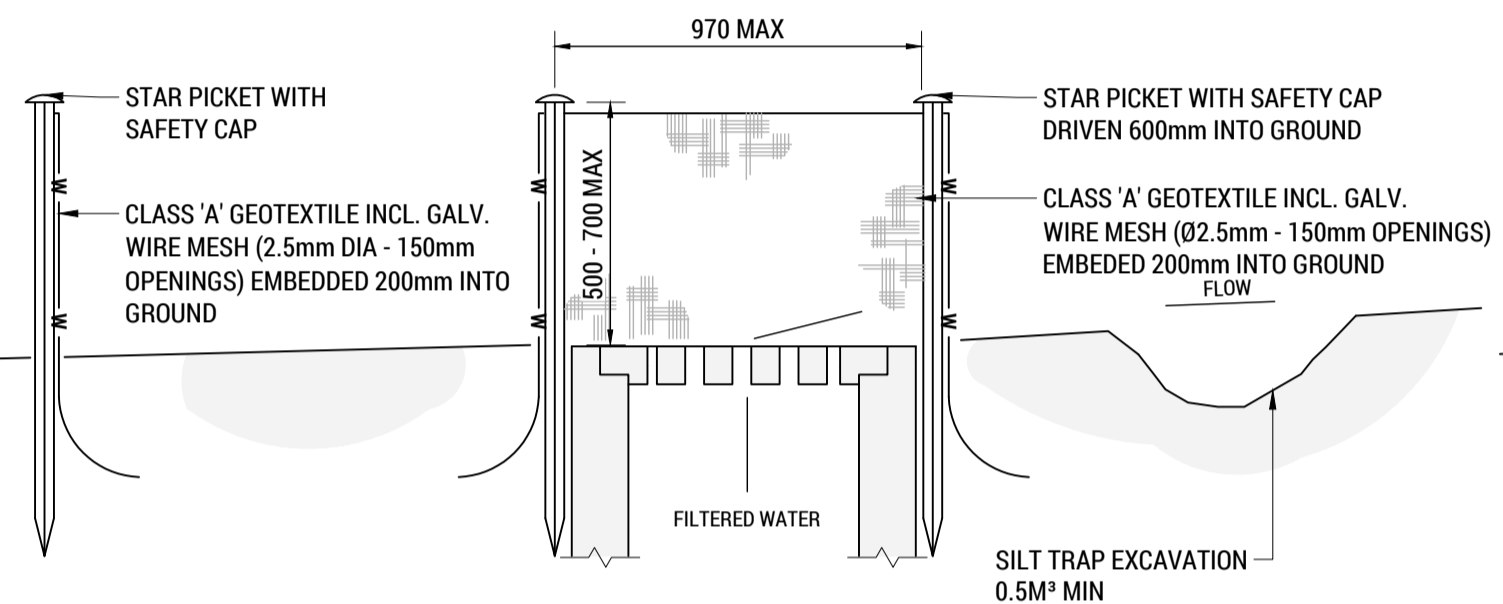
FOR uPVC AND DUCTILE IRON PIPES SAND OR CRUSHED ROCK (STONE DUST)

SIEVE APERTURE (mm)	% PASSING (BY MASS)
TO AS 1152	
6.7	100
2.36	70-100
0.6	20-90
0.3	8-50
0.15	0-20
0.075	0-10

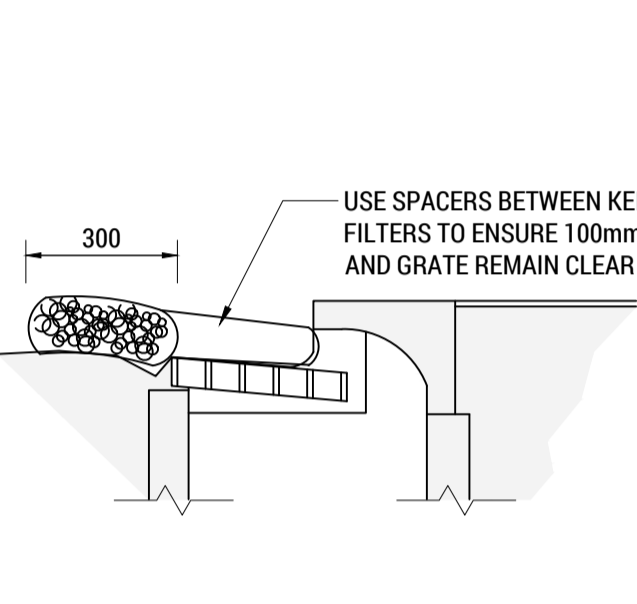
FOR CONCRETE PIPES CRUSHED ROCK

SIEVE APERTURE (mm)	% PASSING (BY MASS)
TO AS 1152	
19	100
2.36	50-100
0.6	20-90
0.3	10-50
0.15	0-25
0.075	0-10

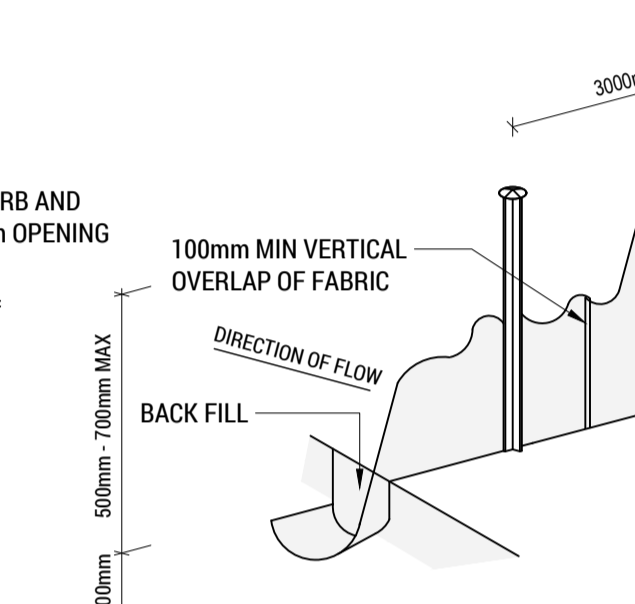
ALL MATERIAL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH AS 3725 AND TO THE SATISFACTION OF THE SUPERINTENDENT.



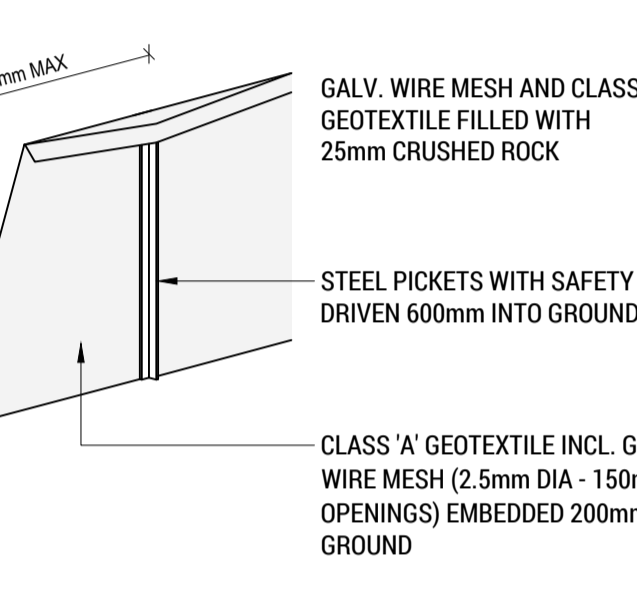
D04 SILT FENCE AT GRATED PIT
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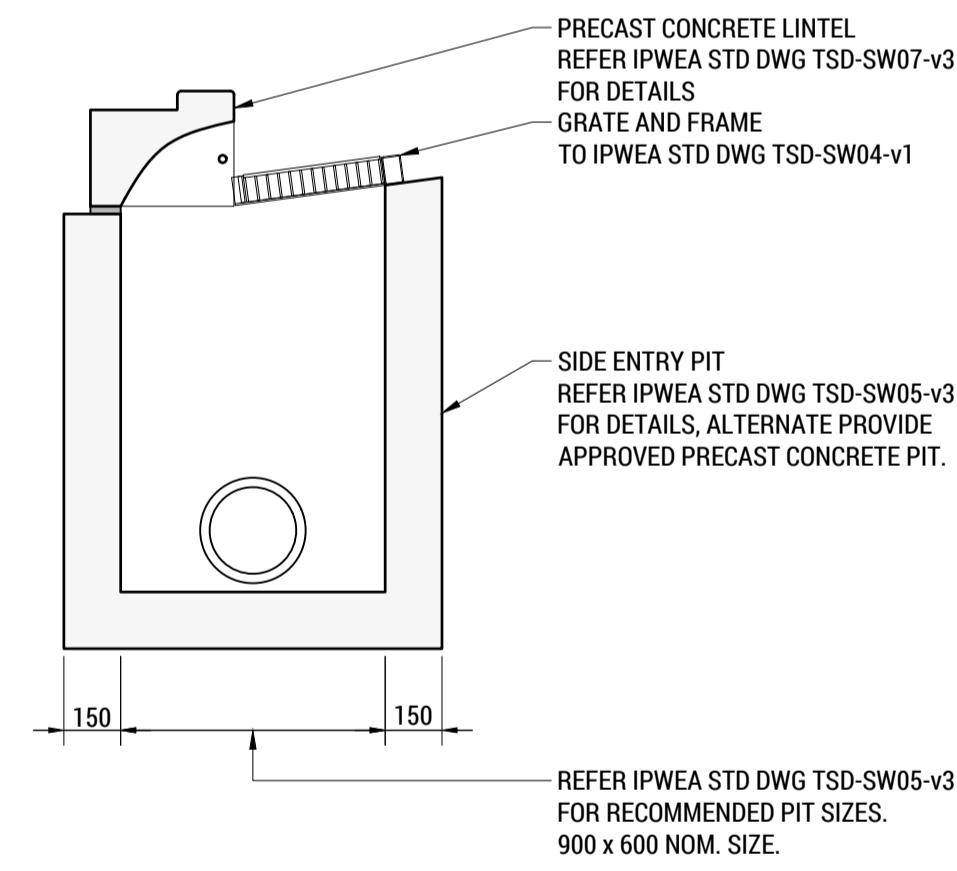
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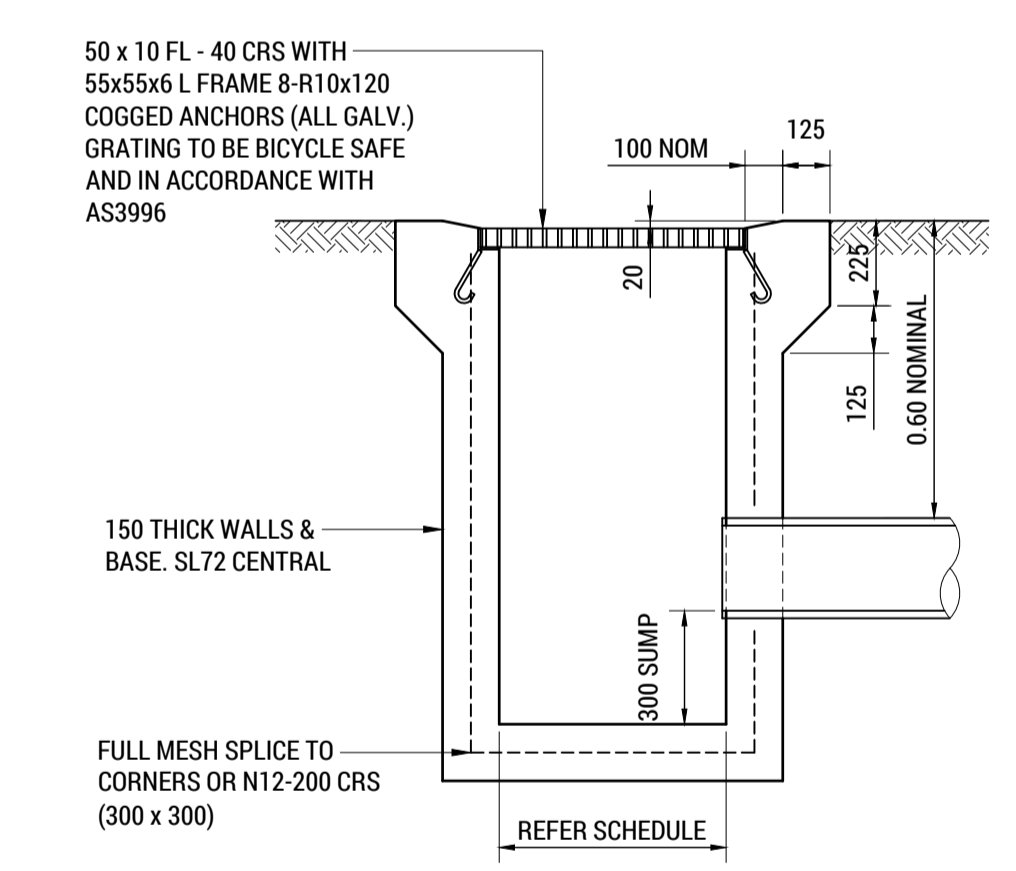
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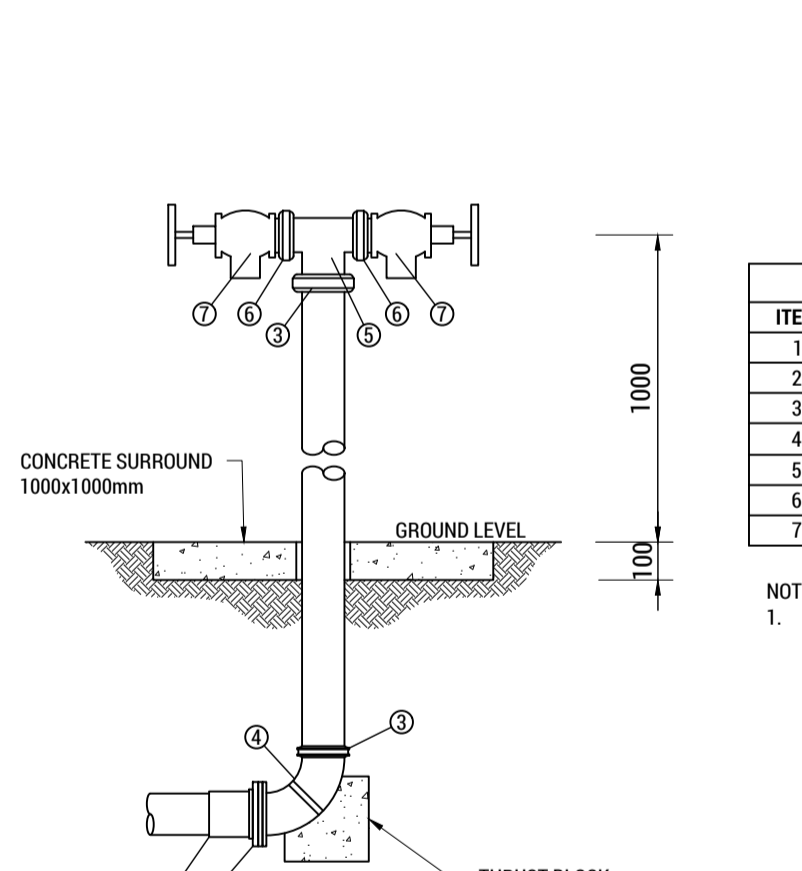
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D08 SECTION DETAIL - SIDE ENTRY PIT 'TYPE 1'
SCALE 1:20
REFER IPWEA STANDARD DRAWINGS FOR ADDITIONAL SIDE ENTRY PIT DETAILS



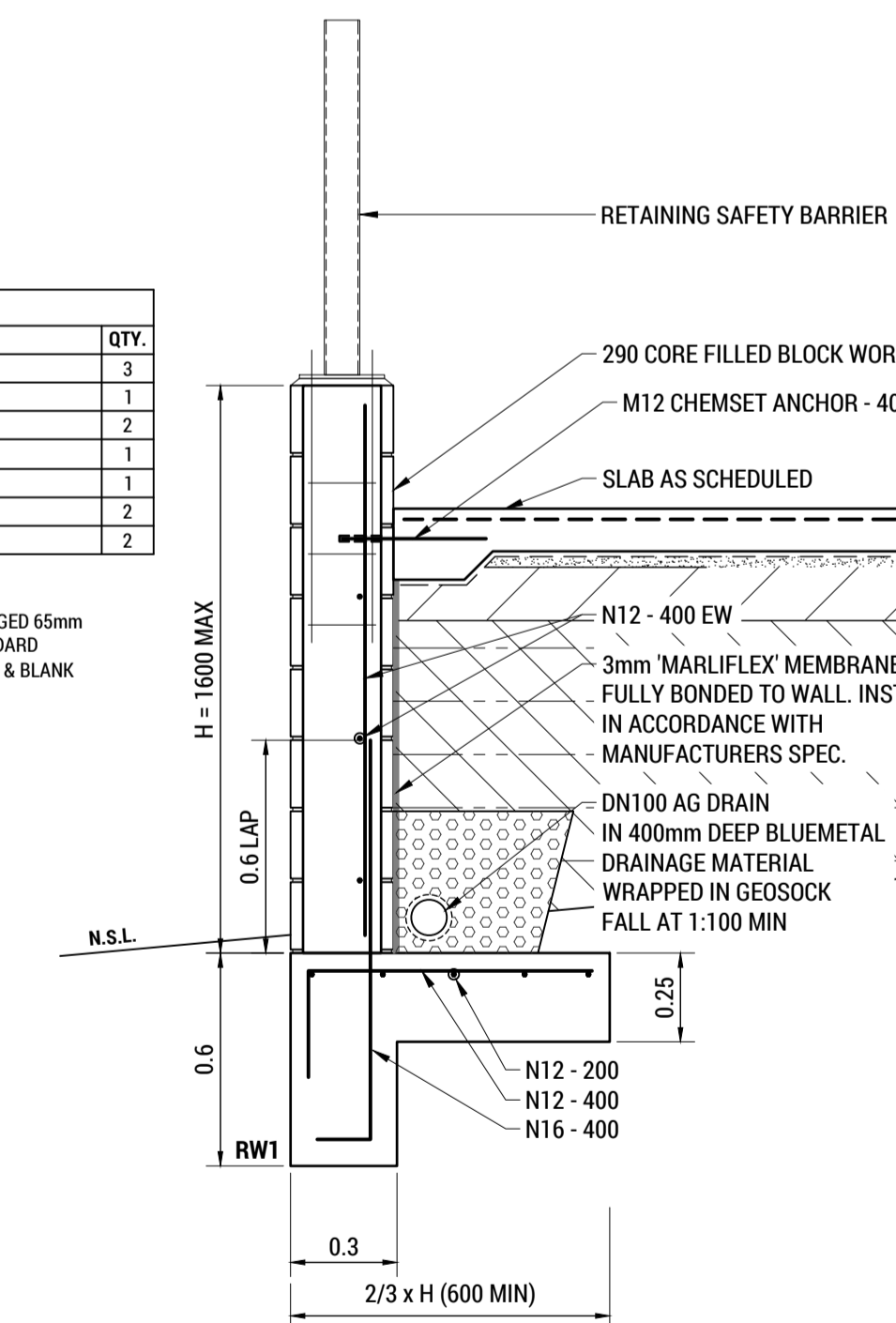
D09 GRATED PIT - TRAFFICABLE
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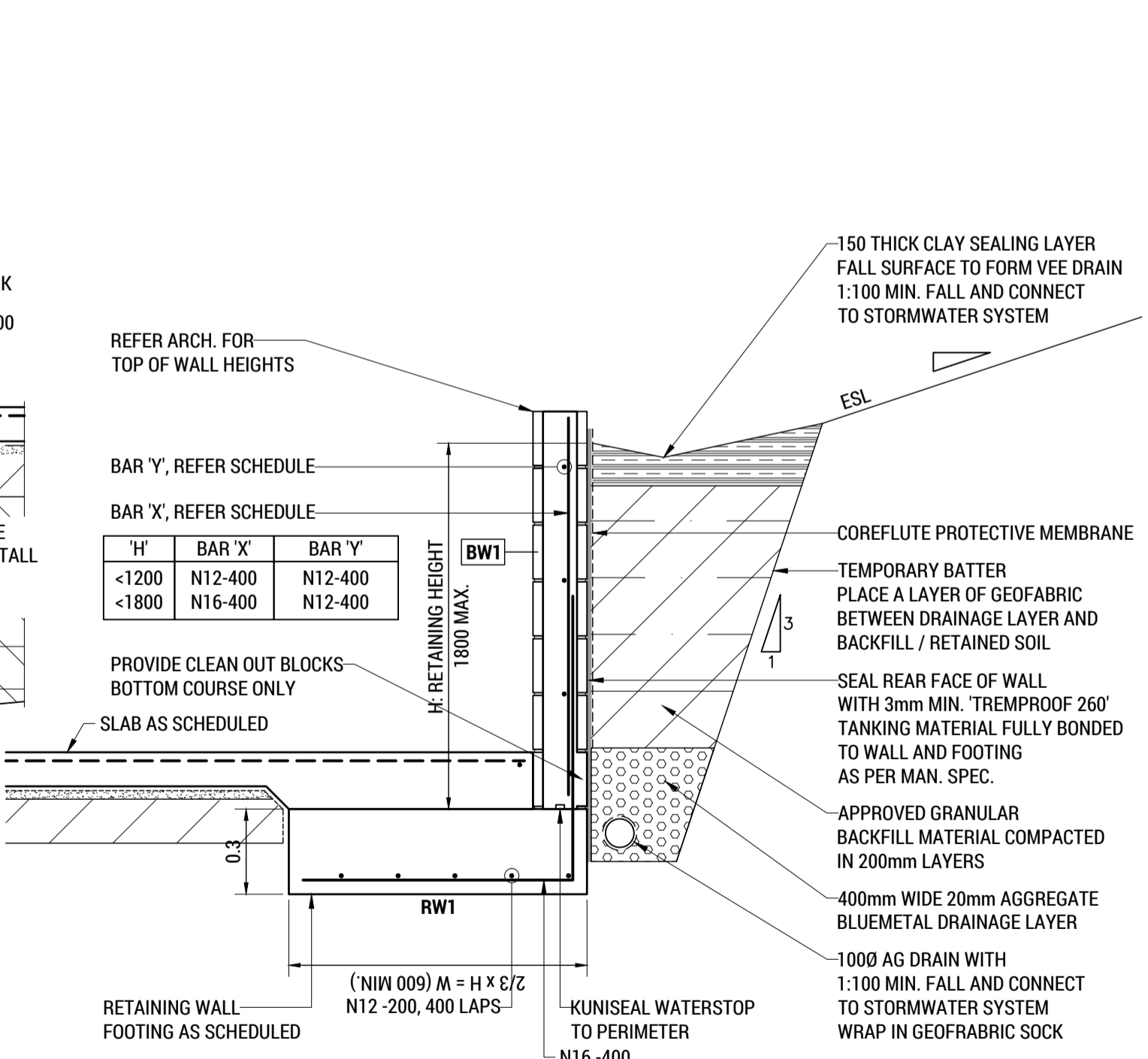
D10 FIRE FIGHTING WATER MAIN & HYDRANT DETAIL
SCALE N.T.S.

EQUIPMENT SCHEDULE (PER UNIT)		
ITEM	DESCRIPTION	QTY.
1	CAP P16 100mm FLANGED SPIGOT WITH TABLET'SLIP ON FLANGE	3
2	VICTAULIC STYLE 741 DN100 TABLE 'E' FLANGE ADAPTOR	1
3	VICTAULIC STYLE 005 DN100 FIRELOCK RIGID COUPLING	2
4	VICTAULIC GROOVED DN100 LONG RADIUS ELBOW (GAL)	1
5	BULL HEAD TEE 100/80 - DIXON FWG-BHT888114	1
6	VICTAULIC STYLE 80NB RIGID COUPLING	2
7	65mm STORTZ HERMAPHRODITE FITTING	2

NOTES:
1. FIRE HYDRANT TO COMPLY WITH DIN or NEN STANDARD COMPLIANT FORGED 65mm STORTZ HERMAPHRODITE FITTING. FITTING MUST BE FITTED WITH STANDARD (DELIVERY) WASHER, RATED TO 1800kpa AND 2400kpa BURST PRESSURE & BLANK CAPS



D09 FILL RETAINING WALL 'RW-1' DETAIL
SCALE 1:20

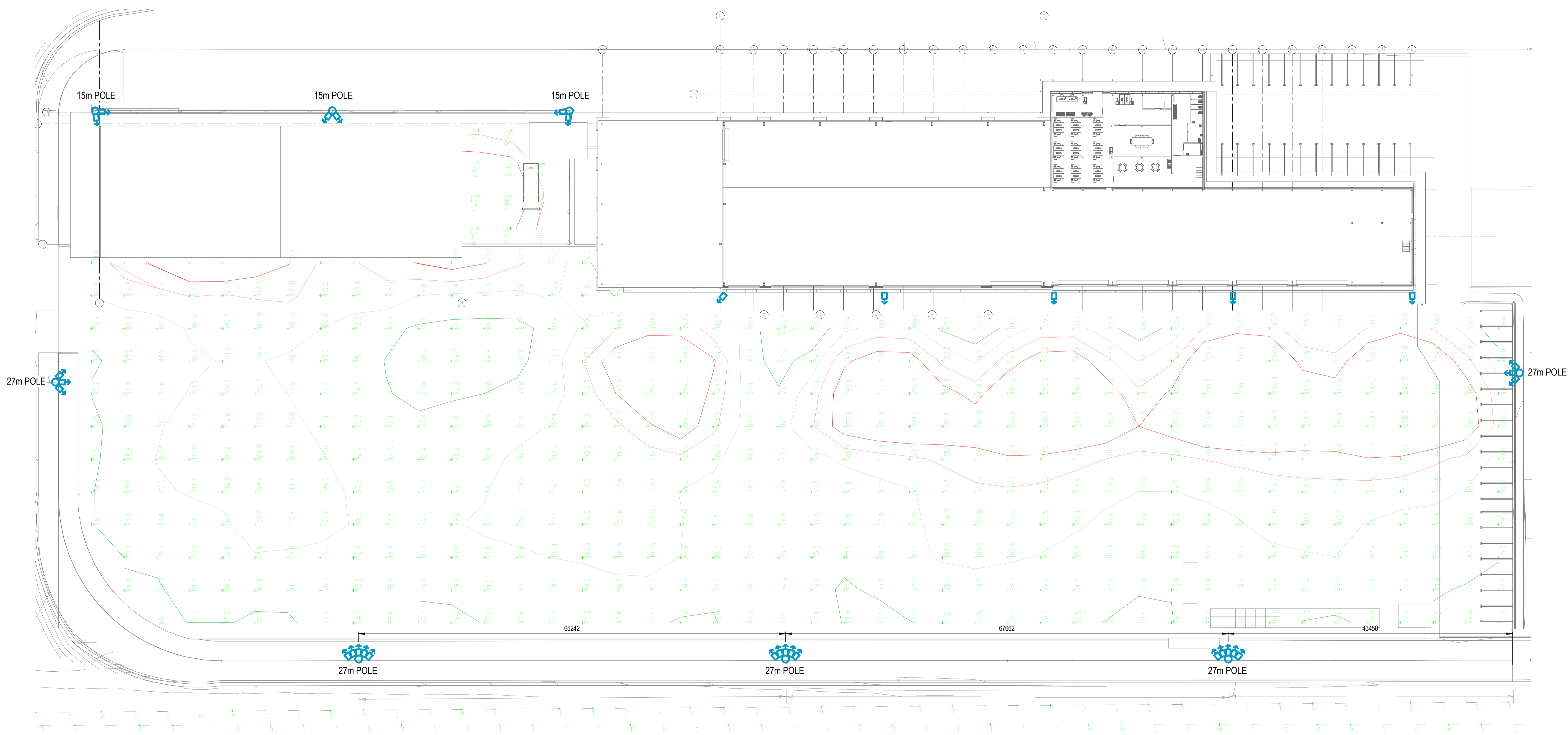


D01 CUT RETAINING WALL 'RW2' DETAIL
SCALE 1:20

REVISIONS				STATUS:		DESIGN BY:		CLIENT:		TITLE:		
E	REVIEW / INFORMATION	JQ	25-07-25	PRELIMINARY / INFORMATION		JQ		STUBBS & LIGHT INVESTMENTS PTY LTD.		SECTIONS & DETAILS		
D	REVIEW / INFORMATION	JQ	10-07-25			BS						
C	REVIEW / INFORMATION	JQ	17-06-25			JQ		PROJECT: STRAITLINK - BURNIE TERMINAL				
B	REVIEW / INFORMATION	JQ	06-06-25			JS		ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE		SCALE: 1:20 SHEET SIZE: A1 DWGS IN SET: -		
A	REVIEW / INFORMATION	JQ	28-05-25							PROJECT No: 245019 DWG No: C701 REV: E		
REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:	APPROVED: R.NEVILLE	ACRED. No: CC2080J	DATE: 25-07-25	DESIGN BY: JQ DESIGN CHK: BS DRAWN BY: JQ DRAFT CHK: JS		STUBBS & LIGHT INVESTMENTS PTY LTD. PROJECT: STRAITLINK - BURNIE TERMINAL ADDRESS: MASSY GREENE DRIVE, SOUTH BURNIE		TITLE: SECTIONS & DETAILS SCALE: 1:20 SHEET SIZE: A1 DWGS IN SET: - PROJECT No: 245019 DWG No: C701 REV: E	



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ELECTRICAL SERVICES - CONTAINER YARD LIGHTING - SITE PLAN

1:300

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- THIS DRAWING IS REQUIRED TO BE VIEWED OR PRINTED IN COLOUR FOR THE FULL EXTENT OF THE SCOPE OF WORKS TO BE SHOWN

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M.v.B	27/08/25	PRELIMINARY	P1
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REVISIONS



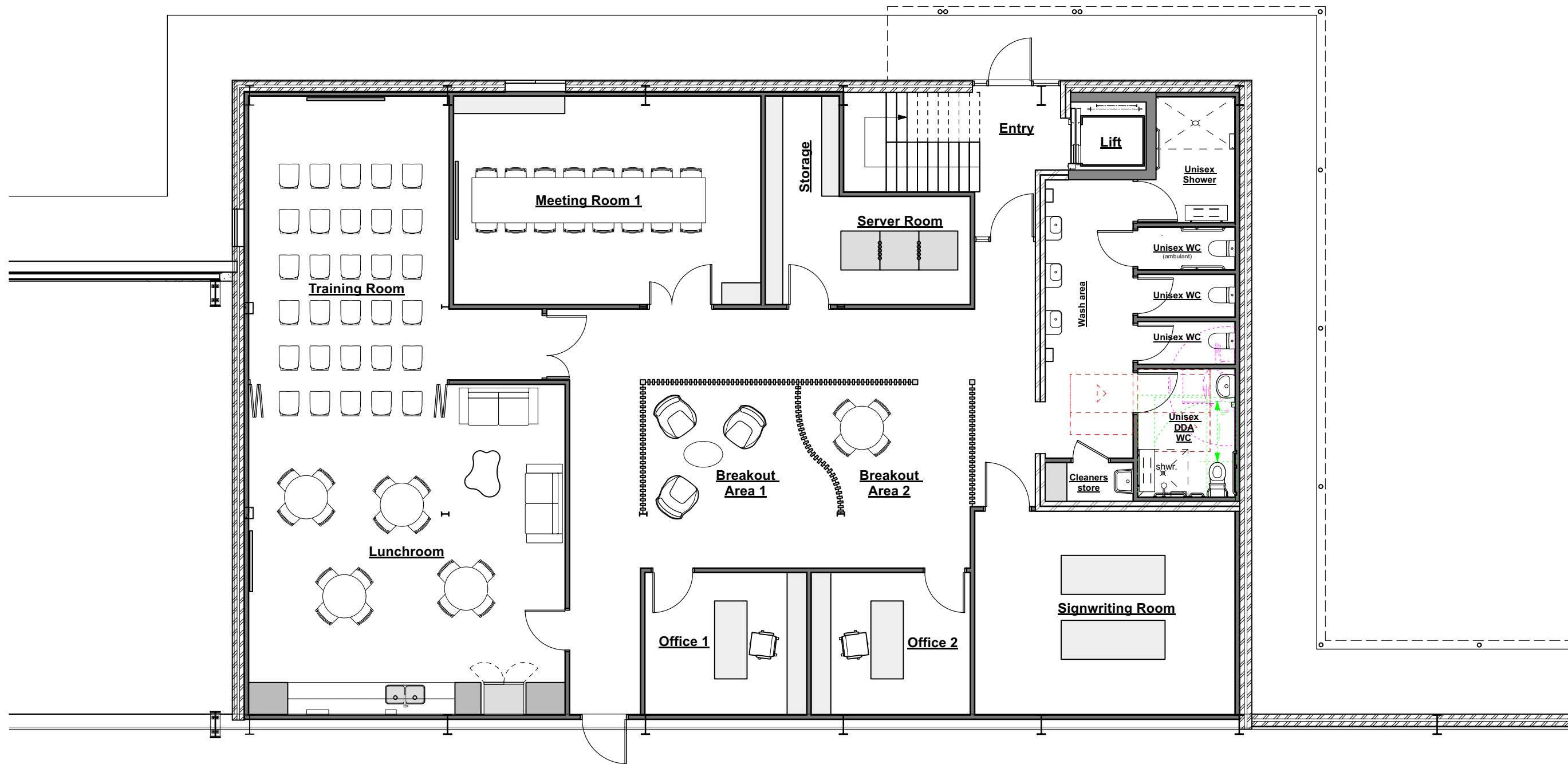
Client
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Project
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Sheet Title
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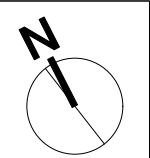
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Designed By M.v.B	Drawn By J.v.N
Checked By -	Revision P1
Drawing No. 25135-EW-E01	
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Ground Office Plan
Scale: 1:100

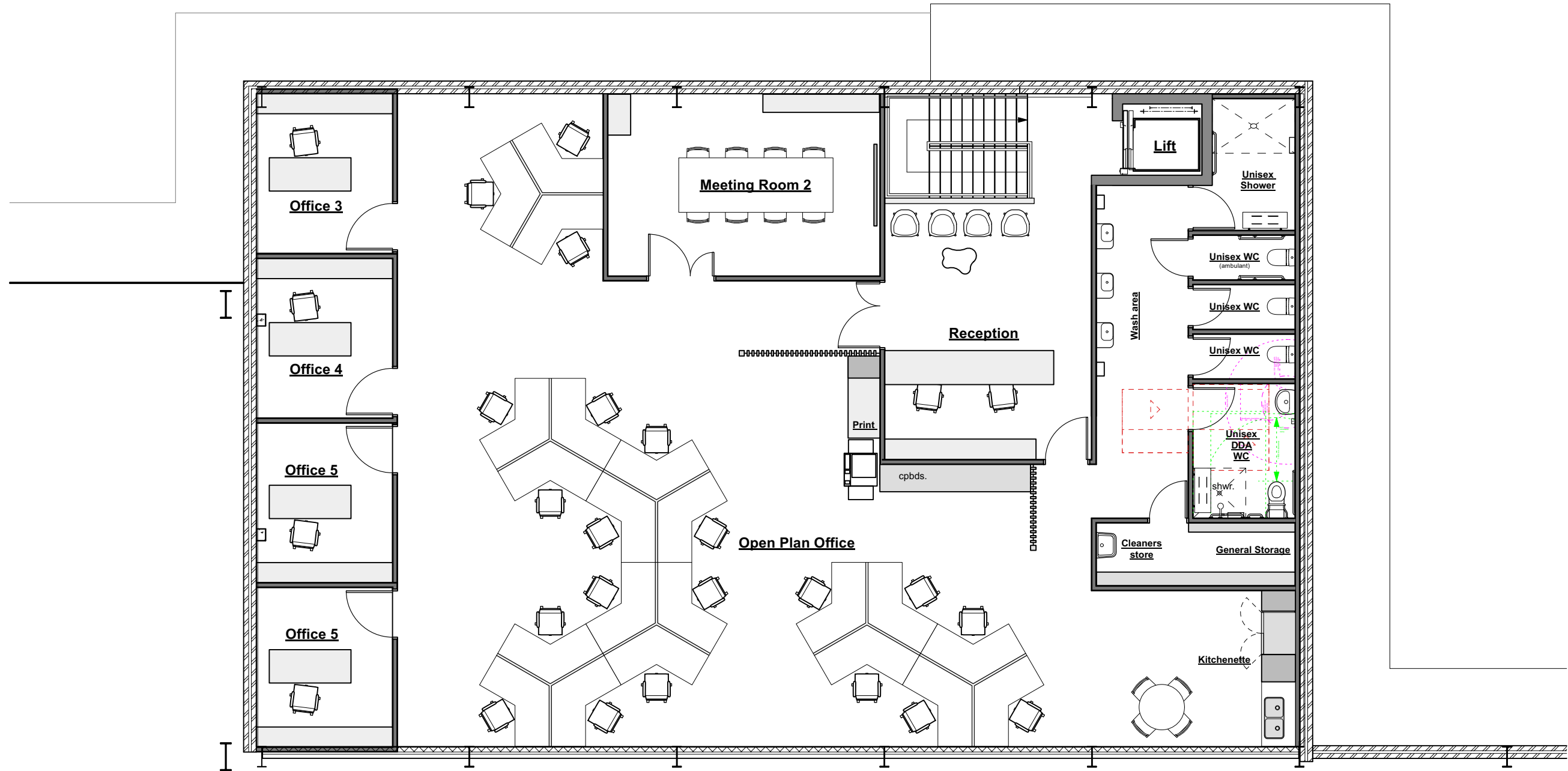


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	date 9/7/2025	project Strait Link Offices Massy-Greene Drive				Notes
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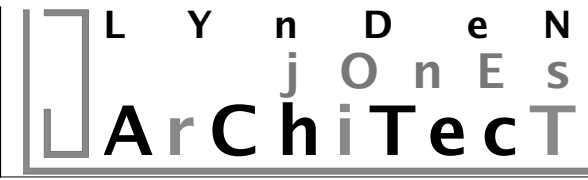
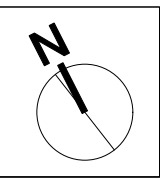
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foresb@plynel.com.au



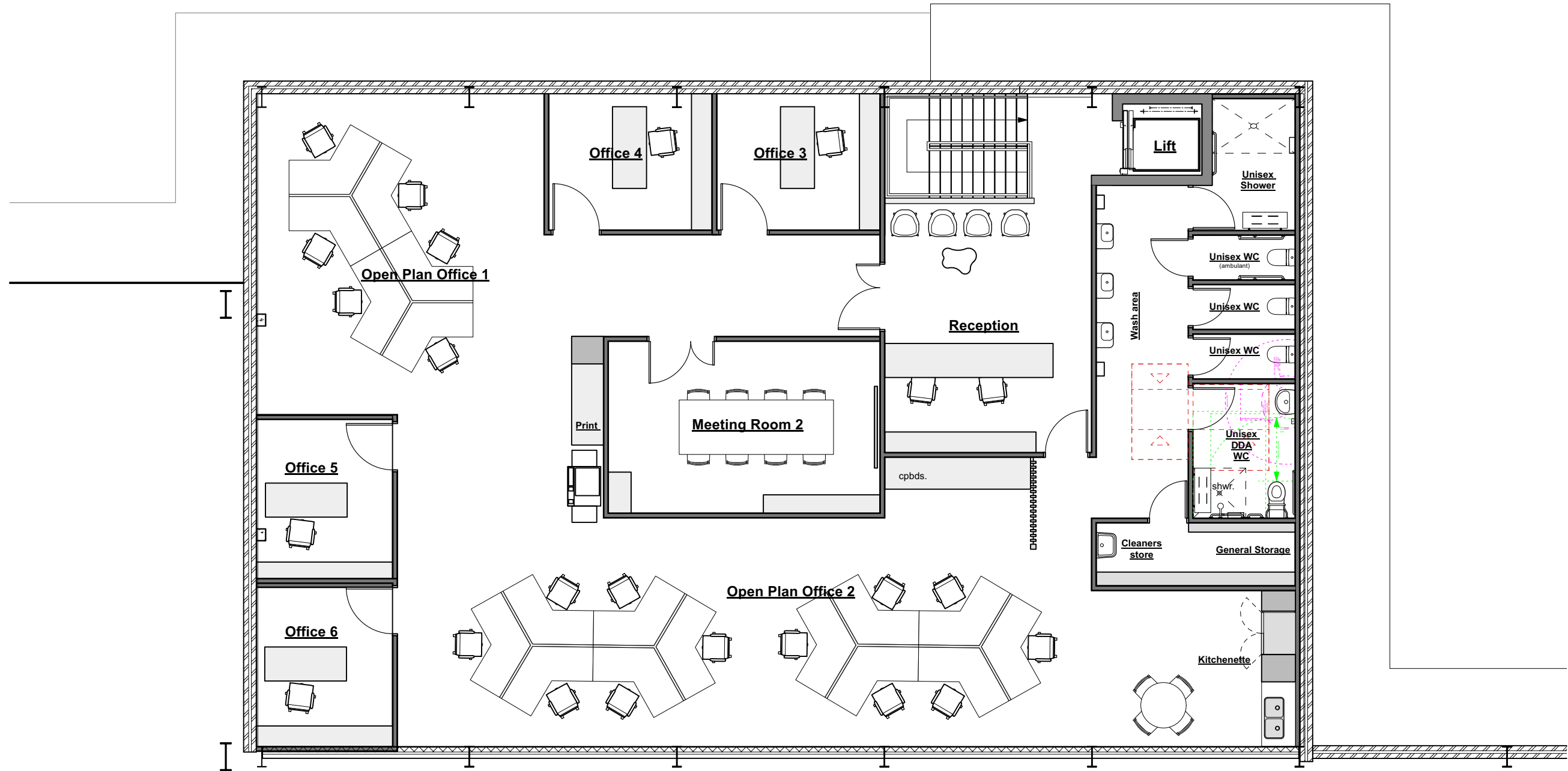
Level 1 Office Plan Option 1
Scale: 1:100



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	project Strait Link Offices Massy-Greene Drive				
	client Striat Link				



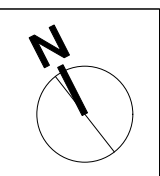
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Level 1 Office Plan Option 2
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	client Striat Link				



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2-10 MASSY-GREENE DRIVE, SOUTH BURNIE

STRAITLINK WAREHOUSE

**TRAFFIC IMPACT ASSESSMENT
AUGUST 2025**





2-10 Massy-Greene Drive South Burnie Straitlink Warehouse

TRAFFIC IMPACT ASSESSMENT

- Final
- August 2025

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Document history and status

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1. Introduction

1.1 Background

Further development of 2-10 Massey-Greene Drive is proposed with a warehouse extension and container storage for Straitlink. This report has been prepared to assess the traffic impact of the proposal.

This TIA has been prepared based on Department of State Growth (DSG) guidelines and responds to Tasmanian Planning Scheme – Burnie Road - Parking & Sustainable Transport Code C2 and Road & Railway Assets Code C3.

1.2 Objectives

A Traffic Impact Assessment is a means for assisting in the planning and design of sustainable development that considers:

- Safety and capacity
- Equity and social justice
- Economic efficiency
- The environment and future development.

This TIA considers the impact of the proposal on projected traffic volumes expected by 2035.

1.3 Scope of Traffic Impact Assessment (TIA)

This TIA considers in detail the impact of the proposal on Massy- Greene Drive.

1.4 References

- RTA Guide to Traffic Generating Development 2002
- Tasmanian Planning Scheme - Burnie
- Austroads Guide Road Design Part 4A: Unsignalised & Signalised Intersections 2021
- Guide to Traffic Management Part 6: Intersections, Interchanges & Crossings 2020.
- AS/NZS 2890.1:2004 Parking facilities Part 1: Off-street car parking
- LGAT Tasmanian Standard Drawings



1.5 Statement of Qualifications and Experience

This TIA has been prepared by Richard Burk, an experienced and qualified traffic engineer in accordance with the requirements of the Department of State Growth's guidelines and Council's requirements. Richard's experience and qualifications include:

- 38 years professional experience in road and traffic engineering industry
 - Manager Traffic Engineering at the Department of State Growth until May 2017.
 - Previous national committee membership with Austroads Traffic Management Working Group and State Road Authorities Pavement Marking Working Group
- Master of Traffic, Monash University, 2004
- Post Graduate Diploma in Management, Deakin University, 1995
- Bachelor of Civil Engineering, University of Tasmania, 1987

A handwritten signature in blue ink, appearing to read 'Richard Burk', is placed over a light blue rectangular background.

Richard Burk

BE (Civil) M Traffic Dip Man. MIE Aust CPEng

Director Traffic and Civil Services Pty Ltd



1.6 Glossary of Terms

AADT	Annual Average Daily Traffic - The total number of vehicles travelling in both directions passing a point in a year divided by the number of days in a year.
Acceleration Lane	An auxiliary lane used to allow vehicles to increase speed without interfering with the main traffic stream. It is often used on the departure side of intersections.
Access	The driveway by which vehicles and/or pedestrians enter and/or leave the property adjacent to a road.
ADT	Average Daily Traffic – The average 24-hour volume being the total number of vehicles travelling in both directions passing a point in a stated period divided by the stated number of days in that period.
Austrroads	The Association of Australian and New Zealand road transport and traffic authorities and includes the Australian Local Government Association.
Delay	The additional travel time experienced by a vehicle or pedestrian with reference to a base travel time (e.g. the free flow travel time).
DSG	Department of State Growth – The Tasmanian Government Department which manages the State Road Network.
GFA	Gross Floor Area
Intersection Kerb	The place at which two or more roads meet or cross. A raised border of rigid material formed at the edge of a carriageway, pavement or bridge.
km/h	Kilometres per hour
Level of Service	An index of the operational performance of traffic on a given traffic lane, carriageway or road when accommodating various traffic volumes under different combinations of operating conditions. It is usually defined in terms of the convenience of travel and safety performance.
m	Metres
Median	A strip of road, not normally intended for use by traffic, which separates carriageways for traffic in opposite directions. Usually formed by painted lines, kerbed and paved areas grassed areas, etc.
Movement	A stream of vehicles that enters from the same approach and departs from the same exit (i.e. with the same origin and destination).
Phase	The part of a signal cycle during which one or more movements receive right-of-way subject to resolution of any vehicle or pedestrian conflicts by priority rules. A phase is identified by at least one movement gaining right-of-way at the start of it and at least one movement losing right-of-way at the end of it.



Sight Distance	The distance, measured along the road over which visibility occurs between a driver and an object or between two drivers at specific heights above the carriageway in their lane of travel.
Signal Phasing	Sequential arrangement of separately controlled groups of vehicle and pedestrian movements within a signal cycle to allow all vehicle and pedestrian movements to proceed.
SISD	Safe Intersection Sight Distance – The sight distance provides sufficient distance for a driver of a vehicle on the major road to observe a vehicle on a minor road approach moving into a collision situation and to decelerate to a stop before reaching the collision point.
Speed	Distance travelled per unit time.
85th Percentile	The speed at which 85% of car drivers will travel slower and 15% will travel faster. A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic-actuated Control	A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic Growth Factor	A factor used to estimate the percentage annual increase in traffic volume.
Trip	A one-way vehicular movement from one point to another excluding the return journey. Therefore, a vehicle entering and leaving a land use is counted as two trips. (RTA Guide to Traffic generating Developments).
Turning Movement	The number of vehicles observed to make a particular turning movement (left or right turn, or through movement) at an intersection over a specified period.
Turning Movement Count	A traffic count at an intersection during which all turning movements are recorded.
Vehicle Actuated Traffic Signals	Traffic signals in which the phasing varies in accordance with the detected presence of vehicles on the signal approaches.
vpd	vehicles per day – The number of vehicles travelling in both directions passing a point during a day from midnight to midnight.
vph	vehicles per hour – The number of vehicles travelling in both directions passing a point during an hour.

1.7 Site Specific Glossary of Terms

BCC	Burnie City Council
SSA	Safe System Assessment

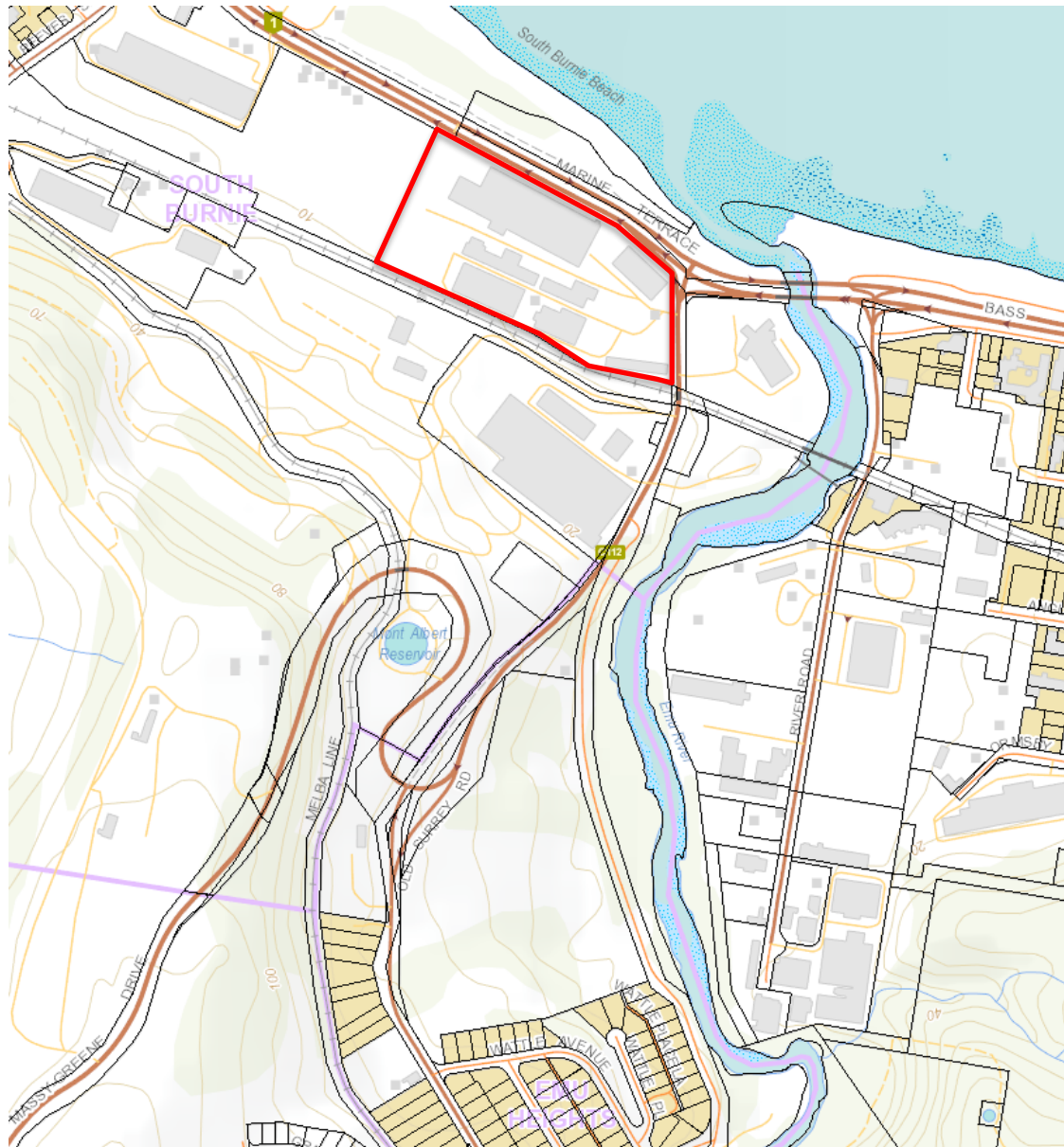
2. Site Description

Figures 1 & 2 show the development location at 2-10 Massey – Greene Drive, Burnie. The development site is flat with:

- Tasrail Western Line along the Southern boundary.
- Massey- Greene Drive along the Eastern boundary.
- Bass Highway along the Northern boundary.

The development site is shown in Figures 1&2 and Appendix A.

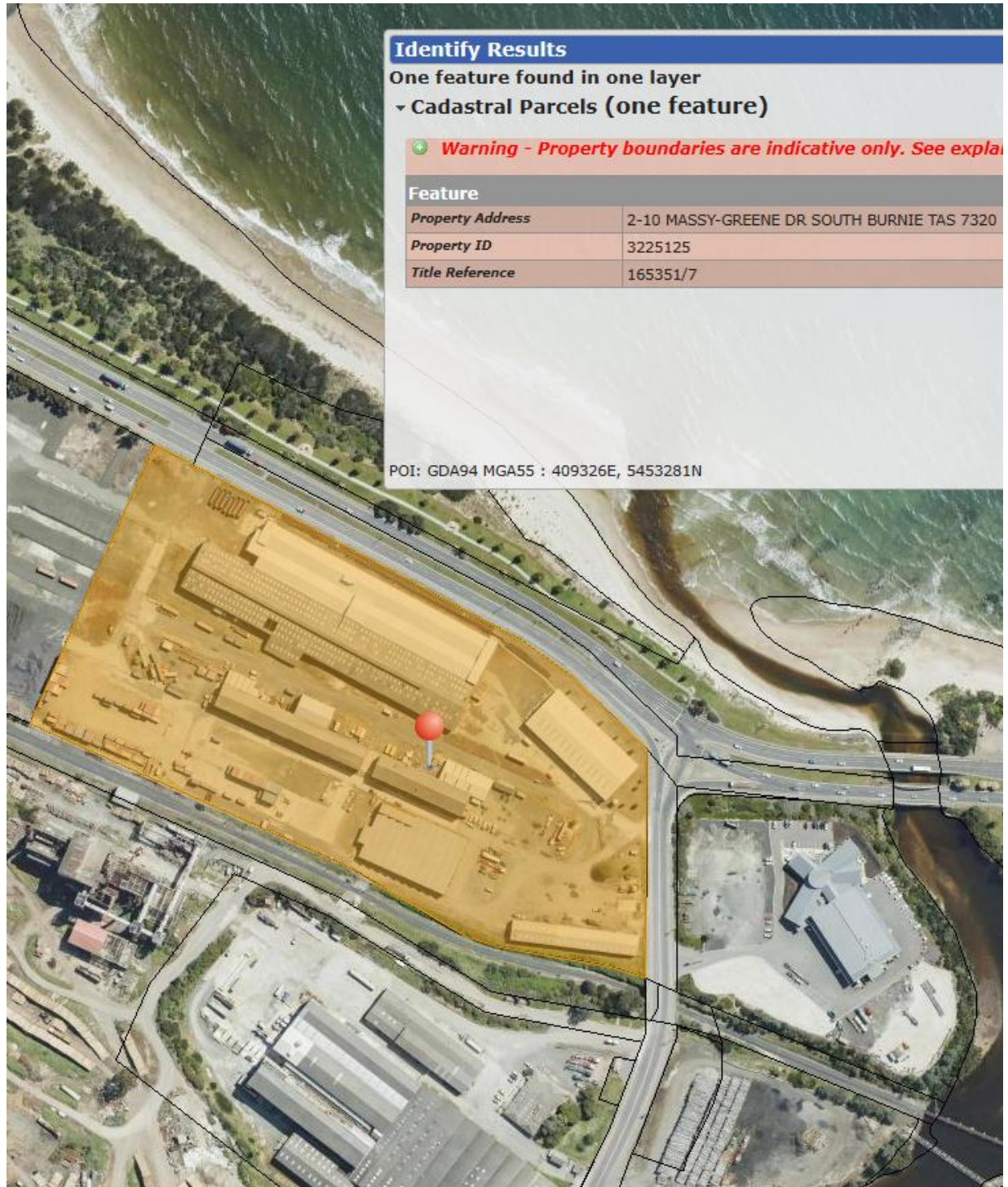
Figure 1 – Development location at 2-10 Massey-Greene Drive



Source: *The List, DPIPWE*



Figure 2 – Aerial view of 2-10 Massey-Greene Drive



Source: The List, DPIPWE



3. Proposal, Planning Scheme and Road Owner objectives

3.1 Description of Proposed Development

The development site at 2-10 Massy-Greene Drive has various large buildings with low occupancy with 7 tenants over 1 hectare of the 7.6 - Hectare site.

Following discussions with potential new tenants requiring large warehouse and workshop facilities as well as storage areas, a master plan has been prepared for the site. The current tenants will remain on the site at least for the period of their current lease agreement which ranges between 2-5 years.

Some of the currently vacant buildings will be occupied in stages over the next five years.

Warehouse expansion and container storage within Southwest portion of the site is now proposed by Straitlink. The warehouse would operate 7AM to 5PM.

The plans show the proposed traffic access and circulation layout for the site. The main access to the site will be via the existing one-way property entrance from Massy-Greene Drive. Exit from the site is proposed via the existing two-way Massy-Green Drive access.

Figures 3.0 to 3.6 show the proposed development and access arrangements.

Figure 4 shows the adjacent rail line. Appendix N shows the proximity of the rail line and proposed driveway extension to the West.

Figure 3.0 – Proposed development site

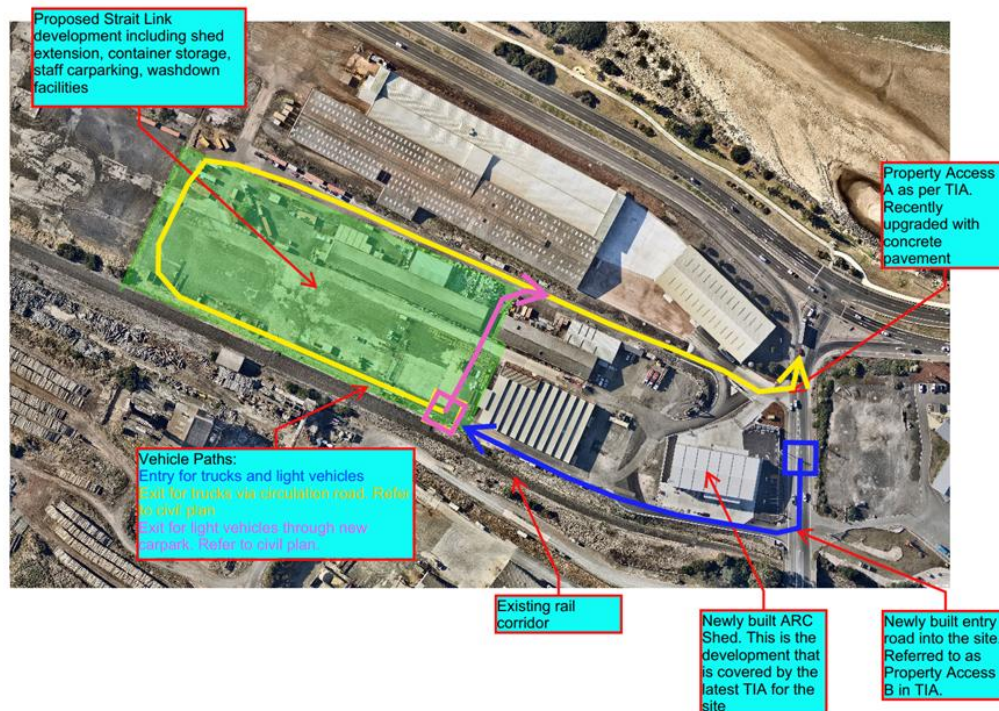


Figure 3.2 – Western end of proposed development site

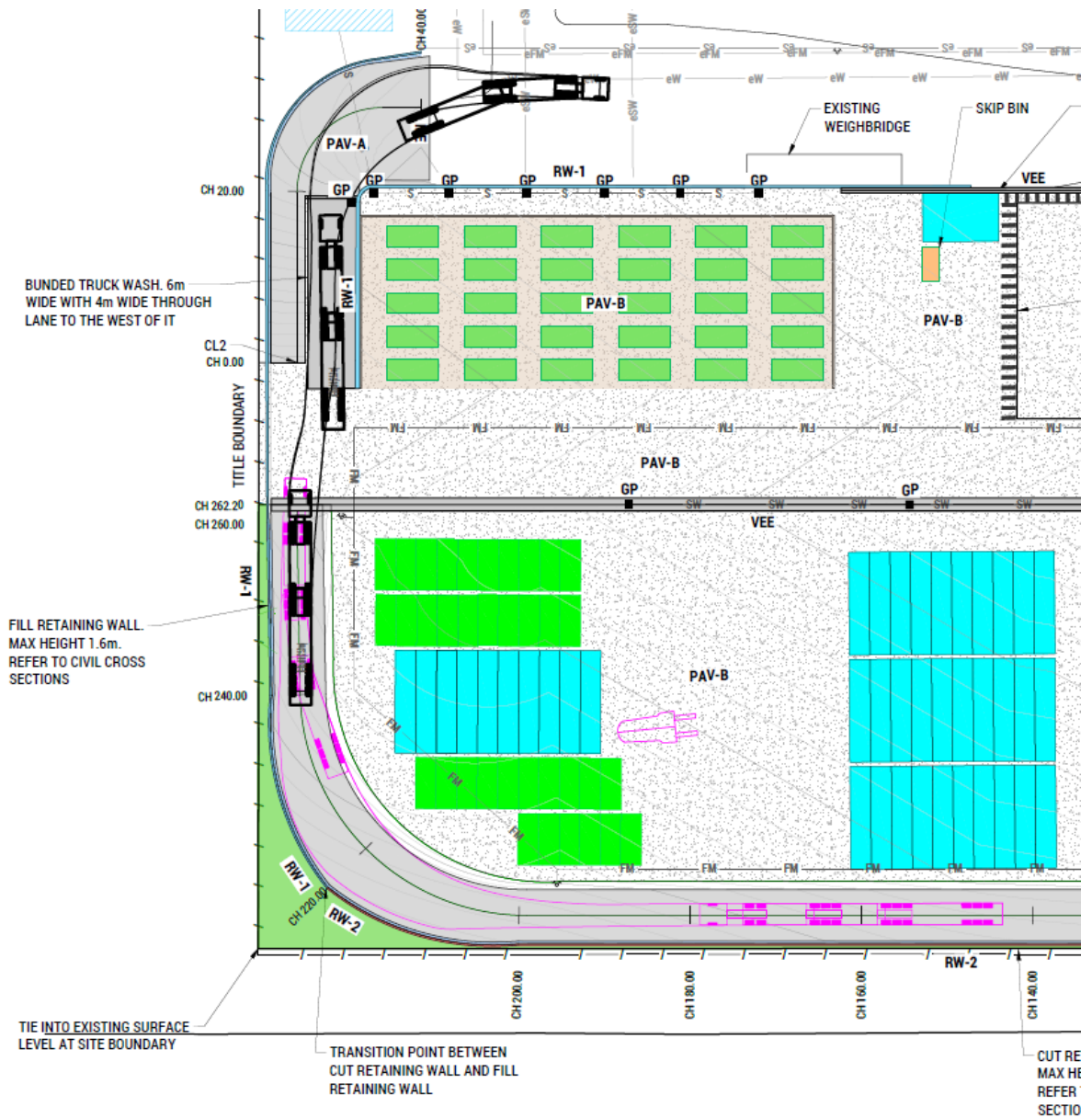




Figure 3.3 – Central section of development site

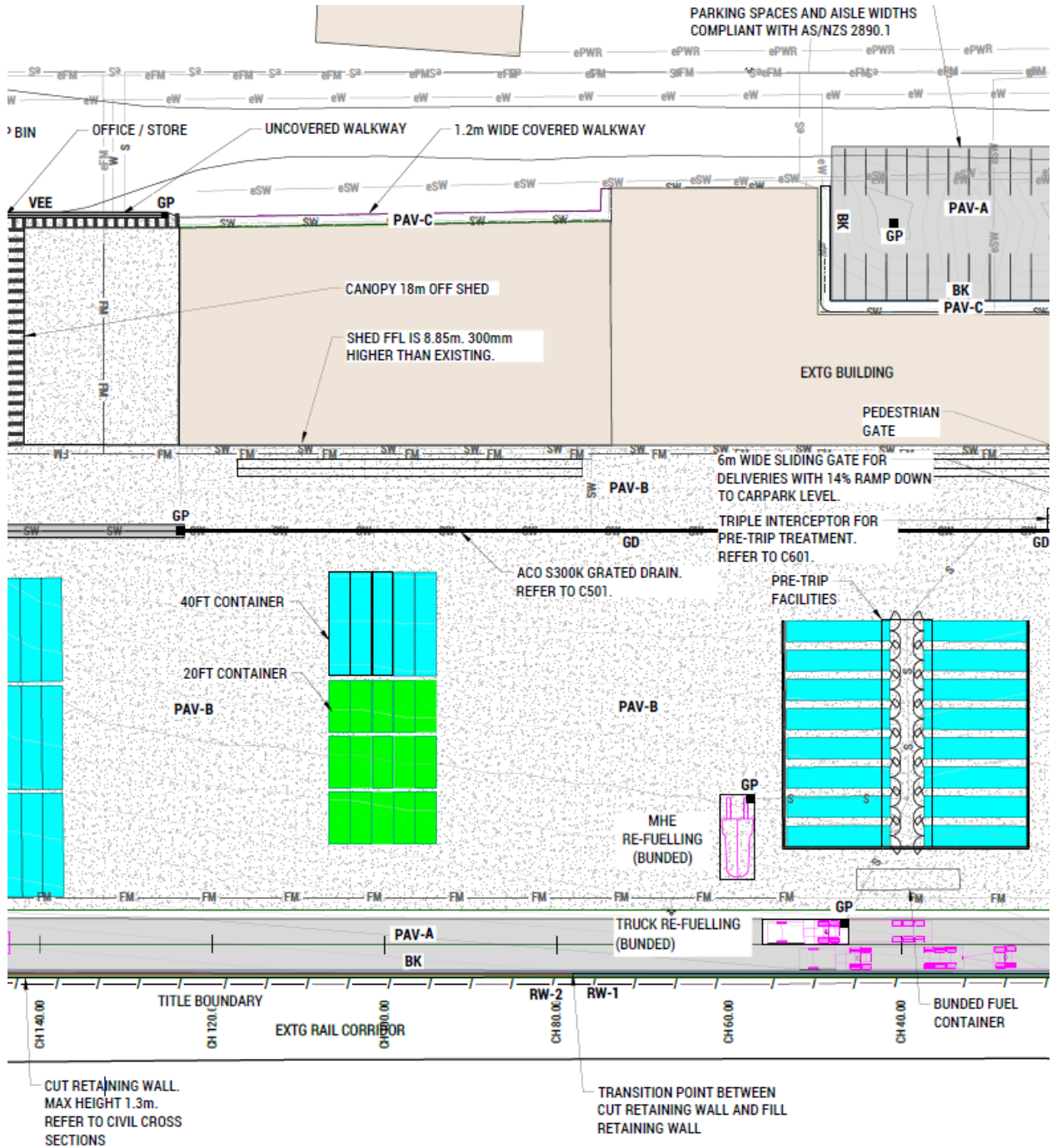




Figure 3.4 – Eastern end of development site

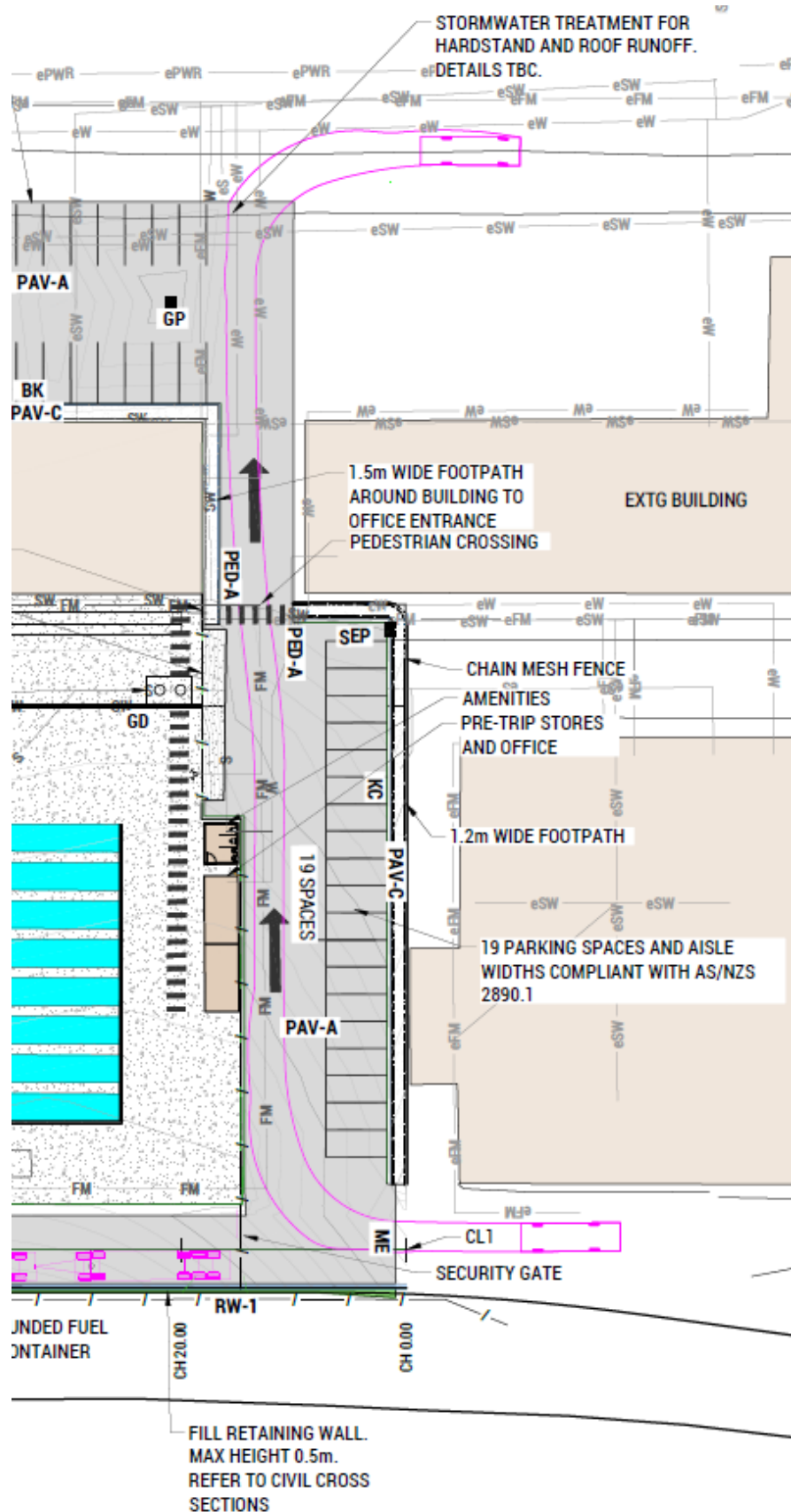
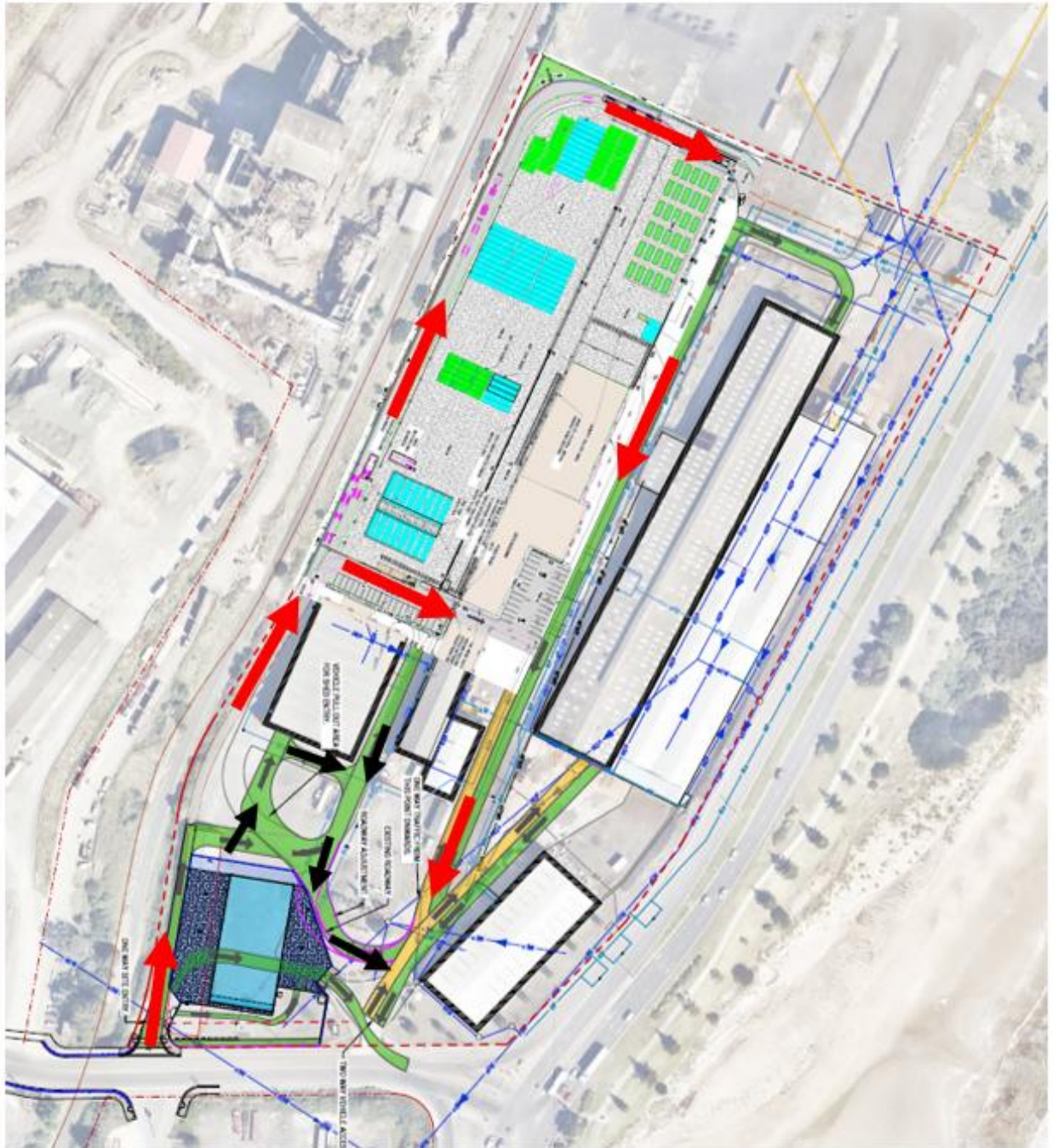




Figure 3.5 – Proposed internal road network at 2-10 Massy-Greene Drive, South Burnie



See Appendix L for proposed entry lane to the development.

Figure 3.6 – Adjacent access conflicts

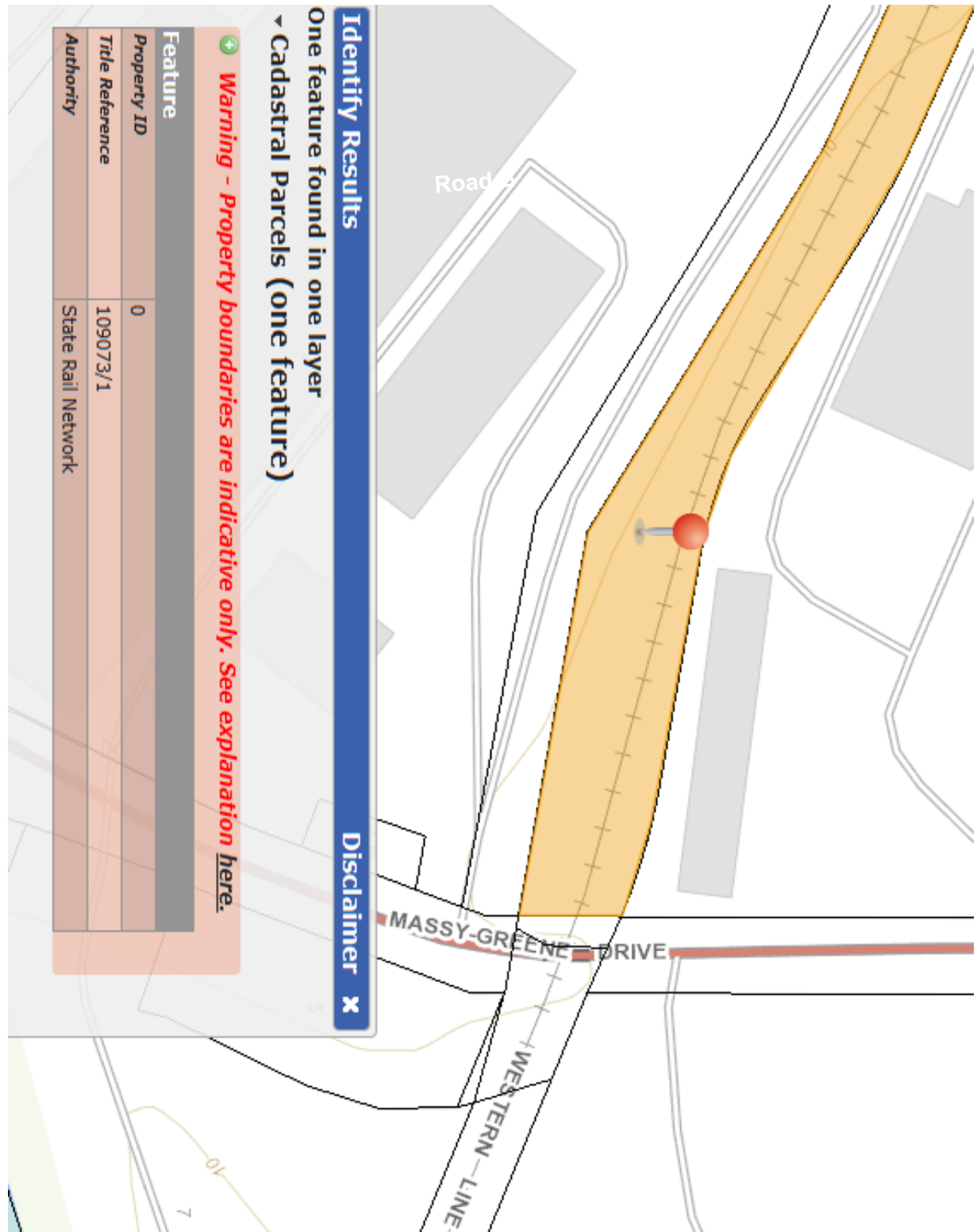


Lane widths are adequate with adequate separation to cater for all heavy movements:

- The Northbound Lane is 5m wide.
- The median turn lane is 4m wide.
- The Southbound Lane is 5m wide.



Figure 4 – Tasrail – Western Line, South Burnie



Also see Appendix N for relative position of driveway to adjacent rail line.



3.2 Council Planning Scheme

The proposed development site zoning is shown in Figure 5 as per the Tasmanian Planning Scheme - Burnie.

Figure 5 – Site zoned General Industrial



Source: *The List*, DPIPWE

3.3 State Road Network Objectives

DSG is the authority responsible for the State Road network impacted by the proposal. DSG objectives are to maintain traffic safety and capacity.

3.4 Local Road Network Objectives

BCC is the authority responsible for the Council Road network impacted by the proposal. BCC objectives are to maintain traffic safety and capacity.



4. Existing Conditions

4.1 Transport Network

The transport system adjacent the development site consists of the Bass Highway and Massy-Greene Drive.

4.1.1 Bass Highway, South of Burnie

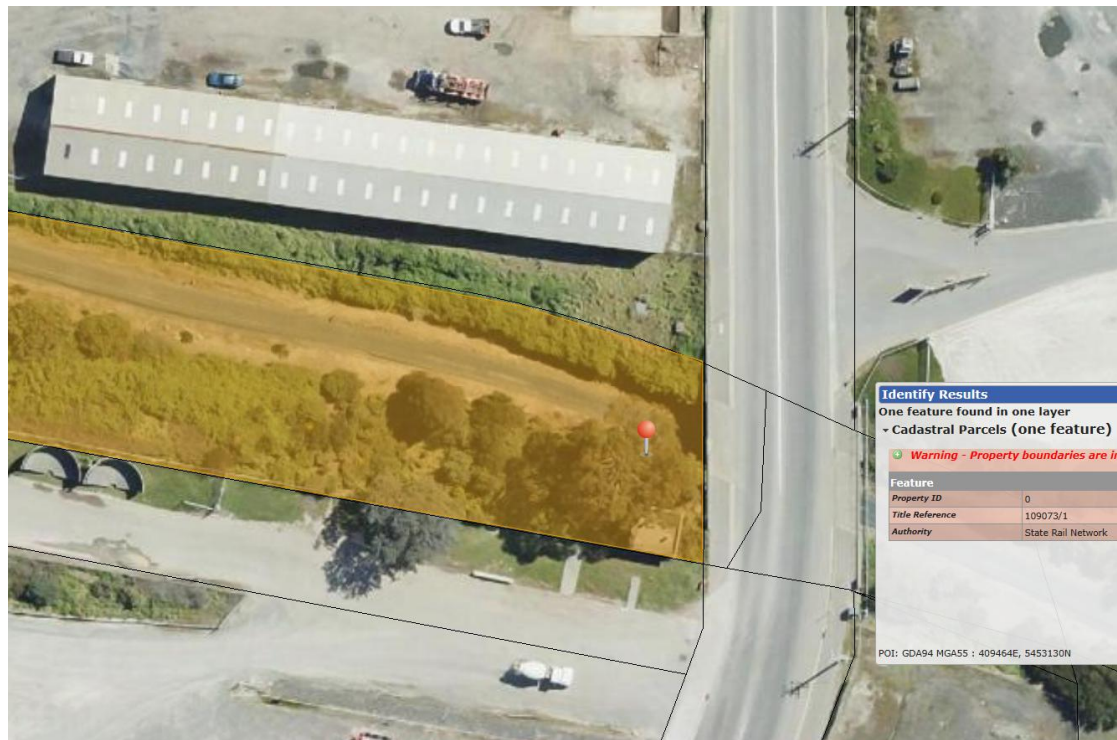
Bass Highway is a Category 1 Trunk Road in the State Road Hierarchy with Limited Access and is part of the Tasmanian 26m Double B Network, see Appendix C.

The Bass Highway has a speed limit of 80km/h in the vicinity of the Massy-Greene Drive junction. The Highway has two lane 7.4m wide carriageways in each direction plus turning lanes at intersections. The road is well delineated with line marking and guideposts.

4.1.2 Western Line 200m South of Bass Highway

The Tasrail Western Lines travels East – West some 175m South of the Bass Highway. The Western Line is a regular freight route. Figure 6 shows the position of the Western Line railway reservation.

Figure 6 – Western Line some 200m South of the Bass Highway





4.1.3 Massy- Greene Drive, South of Burnie

Massy-Greene Drive is a Collector Road in the Council Road Hierarchy in the vicinity of the 2-10 Massy-Greene Drive property and is not a Limited Access Road and is part of the Tasmanian 26m Double B Network, see Appendix C.

Massy-Greene Drive has a speed limit of 60km/hr and the sealed road width ranges between 13.6 and 14m and is delineated with continuous barrier line, see Figure 7.

Figure 7 – Massy - Greene Drive Southern approach to development site.



4.1.4 Bass Highway / Massy- Greene Drive junction

The Bass Hwy / Massy-Greene Drive signalised intersection layout is shown in Figures 8-13.

Figure 8 – Bass Highway / Massy-Greene Drive unjunction



Source: *The List, DPIPWE*



Figure 9 – Massy-Greene Drive approach to Bass Highway



Figure 10 – Looking right along Bass Hwy from Massy- Greene Drive



Figure 11 – Looking right from Bass Hwy off ramp to Massy-Greene Drive





Figure 12 – Bass Highway Western approach at Massy-Greene Drive



Figure 13 – Bass Highway Eastern approach to Massy-Greene Drive





4.1.5 2-10 Massy-Greene Drive Property Access A

Figures 14-19 show the nature of the proposed junction and approaches. The existing access operates as a Basic Right (BAR) and Basic Left (BAL) junction, see Figure 14, also see Appendix M.

Figure 14 – 2-10 Massy-Greene Drive Property Access A



Source: The List, DPIPWE

Figure 15 – Elevation view from Massy-Greene Drive of Access A



Source: Google Maps



Figure 16 – Looking right along Massy-Greene Drive from Access A



Sight distance
right is 150m.

Figure 17 – Looking left along Massy-Greene Drive from Access A



Sight distance
left is 65m.

Figure 18 – Massy-Greene Northern approach at Access A



Source: Google Maps



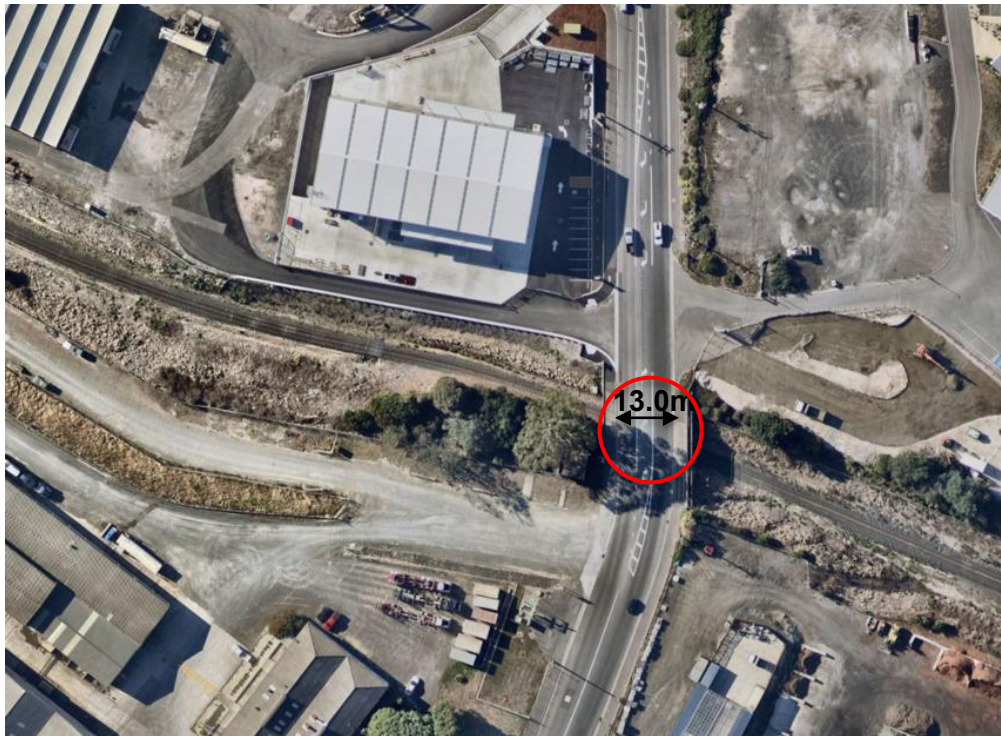
Figure 19 – Massy-Greene Southern approach at Access A



4.1.6 2-10 Massy-Greene Drive Property Access B – Entrance Only

Figures 20-23 show the nature of the proposed junction and approaches.

Figure 20 – Existing Massy-Greene Drive Property Access B



Source: *The List, DPIPWE*



Figure 21 – Elevation view of existing Access B driveway on Massy- Greene Drive



Source: Google Maps

Figure 22 – Massy-Greene Northern approach to Access B



Forward sight distance is 130m.

Figure 23 – Massy-Greene Southern approach to Access B





4.2 Traffic Activity

Detailed traffic data from DSG records for the Bass Highway (Appendix E) and from TCS turning count survey for the Bass Highway / Massy-Greene Drive junction (Appendix F) are summarised as follows:

Bass Highway (East of Massy-Greene Drive)

- AADT: 18,597 vpd (2021) – DSG Data
- AADT: 21,440 vpd (2035) Projected from 1.2% compound annual growth.

Massy Greene Drive (At Bass Highway)

- AADT: 4,300 vpd (2023) – TCS Data
- AADT: 4,850 vpd (2035) Projected from 1.2% compound annual growth.
-

4.3 Crash History

The Department of State Growth is supplied with reported crashes by Tasmania Police. The Department maintains a crash database from the crash reports which is used to monitor road safety, identify problem areas and develop improvement schemes.

The 5-year reported crash history records a reported crash near the proposed and existing accesses with no evidence of a crash propensity in the vicinity of the proposed entrance and existing 2-way access. Figures 24 & 25 summarise reported crash history in the general area.

Figure 24 – 5 Year reported crash history summary for Massy - Greene Drive

Crash Id	Units	Description	Date	Time	Severity	Light	Speed Limit	Location
49492086	HV; LV	130 - Veh. in same lane/ rear end	10-AUG-2018	16:43	Minor	Day	80	Massy-Greene Drive / Bass Hwy jcn.
49577132	LV; LV	130 - Veh. in same lane/ rear end	05-SEP-2018	09:30	PDO	Day	60	Massy-Greene Drive / Bass Hwy jcn.
49670283	LV; LV	130 - Veh. in same lane/ rear end	16-NOV-2018	17:30	PDO	Day	60	Massy-Greene Drive / Bass Hwy jcn.
50608086	LV; LV	130 - Veh. in same lane/ rear end	24-DEC-2019	11:30	PDO	Day	80	Massy-Greene Drive / Bass Hwy jcn.
50628536	LV	184 - Out of control on carriageway	10-JAN-2020	18:12	PDO	Day	60	Massy-Greene Drive
50595297	MC; LV	132 - Veh. in same lane/ right rear	17-FEB-2020	15:27	Minor	Day	60	Massy-Greene Drive / Bass Hwy jcn.
50630207	LV; LV	149 - Other maneuvering	27-APR-2020	15:50	PDO	Day	60	Massy-Greene Drive / Bass Hwy jcn.
50794056	LV; MC	130 - Veh. in same lane/ rear end	25-AUG-2020	15:11	Minor	Day	80	Massy-Greene Drive / Bass Hwy jcn.
50977208	LV	183 - Off left bend into obj./prkd veh.	16-FEB-2021	08:28	PDO	Day	80	Massy-Greene Drive
51496715	LV; LV	121 - Right through	21-DEC-2021	21:19	PDO	Dusk	80	Massy-Greene Drive / Bass Hwy jcn.
51561100	HV	189 - Other curve	10-FEB-2022	15:30	PDO	Day	80	Massy-Greene Drive / Bass Hwy jcn.
51564521	LV	179 - Other straight	15-FEB-2022	09:09	Minor	Day	60	Massy-Greene Drive

Minor	Minor Injury Crash
PDO	Property Damage Only Crash
LV	Light Vehicle
HV	Heavy Vehicle
MC	Motorcycle



Figure 25 – 5 Year reported crash locations for Massy-Greene Drive



4.4 Services

Street lighting is provided along Massy-Greene Drive.

4.5 Road Safety Review

4.5.1 Massy-Greene Drive

No road safety issues were identified.

4.5.2 Massy-Greene Drive approaches to the existing access

No road safety issues were identified.

4.5.3 Massy-Greene Drive approaches to the proposed access

No road safety issues were identified.



4.6 Austroads Safe System Assessment

Massy-Greene Drive approaches to 2-10 Massy-Greene Drive have been assessed in accordance with the Austroads Safe System assessment framework. This framework involves consideration of exposure, likelihood and severity to yield a risk framework score. High risk crash types and vulnerable road user crash types are assessed for each site and aggregated to provide an overall crash risk. Crash risk is considered in terms of three components:

- Exposure (is low where low numbers of through and turning traffic) i.e. 1 out of 4
- Likelihood (is low where the infrastructure standard is high) i.e. 1 out of 4
- Severity (is low where the speed environment is low) i.e. 1 out of 4

The Austroads Safe System Assessment process enables the relative crash risk of an intersection or road link to be assessed. Vulnerable Road users are considered along with the most common crash types. The crash risk score indicates how well the infrastructure satisfies the *safe system objective which is for a forgiving road system where crashes do not result in death or serious injury*.

From safe system assessment the crash risk score has been determined for the approaches to 2-10 Massy-Greene Drive as 37/448.

This core indicates good alignment with the safe system objective, see Figure 26 and Appendix D for the assessment details. There is some crash risk at the Bass Highway / Massy-Greene Drive junction.

Figure 26 – Austroads Safe System Assessment alignment between crash score and risk





5. Traffic Generation and Assignment

This section of the report estimates how traffic generated by the proposal is distributed within the adjacent road network now and ten years future.

5.1 Traffic Growth

Compound annual traffic growth of 1.2% has been assumed for all roads.

5.2 Trip Generation

From site survey traffic generation was 26 vph (2023) see Appendix H.

From the master plan and subsequent discussions with new tenants estimated commercial traffic generation as of 2023 is summarised in Figure 27. The relevant tenancy areas are shown in Appendix A.2 as reported in the Milan Prodanovic TIA (August 2016).

Figure 27 – Pre 2023 Operation

TENANCY	TOTAL DAILY VEHICLE	TYPES OF VEHICLES	HOURS OF OPERATION	PEAK HOUR TRAFFIC
Tenancy A	60	B- Double and Semi-Trailers Freight drop off in smaller trucks	7.30 to 5.00	Varies 9 - 11
Tenancy B	15 service vehicles 1 truck	Service utilities & skell truck with reefer container	8.00 to 4.30	4
Tenancy C	25 to 35 when delivering logs. 200 with ship load.	Log Trucks 66% Skell type 33%B Double	6.00 to 4.40	4 - 20
Tenancy D	60	Utes / light vehicles Trucks say 2 per day	7.30 to 4.30	9
TOTAL	171 - 336	-	-	26 - 44

The proposal replaces the Tenancy C site. Accordingly, the 4 to 20 vph should be deducted.

During 2023 an ARC warehouse proposal was prepared. The total number of employees associated with the development was estimated at 60 with 40 parking on the site. These employees will generate:

- 35vph before 7:30AM
- 35vph from 4:30 to 5:30PM

With the recent development peak traffic generation is estimated at 46 vph as of 2025.

The current proposal (a warehouse extension and container storage for Straitlink), see Figure 3.1, has a floor area of 13,800m².



Applicable traffic generation rates for the proposed warehouse extension are based on RTA guidelines as follows:

- daily trip generation of 4vpd / 100m2 Gross Floor Area (GFA) and
- peak of 0.5vph / 100m2 GFA

Accordingly, the proposal is estimated to generate 552vpd and 70 vph at peak times. The client has advised expected truck movements of 230vpd and adding in staff traffic and other light traffic actual total traffic due to the proposal is likely to be some 300 to 350 vpd and some 50 vph at peak times. To be conservative 70 vph has been used for analysis purpose.

5.3 Trip Assignment

Figures 28 & 29 for AM and PM peak traffic assignments.

Figure 28 – 2035 Traffic Assignment at 2-10 Massy-Greene Drive Access A & B - AM

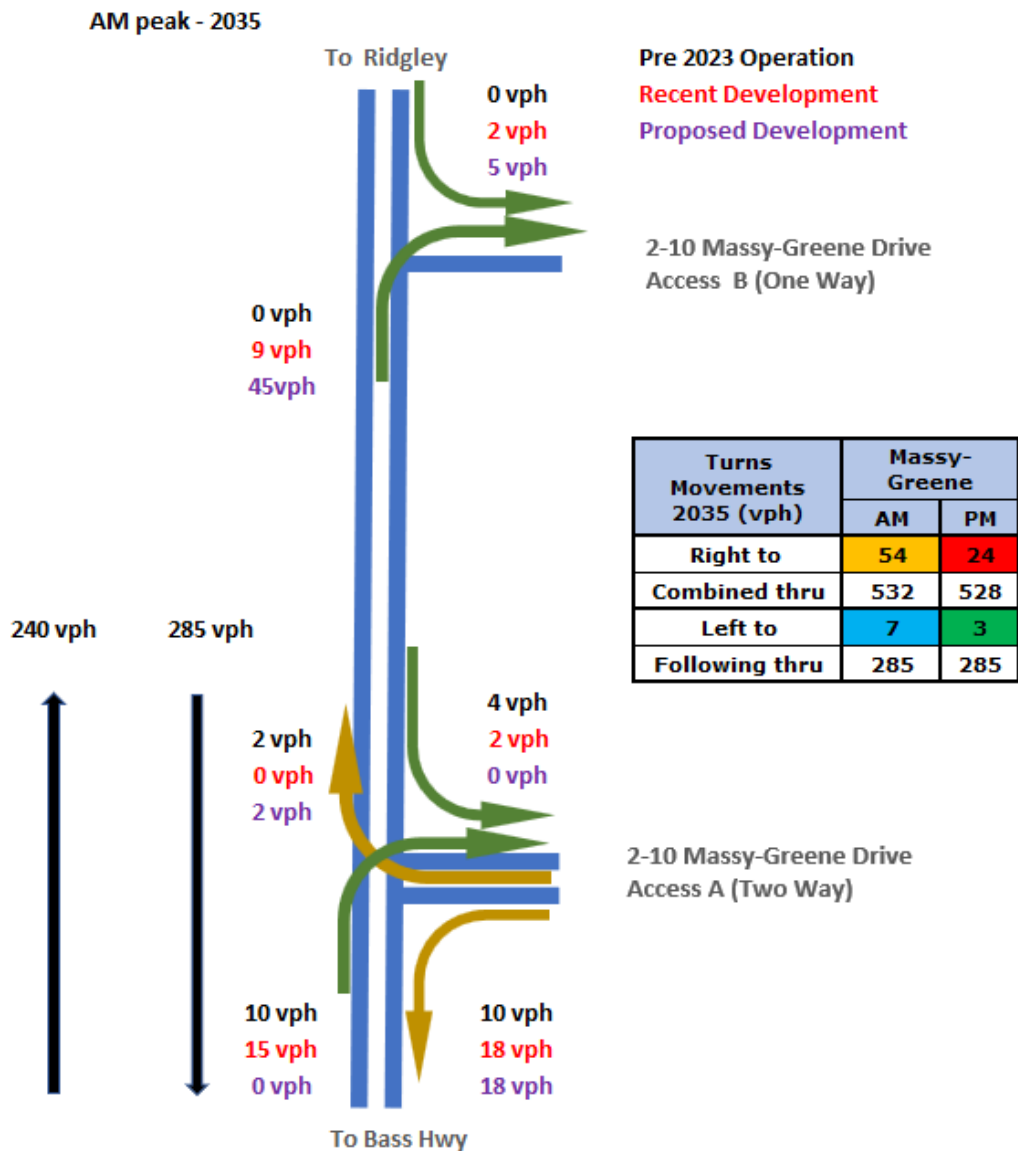




Figure 29 – 2035 Traffic Assignment at 2-10 Massy-Greene Drive Access A & B - PM

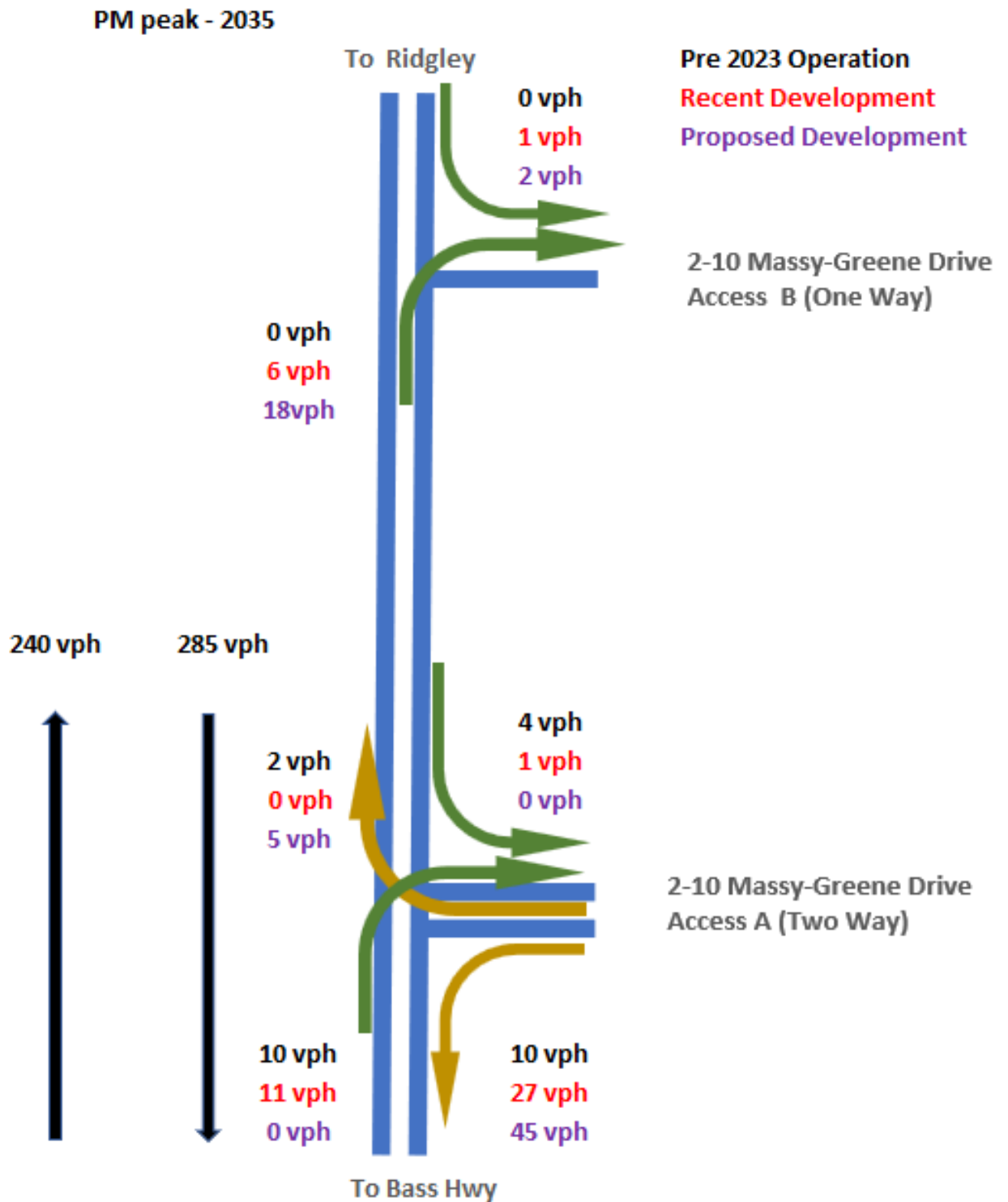
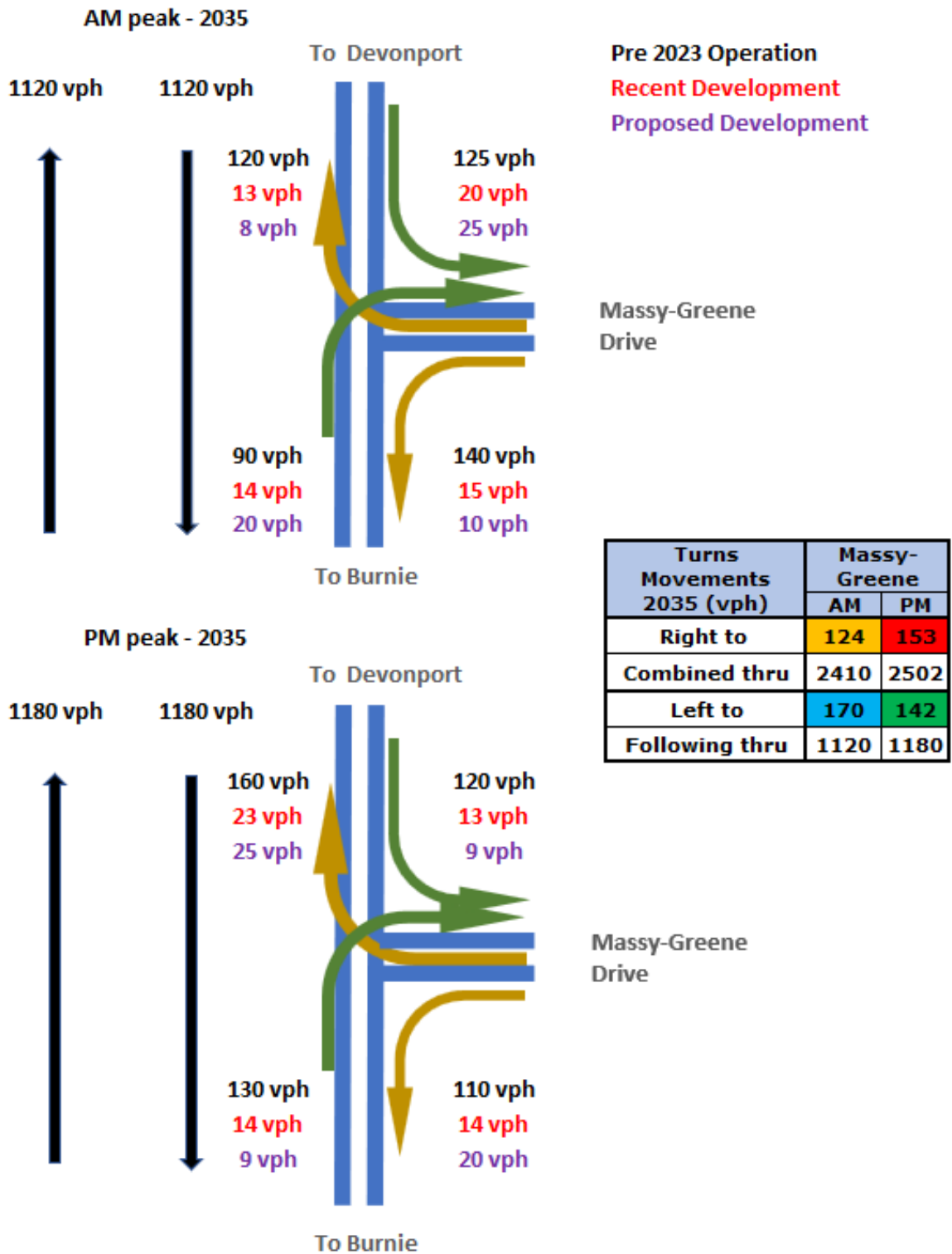




Figure 30 shows estimated peak traffic assignment at the Bass Hwy / Massy Greene Junction.

Figure 30 – 2035 Traffic Assignment at Bass Hwy / Massy-Greene Signalised Junction





6. Impact on Road Network

This section of the report evaluates the impact that the traffic expected to be generated by the proposed industrial development will have on the adjacent roads including:

- Bass Highway / Massy-Greene Drive Intersection
- Existing two-way & one-way access to 2-10 Massy Greene Drive
- Internal site layout

6.1 Bass Highway / Massy-Greene Drive Intersection

The Bass Highway / Massy-Greene Drive signalised intersection will experience an increase in turning traffic activity of up to 64 vph due to the proposal, see Figure 30. Appendix I shows the intersection layout used for modelling.

Signalised intersection with SIDRA 9+ software demonstrates that by 2035 the intersection will operate efficiently at:

- LOS B during the AM Peak
- LOS C- during the PM Peak

These LOS demonstrate that the impact of the proposal is minor, and the intersection will operate efficiently beyond 2035. See Appendix B for Austroads Levels of Service definitions.

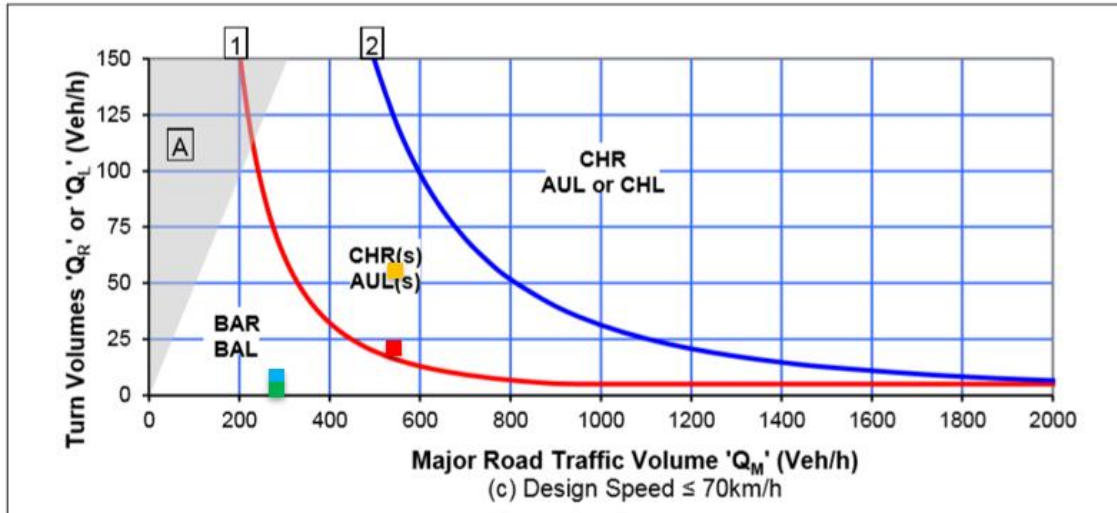
LOS C is a very acceptable LOS, and it is also noted that the traffic volumes used in the analysis are conservative. It is concluded that there is no traffic capacity issue with the proposal.

6.2 Access warrants for 2-10 Massy-Greene Drive

The access layouts for 2-10 Massey-Greene Drive property accesses should take into account the standard of the road, speed limit, through & side road traffic consistent with Austroads Guide to Traffic Management Part 6: Intersections, Interchanges and Crossings – 2020. Figure 31 shows the warranted layout.



Figure 31 – Austroads Warrant for Massy-Greene Drive / Access B junction 2035.



Turns Movements 2035 (vph)	Massy-Greene	
	AM	PM
Right to	54	24
Combined thru	532	528
Left to	7	3
Following thru	285	285

Source: Austroads GTM Part 6-2020

From projected turning data shown in Figure 31, technically a CHR(s) right turn facility and Basic Left (BAL) access layout is warranted at peak times.

An Austroads CHR(s) and BAL junction exists, see Figures 22 & 23.

6.3 Access Sight Distance Summary

Sight distance requirements are summarised in Figure 32.

Figure 32 – Sight distance summary

Junction Major Rd - Minor Rd	Speed Limit (km/h)	Speed Environment (km/h)	Road frontage sight distance			
			Austroads SISD (m)	Available		AS/NZS 2890.1 SSD (m)
				Left(m)	Right(m)	
2-10 Massy-Greene - Access A	60	60	123	65	150	65

Austroads Compliant

Not Relevant - Entrance Only

AS/NZS 2890.1 Compliant



6.4 Tasmanian Subdivision Guidelines and Transport Planning

No issues have been identified.

6.5 Proposed Internal Traffic Management.

Consistent with the General Industrial Zoning of the development site, the proposed roads and internal road layout are designed to accommodate the design vehicles in this case a 19m Triaxle Semitrailer.

The internal junctions provided have adequate width to cater for the design vehicle swept paths. The proposed layout and swept paths for the internal access road at the proposed warehouse are shown on the master plans attached in Appendix A.1.

The one-way internal road will have a minimum width of 4.8m which is sufficient for one-way operation with the design vehicle.

There will be some side accesses leading to the warehouses, workshops and storage areas which will have sufficient width for turning paths of commercial vehicles in line with requirements of AS2890.2 – Off Street Commercial Vehicle Facilities.

No Entry signs are recommended at the proposed One-Way entrance points, see Figure 33, to manage internal traffic circulation. Also see Appendix L and M.

Figure 33 – Proposed One Way Entrance Signage & Pavement Arrows



Appendix L.8 – Looking East along existing driveway



Need No Entry R2-4A signs and Thermoplastic Pavement arrows to clarify One Way

Appendix L.9 – Looking East along driveway towards Massy Greene Drive



Need No Entry R2-4A signs and Thermoplastic Pavement arrows to clarify One Way

6.6 Proposed property access standard

The proposed driveway should be constructed to broadly comply with LGAT standard drawing TSD-R09 Urban Road Driveways with heavy duty reinforcement to cater for design vehicles i.e heavy commercial vehicles. LGAT standard drawings are available online at:

[Tasmanian-Municipal-Standards-Drawings-v3-December-20202.pdf \(lgat.tas.gov.au\)](https://www.lgat.tas.gov.au/standards-drawings)

6.7 Impacts on road users.

6.7.1 Heavy vehicles including Public Transport

No impact.

6.7.2 Pedestrians and Cyclists

No impact.

6.7.3 Motorcyclists

Minimal impact.



6.8 Other impacts

6.8.1 Environmental

No applicable environmental impacts were identified in relation to:

- Noise, vibration or visual impact
- Community severance, pedestrian amenity
- Hazardous loads, air pollution or ecological impacts
- Heritage and Conservation

6.8.2 Street Lighting and Furniture

Adequate street lighting exists on Massy-Greene Drive.



7. Tasmanian Planning Scheme - Burnie

7.1 Parking and Sustainable Transport Code C2

C2.5.1 Car parking numbers

Acceptable Solution A1: *The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:*

- (a) The site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash in lieu) must be in accordance with that plan,*
- (b) The site is contained within a parking precinct plan and subject to Clause C2.7,*
- (c) The site is subject to Clause C2.5.5; or*
- (d) It relates to an intensification of an existing use or development or a change of use where:*
 - i. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional onsite car parking is required; or*
 - ii. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:*

$$N=A+(C-B)$$

N = Number of on-site car parking spaces required

A = Number of existing on-site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1.

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.

Proposed parking and pedestrian provisions are summarised in Figure 34.

Table C2.1 Parking Space requirements are summarised in Figure 35.



Figure 35 – Parking Space Requirements Summary (69 car & 4 bicycle spaces)

Lease Holder	Building Area (m2)	Staff	Use	Car Parking Space Rate	Table C2.1 Parking	
					By Area	By Staff
Proposed warehouse	13,800	40	Manufacturing and Processing	1/200 m2 or 2/3 staff	69	27

Lease Holder	Building Area (m2)	Staff	Use	Bicycle Parking Space Rate	Table C2.1 Parking
Proposed warehouse	13,800	40	Manufacturing and Processing	1/5 staff	8

A1 is not satisfied, see Figure 35, as 69 car parking spaces are required and 43 are proposed.

Performance Criteria P1.1: *The number of on-site car parking spaces for uses excluding dwellings, must meet the reasonable needs of the use, having regard to:*

(a) *The availability of off-street public car parking spaces within reasonable walking distance of the site.*

No other off street public parking is available.

(b) *The ability of multiple users to share spaces because of:*

(i) *variations in car parking demand over time; or*

(ii) *efficiencies gained by consolidation of car parking spaces*

This is not considered likely.

(c) *The availability and frequency of public transport within reasonable walking distance of the site.*

There are no metro bus services on Massy-Greene Drive.

(d) *The availability and frequency of other transport alternatives.* Other transport alternatives are car-pooling, cycling, walking and motorcycling.

(e) *Any site constraints e.g. exist. buildings, slope, drainage, vegetation & landscaping.* Site constraints affect the building layout and design.

(f) *The availability, accessibility, and safety of on -street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity.*

On street parking within walking distance of the site is available.

(g) *The effect on streetscape.* Proposal has negligibly impact.



- (h) *Any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.*

Parking appraisal is part of this Traffic Impact Assessment. Figure 35 shows estimated parking demand based on Table C2.1.

However, the Straitlink proposal is not expected to generate the parking demand predicted using Table C2.1 rates and the proposed 43 car parking spaces are considered adequate for the expected need. **P1.1 is satisfied.**

C2.5.2 Bicycle parking numbers

Acceptable Solution A1: Bicycle parking spaces must:

- (a) Be provided on the site or within 50m of the site; and*
- (b) Be no less than the number specified in Table C2.1.*

Table C2.1 for the proposed warehouse requires 8 bicycle parking spaces which will be provided. **A1 is satisfied.**

C2.5.3 Motorcycle parking numbers

Acceptable Solution A1: The number of on-site motorcycle parking spaces for all uses must:

- (a) Be no less than the number specified in Table C2.4. and*
- (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle spaces is maintained.*

Table C2.4 has requirement for 1 motorcycle space where the number of required car parking spaces is 21-40 plus 1 space for every additional 20 car parking spaces. As 69 car parking spaces are required proposed, 3 motorcycle parking spaces are required. **A1 is satisfied.**

C2.5.4 Loading Bays

Acceptable Solution A1: A loading bay must be provided for uses with a floor area of more than 1,000m² in a single occupancy.

Loading areas are proposed. **A1 is satisfied.**



C2.6.1 Construction of parking areas

Acceptable Solution A1: All parking, access ways, manoeuvring and circulation spaces must:

- (a) be constructed with a durable all-weather pavement,*
- (b) be drained to the public stormwater system, or contain stormwater on the site; and*
- (c) excluding all uses in the Rural Zone, Agricultural Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Public Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.*

Sealed parking spaces and driveways are proposed with drainage to the public stormwater system. **A1 is satisfied.**

C2.6.2 Design and layout of parking areas

Acceptable Solution A1.1: Parking, accessways, manoeuvring and circulation spaces must:

(a) comply with the following:

- i. have a gradient in accordance with Australian Standard AS 2890 Parking facilities, Parts 1-6. Site has sufficient grade for driveway and carpark drainage purposes.*
- ii. Provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces. Satisfied.*
- iii. Have an access width not less than the requirements in Table C2.2.*
Table C2.2 requires access width of 5.5m for two-way operation where number of parking spaces is over 21. The existing one-way entry access width is 5.0m. The existing two-way access width is some 8m.
- iv. Have car parking space dimensions which satisfy Table C2.3.*
Table C2.3 compliant spaces are proposed.
- v. Have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces.*
Manoeuvre space requirements will be compliant with Table C2.3 are proposed. Also see Appendix K regarding turning templates.
- vi. Vertical clearance of not less than 2.1 metres above the parking surface level, Satisfied.*
- vii. Excluding a single dwelling, be delineated by line marking or other clear physical means. Satisfied.*

A1.1 is satisfied.



Acceptable Solution A1.2

Parking spaces provided for use by persons with a disability must satisfy the following:

- (a) Be located as close as practical to the main entry point to the building. Satisfied.*
- (b) be incorporated into the overall car park design. Satisfied.*
- (c) be designed and constructed in accordance with Australian/ New Zealand Standard AS/NZS 2890.6-2009 Parking facilities - Off-street parking for people with disabilities.*

AS/NZS 2890.6 indicates 1 accessible space should be provided per 50 car parking spaces as a guideline. 43 car parking spaces are proposed including one accessible space. **A1.2 is satisfied.**

C2.6.3 Number of accesses for vehicles

Acceptable Solution A1

The number of accesses provided for each frontage must:

- (a) be no more than 1; or*
- (b) no more than the existing number of accesses whichever is greater.*

The proposal involves an existing two-way access and one-way access.

A1 is satisfied.

C2.6.5 Pedestrian access

Acceptable Solution A1.1

Applies to uses that require 10 or more car parking space must:

- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
 - i. a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or*
 - ii. protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and**
- (b) be signed & line marked at points where pedestrians cross access ways or parking aisles.*

69 car parking spaces are required accordingly compliant footpath is required and has been provided, see Figure 34. **A1.1 is satisfied.**

Acceptable Solution A1.2: *In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a minimum width of 1.5m and a gradient not exceeding 1 in 14 is required from those spaces to the main entry point to the building.*

A1.2 is satisfied, see Figure 34.



Code C2.6.6 Loading Bays

Acceptable solution A1: The area and dimensions of loading bays and access way areas must be designed in accordance with AS2890.2 – 2002, Parking Facilities, Part2: Parking facilities -Off Street commercial vehicle facilities, for the type of vehicles likely to use the site.

Loading Bays compliant with AS2890.2 are proposed with adequate width and grades for design vehicle access i.e 26m B Double. **A1 is satisfied.**

Acceptable solution A2: The type of commercial vehicles likely to use the site must be able to enter, park and exit the site in a forward direction in accordance with AS2890.2-2002, Parking Facilities, Part2: Parking Facilities -Off Street commercial vehicle facilities.

Adequate road width and grades are provided for the design vehicle to enter and exit the site in a forward direction. **A2 is satisfied.**

7.2 Road and Railway Assets Code C3

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction.

Acceptable Solution A1.1 – Satisfied as no new access to the Bass Highway or Massy Greene Drive is proposed. See Appendix O regarding BCC Statement of Compliance Request. A Statement of Compliance Request was submitted 26th Aug 2025, see Appendix P.

Acceptable Solution A1.2 – For a road, excluding a Category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.

A1.2 is not applicable as existing accesses are proposed.

Acceptable Solution A1.3 – For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.

Not applicable as no rail network is impacted, see Figures 3.0 to 3.6 and Appendix N, especially Appendix N.7. The developer does not propose to use the existing gate to the railway line. & 3.1.

Acceptable solution A1.4: Vehicular traffic to and from the site, using and existing vehicle crossing or private level crossing will not increase by more than:

(a) *The amounts in Table C3.1*

(b) *Allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road; and*



- **Bass Highway / Massy-Greene Drive junction**

Existing turning movements total to 527 & 572 vph during the AM & PM peaks respectively. Estimated turning movement increase due to the proposal is 63 vph during the AM & PM peaks respectively.

- **Existing Two-Way access to 2-10 Massy-Greene Drive (Access A)**

Existing turning movements total 65 vph during the AM & PM peaks respectively. Estimated turning movement increase due to the proposal is estimated at 20 & 50 vph during the AM & PM peaks respectively.

- **Proposed One-Way access to 2-10 Massy-Greene Drive (Access B)**

Estimated turning movement increase due to the proposal is estimated at 20 & 50 vph during the AM & PM peaks respectively.

A1.4 is not satisfied for any of the impacted accesses and junctions, from Table C3.1 as the proposal involves more than 10 vehicle movements per day for vehicles up to 5.5m in length on a major road (Bass Highway & Massey-Greene Drive).

Performance Criteria P1: *Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:*

- (a) any increase in traffic caused by the use.
- (b) the nature of the traffic generated by the use.
- (c) the nature of the road.
- (d) the speed limit and traffic flow of the road.
- (e) any alternative access to a road.
- (f) the need for the use.
- (g) any traffic impact assessment; and
- (h) any advice received from the rail or road authority.

A signalised and fully channelised intersection exists at the Bass Highway / Massy-Greene Drive junction with capacity to cope with increased traffic due to the proposal and continue to operate at between LOS B and C by 2035.

Austroroads Channelised Right short CHR(s) and Basic Left turn BAL access layouts are warranted at the 2-10 Massy-Greene Drive property. The existing One-Way entry access meets these requirements. **P1 is satisfied.**

Acceptable solution A1.5: *Vehicular traffic must be able to enter and leave a major road in a forward direction.* **A1.5 is satisfied.**



C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area - Not applicable as the proposal does not involve sensitive uses.

C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area

Acceptable Solution A1: A lot or a lot proposed in a plan of subdivision, intended for a sensitive use must have a building area for the sensitive use that is not within a road or railway attenuation area.

Development is proposed within a General Industrial Zone within 20m of the Tasrail Western Line ie within the 50m railway attenuation area. **A1 is not satisfied.**

Performance Criteria P1

A lot, or a lot proposed in a plan of subdivision, intended for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise the effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:

- (a) the topography of the site;
- (b) any buffers created by natural or other features;
- (c) the location of existing or proposed buildings on the site;
- (d) the frequency of use of the rail network;
- (e) the speed limit and traffic volume of the road;
- (f) any noise, vibration, light and air emissions from the rail network or road;
- (g) the nature of the road;
- (h) the nature of the intended uses;
- (i) the layout of the subdivision;
- (j) the need for the subdivision;
- (k) any traffic impact assessment;
- (l) any mitigating measures proposed;
- (m) any recommendations from a suitably qualified person for mitigation of noise; and
- (n) any advice received from the rail or road authority.



- a. The topography of the site is relatively flat with the Western Rail line grade separated from Massy-Greene Drive with a road overpass.
- b. The Western Rail Line / Massy Greene Drive Overpass acts as a buffer.
- c. The proposed driveway would be on the North side of the Western Line railway corridor.
- d. The Western Rail line operates as a freight line.
- e. Massy-Greene Drive traffic volume is estimated at 4,300 vpd (2023) in an 60km/h zone.
- f. Noise less than 63 dB is estimated due to grade separation between the proposed driveway and Western Line.
- g. The proposed access is One Way providing for some 50 vph of traffic composed mainly of commercial vehicles.
- h. The proposed development is for workshop, warehouse and storage consistent with the Tasmanian Planning Scheme Land Use Zoning – Burnie for General Industrial Land Uses.
- i. The proposed development layout suits the contours and space available at the site.
- j. The development need is based on commercial business rationale.
- k. This traffic impact assessment determines that subject to the recommendations contained in this report, the proposal will allow continued safe and efficient operation of Massy-Greene Drive and is supported on traffic grounds.
- l. No mitigations are expected to be necessary to mitigate road noise concerns.
- m. A noise assessment report is not necessary as habitable development is not proposed.
- n. No advice has been received from the DSG or Tasrail

P1 is satisfied.



7.3 Department of State Growth requirements

DSG review of TIA

These reviews are required to:

- consider proposals and whether the TIA prepared satisfies DSG requirements.
- resolve any issues so the TIA can be finalised.
- enable the TIA endorsement provided by DSG to be communicated to Council as part of the Development application process.

These reviews are usually arranged by the TIA author. The email address for submissions is:

Development@stategrowth.tas.gov.au

Crown landowner consent

This is to provide DSG to opportunity to check alignment of proposals with DSG objectives for the road. If the proposal aligns with DSG objectives Crown Land Consent is issued by DSG. Crown Landowner Consent is required where there is a proposed change in use of property adjacent to a state road. The website for Crown Landowner Consent is:

https://www.transport.tas.gov.au/road/permits/crown_landownerconsent

Access works permits

Developers must obtain an access works permit from DSG for proposed work within a state road reservation. Applications need to include:

- suitably design plans detailing the proposal and services affected.
- relevant design calculations for stormwater management and pavement design
- a traffic impact assessment

The website for access works permit applications is:

<https://www.transport.tas.gov.au/road/permits/road-access>

Summary of DGS requirements

The section of Massy-Greene Drive impacted by the proposal is not part of the State Road network, see Appendix C.

The developer will not need to apply for Crown Landowner consent.

An access works permit is not required from DSG as no works are proposed in a State Road reservation.

DSG confirmation of acceptability of this TIA is not required to be added in Appendix G.



8. Recommendations and Conclusions

This traffic impact assessment has been prepared to assess the proposed warehouse expansion and container storage development at 2-10 Massy-Greene Drive, South Burnie.

The assessment has reviewed traffic activity at the site, existing road conditions, road safety, crash history, Austroads junction warrants and Tasmanian Planning Scheme – Burnie - Parking & Sustainable Transport Code C2 and Road & Railway Assets Code C3 requirements.

The proposal is projected to increase peak traffic activity at the site to some 70 vph across the existing accesses.

5 year reported crash history provides no evidence of a crash propensity at the proposed accesses to the property. From road safety review no traffic hazards or safety concerns were detected. From Austroads Safe System Assessment, the Massy-Greene Drive approaches are assessed with a low crash risk.

The existing two-way access has a CHR(s) and BAL layout adequate for the proposed use.

The existing one-way access has a CHR(s) and BAL layout adequate for the proposed use.

Intersection analysis of the Bass Hwy / Massy Greene Drive intersection demonstrates that the intersection will operate at LOS B & C during the AM and PM peaks by 2035. This indicates that the intersection will operate safely and efficiently beyond 2035.

Evidence is provided to demonstrate the development satisfies the Tasmanian Planning Scheme - Burnie Codes C2 & C3.

A Statement of Compliance Request has been submitted to BCC via the BCC website.

Recommendations:

- Install No Entry Signs and thermoplastic pavement arrows at the One-Way entrance points to prevent contra flow & head on crashes. See Appendix L.8 & L.9.
- Install No Entry Signs and thermoplastic pavement arrows at the site exit to prevent contra flow and head on crashes. See Appendix M.3.
- 8 bicycle parking spaces for staff to be provided / shown on the plans.

Overall, it has been concluded that Massy -Greene Drive will continue to operate safely and efficiently with the proposal subject to the above recommendations.

Based on the findings of this report the proposal is supported on traffic grounds.



Appendices



Appendix A – Development Plans



Appendix A.1 – Proposed Warehouse Plans

CLIENT:
STUBBS & LIGHT INVESTMENTS PTY LTD.

PROJECT:
STRAITLINK - BURNIE TERMINAL

ADDRESS:
MASSY-GREENE DRIVE, SOUTH BURNIE

PROJECT No:
245019

STATUS:
PRELIMINARY / INFORMATION

ISSUED FOR / DESCRIPTION:
REVIEW / INFORMATION

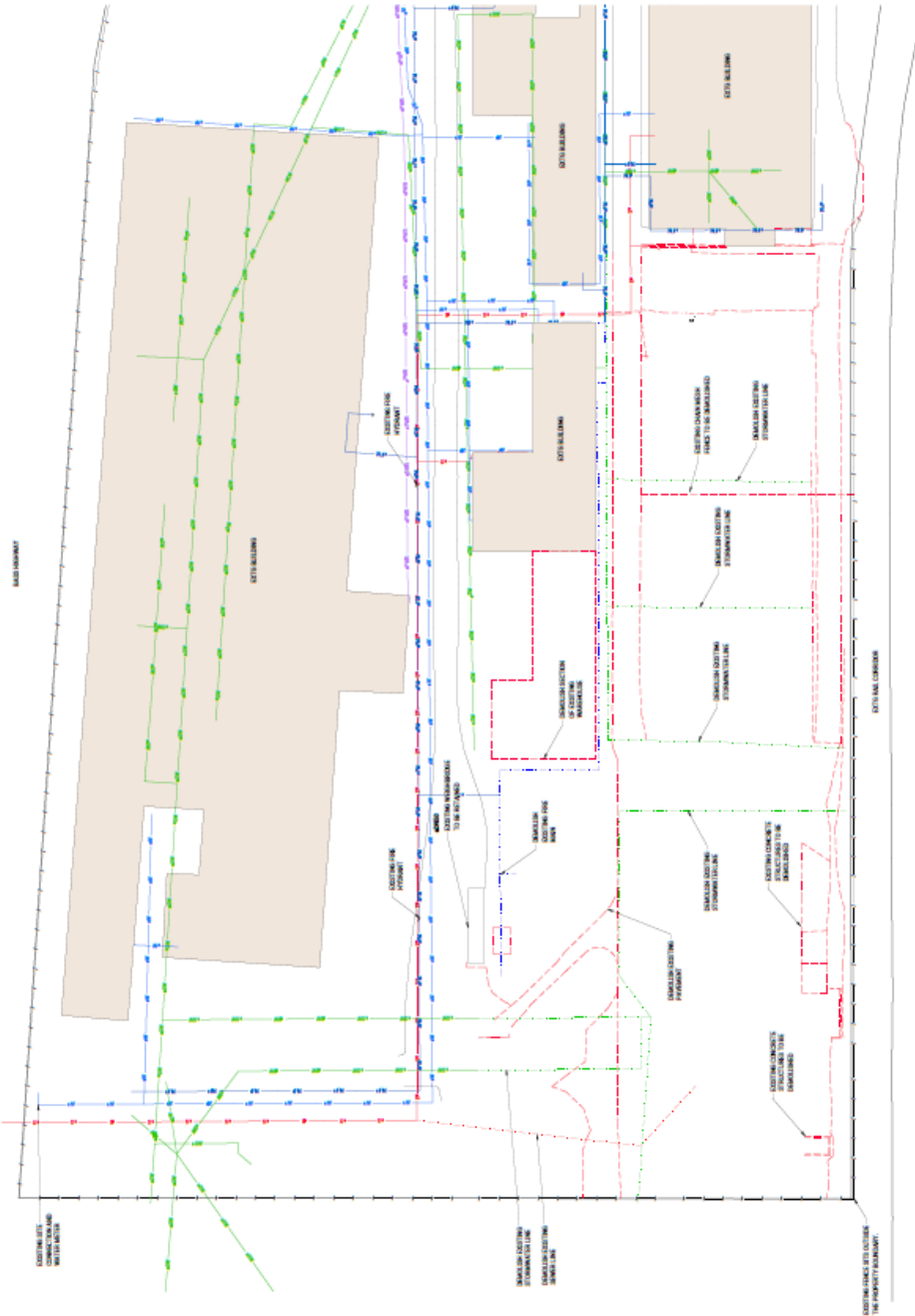
DRAWINGS:

- CW - COVER SHEET
- CW1 - CIVIL NOTES
- C101 - EXISTING SITE / DEMOLITION PLAN
- C201 - BULK EARTHWORKS PLAN
- C401 - CIVIL WORKS PLAN
- C402 - CIVIL LEVELS PLAN
- C411 - CIVIL WORKS LONG SECTIONS
- C421 - CIVIL WORKS CROSS SECTIONS - SHEET 1
- C422 - CIVIL WORKS CROSS SECTIONS - SHEET 2
- C423 - CIVIL WORKS CROSS SECTIONS - SHEET 3
- C501 - DRAINAGE PLAN
- D001 - WATER RETENTION PLAN
- C101 - SECTIONS & DETAILS

1	COVER SHEET	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: COVER SHEET
2	CIVIL NOTES	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: CIVIL NOTES
3	EXISTING SITE / DEMOLITION PLAN	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: EXISTING SITE / DEMOLITION PLAN
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6	CIVIL LEVELS PLAN	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: CIVIL LEVELS PLAN
7	CIVIL WORKS LONG SECTIONS	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: CIVIL WORKS LONG SECTIONS
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11	DRAINAGE PLAN	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: DRAINAGE PLAN
12	WATER RETENTION PLAN	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: WATER RETENTION PLAN
13	SECTIONS & DETAILS	DATE: 24/05/2024	SCALE: AS SHOWN	STATUS: PRELIMINARY / INFORMATION	DATE: 24/05/2024	PROJECT: STRAITLINK - BURNIE TERMINAL	CLIENT: STUBBS & LIGHT INVESTMENTS PTY LTD.	TITLE: SECTIONS & DETAILS

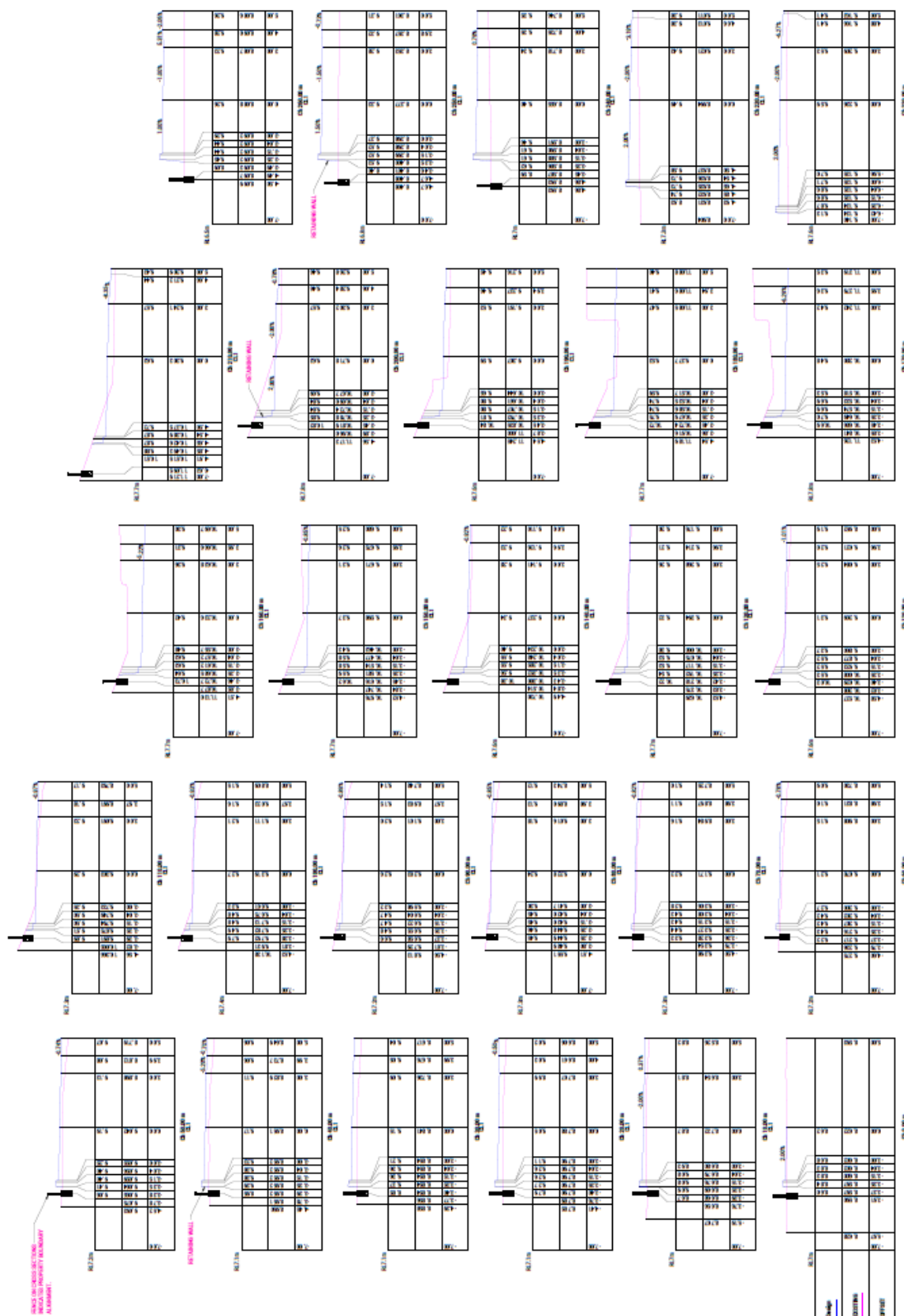


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 2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL COUNCIL'S REQUIREMENTS AND THE LOCAL COUNCIL'S TRAFFIC AND TRANSPORTATION POLICY.
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 10. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL COUNCIL'S REQUIREMENTS AND THE LOCAL COUNCIL'S TRAFFIC AND TRANSPORTATION POLICY.
- LEGEND:**
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 - EXISTING SEWER MAIN
 - EXISTING WATER MAIN
 - EXISTING COMMUNICATIONS MAIN
 - EXISTING ELECTRICAL MAIN
 - EXISTING SERVICES LINE TO BE DEMOLISHED

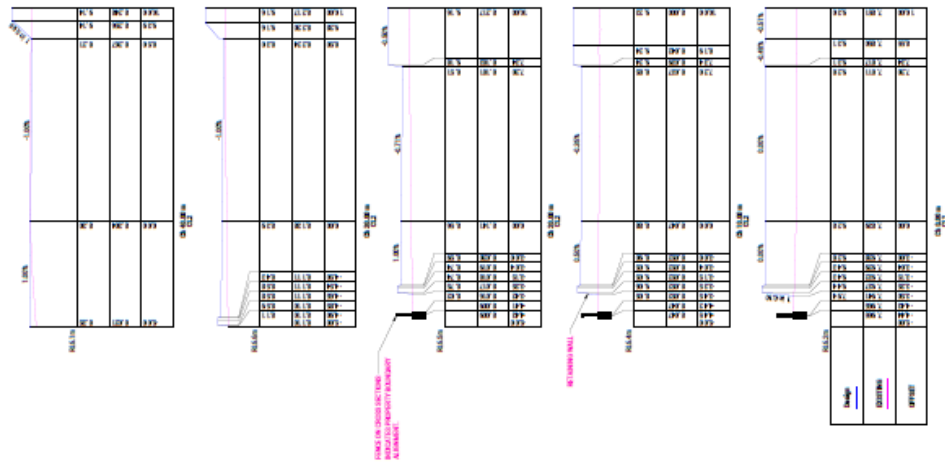


EXISTING SITE UTILITIES PLAN
SCALE 1:500

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DATE: 12/12/2023	DATE: 12/12/2023	APPROVED: MARY GREENE (DRA), DANIEL BURRAGE	APPROVED: MARY GREENE (DRA), DANIEL BURRAGE	APPROVED: MARY GREENE (DRA), DANIEL BURRAGE
PROJECT: STANTON - BEING TERMINAL	PROJECT: STANTON - BEING TERMINAL	PROJECT: STANTON - BEING TERMINAL	PROJECT: STANTON - BEING TERMINAL	PROJECT: STANTON - BEING TERMINAL



CLIENT: STUBBS ALBERT INVESTMENTS PTY LTD. PROJECT: STUBBS ROAD - BOUNDARY TERMINAL ADDRESS: MARKET GARDENS DRIVE, SKEETVILLE		TITLE: CIVIL WORKS CROSS SECTIONS - SHEET 1 SCALE: 1:50 DRAWN BY: M. J. HARRIS CHECKED BY: M. J. HARRIS PROJECT NO.: 246019 DRAWING NO.: CA21	
rare. CONSULTANTS 27-31 LINDSEY STREET, SYDNEY NSW 1585 PH: (02) 9550 6200 WWW.RARECONSULTANTS.COM.AU			
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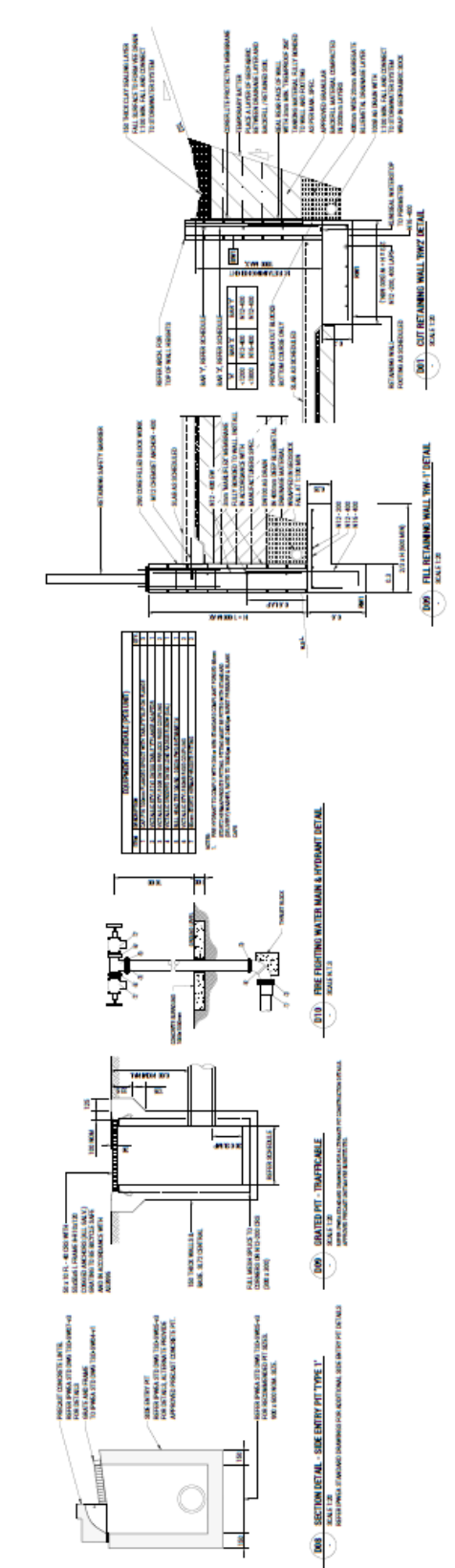
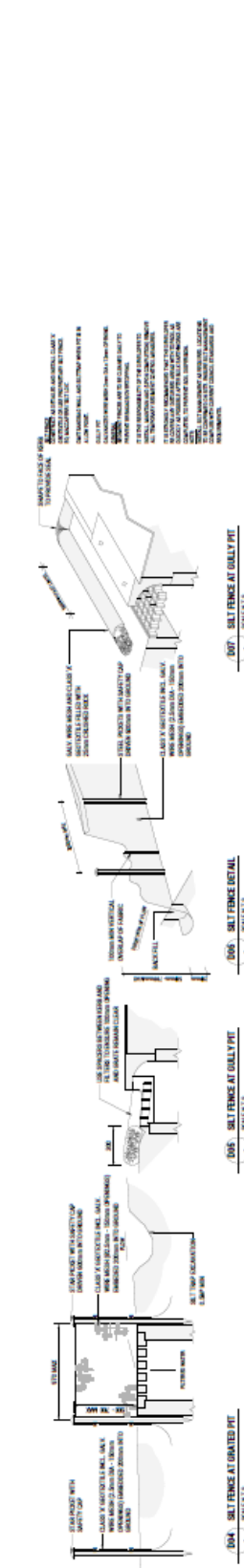
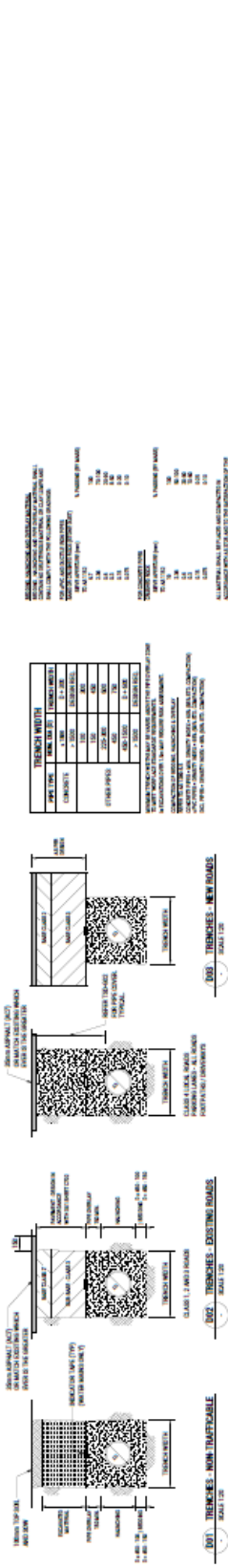
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CLIENT: STURGES & BART INVESTMENTS PPTY LTD. TITLE: CYRIL MORGAN CROSSING SETTINGS - SHEET 3
 PROJECT: STRATFORD - BUSINESS TERMINAL
 DRAWN: MADDY GREENE (DRAFT, 08/15/2024)
 CHECKED: [Name] (DATE) [Date]
 PROJECT No: 240019 SHEET No: C422 REV: E



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DATE: 08/15/2024
 DRAWN: MADDY GREENE
 CHECKED: [Name]
 PROJECT No: 240019 SHEET No: C422 REV: E



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22-23 THE BUILT TRUST
L11-L12, TRUST
NO. 100, 1000

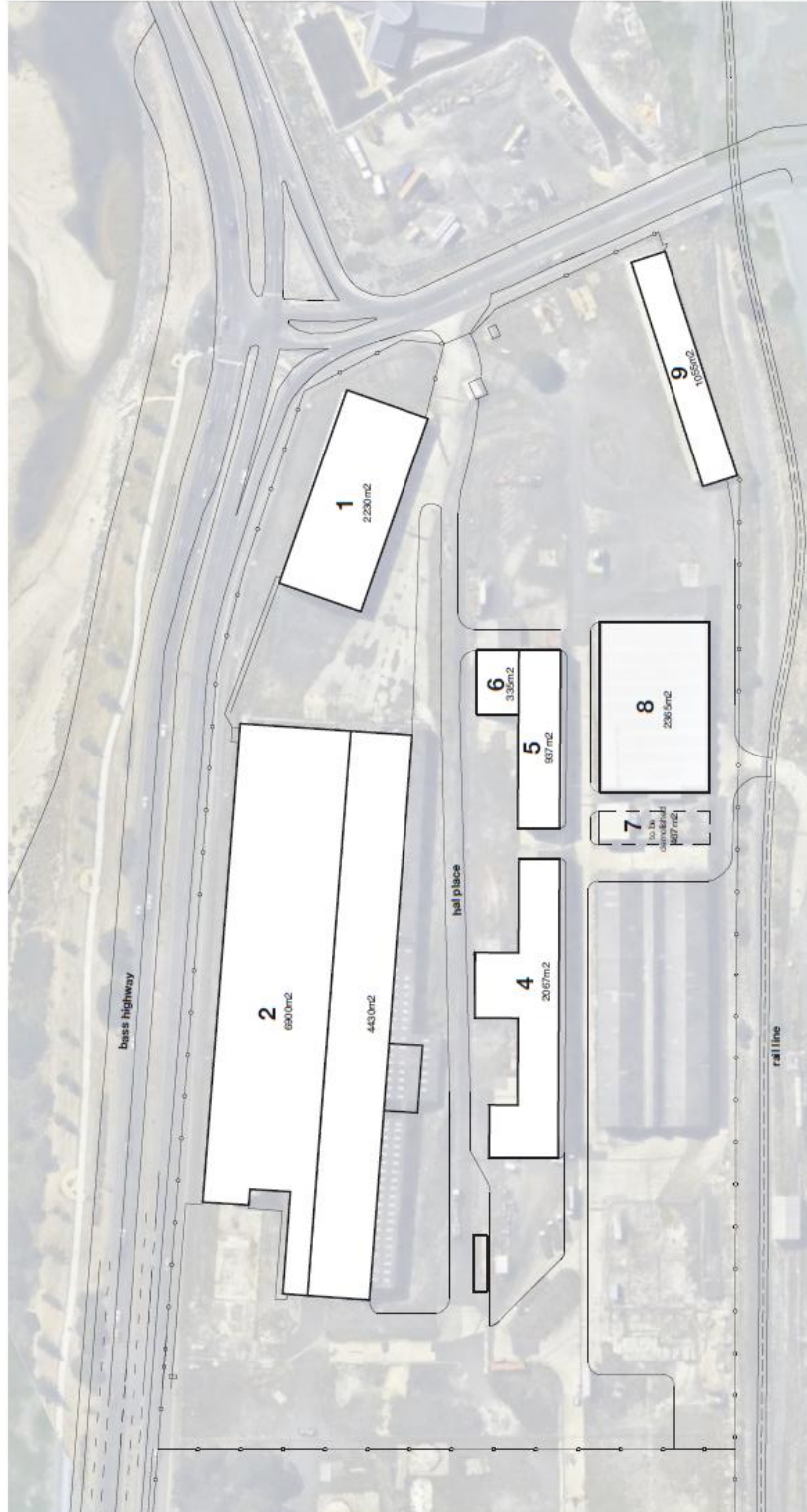
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ADDRESS: MADDY GEESE DRIVE, SOUTH COAST
PROJECT NO: 240019
DATE: 20/01/2024
SCALE: 1:20

TITLE: SECTION & DETAILS
SHEET NO: 001
SHEET SET: 01 OF 01
REV: E



info@cumulus-studio.net
 1000 Cumulus Street
 Suite 100
 Durham, NC 27701
 Tel: 919.487.1111
 Fax: 919.487.1112

Project: Concept Design
Client: Massey Greene Drive
Site Plan - Existing
 plan
 drawing no: 168358-st-03
 date: Friday, 29 July 2016
 issue: C



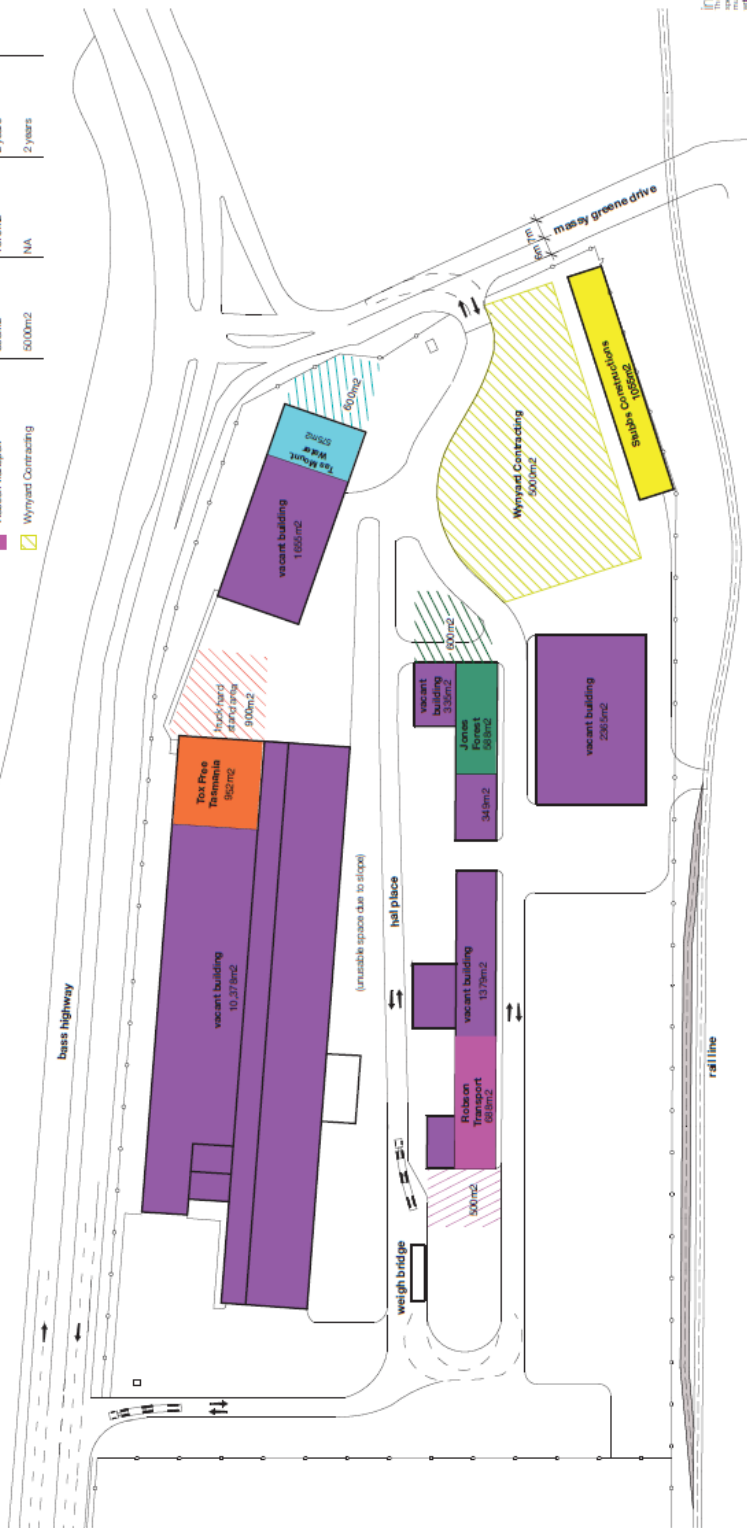
Site Plan - Existing
 1:1500



location of buildings, infrastructure and site boundaries are approximate, only detailed site survey required

Stage 2:
To be considered remainder of site as existing leases expire

Lease holder	Leased area	Area in building vacant	Leased time remaining
Terrence Mountain Water	57.5m ²	1655m ²	2 years
Tox Free Tamara	96.2m ²	1037m ²	5 years
Jones Forest Management	68.9m ²	684m ²	2 years
Robson Transport	68.9m ²	1379m ²	2 years
Wynted Contracting	50.0m ²	N/A	2 years



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Project: Concept Design
 Street: Missy Greene Drive
 Site Plan - Stage 3
 Date: 16 Feb 2018
 Drawing No: 16238-sk06
 Scale: 1:1500
 Status: C

Site Plan - Stage 3
1:1500



Location of infrastructure are approximate only detailed site survey required



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Site
 Concept Design
Address
 Messy Greene Drive
Site Services - Stormwater plan
 drawing no
 16238-sk07
 date
 Friday, 20 July 2018
 sheet
 C



Site Services - Stormwater
 1:1500

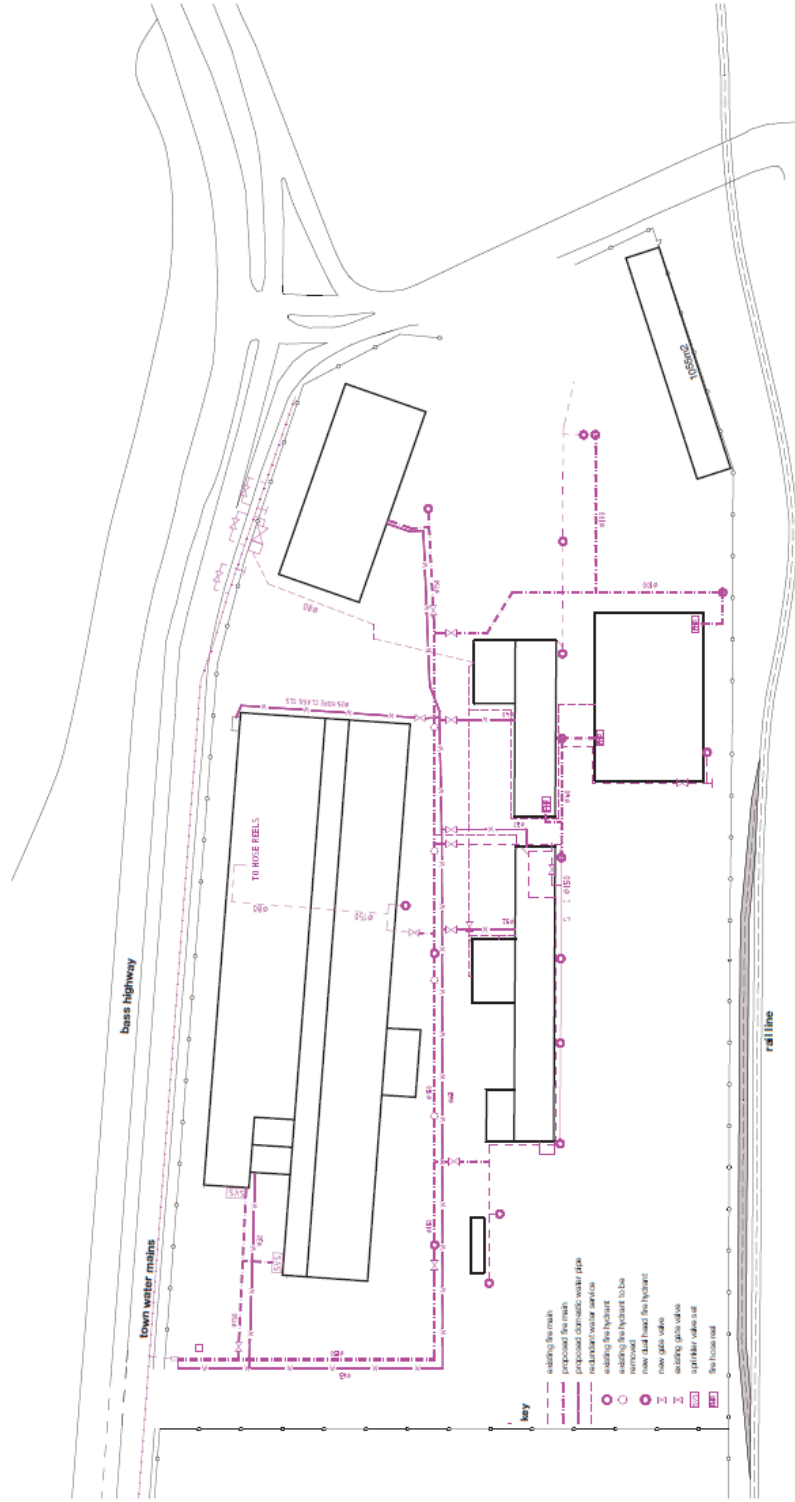


location of lift structures are approved and site survey required



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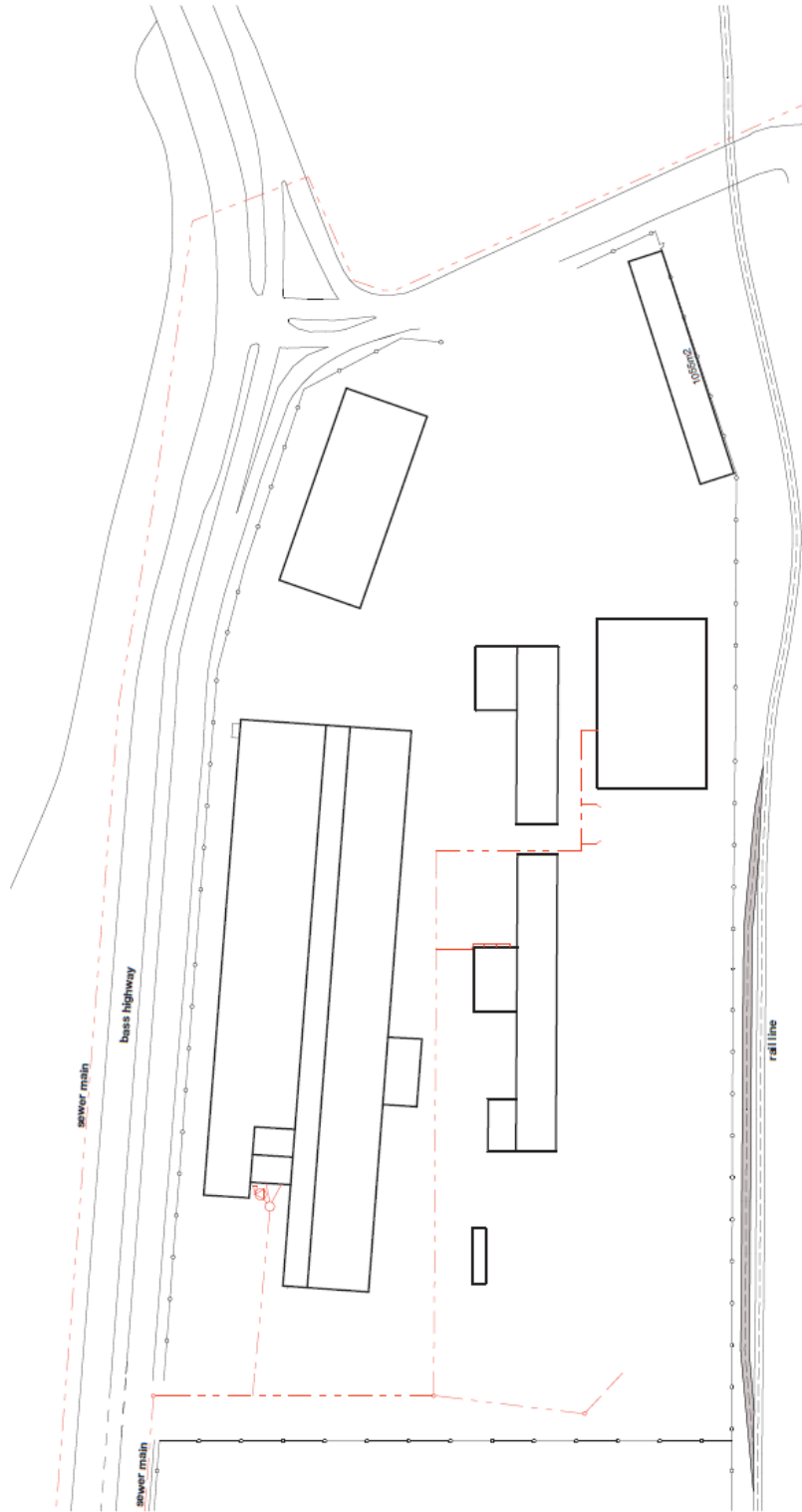
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 Project: Missy Greene Drive
 Site Services - Water
 Date: 21 July 2018
 Drawing #: 16236-sk08
 Scale: C



Site Services - Water
 1:1500



Location of infrastructure are approximate only detailed site survey required



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Site
 Concept Design

Client
 Missy Greene Drive

Site Services - Sewer plan

DATE
 Friday, 25 July 2016

DRAWING NO.
 16238-sk09

SCALE
 C

Site Services - Sewer
 1:1500

Location of infrastructure are
approximate only detailed site
study required.



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Project
Concept Design

Client
Massey Greene Drive

Site Services - Power plan

DATE
16/2/2018

DESIGNED BY
AS

DATE
Friday, 23 July 2016

SCALE
1:1500

PROJECT NO.
162238-sk.10

REVISION
C

Site Services - Power 1:1500



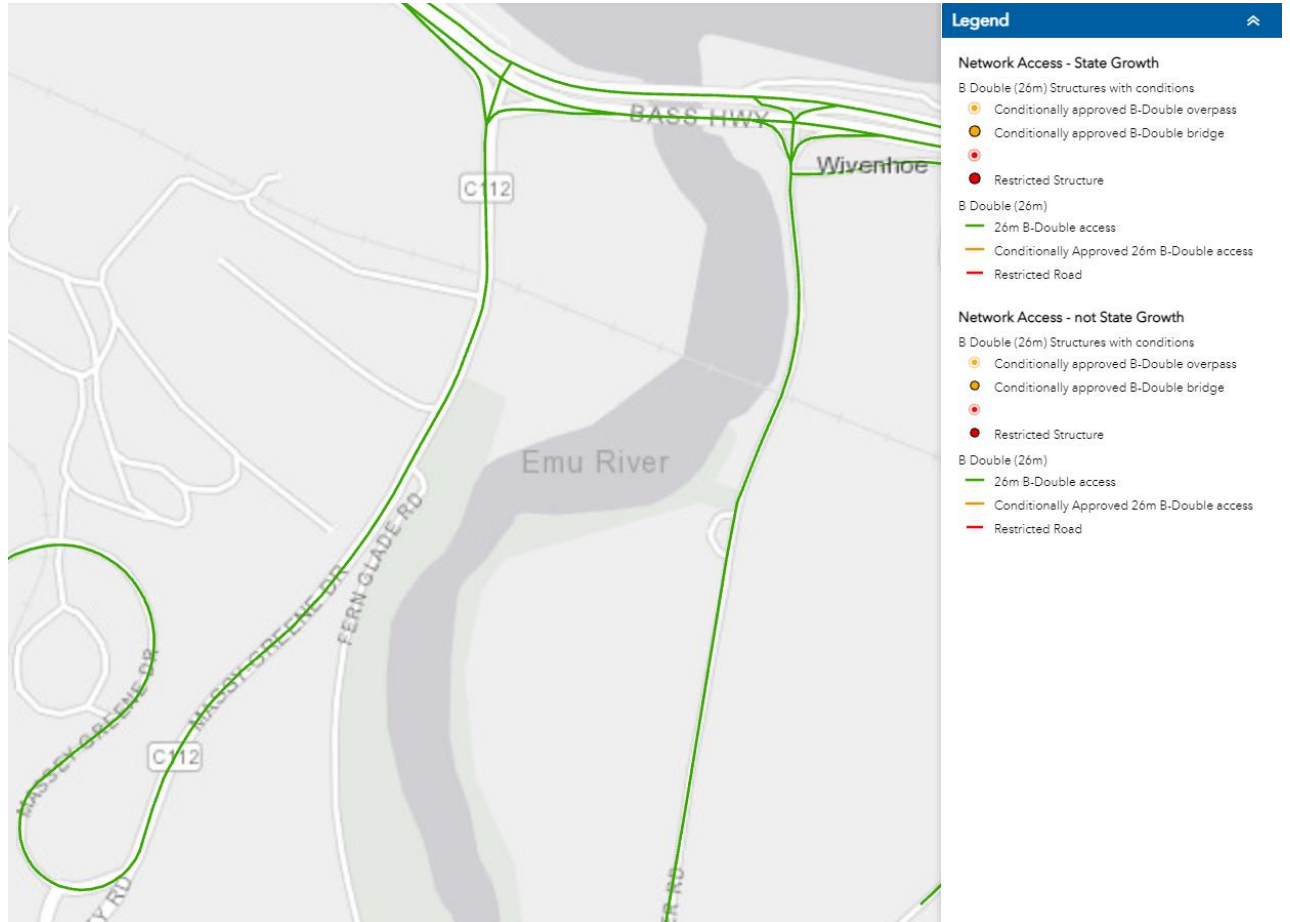
Appendix B – Austroads Level of Service descriptions

Level of service A	A condition of free-flow in which individual drivers are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to manoeuvre within the traffic stream is extremely high, and the general level of comfort and convenience provided is excellent.
Level of service B	In the zone of stable flow where drivers still have reasonable freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is a little less than with level of service A.
Level of service C	Also in the zone of stable flow, but most drivers are restricted to some extent in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of service D	Close to the limit of stable flow and approaching unstable flow. All drivers are severely restricted in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is poor, and small increases in traffic flow will generally cause operational problems.
Level of service E	Traffic volumes are at or close to capacity, and there is virtually no freedom to select desired speeds or to manoeuvre within the traffic stream. Flow is unstable and minor disturbances within the traffic stream will cause breakdown.
Level of service F	In the zone of forced flow, where the amount of traffic approaching the point under consideration exceeds that which can pass it. Flow breakdown occurs, and queuing and delays result.



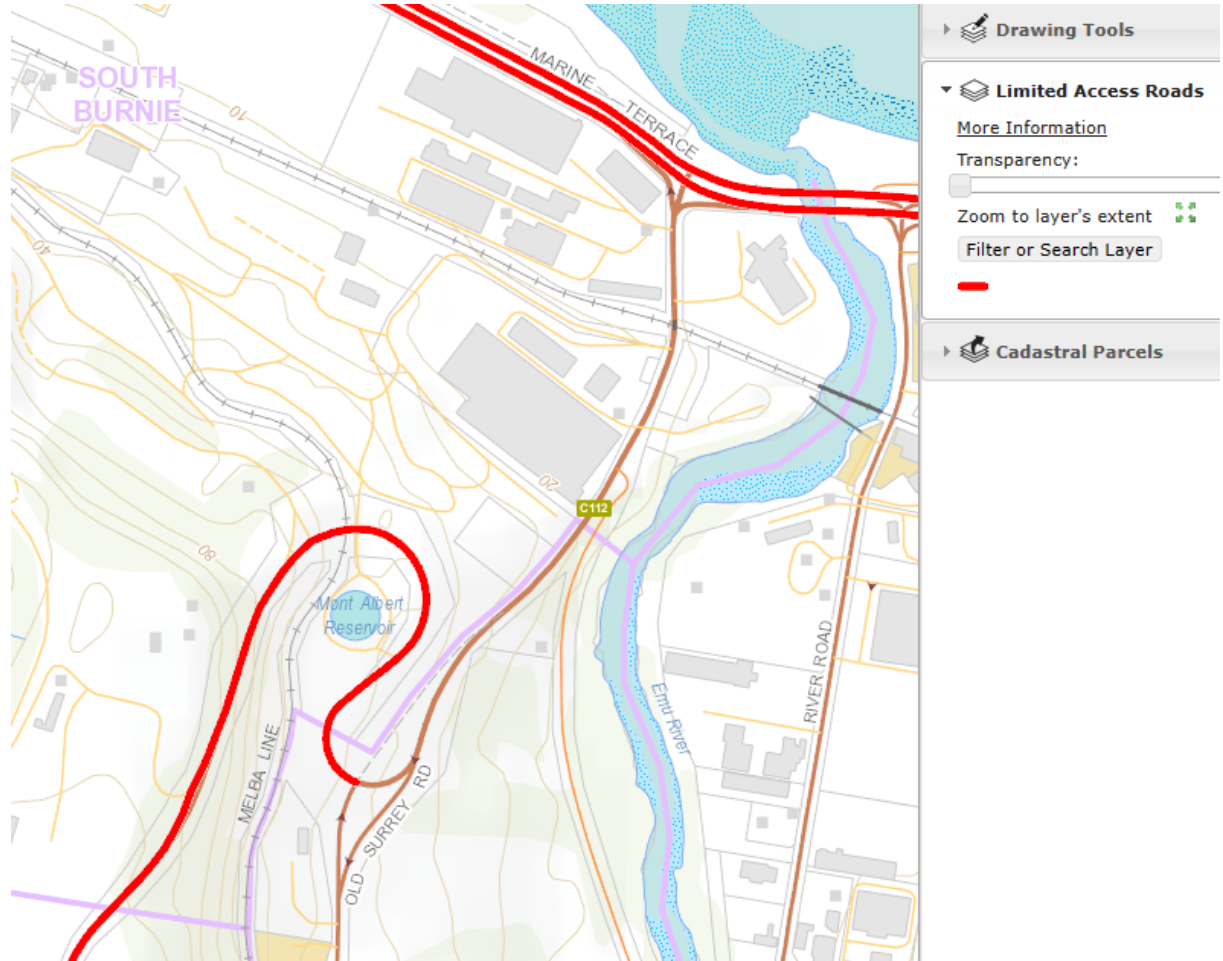
Appendix C – State Road Information

Tasmanian 26m B Double Network



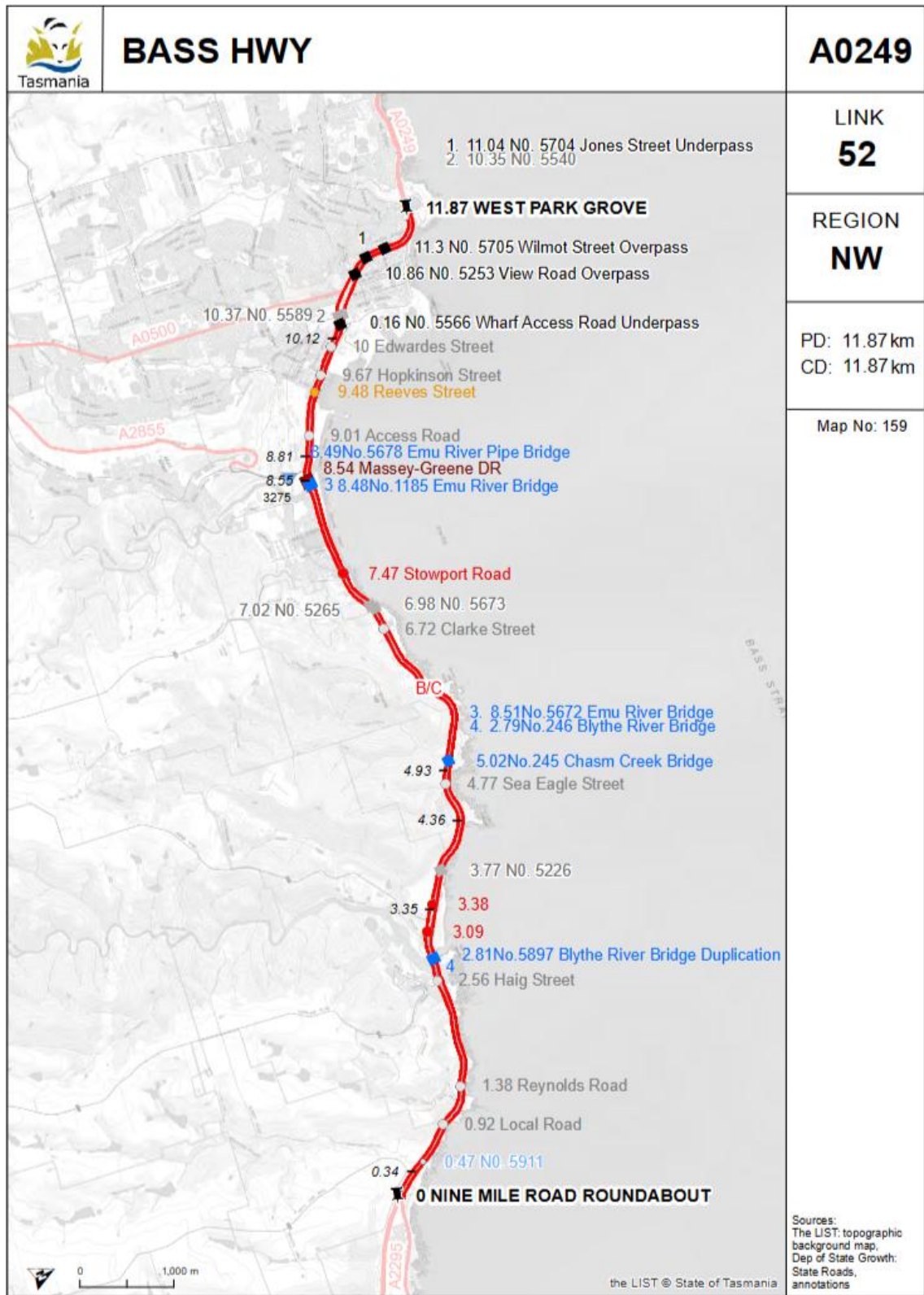


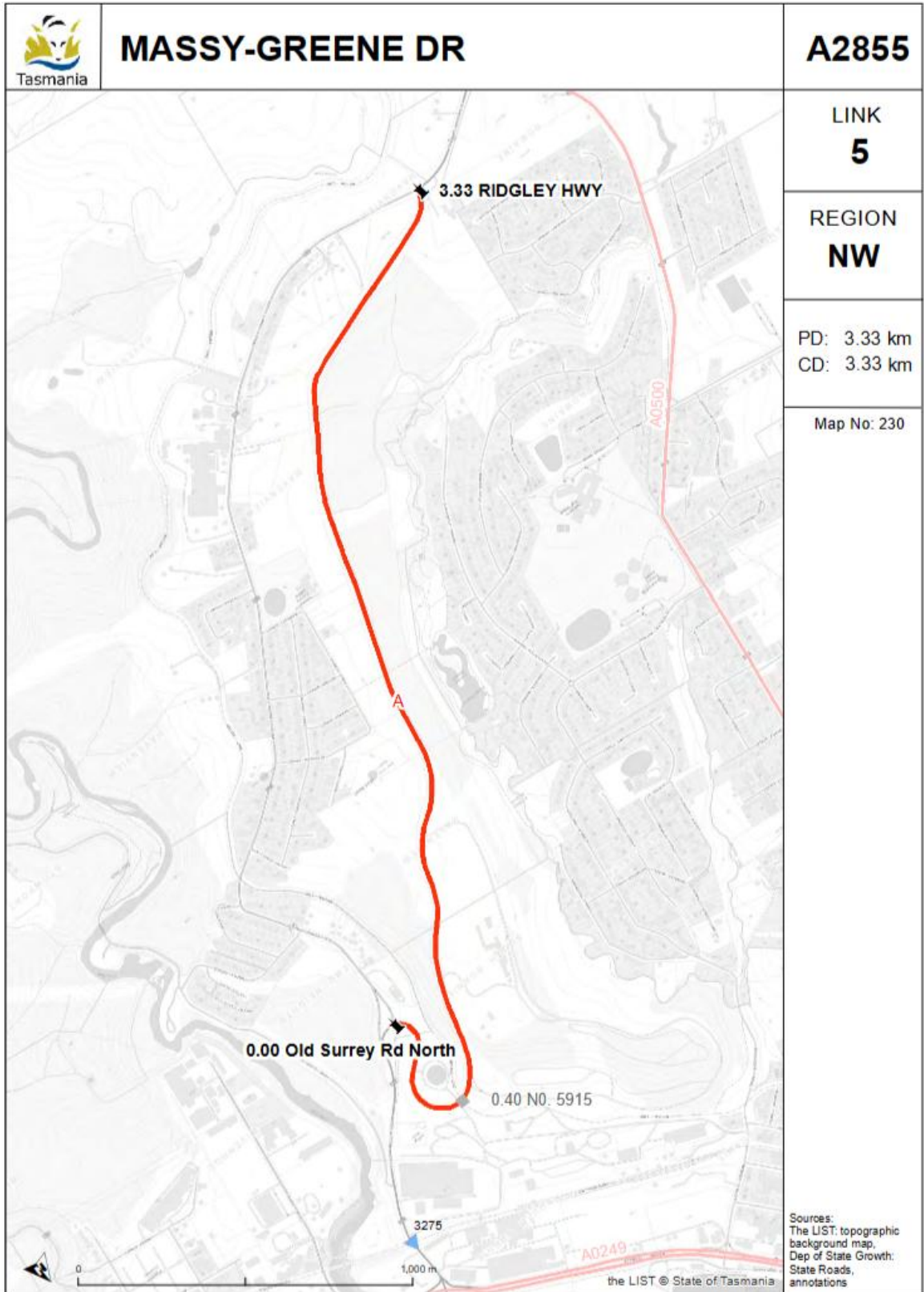
Limited Access State Road Network





Department of State Growth Link Maps





Map prepared by: State Roads, Department of State Growth.

Automated Link Map V2.1, Map created: 03/2017

Appendix D – Safe Systems Assessment

Safe System Assessment		2-10 Massy-Greene Drive Approaches						
Exposure	Run-off-road	Head-on	Intersection	Access to 2-10	Pedestrian	Cyclist	Motorcyclist	
Justification AADT 4,300 vpd (2023)	Low traffic volume, 1 reported PDO crash.	Low traffic volume, no reported crashes	High traffic volume on Bass Hwy estimated at 19,030 vpd (2023) 9 reported PDO & 3 Minor Injury Crashes. 6 of the crashes involving same lane rear end type.	Commercial vehicle route with Industrial land use	Urban Speed Limit with very low pedestrian activity	Low cyclist activity	Low motorcyclist activity	
	Score / 4	2	4	1	1	1	1	
Likelihood	12.3m wide Urban standard Council Collector Road with suitable delineation, alignment and sight distance for industrial use.	12.3m wide Urban standard Council Collector Road with suitable delineation, alignment and sight distance for industrial use.	Signalised Bass Highway/ Massy-Greene Drive junction with separate turn lanes.	General Industrial land use designed for heavy commercial vehicle access	Footpaths both sides of Massy-Greene Drive.	12.3m wide Urban standard Council Collector Road with suitable delineation, alignment and sight distance for industrial use.	12.3m wide Urban standard Council Collector Road with suitable delineation, alignment and sight distance for industrial use.	
Justification (60 km/h speed limit)	Low speed environment	Low speed environment	80 km/h speed limit on the Bass Hwy.	Low speed environment	High speed environment for pedestrians	High speed environment for cyclists	High speed environment for motorcyclists	
	Score / 4	1	3	1	3	3	3	
Severity	Score / 4	2	24	1	3	3	3	
Product	Total Score /64						Total /448	
							37	



Appendix E – Bass Highway Traffic Data

Bass Hwy, 620m East of Massy-Greene Drive

G Site 0000A0249590

A0249590

Description: Bass Highway 508m V

City: Wiver

Route number

Site Data

80

km/h

Bass Hwy (East of Massy-Greene Drive)

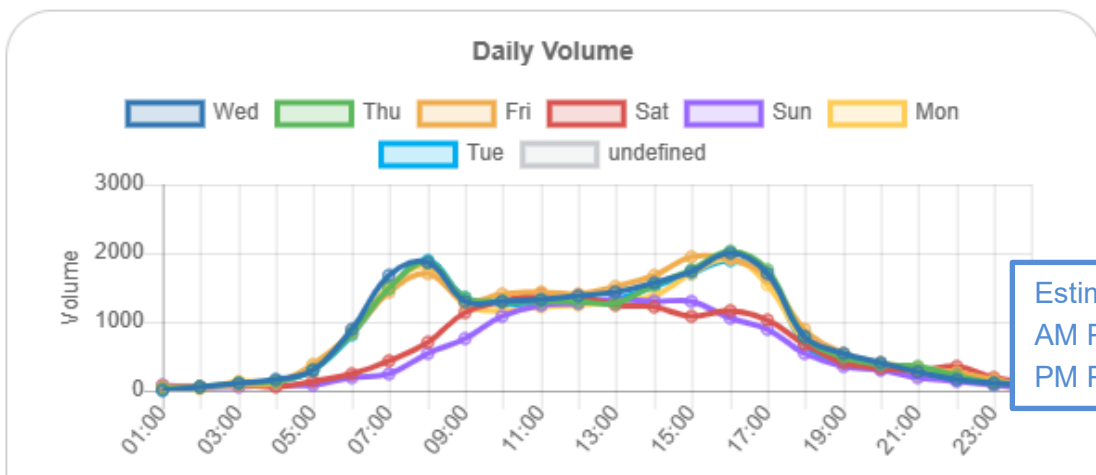
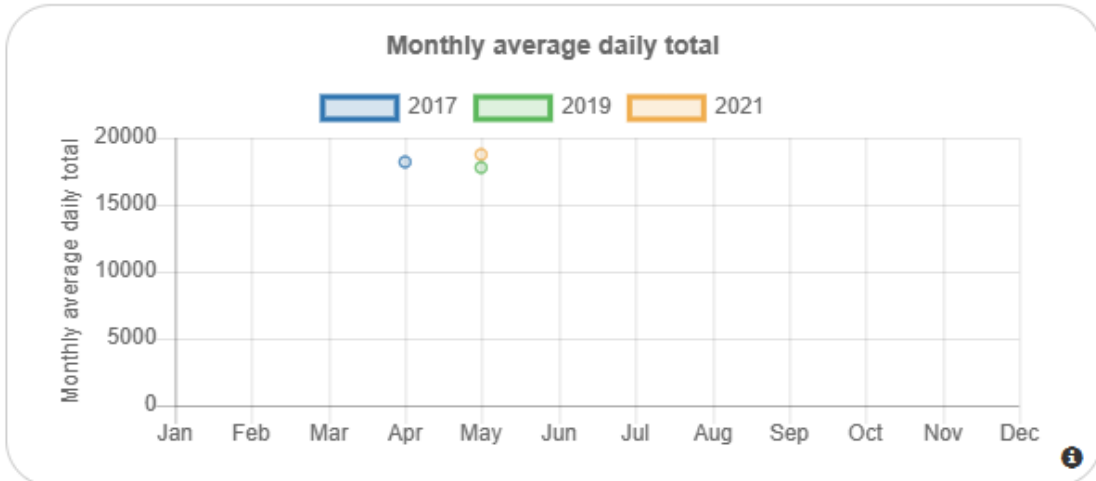
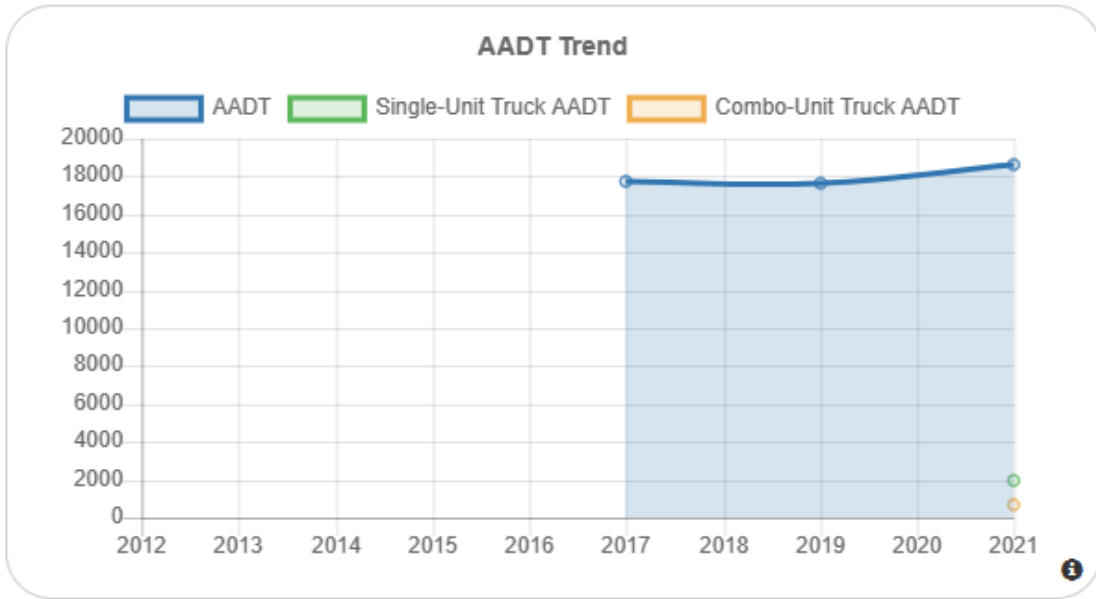
- Compound Annual Growth: 1.2%
- Commercial Veh. – 14.4 %
- 17,713 vpd (2017)
- 18,597 vpd (2021)
- 19,030 vpd (2023) -Projected
- 22,000 vpd (2035) - Projected

Traffic Statistics by Direction

Direction	Weekday average total traffic	7-day average traffic	Weekly traffic total
East	10,617	9,613	67,288
West	10,008	9,158	64,108
Total	20,625	18,771	131,396

Annual Statistics

Data Item	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021
AADT	-	-	-	-	-	17,713	-	17,662	-	18,597
% HV	-	-	-	-	-	14.9%	-	14.6%	-	14.4%



- 2021**
- AM 1,900 vph
 - East Bound 950 vph.
 - West Bound 950 vph.
 - PM Peak 2,000 vph.
 - East Bound 1,000 vph.
 - West Bound 1,000 vph.

- 2035 (Projected)**
- AM 2,240 vph
 - East Bound 1,120 vph.
 - West Bound 1,120 vph.
 - PM Peak 2,360 vph.
 - East Bound 1,180 vph.
 - West Bound 1,180 vph.



Appendix F – Massy Greene Drive Traffic Data

Turn Count Summary AM Peak April 2023

Location: Massy Greene Drive at Bass Highway, South Burnie
 GPS Coordinates: Lat=41.443854, Lon=147.141727
 Date: 2023-04-19
 Day of week: Wednesday
 Weather:
 Analyst: Sid Saxby

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
08:00	0	0	0	7	0	0	7	0	12	0	0	7	33
08:05	0	0	0	13	0	0	5	0	12	0	0	6	36
08:10	0	0	0	9	0	1	6	0	11	0	0	11	38
08:15	0	0	0	4	0	0	8	0	4	0	0	10	26
08:20	0	0	0	9	0	0	15	0	10	0	0	6	40
08:25	0	0	0	14	0	0	14	0	12	0	0	2	42
08:30	0	0	0	6	0	0	13	0	6	0	0	3	28
08:35	0	0	0	8	0	0	15	0	5	0	0	4	32
08:40	0	0	0	10	0	1	9	0	8	0	0	10	38
08:45	0	0	0	12	0	0	14	0	11	0	0	11	48
08:50	0	0	0	9	0	0	7	0	4	0	0	3	23
08:55	0	0	0	7	0	0	9	0	9	0	0	6	31
09:00	0	0	0	1	0	0	0	0	0	0	0	0	1

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
08:00	0	0	0	3	0	0	5	0	9	0	0	3	20
08:05	0	0	0	6	0	0	1	0	8	0	0	5	20
08:10	0	0	0	7	0	0	5	0	9	0	0	8	29
08:15	0	0	0	4	0	0	7	0	4	0	0	4	19
08:20	0	0	0	9	0	0	9	0	4	0	0	2	24
08:25	0	0	0	10	0	0	9	0	7	0	0	1	27
08:30	0	0	0	3	0	0	10	0	5	0	0	1	19
08:35	0	0	0	4	0	0	10	0	1	0	0	3	18
08:40	0	0	0	9	0	1	8	0	4	0	0	3	25
08:45	0	0	0	8	0	0	10	0	9	0	0	6	33
08:50	0	0	0	6	0	0	6	0	4	0	0	3	19
08:55	0	0	0	5	0	0	2	0	8	0	0	4	19
09:00	0	0	0	0	0	0	0	0	0	0	0	0	0

Truck traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
08:00	0	0	0	4	0	0	2	0	3	0	0	4	13
08:05	0	0	0	7	0	0	3	0	4	0	0	1	15
08:10	0	0	0	2	0	1	1	0	2	0	0	3	9
08:15	0	0	0	0	0	0	1	0	0	0	0	6	7
08:20	0	0	0	0	0	0	6	0	6	0	0	4	16
08:25	0	0	0	4	0	0	5	0	5	0	0	1	15
08:30	0	0	0	3	0	0	3	0	1	0	0	2	9
08:35	0	0	0	4	0	0	5	0	4	0	0	1	14
08:40	0	0	0	1	0	0	1	0	4	0	0	7	13
08:45	0	0	0	4	0	0	4	0	2	0	0	5	15
08:50	0	0	0	3	0	0	1	0	0	0	0	0	4
08:55	0	0	0	2	0	0	7	0	1	0	0	2	12
09:00	0	0	0	1	0	0	0	0	0	0	0	0	1

Massy-Greene Drive (At Bass Hwy) 2023

- AM
 - South Bound – 187vph
 - Northbound – 225 vph
- PM
 - South Bound – 217vph
 - Northbound – 235 vph

Estimated AADT – 4,300 vpd (2023)

Massy-Greene Drive (At Bass Hwy) 2035

- AM
 - South Bound – 215vph
 - Northbound – 260 vph
- PM
 - South Bound – 250vph
 - Northbound – 270 vph

Estimated AADT – 4,950 vpd (2035)



Intersection Peak Hour

08:00 - 09:00

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	0	0	0	108	0	2	122	0	104	0	0	79	415
Factor	0.00	0.00	0.00	0.64	0.00	0.17	0.68	0.00	0.72	0.00	0.00	0.60	0.72
Approach Factor	0.00			0.65			0.72			0.60			

Peak Hour Vehicle Summary

Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	0	0	0	74	0	1	82	0	72	0	0	43	272
Truck	0	0	0	34	0	1	39	0	32	0	0	36	142
Bicycle	0	0	0	0	0	0	1	0	0	0	0	0	1

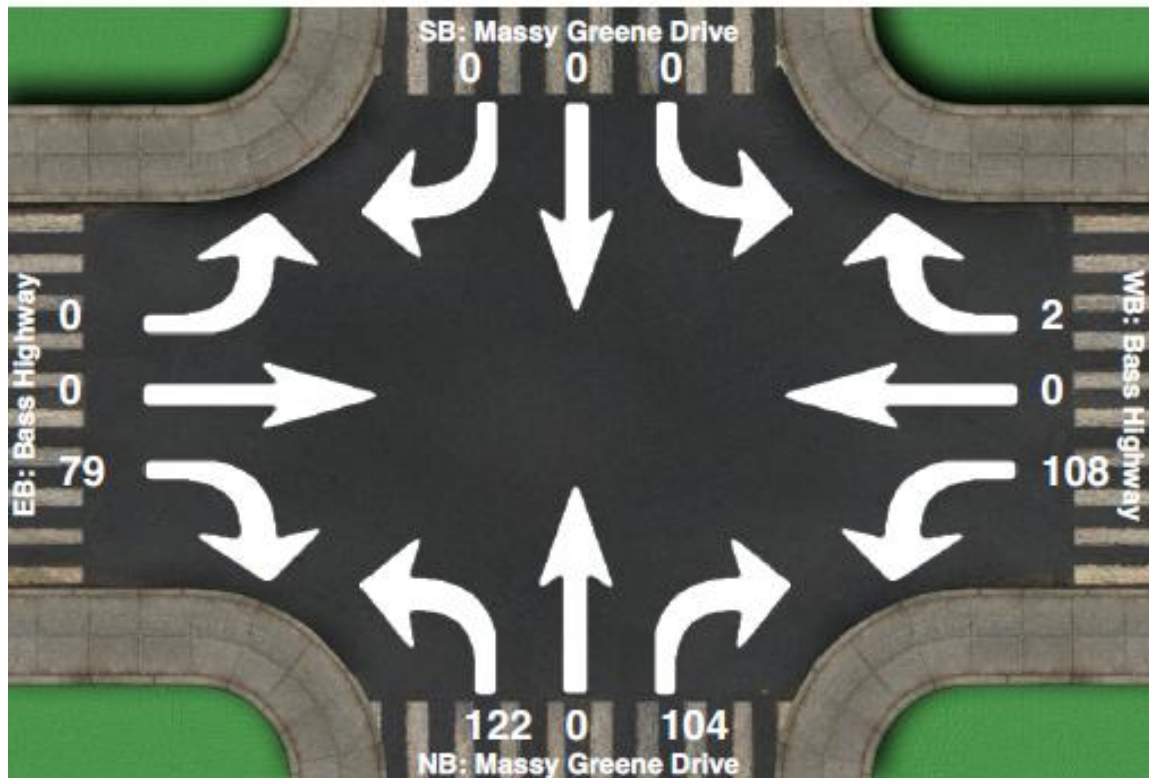
Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	1	0	1	1	0	1	2



Intersection Peak Hour

Location: Massy Greene Drive at Bass Highway, South Burnie
GPS Coordinates: Lat=-41.443854, Lon=147.141727
Date: 2023-04-19
Day of week: Wednesday
Weather:
Analyst: Sid Saxby



Intersection Peak Hour

08:00 - 09:00

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	0	0	0	108	0	2	122	0	104	0	0	79	415
Factor	0.00	0.00	0.00	0.64	0.00	0.17	0.68	0.00	0.72	0.00	0.00	0.60	0.72
Approach Factor	0.00			0.65			0.72			0.60			



Turn Count Summary PM Peak April 2023

Location: Massy Greene Drive at Bass Highway, South Burnie
GPS Coordinates: Lat=-41.443854, Lon=147.141727
Date: 2023-04-20
Day of week: Thursday
Weather:
Analyst: Sid Saxby

Total vehicle traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:30	0	0	0	15	0	1	10	0	15	0	0	4	45
16:35	0	0	0	12	0	0	14	0	13	0	0	11	50
16:40	0	0	0	5	0	0	4	0	8	0	0	14	31
16:45	0	0	0	6	0	0	5	0	7	0	0	6	24
16:50	0	0	0	4	0	1	5	0	17	0	0	14	41
16:55	0	0	0	7	0	0	11	0	14	0	0	11	43
17:00	0	0	0	10	0	0	4	0	5	0	0	7	26
17:05	0	0	0	13	0	0	8	0	15	0	0	5	41
17:10	0	0	0	9	0	0	5	0	11	0	0	5	30
17:15	0	0	0	7	0	0	14	0	14	0	0	11	46
17:20	0	0	0	9	0	0	8	0	13	0	0	12	42
17:25	0	0	0	10	0	0	5	0	10	0	0	10	35

Car traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:30	0	0	0	15	0	1	7	0	11	0	0	4	38
16:35	0	0	0	12	0	0	14	0	12	0	0	9	47
16:40	0	0	0	4	0	0	2	0	8	0	0	9	23
16:45	0	0	0	6	0	0	5	0	7	0	0	4	22
16:50	0	0	0	3	0	1	3	0	14	0	0	9	30
16:55	0	0	0	7	0	0	7	0	12	0	0	11	37
17:00	0	0	0	9	0	0	2	0	5	0	0	5	21
17:05	0	0	0	9	0	0	6	0	14	0	0	3	32
17:10	0	0	0	9	0	0	3	0	11	0	0	5	28
17:15	0	0	0	7	0	0	9	0	12	0	0	9	37
17:20	0	0	0	9	0	0	6	0	11	0	0	9	35
17:25	0	0	0	10	0	0	5	0	9	0	0	10	34

Truck traffic

Interval starts	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
16:30	0	0	0	0	0	0	3	0	4	0	0	0	7
16:35	0	0	0	0	0	0	0	0	1	0	0	2	3
16:40	0	0	0	1	0	0	2	0	0	0	0	5	8
16:45	0	0	0	0	0	0	0	0	0	0	0	2	2
16:50	0	0	0	1	0	0	2	0	3	0	0	5	11
16:55	0	0	0	0	0	0	4	0	2	0	0	0	6
17:00	0	0	0	1	0	0	2	0	0	0	0	1	4
17:05	0	0	0	4	0	0	2	0	1	0	0	2	9
17:10	0	0	0	0	0	0	2	0	0	0	0	0	2
17:15	0	0	0	0	0	0	5	0	2	0	0	2	9
17:20	0	0	0	0	0	0	2	0	2	0	0	3	7
17:25	0	0	0	0	0	0	0	0	1	0	0	0	1



Intersection Peak Hour

16:30 - 17:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	0	0	0	107	0	2	93	0	142	0	0	110	454
Factor	0.00	0.00	0.00	0.59	0.00	0.17	0.55	0.00	0.70	0.00	0.00	0.65	0.76
Approach Factor	0.00			0.57			0.70			0.65			

Peak Hour Vehicle Summary

Vehicle	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Car	0	0	0	100	0	2	69	0	126	0	0	87	384
Truck	0	0	0	7	0	0	24	0	16	0	0	22	69
Bicycle	0	0	0	0	0	0	0	0	0	0	0	1	1

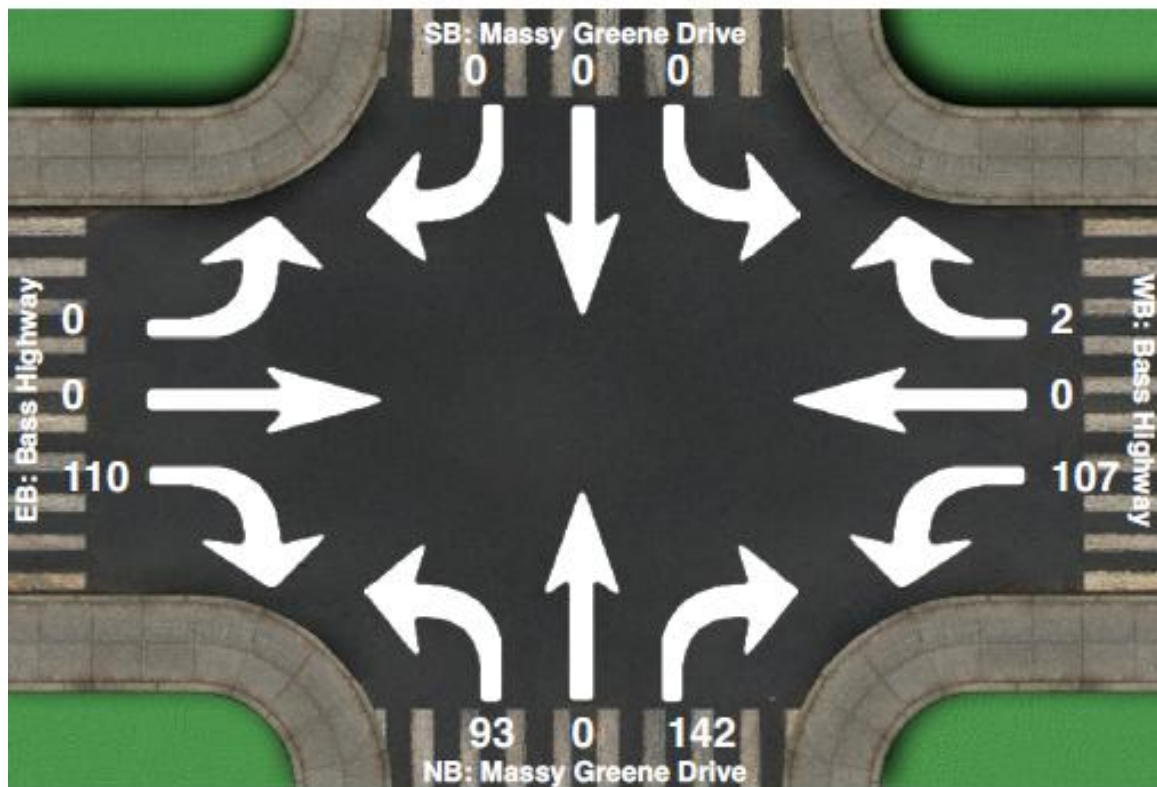
Peak Hour Pedestrians

	NE			NW			SW			SE			Total
	Left	Right	Total	Left	Right	Total	Left	Right	Total	Left	Right	Total	
Pedestrians	0	0	0	0	0	0	0	0	0	1	0	1	1



Intersection Peak Hour

Location: Massy Greene Drive at Bass Highway, South Burnie
GPS Coordinates: Lat=-41.443854, Lon=147.141727
Date: 2023-04-20
Day of week: Thursday
Weather:
Analyst: Sid Saxby



Intersection Peak Hour

16:30 - 17:30

	SouthBound			Westbound			Northbound			Eastbound			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
Vehicle Total	0	0	0	107	0	2	93	0	142	0	0	110	454
Factor	0.00	0.00	0.00	0.59	0.00	0.17	0.55	0.00	0.70	0.00	0.00	0.65	0.76
Approach Factor	0.00			0.57			0.70			0.65			



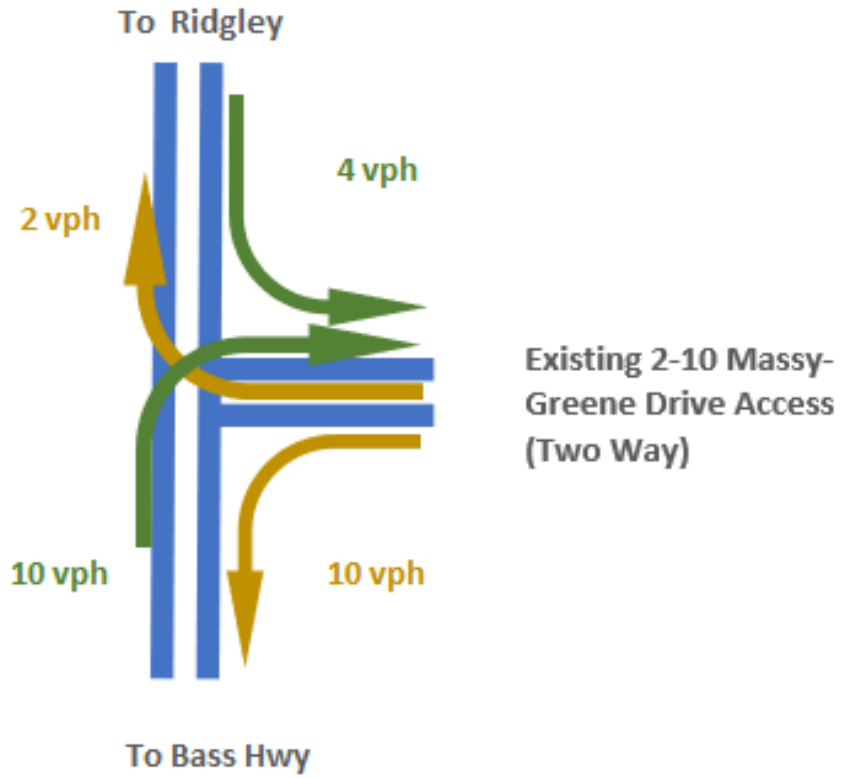
Appendix G – DSG advice on TIA acceptability

DSG advice on acceptability of this TIA is not required.



Appendix H – 2-10 Massy-Greene Drive Traffic Access Turning movements Survey 2016

Hourly Operation 2016





Appendix I – Intersection Analysis

Bass Highway / Massy-Greene Drive Signalised Intersection

Intersection Model



SITE LAYOUT

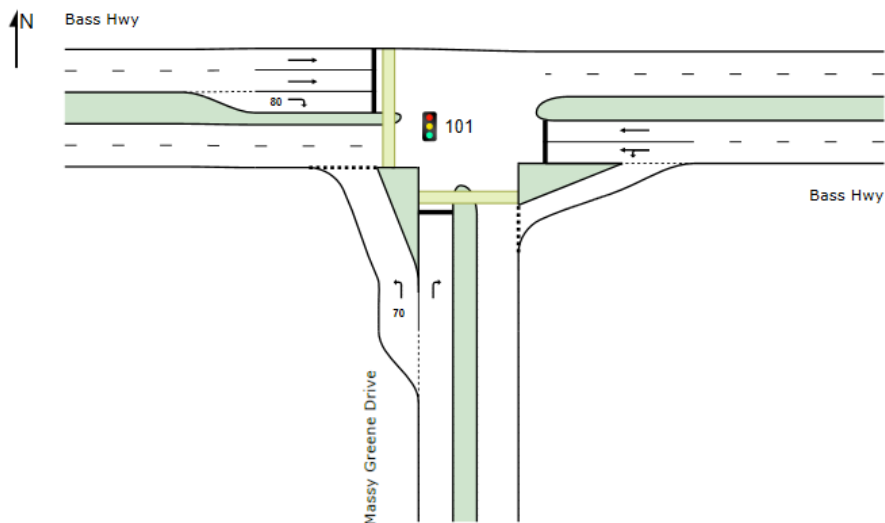
 **Site: 101 [Bass Hwy - Massy Greene Drive (Site Folder: General)]**

Bass Hwy - Massy Greene Drive

Site Category: (None)

Signals - EQUISAT (Fixed-Time/SCATS) Isolated

Layout pictures are schematic functional drawings reflecting input data. They are not design drawings.





Movement Summary AM Peak 2035 - Overall Performance - LOS B

MOVEMENT SUMMARY

Site: 101 [Bass Hwy - Massy Greene Drive (Site Folder: General)]

Output produced by SIDRA INTERSECTION Version: 9.1.6.228

Bass Hwy - Massy Greene Drive

Site Category: (None)

Signals - EQUISAT (Fixed-Time/SCATS) Isolated Cycle Time = 60 seconds (Site Practical Cycle Time)

Vehicle Movement Performance										
Mov ID	Turn	Mov Class	Demand Flows [Total HV]	%	Arrival Flows [Total HV]	%	Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue [Veh. Dist]
			veh/h		veh/h		v/c	sec		m
South: Massy Greene Drive										
1	L2	All MCs	174	18.0	174	18.0	0.220	13.4	LOS B	2.7
3	R2	All MCs	148	18.0	148	18.0	*0.710	36.9	LOS D	4.7
Approach			322	18.0	322	18.0	0.710	24.3	LOS C	4.7
East: Bass Hwy										
4	L2	All MCs	174	18.0	174	18.0	0.816	11.2	LOS B	20.1
5	T1	All MCs	1179	14.0	1179	14.0	*0.816	21.9	LOS C	20.1
Approach			1353	14.5	1353	14.5	0.816	20.5	LOS C	20.1
West: Bass Hwy										
11	T1	All MCs	1179	14.0	1179	14.0	0.722	15.7	LOS B	14.8
12	R2	All MCs	131	18.0	131	18.0	*0.689	36.5	LOS D	4.1
Approach			1309	14.4	1309	14.4	0.722	17.7	LOS B	14.8
All Vehicles			2984	14.8	2984	14.8	0.816	19.7	LOS B	20.1

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

Delay Model: SIDRA Standard (Control Delay; Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Green.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

* Critical Movement (Signal Timing)



Movement Summary PM Peak 2035 - Overall Performance - LOS C

MOVEMENT SUMMARY

Site: 101 Bass Hwy - Massy Greene Dr PM (Site Folder: General)

Output produced by SIDRA INTERSECTION Version: 9.1.6.228

Bass Hwy - Massy Greene Drive

Site Category: (None)

Signals - EQUISAT (Fixed-Time/SCATS) Isolated Cycle Time = 60 seconds (Site Practical Cycle Time)

Vehicle Movement Performance													
Mov ID	Turn	Mov Class	Demand Flows [Total HV]	%	Arrival Flows [Total HV]	%	Deg. Satn	v/c	Aver. Delay	sec	Level of Service	95% Back Of Queue [Veh. Dist.]	
			veh/h		veh/h							veh	m
South: Massy Greene Drive													
1	L2	All MCs	152	18.0	152	18.0	0.183		14.0		LOS B	2.4	19.3
3	R2	All MCs	219	18.0	219	18.0	* 0.815		38.2		LOS D	7.2	58.6
Approach			371	18.0	371	18.0	0.815		28.3		LOS C	7.2	58.6
East: Bass Hwy													
4	L2	All MCs	149	18.0	149	18.0	0.909		21.3		LOS C	26.7	210.8
5	T1	All MCs	1242	14.0	1242	14.0	* 0.909		34.0		LOS C	26.7	210.8
Approach			1392	14.4	1392	14.4	0.909		32.7		LOS C	26.7	210.8
West: Bass Hwy													
11	T1	All MCs	1242	14.0	1242	14.0	0.821		22.0		LOS C	18.9	148.2
12	R2	All MCs	161	18.0	161	18.0	* 0.826		40.4		LOS D	5.5	44.2
Approach			1403	14.5	1403	14.5	0.826		24.1		LOS C	18.9	148.2
All Vehicles			3165	14.9	3165	14.9	0.909		28.4		LOS C	26.7	210.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Green.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akcelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

* Critical Movement (Signal Timing)



Appendix J – Warehouse Parking Demand

ITE Parking Generation Rates - 4th Edition		ITE Vehicle Parking Generation Rates					
		Average		85th Percentile			
Description/ITE Code	Units	Weekday	Saturday	Sunday	Weekday	Saturday	Sunday
		Commercial Airport 021	Daily enplanements	0.4	0.84	0.57	0.61
Light Rail Transit Station w/ Parking, Suburban 093	1000 Daily Boardings	136	NA	NA	212	NA	NA
Light Rail Transit Station w/ Parking, Urban 093	1000 Daily Boardings	58	NA	NA	94	NA	NA
General Light Industrial 110	KSF	0.75	NA	NA	1.13	NA	NA
General Light Industrial 110	Employees	0.64	NA	NA	0.81	NA	NA
Industrial Park 130	KSF	1.27	NA	NA	1.85	NA	NA
Industrial Park 130	Employees	0.89	NA	NA	0.98	NA	NA
Manufacturing 140	KSF	1.02	NA	NA	1.18	NA	NA
Manufacturing 140	Employees	0.97	NA	NA	1.14	NA	NA
Warehousing 150	KSF	0.51	NA	NA	0.81	NA	NA
Warehousing 150	Employees	0.78	NA	NA	1.01	NA	NA
Mini-Warehouse 151	KSF	0.14	0.11	NA	0.17	0.13	NA
Mini-Warehouse 151	100 Storage Units	1.35	0.77	NA	1.66	NA	NA



Appendix L – Proposed entry lane

Appendix L.1 – Looking West from Massy Greene Drive



Driveway width 5.0m
from face to face of kerb.

Appendix L.2 – Looking West along driveway



Driveway width 4.8m
from face to face of kerb.

Appendix L.3 – Looking West along proposed driveway





Appendix L.4 – Looking West along proposed driveway



Appendix L.5 – Looking West along proposed driveway



Driveway and rail line at
above same level.

Appendix L.6 – Looking East along proposed driveway





Appendix L.7 – Looking East along existing driveway



Need No Entry signs and Pavement arrows to clarify One Way Operation

Appendix L.8 – Looking East along existing driveway



Need No Entry R2-4A signs and Thermoplastic Pavement arrows to clarify One Way

Appendix L.9 – Looking East along driveway towards Massy Greene Drive



Need No Entry R2-4A signs and Thermoplastic Pavement arrows to clarify One Way



Appendix M – Proposed exit lane

Appendix M.1 – Looking West along proposed driveway



Appendix M.2 – Looking East along proposed driveway



Appendix M.3 – Looking West along proposed driveway



Need No Entry R2-4A signs and Thermoplastic Pavement arrows to clarify One Way



Appendix M.4 – Looking East at exit to Massy Greene Drive



Appendix N – Rail Line South of access lane

Appendix N.1 – Looking East along rail line towards Massy Greene Drive



Appendix N.2 – Looking East along rail line towards Massy Greene Drive



Driveway clearance to rail line is adequate.

Appendix N.3 – Looking West along rail line



Driveway clearance to rail line is adequate.



Appendix N.4 – Looking West along rail line



Driveway clearance to rail line is adequate.

Appendix N.5 – Looking West along rail line



Driveway clearance to rail line is adequate.

Appendix N.6 – Looking West along rail line



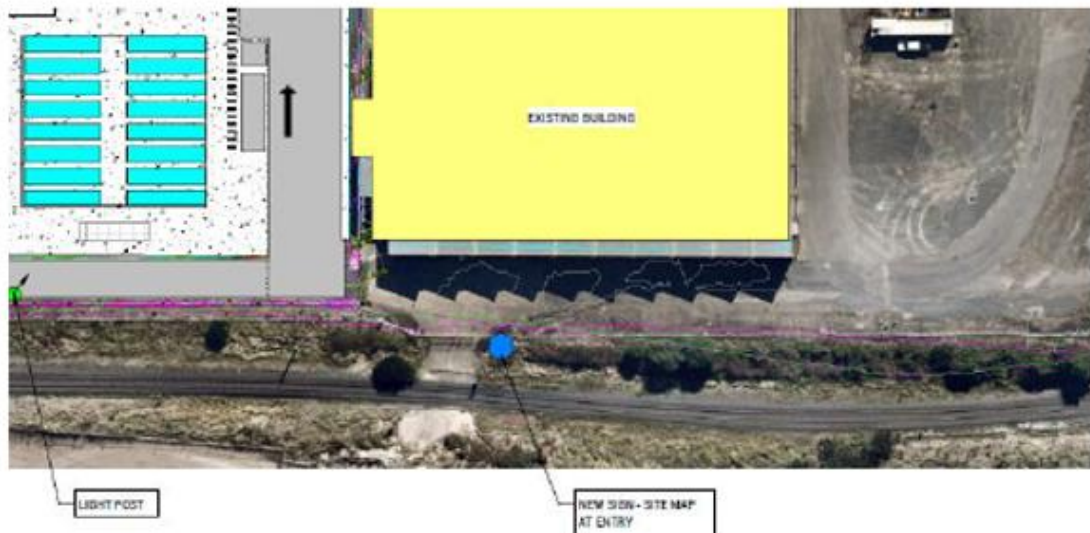
Appendix N.7 – Rail Line access site



The railway line access, see gate in the above & below images is not proposed to be used by the developer.

7. TasRail request

The application has been referred to TasRail (as the relevant authority for the nearby railway). They have requested further information regarding the interaction with their disused railway line. Could you please advise if the access in the picture below is proposed to be used.





Appendix O – BCC Response to DA 2025/48

PLEASE QUOTE

Your Ref:

Our Ref: DA 2025/48

Enquiries: Planning

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690

Phone: (03) 6430 5700

Email: burnie@burnie.tas.gov.au

Web: www.burnie.tas.gov.au

We value your feedback on our service.
Tell us about it at www.burnie.net/feedback



12 August 2025

Rare Innovation
2224 Patterson Street
LAUNCESTON TAS 7250

Dear Sir/Madam

RE: DA 2025/48 – PERMIT APPLICATION FOR TRANSPORT DEPOT AT 2-10 MASSY-GREENE DRIVE SOUTH BURNIE

Burnie City Council received your permit application for the Transport Depot at 2-10 Massy-Greene Drive SOUTH BURNIE. Following our previous request made on 5 August 2025, Council's Technical Services Department, as the road authority, requested further information. The original request is amended below.

Under Section 54 of the *Land Use Planning and Approvals Act, 1993* Council seeks the following additional information, in accordance with the Tasmanian Planning Scheme –

1. Access, parking & vehicle movements

The application does not provide enough information to assess access, parking and truck movements. In order to address the requirements of C2.0, you will need to obtain a Traffic Impact Assessment that is completed by a suitably qualified person. Please note: the existing uses will need to be considered and addressed as part of the traffic assessment.

2. Vehicular access

The application does not include written permission nor any commentary/conditions from the road authority. To obtain road authority approval, you will need to request a statement of compliance from Council's Technical Services team (the road authority). The TIA requested under Point 1 will also need to address any requirements made by the road authority. It is recommended that your traffic engineer contact Simon Hughes to discuss prior to the finalisation of the document. The statement of compliance request can be done at:

<https://www.burnie.tas.gov.au/Development/Planning/Apply-for-a-Planning-Permit/Statement-of-Compliance-Request>

3. Signage

Signage is proposed; however, there is insufficient information to assess the relevant provisions of C1.0 Signs Code. Council requests that you provide additional information to address Clauses C1.6.1 & C1.6.2.

4. Light poles

The height of light poles must be provided on the plan set. Given that they are likely above 10m high, the application will need to demonstrate that there is no unreasonable impact on adjoining properties, including the road (see Clause 19.4.1 P1).

5. Landslip Hazard Code

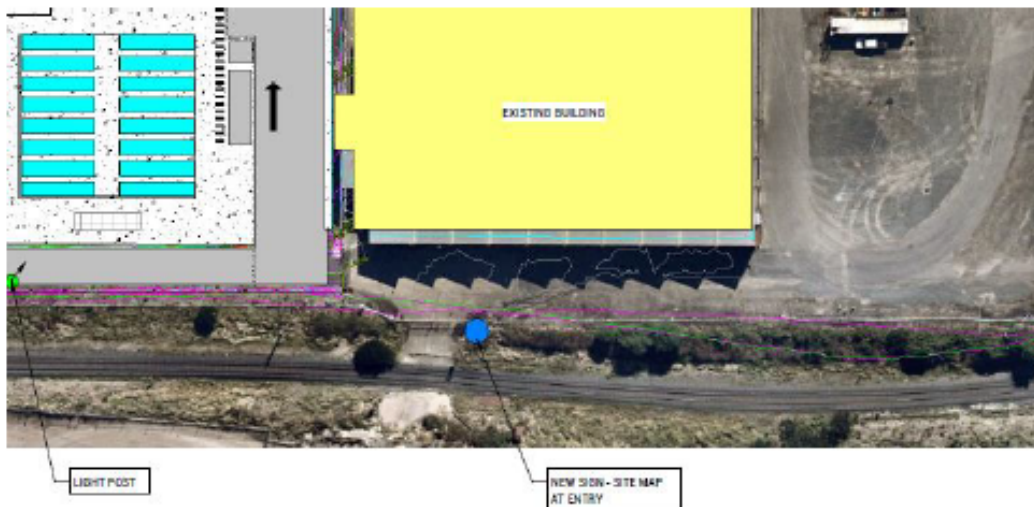
There is a small area of the site under the medium level landslip hazard overlay in the south-western corner of the site. There is insufficient information in the application to assess any proposed works in this area. Council requests that you review the overlay area and respond to the relevant provisions in the Code.

6. TasWater Request for Additional Information

The application has been referred to TasWater (as the water authority). They have issued a request for additional information (RAI) which is attached to this letter. All information requested in the RAI must be addressed and provided to Burnie City Council. Council will forward the information to TasWater for their response.

7. TasRail request

The application has been referred to TasRail (as the relevant authority for the nearby railway). They have requested further information regarding the interaction with their disused railway line. Could you please advise if the access in the picture below is proposed to be used.



It is advised section 61(3) *Land Use Planning and Approvals Act 1993* allows that a person who is dissatisfied with an additional information request made under section 54 may appeal the request to the Tasmanian Civil and Administrative Tribunal within 14 days from the date on which the request was made.

If the additional information requested is not provided within 2 years from the date of this request (or such longer period as the planning authority may allow) the permit application to which the request relates will lapse.



Please be advised your permit application has been placed on hold until all relevant documentation has been received to the satisfaction of Council.

If you have any further queries please do not hesitate to contact Troy McCarthy on (03) 6430 5700.

Yours faithfully

A handwritten signature in black ink that reads 'Troy McCarthy'. The signature is written in a cursive style with a large, sweeping initial 'T'.

Troy McCarthy
Principal Planner



Appendix P – Statement of Compliance Request

Application request submitted 26th August 2025.

Mr Simon Hughes (ph 6430 5757) was contacted by Richard Burk (TCS) on Wednesday morning the 27th August 2025 as per BCC recommendation in the RFI of the 12th August 2025, see extract below.

2. Vehicular access

The application does not include written permission nor any commentary/conditions from the road authority. To obtain road authority approval, you will need to request a statement of compliance from Council's Technical Services team (the road authority). The TIA requested under Point 1 will also need to address any requirements made by the road authority. It is recommended that your traffic engineer contact Simon Hughes to discuss prior to the finalisation of the document. The statement of compliance request can be done at:

<https://www.burnie.tas.gov.au/Development/Planning/Apply-for-a-Planning-Permit/Statement-of-Compliance-Request>

Mr Hughes advised of Council's concerns with the structural strength of the Massy Green Drive Rail Overpass structure just South of the entrance to 2-10 Massy Greene Drive.

With recent line marking changes head-to-head right turn lanes were introduced to cater for right turns to the ARC and AJL accesses which are opposite each other.

This line marking work means there are now three traffic lanes on the rail overpass, exceeding regular load bearing capacity.

Accordingly, Council is in the process of removing the right turn lane to AJL (on the East side of Massy Greene Drive) to return to two traffic lanes over the rail overpass.

Council is planning to replace the overpass to enable three traffic lanes to be provided.

Given that the Strait link proposal does not require upgrading of the left turn entry facility to #2-10 Massy Greene Drive, as the existing BAL left turn facility is adequate, the proposal may proceed as proposed.

It is noted however that the overpass replacement will be disruptive for transport operations as Massy Greene Drive functions as a major heavy vehicle route and is part of the Tasmanian 26m B Double Network, see Appendix C.

Our Ref: 245019

20th May 2025

Planning Department
Burnie City Council
80 Wilson Street, Burnie TAS 7320

TO WHOM IT MAY CONCERN,

PLANNING APPLICATION COVER LETTER – STRAITLINK BURNIE TERMINAL BY STUBBS & LICHT INVESTMENTS P/L – ON 2-10 MASSY GREENE DRIVE, SOUTH BURNIE (TITLE REFERENCE 165351/7)

This letter is provided to support a planning permit for a logistics development project located at the subject property, in accordance with the requirements of the Tasmanian Planning Scheme and the Land Use Planning and Approvals Act 1993.

The application has been assessed and, in our opinion, does not trigger any need for assessment by the Environmental Protection Authority (EPA). The application does include elements relating to washdown that will require consideration by TasWater in relation to Trade Waste.

This letter should be read in conjunction with supporting documents and material as outlined in the application form.

The existing site will be partially demolished to facilitate the proposal. It is a long standing industrial site established circa 1960 and has retained various industrial uses to date. The proposal is in keeping with its industrial history and character.

There are multiple tenancies on the site. Some of these have been upgraded as recently as 2024 and in general it is intended that the unaffected tenancies/buildings on the site will be retained and used for their existing purposes.

The proposed development provides for the upgrade, expansion and construction of an industrial/logistics facility at the subject site. The upgrades will include the provision of access and hardstand for logistics, handling and storage of containers. They will also provide for specialist activities including refueling for trucks, cleaning/washdown of trucks and containers, and maintenance of containers and forklifts.

Key operational and functional description of the proposal includes:

- Container storage for 500 containers (combination of 20ft 40ft and refrigerated) in a stacked configuration
- Working platforms for container forklifts (MHE's)
- Circulating access roads for delivery vehicles (semi trailers)
- Yard lighting and signage
- Site drainage
- Facilities for container maintenance (workshops)
- Facilities for container washdown (washdown bay including waste water treatment)
- Offices, amenities, facilities and parking for 50 employees
- Normal Operating Hours 6am to 6pm 7 days a week

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Outside Normal Operating Hours: Occasional traffic 6pm – 6am 7 days a week

Land Use and Zoning: The site is zoned General Industrial under the Burnie Local Provisions Schedule and the proposed development and usage is generally in keeping with this. There are no relevant codes or overlays identified on the site generally. There is a coastal erosion area in the North East corner which is not considered to be relevant to the proposal given the only new infrastructure proposed in the affected area is signage. The surrounding areas are all similarly zoned.

Environmental Considerations: The application has been assessed in relation to potential impacts on water management and no triggers have been identified for further consultation with the EPA. Air quality and noise have not been investigated in detail however the proposed usage is in keeping with the existing use on the site and in the surrounding areas.

Significant Infrastructure: There is a railway corridor immediately to the South of the site. The intent of the proposal is to protect this corridor by minimizing cuts at this boundary. Yard lighting will be directed back into the site to mitigate light spill into this corridor.

Existing Services and Infrastructure: The site is currently fully serviced with reticulated supply/discharge for sewer, water and stormwater. The proposal will increase demands on sewer and water but will not significantly alter demands for stormwater runoff.

Traffic and Access: The development includes provisions for vehicle access, parking, and traffic flow. There are 230 truck movements per day during normal operating hours and 20 truck movements outside of normal operating hours. The access and parking provisions are generally minor improvements in relation to safety and efficiency and will not substantially alter the current characteristics of the overall site.

Community and Economic Benefits: The project is expected to sustain jobs currently within the local region and improve working conditions and efficiencies. This will continue to contribute to the local economy and lead to improved outcomes for workplace conditions.

Supporting documentation accompanying this application includes:

- Completed planning application form
- Site and development plans
- Other relevant reports and assessments

We respectfully request that the application be assessed under the provisions of the Tasmanian Planning Scheme and welcome the opportunity to provide further information or clarification as required.

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Thank you for considering this application. We look forward to working collaboratively with your office to ensure the successful delivery of this project.

Should you have any further queries, please do not hesitate to contact us.

Yours faithfully,



Brendan Stanborough
Division Head Structural and Engineering Infrastructure

P: 03 6388 9200

M: 0417 178 722

A: 22-24 Paterson Street, Launceston TAS 7250

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Memorandum No 001

Project Strait Link Washdown / 245019
For: Strait Link C/- Stubbs Construction
From: B. Stanborough
Date 5 May 2025
Revision A
Status For Information
Subject: Preliminary Trade Waste Evaluation - Proposed Washdown Bay
Attention: TasWater Trade Waste Enquiries (tradewaste@taswater.com.au)

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Introduction

This evaluation has been provided in relation to a proposed new Straitlink logistics facility at 2-10 Massey-Green Drive in South Burnie. The facility will receive containers from shipping operations which will be stored on site. The function of the washdown is to receive washdown from empty "dirty" 40ft containers (both refrigerated and standard), capture and treat the contaminated washdown effluent and then discharge to a suitable receiving network. Based on the contamination profile of the washdown effluent it is proposed that this receiving network is a TasWater Sewer network under a Trade Waste Agreement.

Objective

The purpose of this memorandum is to provide sufficient context to the project to enable and initiate early engagement/discussions with TasWater tradewaste prior to (and then in parallel with) development application processes. This document does not contemplate any final trade-waste agreement or pre-treatment solution. Desirable outcomes will include creation of an application reference within TasWater assessment system, identification of probable classification outcomes (i.e. commercial or industrial tradewaste) and preliminary statements of local infrastructure capacity to receive new trade waste.

Proposed Operations and Waste Stream Profile Description

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TasWater – tradewaste@taswater.com.au
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The facility is intended to handle around 120 containers per day through the wash down process.

Each container requires an average range of 50-100 litres per container to wash down. This can vary significantly on a per container basis depending on the nature of the contained material. The estimated total volume of annualized waste washdown is between 1-3 ML/year.

Strait Link are a logistics company and the goods that they receive can vary significantly). Goods received are new products, Strait Link do not receive waste material. There are materials that are regular and recurrent which have been used to characterise the waste stream for this evaluation. These substances are:

- Gross Solids / General purpose non-dissolvable solids (e.g. packaging)
- Plant Products
- Hydrocarbons
- Meat products / Fish meal / Agricultural feed
- Grains / Nuts
- Fertilisers

A list of SDS data has been provided with this memo for completeness.

In general incoming goods are products and may be subject to quarantine. The operation aims to retain and dispose of solid waste streams in accordance with strict quarantine requirements (which typically requires capture and disposal).

Treatment Train Proposal Description

The proposed washdown treatment system will be designed/specified by suitably qualified persons in accordance with TasWater Trade Waste Policy and pre-treatment guidelines (where applicable). The aim of the treatment system will be to provide at-source capture and treatment to enable a target quality of effluent which can then be discharged at a controlled rate to a TasWater receiving sewer.

A pre-treatment device(s) is proposed to be installed downstream of the washdown. It is likely to include the following key features:

- High volume solids capture (e.g. waste basket, raking, backwashing system) down to 3.5mm aperture
- Buffer tank
- Filtration System
- Hydrocarbon separator

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TasWater – tradewaste@taswater.com.au
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Due to the large area and handling methods roofing is not practical. An automated stormwater diversion first flush system may be appropriate to minimise stormwater volumes entering the sewer.

TasWater Trade Waste Guidelines Review

The nearest assessment identified within TasWaters "Commercial Trade Waste Customer Pre-Treatment Guideline 2022 (V3)" under *section 12. Other commercial trade waste activity pre-treatment* requirements is "Garbage Bin Washing". The required action is to Contact TasWater (Pre-treatment will depend on substances of concern).

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Memorandum No 02

Project: Strait Link / 245019

For: Strait Link C/- Stubbs Construction

From: Jordan Quaile

Date: 10 June 2025

Revision: A

Status: For Information

Subject: Strait Link Development – Pre Development vs Post Development Impervious Surfaces

Attention: Burnie City Council

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Introduction

This evaluation has been provided in relation to a proposed new Strait Link logistics facility at 2-10 Massey-Green Drive in South Burnie. The development is in a leased area within the former Pulp Mill site and involves the extension of an existing shed, construction of an external concrete hardstand area for forklift operations and container storage, and construction of a new circulation road within the site.

Objective

The purpose of this memorandum is to provide context around the past and present surface conditions of the development site, with a particular focus on the percentage impervious surface area of the development site pre and post development. This is to initiate discussions with the Burnie City Council and ultimately inform the stormwater permit conditions specified in the Building Permit.

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Historical Aerial Images



Figure 1: 2007 Aerial



Figure 2: 2017 Aerial

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Figure 3: 2025 Aerial



Figure 4: Photo from the 1960s

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2025 Site Photos



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Assessment from Photos

- A majority of the development area has been either sheds or hardstand area for a significant period of time. Photo evidence indicates minimal pervious surfaces back to the 1960 other than the strip grass adjacent to the rail corridor and some garden beds towards the north western corner.
- A 3650 m² shed was demolished in 2017.
- There are a number of large existing concrete slabs/footings within the development site. Some remain as exposed concrete and some are covered in gravel.
- There are a number of puddles within the existing gravel yard which indicates that stormwater is not infiltrating through the gravel and it is generally impervious.

Pre vs Post Development Conditions

The development area is 21,030 m².

1. Estimated Pre Development (2017 prior to shed demolition):

Roof: 5,820 m² (28%)

Concrete/Gravel Hardstand: 13,930 m² (66%)

Landscaping: 1,280 m² (6%)

2. Proposed Post Development

Roof: 2,480 m² (12%)

Concrete Hardstand: 13,750 m² (65%)

Carparking and Circulation Road (Asphalt): 3,450 m² (16%)

Footpaths: 180 m² (1%)

Landscaping: 320 m² (2%)

Unknown: 850 m² (4%)

Recommended Evaluation

The overall change in "landscaping" is 1170-1280 = -110 m² or 0.5% leading to a very slight/negligible overall increase in impermeability across the proposed development area. It is recommended that some amount of attenuation is provisioned to this increase. Further more we also recommend that some consideration is given to the effects of overall ageing leading to very minor increases permeability on some surfaces (e.g.

cracking in pavements) which we recommend to be no more than 0.5%. In total we propose that a 1% attenuation of peak site discharge for the 5% AEP is provided for by the development.

Conclusion

Although the development is increasing the area of concrete hardstand, the increase in impervious surfaces post-development development is not significant compared to pre-development. The large building formally on this site and the gravel hardstand with concrete underneath it has made this site largely impervious for at least the last 70 years. This historical context should be considered when determining the Building Permit conditions related to stormwater for this development.

A recommended 1% attenuation of peak site discharge is recommended to manage stormwater runoff for the site.

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Memorandum No 03

Project: Strait Link / 245019

For: Strait Link C/- Stubbs Construction

From: Jordan Quaile

Date: 11 June 2025

Revision: A

Status: For Information

Subject: Strait Link Development – Trade Waste Risk Categorisation

Attention: TasWater Trade Waste Enquiries (tradewaste@taswater.com.au)

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Introduction

This evaluation has been provided in relation to a proposed new Strait Link logistics facility at 2-10 Massey-Green Drive in South Burnie. The facility will receive containers from shipping operations which will be stored on site. The function of the washdown is to receive washdown from empty "dirty" 40ft containers (both refrigerated and standard), capture and treat the contaminated washdown effluent and then discharge to a suitable receiving network. Based on the contamination profile of the washdown effluent it is proposed that this receiving network is a TasWater Sewer network.

Objective

The purpose of this memorandum is to determine a Trade Waste categorisation for the Strait Link wash down operations. This is to provide precedent to inform the required level of pre-treatment and volume limitations for discharge to TasWater sewer infrastructure. This determination is part of the process towards obtaining a Trade Waste Agreement for the Strait Link development.

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Reference Documents

1. 245019-MEM-001_250505 Preliminary Waste Evaluation – Proposed Washdown Bay. Refer to this memorandum for an overview of proposed washdown operations including estimated volume of washdown waste and SDS data for potential waste products.
2. TasWater Trade Waste Customer Category Guideline (V3)
3. TasWater Commercial Trade Waste Customer Pre-Treatment Guideline 2022 (V3)

Trade Waste Categorisation

As per the Section 7.1 of the *TasWater Trade Waste Customer Category Guideline (V3)* the calculation of a risk score is based on the business activity, substance of most concern, pre-treatment equipment required and trade waste volume. Scores for each of these are added together to give the overall risk score and ultimately the Trade Waste Categorisation.

Business Activity Score - 10

As per Appendix A Table 2 the Trade Waste Code of BE08P Plant/Machinery Hire/Truck Wash is the most appropriate business activity code for the washdown facility.

Refers to businesses hiring plant and equipment or washing down larger scale vehicles (e.g. Trucks). Main activity is service and wash down of equipment.

Substance Score - 15

As per the info provided in MEM-001 the substance of most concern is hydrocarbons. For example, the SDS for Stabilised Fish Meal states:

Combustible. May evolve toxic gases (carbon oxides, nitrogen oxides, sulphur oxides, phosphorous oxides, hydrocarbons) when heated to decomposition. Finely divided dust may form explosive mixtures with air. May evolve ammonia in contact with water.

Subsequently the Substance Score is 15 for High Impact as per Table 4.

Pre-Treatment Score - 18

Table 11 of *TasWater Commercial Trade Waste Customer Pre-Treatment Guideline 2022* states that the pre-treatment types for Business Code BE08P could include:

- Silt sump with litter basket
- Approved oil water separators
- Holding tank
- Solid settlement pit.

Additionally, the following is specified:

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Burnie City Council
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- A non-emulsifying pump with suction inlet at least 300mm above the bottom of the collection well.
- All cleaning and washing must occur in an area isolated for drainage to exclude stormwater run-off using a combination of bunding or cut-off drains.
- Runoff be roofed in accordance with - National Construction Code 2019 Volume 3. Where roofing cannot be achieved or is not practical an automated stormwater diversion first flush system shall be designed to minimise stormwater volumes entering the sewer.

The washdown area stormwater catchment will be separated from the wider concrete hardstand catchment. Only runoff from the washdown area will be pre-treated and then permitted to discharge into the sewer network.

This development is proposing to use a first flush system in the washdown area to be designed by a suitably qualified person. A roof is not practical given the large area, high clearances needed and very high risk of impact to structures associated with the use (container handling equipment)

An Oil/Separator is classed as High Risk with a Pre-Treatment score of 18.

Trade Waste Volume Score – 30

It is the intention of the development to limit waste discharge from the washdown facility following the commercial pre-treatment guidelines by limiting discharge to a maximum amount of 1500 kL/annum. This will be achieved through re-circulation and reuse of washdown water which is to be treated on-site using a suitably designed treatment system. Subsequently the Volume Score is 30 as per Table 6.

Total Risk Score – 10 + 15 + 18 + 30 = 73

Given the Total Risk Score is >60, the Trade Waste Risk Category of 2C can be applied to the development.

This assessment is for the proposed development area only and does not evaluate or limit discharge to sewer from any other use on other parts of the site not affected by the proposed development.

Next Steps

- Approval of Trade Waste Categorization 2C for the Strait Link washdown operations.
- Engagement of suitably qualified person to design a system that incorporates the first flush system, necessary pre-treatment and re-circulation of washdown water to limit the discharge into the sewer network to 1500 kL/annum.
- Trade Waste Agreement signoff

Memorandum No 04

Project: Strait Link / 245019
 For: Strait Link C/- Stubbs Construction
 From: Jordan Quaile
 Date: 19th August 2025
 Revision: A
 Status: For Information
 Subject: Strait Link Development – ET Calculation
 Attention: David Boyle – TasWater

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Introduction

A Request for Additional Information from TasWater has been received regarding the Strait Link development. For TasWater to determine potential hydraulic service capacity limitations they require the calculation of equivalent tenements (ETs) for the development.

The Strait Link development is occurring within the 2-10 Massey Greene Dr South Burnie site. All existing, demolished, and proposed infrastructure within the property will be considered.

Classification of Existing and Proposed Buildings

Table A1 of the TasWater Supplement to Water Supply Code of Australia WSA03-2011-3.1 MRWA Edition V2.0 is the basis for the ET calculations for both water and sewer.

The most appropriate ET codes for the existing and proposed infrastructure on site are:

ET Code	Development Type	Water	Sewer	Units	Comments/ Changes
MM	Metal Processing & Manufacturing				
MM00	Industrial/commercial manufacture and fabrication including workshop and warehouse	Use CF00 Rate	Use CF00 Rate	Shower/WC/Urinal	MM's all merged & use CF00 rate as per PSP
CF00	Amenities	0.4	0.6	Shower/WC/Urinal	Updated units & Merger of CF08 & CF09
TW	Biological loading exceeding domestic sewage	N/A	Case-by-case	Case-by-case	Assessed by TasWater Trade Waste Team
BE04	Office	0.004	0.006	GBFA(sqM)	

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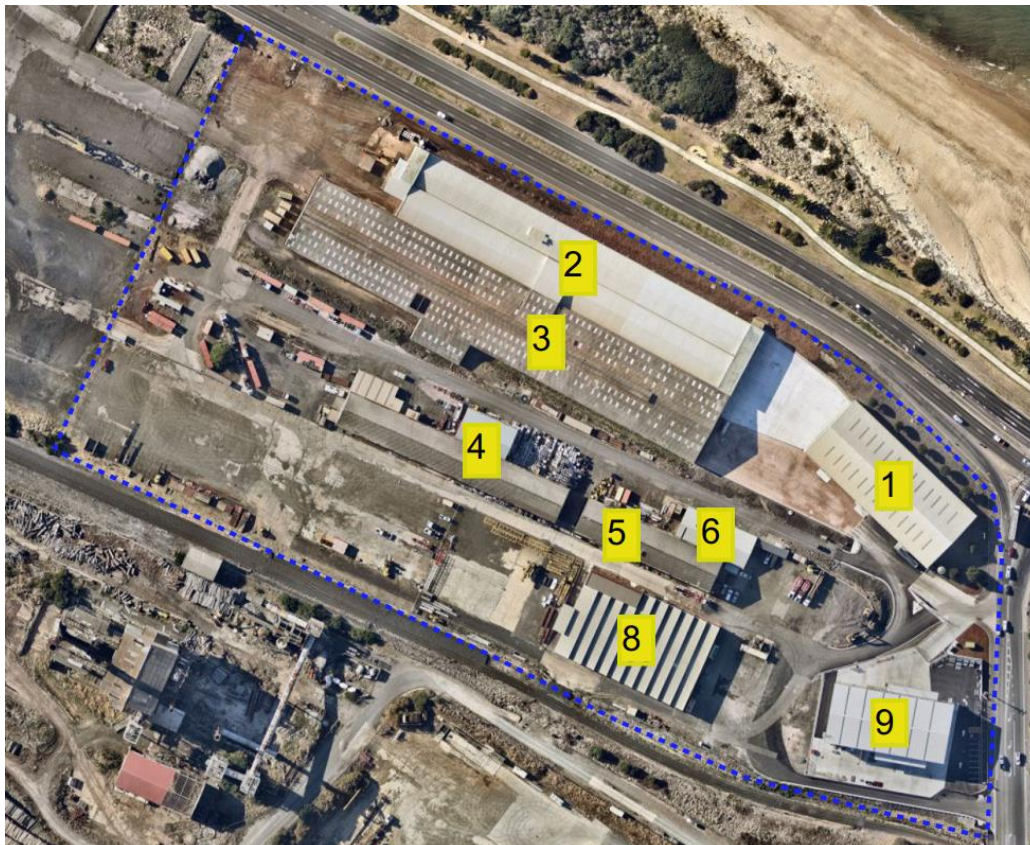
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Note that the office code has been used for the Strait Link development.

Existing Infrastructure

There are a number of existing buildings on site as per the figure below:



Building Number	WC/Shower/Urinal Units
Building 1	0 toilets, 0 showers, 0 urinals
Building 2	2 toilets, 0 showers, 1 urinal
Building 3	0 toilets, 0 showers, 0 urinal
Building 4 (Existing)	332m2 ground floor office, 332m2 first floor office
Building 5	0 toilets, 0 showers, 0 urinal
Building 6	1 toilet, 0 showers, 0 urinals

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Building 7	0 toilets, 0 showers, 0 urinal
Building 8	3 toilets, 0 showers, 1 urinal
Building 9	2 toilets, 0 showers, 0 urinals

Proposed Infrastructure

The proposed Strait Link development involves the demolition of a section of Building 4, construction of a new connecting warehouse, development of the existing office space, and development of the external areas including construction of a new wash down bay. The calculation for the ETs will be based on the gross building floor area of the office space.

Building 4 (Proposed)	<p>Ground floor: 332m2 (no change from existing)</p> <p>First floor: 332m2 (no change from existing)</p> <p>Wash down bay: Discharge to sewer limited to 1500 kL/annum. Water to be recycled.</p>
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Existing and Proposed ET Calculation

The table below shows the existing and proposed ETs for the site.

Location	TasWater Code	Unit of Measurement	Water ETs per Unit	Sewer ETs per Unit	PRE DEVELOPMENT			POST DEVELOPMENT		
					Units	Water ETs	Sewer ETs	Units	Water ETs	Sewer ETs
Building 1	MM00	Shower/WC/Urinal	0.4	0.6	0	0	0	0	0	0
Building 2	MM00	Shower/WC/Urinal	0.4	0.6	3	1.2	1.8	3	1.2	1.8
Building 3	MM00	Shower/WC/Urinal	0.4	0.6	0	0	0	0	0	0
Building 4 (Strait Link)	BE04	GBFA (m2)	0.004	0.006	664	2.7	4	664	2.7	4
	TW	Case-by-case								1500kL/annum
Building 5	MM00	Shower/WC/Urinal	0.4	0.6	0	0	0	0	0	0
Building 6	MM00	Shower/WC/Urinal	0.4	0.6	1	0.4	0.6	1	0.4	0.6
Building 7	MM00	Shower/WC/Urinal	0.4	0.6	0	0	0	0	0	0
Building 8	MM00	Shower/WC/Urinal	0.4	0.6	4	1.6	2.4	4	1.6	2.4
Building 9	MM00	Shower/WC/Urinal	0.4	0.6	2	0.8	1.2	2	0.8	1.2
TOTAL						6.7	10		6.7	10

Results

The Strait Link development leads to no change to the sewer and water ETs at 2-10 Massey Greene Drive. The proposed washdown bay introduces an additional discharge to sewer. Discharge will be limited to 1500 kL/annum. Refer to MEM-003 for Trade Waste Risk Categorisation Memo which outlines the proposal for

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tradewaste. Water used in the wash down bay will be re-circulated wash down water that has been treated by a Syrx Environmental system.

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Memorandum No 05

Project: Strait Link / 245019
For: Strait Link C/- Stubbs Construction
From: Brendan Stanborough
Date: 20th August 2025
Revision: A
Status: For Information
Subject: Strait Link Development – Landslide Risk Assessment
Attention: Burnie City Council Planning Department

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Landslide Risk Assessment

Proposed Industrial Development – 2-10 Massey Greene Drive, Burnie, TAS

1.0 INTRODUCTION

This Landslide Risk Assessment report has been prepared to support a development application for a new industrial facility at 2-10 Massey Greene Drive, Burnie, Tasmania.

The overall development proposal includes the construction of new buildings, pavements, signage, and light poles. A portion of the site works in the South West corner involves the construction of a small retaining wall near a property boundary, which will have a maximum retained height in cut of 1.2 metres.

A review of the Tasmanian Planning Scheme's Landslip Hazard Code overlay map shows that only the proposed retaining wall is located within a mapped landslip hazard area. The other proposed building works are outside this designated zone.

Therefore, the purpose of this report is to specifically assess the risk to property from potential landslip events associated with the retaining wall, in accordance with the requirements of the Landslip Hazard Code (C15.0). This assessment demonstrates that this component of the development satisfies the relevant performance criteria of the code.

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2.0 RISK ASSESSMENT METHODOLOGY

The risk assessment principles applied in this report are based on the Australian Geomechanics Society (AGS) "Practice Note Guidelines for Landslide Risk Management 2007". This qualitative methodology involves answering the following questions:

- **Hazard Identification:** What might happen?
- **Likelihood Analysis:** How likely is it?
- **Consequence Analysis:** What damage or injury might result?
- **Risk Evaluation:** How important is it?

The risk is evaluated as a combination of the likelihood of a landslip event occurring and the potential consequences. This report uses the qualitative risk terminology provided in the AGS (2007) guidelines.

3.0 SITE DESCRIPTION & GEOTECHNICAL MODEL

3.1 Site Location and Topography

The subject site, 2-10 Massey Greene Drive, is located in an established **industrial area** of Burnie. The site exhibits a **moderate to minor slope with a Northerly aspect**.

The slope becomes much more severe South of the site toward the centre of the landslip hazard zone.

3.2 Regional Geology

The geology of the Burnie area is dominated by Tertiary-aged basalt and associated volcanic soils and sediments. These materials, when weathered, can form clays and silts that are susceptible to instability, particularly on steeper slopes or where groundwater and surface water are concentrated.

An extract below shows the varying geology within the site area. The geology of concern is (Qxt) being primarily located to the South of the property. The majority of the site area is underlain with more benign geology formations being sands and sediments consistent with the low lying areas around the Burnie adjacent the coastline.



Figure 1 Regional geology extract from ListMAP

3.3 Site Context and Description of Works

The retaining wall is located in the South West corner of the site. It extends for approximately 120 m and retains up to 1.2m of earth. The proximity of the wall to the boundary is 450mm. The wall extends along the Southern boundary along the fence line and transitions around to the Western boundary into a fill configuration to support the circulating road.

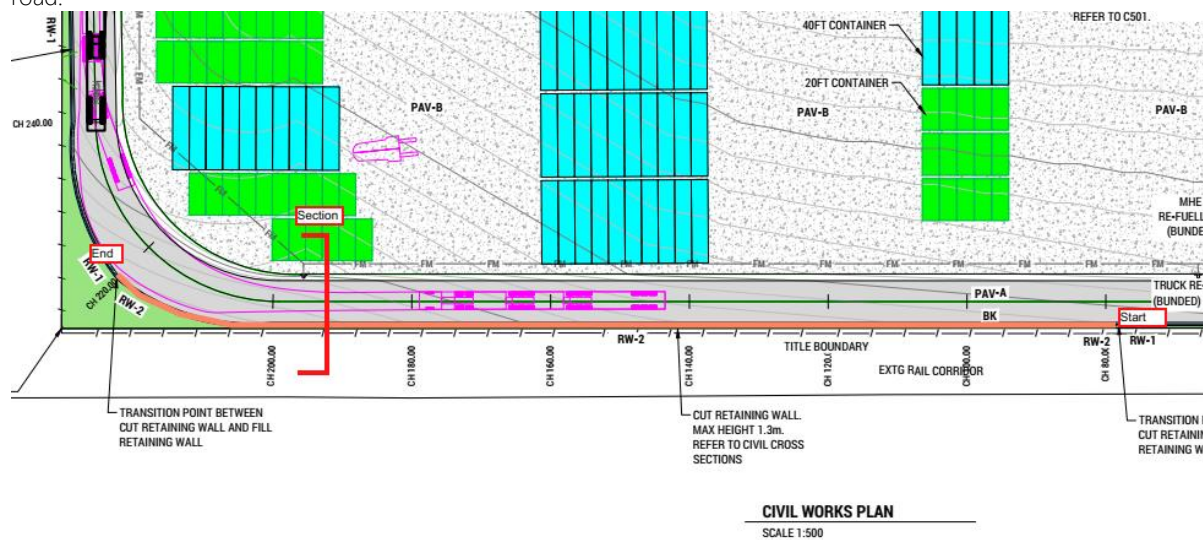


Figure 2 Site Plan showing wall and section location

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4.2 Risk to Property

The risk to property from the identified hazard is assessed below, assuming the retaining wall is designed and constructed in accordance with standard engineering practice, including the recommendations in Section 6.0 of this report.

Table 1 Landslide Risk Profile

Scenario	Likelihood	Consequence	Risk Profile
Small-scale instability impacting the proposed wall or adjacent property.	Unlikely	Minor: Potential for localised damage to the wall, fences, or non-critical paved areas. No impact on buildings is anticipated.	Low

This assessment concludes that the proposed retaining wall presents a **Low** level of risk to property.

5.0 ASSESSMENT AGAINST THE LANDSLIP HAZARD CODE (C15.0)

Clause C15.2.1 of the Tasmanian Planning Scheme states that the Landslip Hazard Code applies to use or development of land *within a landslip hazard area*. As the proposed new buildings are located a considerable distance from the mapped hazard area (and the related geological hazards), they are not functionally important to this risk assessment. Other related infrastructure i.e. lightpoles, pavements and signage are also functionally irrelevant to the risk assessment as they do exacerbate hazards by nature of having no impact on prevailing ground surfaces. Therefore this assessment focuses solely on the retaining wall.

As there is no Acceptable Solution for development within a landslip hazard area, the proposal is assessed against the Performance Criteria of Clause C15.6.1.

Objective: *That building and works on land within a landslip hazard area can: (a) minimise the likelihood of triggering a landslip event; and (b) achieve and maintain a tolerable risk from a landslip.*

Performance Criterion P1.1: *"Building and works within a landslip hazard area must not cause or contribute to a landslip on the site or on adjacent land, and must achieve and maintain a tolerable risk from a landslip..."*

Assessment: The proposed retaining wall is a minor engineering structure with a maximum height of 1.2m. A structure of this scale, when designed and constructed to appropriate engineering standards, will not increase the risk of landslip. In fact, by providing formal support to a localised slope, it is considered to improve stability in its immediate vicinity.

To ensure compliance with this performance criterion, the following will be incorporated into the design and construction:

1. **Engineering Design:** The retaining wall should be designed by a qualified structural or geotechnical engineer in accordance with Australian Standards (e.g., AS 4678-2002 Earth-retaining structures).
2. **Drainage:** Subsoil drainage (e.g., aggregate and slotted pipe) should be installed behind the wall to prevent the buildup of hydrostatic pressure, which is a key trigger for instability.
3. **Foundation:** The wall's footings will be founded on firm, competent material below any weak soils or fills encountered near surface.
4. **Surface Water Management:** Surface water from upslope areas will be collected and directed away from the rear of the wall to prevent saturation of the retained soil.

By implementing these standard engineering measures, the retaining wall will not cause or contribute to a landslide. The resulting risk level is assessed as **Low**, which is considered a **tolerable risk** for this type of minor development in an industrial setting, thereby satisfying Performance Criterion P1.1.

6.0 RECOMMENDATIONS & CONCLUSION

The landslide risk assessment demonstrates that the proposed 1.2m high retaining wall at 2-10 Massey Greene Drive, Burnie, presents a **Low** risk to property. This assessment pertains only to the retaining wall, as all other proposed works are outside the mapped landslide hazard area.

The development satisfies the Performance Criteria of the Tasmanian Planning Scheme – Landslip Hazard Code (C15.0), provided that the following recommendations are implemented for the retaining wall:

1. The final design of the retaining wall is undertaken by a suitably qualified engineer.
2. The design explicitly includes measures for managing both subsoil and surface water.
3. Construction is carried out in accordance with the engineer's design and specifications.

Based on this assessment, the proposed retaining wall is considered appropriate for the site and the landslip hazard context.