

# PLANNING APPLICATION

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## Status:

### Reference

PLN-HOB-2025-0609

### Address

16 CLARE ST NEW TOWN TAS  
7008

### Titles

45950/1

## Before you start

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Before you start your application, you will need to know if you require planning approval or not.

If you are unsure if you require a permit, use the [PlanBuild Tasmania Enquiry Service](#) to lodge a request for advice from the relevant Council.

Once your application has been submitted the Council will review your application. If payment has not been made, you will be sent a request for the payment of application fees via PlanBuild Tasmania.

Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification containing a task to complete.

## Pre-Application Advice

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Have you spoken with anyone at Council about this application?

Yes - enter details below

No - continue to the next section

If yes, provide the name of the person you contacted

## Applicant

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Name	Email	Phone	Address	Involvement
Personal Information Removed				

## Owners

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Name	Email Address	Address
Personal Information Removed		

## Certificate(s) of Title

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Selected Titles

45950/1

Total Area: 481m<sup>2</sup>

## Owner Notification

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Are you the sole owner of the land?

Yes - continue to the next section

No - answer question below

If no, have you notified all owners, joint or part owners of your intention to submit this application?

Yes - enter owner details below

No - you must notify all owners before proceeding with this application

List all owners, joint or part owners as recorded on the Title documents notified:  
MCFENG INVESTMENTS PTY LTD (Client Beth McDonald is a listed Director)

Enter the date that the last owner, joint or part owner was notified  
21/11/2025

#### Declaration

I declare that all land owners, joint or part owners have been notified of this planning application.

## Crown Land Consent

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Is Crown Land involved in the proposed use or development?

- Yes - complete question below  
 No - continue to the next section - see further information below  
 Unsure

If yes, has written Crown Land consent been obtained?

- Yes - upload written consent  
 No - application will not be progressed until consent has been provided

## General Manager Consent

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Is Council-owned or administered land involved in the proposed use or development?

- Yes - complete question below  
 No - continue to the next section  
 Unsure

If yes, has written consent been obtained from the Council General Manager?

- Yes - upload written consent  
 No - application will not be progressed until consent has been provided

## Proposed Use or Development

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What is the reason for your planning application?

- I want to change how the property is used  
 I want to use the property for visitor accommodation  
 I want to subdivide  
 I want to undertake a new development or alteration  
 I want to do a minor boundary adjustment  
 I want to put up a sign(s)  
 I want to demolish  
 I want to do works only  
 Other

If your application is to subdivide, please enter the number of proposed lots.  
0

If your application is for signage, please enter the number of signs.

Is the property a Tasmanian Heritage Listed Property?

- Yes  
 No

Is the application for an EPA Activity under the Environmental Management and Pollution Control Act 1994?

- Yes  
 No  
 Unsure

Is the proposed use or development permitted or discretionary?

- Permitted  
 Discretionary  
 Unsure if permitted or discretionary

**Provide a full description of the proposed use or development**  
See planning report.

**Will the proposed use or development involve a road reserve?**

Yes - complete the section below

No - continue to the next section

Unsure

**If yes, enter the address(es) or locations below:**

**If yes, how will the road reserve be affected?**

## Value of Works

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**What is the estimated value of the works?**

750000

## Supporting Documents

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Version	Document Date	Document Type	Description	Prepared By
1	21 Nov 2025	Planning Assessment Report	Supporting planning report	Era Advisory
1	18 Nov 2025	Architectural Plans	Proposal plans	Cumulus Studio
1	21 Nov 2025	Property Title Document	URDSReportLong-45950-1.pdf	Era Advisory
1	21 Nov 2025	Property Title Document	FolioPlan-45950-1.pdf	Era Advisory
1	11 Nov 2025	Other	Traffic Impact Assessment	Ratio Consultants
1	21 Nov 2025	Property Title Document	FolioText-45950-1.pdf	Era Advisory

## Next steps

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When you have completed all the necessary fields and attached all required documents to support your application, click on the green 'Save & Submit' button at the top right of this form.

Once submitted, the Council will review your application. A request for the payment of application fees will be sent to you via PlanBuild Tasmania.

Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification from PlanBuild which will tell you what you need to provide to continue the application.

Form published: 14/05/2025 15:58

SEARCH OF TORRENS TITLE

VOLUME 45950	FOLIO 1
EDITION 3	DATE OF ISSUE 25-Nov-2025

SEARCH DATE : 16-Dec-2025

SEARCH TIME : 09.29 am

DESCRIPTION OF LAND

City of HOBART

Lot 1 on Diagram [45950](#)

Derivation : Part of 0A-2R-18Ps. Gtd to I. Hyams

Prior CT [4700/74](#)

SCHEDULE 1

[N284407](#) TRANSFER to MCFENG INVESTMENTS PTY LTD Registered  
25-Nov-2025 at 12.01 pm

SCHEDULE 2

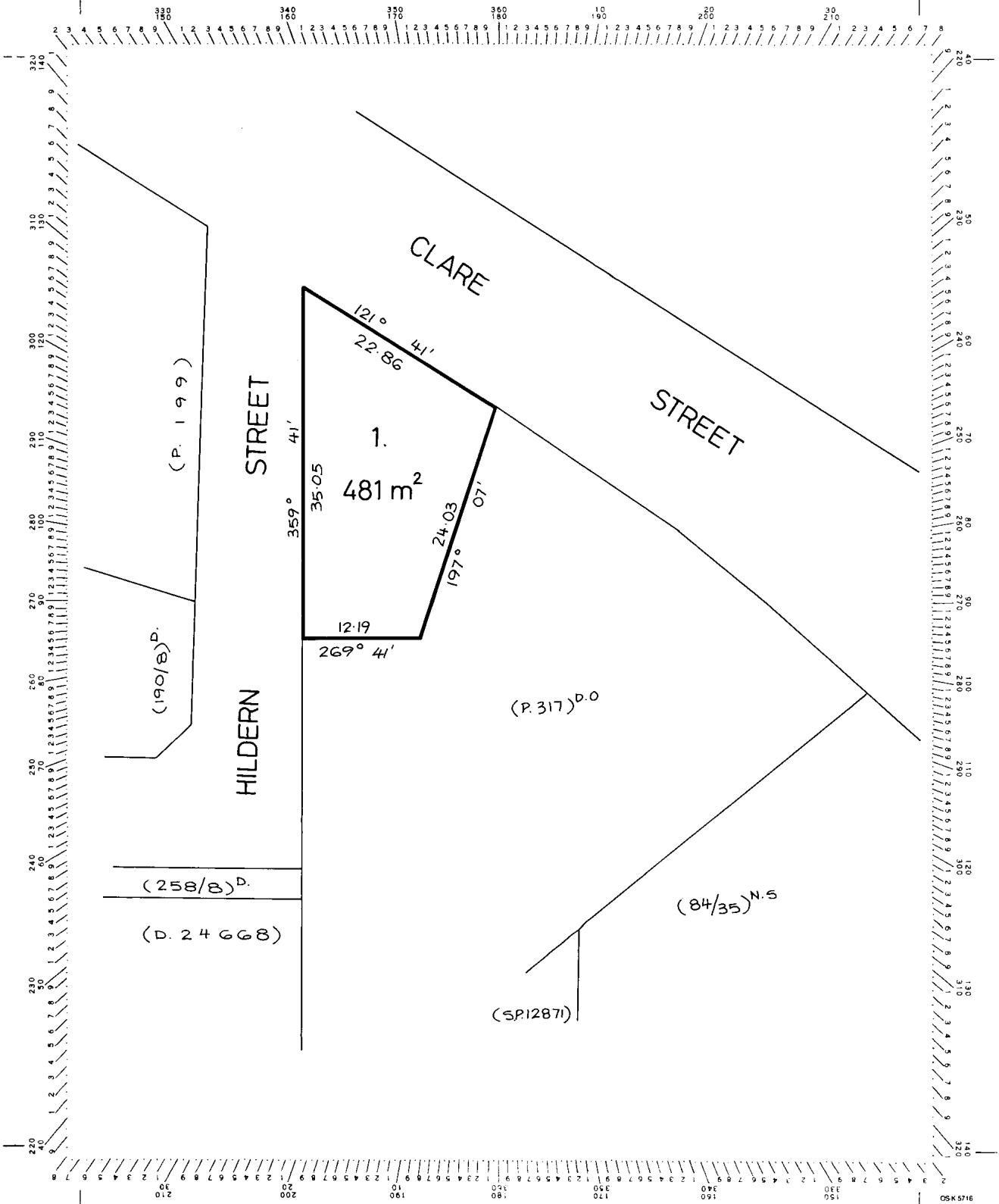
Reservations and conditions in the Crown Grant if any

[E431505](#) MORTGAGE to Westpac Banking Corporation Registered  
25-Nov-2025 at 12.02 pm

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

Owner: L.T. ACT 1980.	PLAN OF SURVEY of land situated in the <b>CITY OF HOBART</b>	Registered Number <b>D.45950</b>
Title Reference: Y.12800		Approved <b>11 SEP 1990</b>
Grantee: PART OF 0-2-18 GTD. TO ISRAEL HYAMS.	COMPILED FROM P.317 D.O.	<i>Michael P. ...</i> Recorder of Titles
SCALE 1: 400 MEASUREMENTS IN METRES		



# GENERAL MANAGER CONSENT

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**Reference**

GMC-HOB-2025-0056

**Address**

16 CLARE ST NEW TOWN TAS 7008

**Titles**

45950/1

## Applicant

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Name	Email	Phone	Address	Involvement
Era Advisory	enquiries@era-advisory.com.au	0361650443	Level 1, 125A Elizabeth Street, Hobart, Tasmania, Australia, 7000	Applicant

## Council Reference

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**Council Proposed Use or Development Description**

## Consent Information

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**Information**

I advise that pursuant to Section 52 of the Land Use Planning and Approvals Act 1993, I grant my consent on behalf of the Council as the owner/administrator of the above land for you to make application to the Council for a planning permit for the development described above.

Please note that the granting of the consent is only for the making of the application and in no way should such consent be seen as prejudicing any decision the Council is required to make as the statutory planning authority.

This consent does not constitute an approval to undertake any works and does not authorise the owner, developer or their agents any right to enter or conduct works on any Council managed land whether subject to this consent or not.

If planning approval is granted by the planning authority, you will be required to seek approvals and permits from the Council as both landlord, land manager, or under other statutory powers (such as other legislation or Council By-Laws) that are not granted with the issue of a planning permit under a planning scheme. This includes the requirement for you to reapply for a permit to occupy a public space under the Council's Public Spaces By-Law if the proposal relates to such an area.

Accordingly, I encourage you to continue to engage with the Council about these potential requirements.

## Signatory

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**Signed:** Michael Stretton

**Signature applied by:** Nicole Spooner

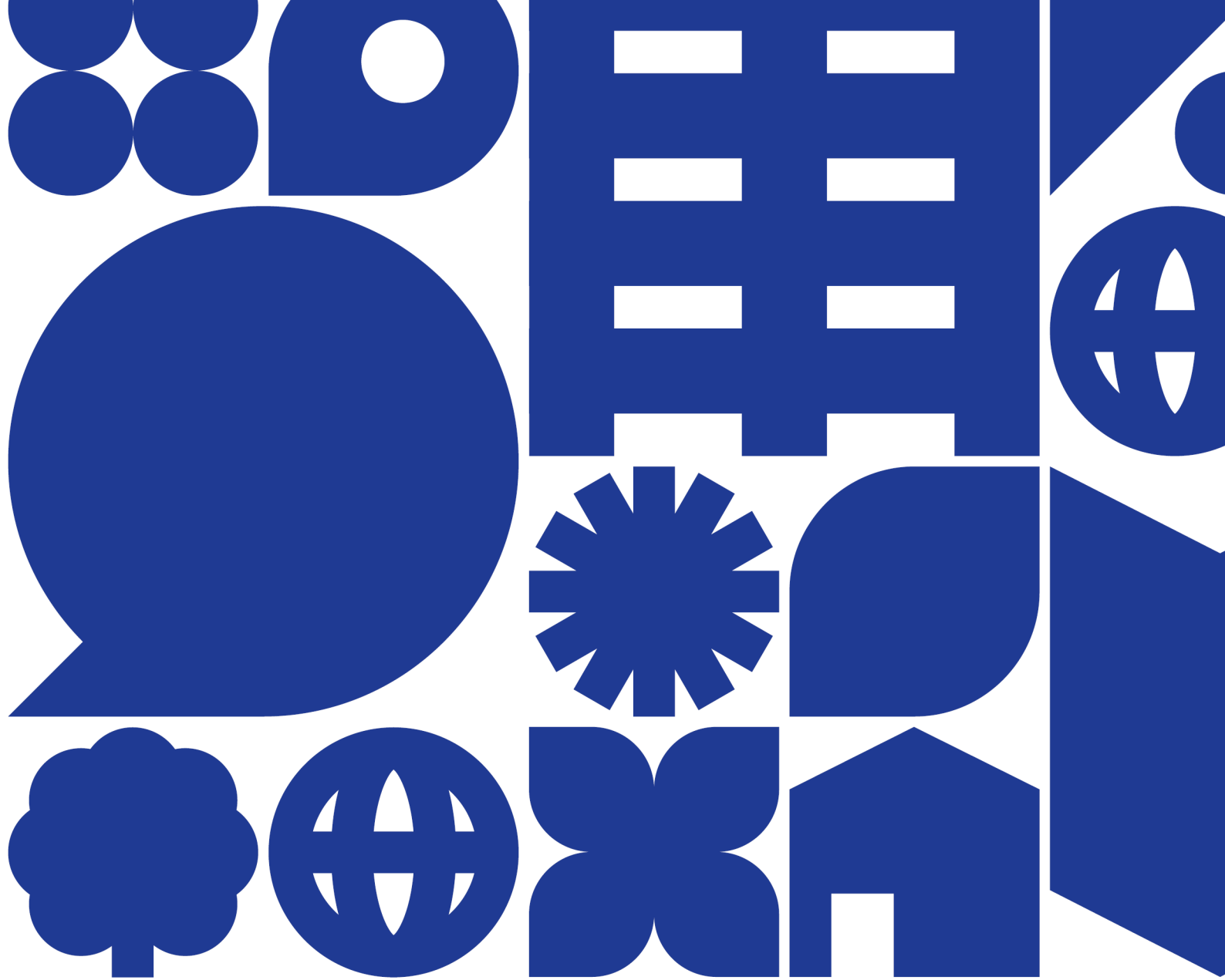
## Supporting Documents

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No Documents.

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Submitted on **03/02/2026**



Tasmanian Lung Centre  
Redevelopment  
Supporting planning report

Era Advisory acknowledge palawa as the Traditional Owners of lutruwita (Tasmania).

They are the original custodians of our land, sky and waters.

We respect their unique ability to care for country and deep spiritual connection to it.

We honour and pay our respect to Elders past and present, whose knowledge and wisdom has and will ensure the continuation of culture and traditional practices.

We acknowledge that their sovereignty has never been ceded.

Always was, always will be.

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Era Advisory Pty Ltd

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ABN 21 681 443 103

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Level 1, 125A Elizabeth St Hobart 7000  
(03) 6165 0443

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[enquiries@era-advisory.com.au](mailto:enquiries@era-advisory.com.au)  
[era-advisory.com.au](http://era-advisory.com.au)

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Client                      Beth McDonald

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Document version      Council submission (Revision 2)

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Date                        23/02/2026

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Author                    Grace Elliott

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Reviewer                Emma Riley

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Job number              2526-036

# Permit overview

## Permit application details

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<b>Applicant</b>	Era Advisory on behalf of Beth McDonald
<b>Owner</b>	MCFeng Investments Pty Ltd
<b>Address</b>	16 Clare Street New Town, TAS 7008
<b>Lot description</b>	Folio of the Register 45950, Lot 1
<b>Description of proposal</b>	Alterations and additions at 16 Clare Street to accommodate Tasmanian Lung Centre medical practice.

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## Relevant Planning Provisions

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<b>Applicable planning scheme</b>	<i>Tasmanian Planning Scheme - Hobart</i>
<b>Zone(s)</b>	Inner Residential Zone
<b>Codes</b>	C2.0 Parking and Sustainable Transport Code C3.0 Road and Railway Assets Code C6.0 Local Historic Heritage Code
<b>Discretions</b>	<ul style="list-style-type: none"><li>· 9.3.1 Discretionary uses (A4/P4)</li><li>· 9.5.1 Non-dwelling development (A1/P1)</li><li>· 9.5.1 Non-dwelling development (A5/P5)</li><li>· 9.5.1 Non-dwelling development (A6/P6)</li><li>· C.2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone (A1/P1)</li><li>· C2.6.2 Design and layout of parking areas (A1/P1)</li><li>· C6.7.1 Demolition within a local heritage precinct (A1/P1)</li><li>· C6.7.3 Buildings and works, excluding demolition (A1/P1)</li><li>· C6.7.3 Buildings and works, excluding demolition (A2/P2)</li></ul>

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# 1 Introduction

## 1.1 Purpose of the report

Era Advisory has been engaged by Beth McDonald from Tasmanian Lung Centre to provide a supporting planning report for alterations and additions to 16 Clare Street, New Town. The alterations and additions will accommodate the Tasmanian Lung Centre who will use the site as a medical clinic.

This report provides relevant background material, proposal details and an assessment against the relevant planning scheme provisions.

## 1.2 Enquiries

Enquiries relating to this planning should be directed to:

Grace Elliott  
Planner  
Era Advisory  
[enquiries@era-advisory.com.au](mailto:enquiries@era-advisory.com.au)  
(03) 6165 0443

## 1.3 Planning authority

The relevant planning authority is the City of Hobart.

## 1.4 Planning scheme

The proposal must be assessed against the provisions of the *Tasmanian Planning Scheme – Hobart*.

## 1.5 Title documentation

The project site consists of the following lot. Title documentation is provided in Appendix A.

Table 1 - Land comprising the subject site

Address	Owner	Title reference	Area
16 Clare Street, New Town TAS 7008	MCFeng Investments Pty Ltd	45950/1	479.7m <sup>2</sup>

# 2 Proposal description

## 2.1 Overview

The proposal includes alterations and additions to 16 Clare Street, New Town. The works will accommodate the Tasmanian Lung Centre who will move into the building. The Tasmanian Lung Centre is a respiratory medical practice and lung function testing service, helping to assess, monitor and treat adults with diseases of the respiratory system. The proposed works will create a medical practice that is accessible, cohesive and welcoming for both patients and staff.

## 2.2 Revisions

Revisions of the proposal plans are summarised as follows:

- Plans dated 18/11/2025 were submitted with the original planning application and are now superseded.
- Plans dated 05/12/2025 and 11/12/2025 were submitted as part of the General Manager Consent process and are now superseded.
- Plans dated 20/02/2026 are the current set of plans, responding to GMC and building surveyor requirements.

## 2.3 The site

The subject site is located at 16 Clare Street, New Town (Figure 1). The site has an asymmetrical four-sided shape, and is bounded by Clare Street to the north, Hildern Street to the west, and a residential property to the south and east.

The site is on a small slope and has an area of 481.6m<sup>2</sup>. It currently contains one building that was built as a house and has historically been used for Business and Professional Services as a medical centre. It has most recently been used as a medical clinic for obstetric and gynaecological services.

The surrounding area is predominantly characterised by residential development, with the health and community services industry also having a significant presence in the broader area. The site is approximately 400 m away from the Calvary Hospital site at 49 Augusta Road, Lenah Valley. It is not within the boundaries of the Calvary Hospital Surrounds Specific Area Plan. Facilities in the immediate area include the Relationships Australia Family Relationship Centre and the Health and Community Services Union office.



Figure 1 Aerial image of site and surrounds (Source: LISTmap)

## 2.4 Demolition

It is proposed to undertake demolition of the following:

- Internal demolition (exempt from planning scheme pursuant to clause 4.3.2)
- Frontage fences and gates
- Crossover and landscaping associated with the Hildern Street vehicle access (to be replaced)
- Trees as required
- Retaining wall on the northern end of the car park
- Access door, window and part of wall on the southern elevation
- Window and part of wall on the western elevation (Hildern Street frontage)
- Balustrade and entry ramp on the northern elevation (Clare Street frontage)
- Security bars from existing windows
- Existing roof sheeting, flashings and associated roof plumbing (to be replaced)
- Existing second-floor windows (to be replaced)
- Existing skylights (to be replaced)

A demolition plan is included within the proposal plans at Appendix B.

## 2.5 Built form

The proposed development includes the following:

- Internal works (exempt from planning scheme pursuant to clause 4.3.2)

- Minor extension towards the Clare Street frontage
- New entry walkway and DDA access ramp
- New awnings above entrances
- Replacement vehicle crossover at the Hildern Street frontage
- Upgraded car park with formalised car parking arrangement
- Gas bottle and rubbish enclosure
- Bicycle parking
- Landscaping

Proposal plans are included at Appendix B.

# 3 Planning controls

## 3.1 Statutory controls

The site is subject to the provisions of the *Tasmanian Planning Scheme – Hobart* (the planning scheme).

The subject site is zoned Inner Residential under the planning scheme, as shown in Figure 2.

The site is within the Clare Street local heritage precinct (HOB-C6.2.47). It is not listed as a local heritage place, nor is it on the Tasmanian Heritage Register.



Figure 2 Zoning of site and surrounds (Source: LISTmap)



Figure 3 Code overlays at the site with local historic heritage precinct shown in green hatching and Royal Hobart Hospital Helipad Specific Area Plan shown in black hatching (Source: LISTmap)

## 3.2 Specific Area Plan

The site is subject to the Royal Hobart Hospital Helipad Airspace Specific Area Plan (HOB-S10.0).

## 3.3 Relevant Codes

The following codes from the planning scheme are applicable to the application:

- C2.0 Parking and Sustainable Transport Code
- C3.0 Road and Railway Assets Code
- C6.0 Local Historic Heritage Code

## 3.4 Use status

Clause 6.2.1 of the planning scheme requires each proposed use or development to be categorised into one of the use classes in Table 6.2 of the planning scheme. It is considered that the proposal falls under Business and Professional Services, which is the existing use at the site, and is defined as follows:

*use of land for administration, clerical, technical, professional or similar activities. Examples include a bank, call centre, consulting room, funeral parlour, medical centre, office, post office, real estate agency, residential support services, travel agency and veterinary centre.*

Based on the services provided by Tasmanian Lung Centre, the proposal falls under the subclass of 'medical centre', which is defined in Table 3.1 of the planning scheme as:

*means use of land to provide health services (including preventative care, diagnosis, medical and surgical treatment, and counselling) to out-patients only.*

Business and Professional Services, if for a medical centre, is a discretionary use within the Inner Residential Zone.

# 4 Zone assessment

## 4.1 Zone purpose

The purpose of the Inner Residential Zone is:

- 9.1.1 *To provide for a variety of residential use or development that accommodates a range of dwelling types at higher densities.*
- 9.1.2 *To provide for the efficient utilisation of available social, transport and other service infrastructure.*
- 9.1.3 *To provide for non-residential use that:*
  - (a) primarily serves the local community; and*
  - (b) does not cause an unreasonable loss of amenity, through scale, intensity, noise, activity outside of business hours, traffic generation and movement, or other off site impacts.*
- 9.1.4 *To provide for Visitor Accommodation that is compatible with residential character.*

The proposal is not inhibiting residential use, as the site is already used as a medical practice. The proposal will provide for the efficient utilisation of existing service infrastructure. The Tasmanian Lung Centre serves the local community, providing respiratory care and lung function testing services to Hobart residents. The proposal will not cause an unreasonable loss of amenity to the residential zone, as further assessed in Section 4.2.1.

## 4.2 Applicable standards

Clause	Applicability
<b>Use Standards</b>	
9.3.1 Discretionary uses	A1/P1    Applicable.
	A2/P2    Applicable.
	A3/P3    Applicable.
	A4/P4    Applicable.
9.3.1 Visitor Accommodation	Not applicable. No Visitor Accommodation proposed.
<b>Development Standards for Dwellings</b>	
9.4 Development Standards for Dwellings	Not applicable. No dwelling proposed.

Clause	Applicability
<b>Development Standards for Non-Dwellings</b>	
9.5.1 Non-dwelling development	A1/P1 Applicable.
	A2/P2 Applicable.
	A3/P3 Applicable.
	A4/P4 Not applicable. No new fencing proposed.
	A5/P5 Applicable.
	A6/P6 Applicable.
9.5.2 Non-residential garages and carports	Not applicable. No garage or carport proposed.
<b>Development Standards for Subdivision</b>	
9.6 Development Standards for Subdivision	Not applicable. No subdivision proposed.

## 4.3 Clause 9.3.1 Discretionary uses

Acceptable Solutions	Performance Criteria
<b>Objective</b>	
That Discretionary uses do not cause an unreasonable loss of amenity to adjacent sensitive uses.	
<p><b>A1</b></p> <p>Hours of operation of a use listed as Discretionary, excluding Emergency Services, must be within the hours of:</p> <p>(a) 7.00am to 7.00pm Monday to Friday; and</p> <p>(b) 8.00am to 6.00pm Saturday and Sunday.</p>	<p><b>P1</b></p> <p>Hours of operation of a use listed as Discretionary, excluding Emergency Services, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:</p> <p>(a) the timing, duration or extent of vehicle movements; and</p> <p>(b) noise, lighting or other emissions.</p>
<b>Planner Response</b>	
Hours of operation at the site will be 8 am to 4:30 pm Monday to Friday. The acceptable solution (A1) is met.	
<p><b>A2</b></p> <p>External lighting for a use listed as Discretionary:</p> <p>(a) must not operate within the hours of 8.00pm to 6.00am, excluding any security lighting; and</p> <p>(b) security lighting must be baffled so that direct light does not extend into the adjoining property.</p>	<p><b>P2</b></p> <p>External lighting for a use listed as Discretionary, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:</p> <p>(a) the number of proposed light sources and their intensity;</p> <p>(b) the location of the proposed light sources;</p> <p>(c) the topography of the site; and</p> <p>(d) any existing light sources.</p>

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**Planner Response**

Any external lighting will be determined at a later stage. It is recommended that the planning permit places a condition on external lighting to address this standard by requiring compliance with the acceptable solution. The acceptable solution (A2) is met.

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**A3**

Commercial vehicle movements and the unloading and loading of commercial vehicles for a use listed as Discretionary, excluding Emergency Services, must be within the hours of:

- (a) 7.00am to 8.00pm Monday to Friday;
- (b) 9.00am to 12 noon Saturday; and
- (c) nil on Sunday and public holidays.

**P3**

Commercial vehicle movements and the unloading and loading of commercial vehicles for a use listed as Discretionary, excluding Emergency Services, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:

- (a) the extent and timing of traffic generation;
  - (b) the dispatch of goods and materials; and
  - (c) existing levels of amenity.
- 

**Planner Response**

There are no regular commercial vehicle movements associated with the existing and proposed use. Consumable deliveries for the practice are received approximately twice a month, within the hours specified in A3. The acceptable solution (A3) is met.

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**A4**

No Acceptable Solution.

**P4**

A use listed as Discretionary must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:

- (a) the intensity and scale of the use;
  - (b) the emissions generated by the use;
  - (c) the type and intensity of traffic generated by the use;
  - (d) the impact on the character of the area; and
  - (e) the need for the use in that location.
- 

**Planner Response**

There is no acceptable solution. Assessment against the performance criteria is required.

Under the planning scheme, 'adjacent' means near to, and includes adjoining. A 'sensitive use' means a residential use or a use involving the presence of people for extended periods except in the course of their employment such as a caravan park, childcare centre, dwelling, hospital or school.

Adjacent sensitive uses therefore include the adjoining dwelling at 14 Clare Street and other nearby dwellings on Hildern Street and Clare Street.

The proposed development will not cause an unreasonable loss of amenity to adjacent sensitive uses. The site is already used as a medical centre. The design is sympathetic to the residential nature of the area, and to the historic heritage value of the precinct.

The proposal includes two relocated air conditioning condenser units on the eastern elevation, approximately 0.9 m from the shared boundary with 14 Clare Street. The air conditioning condenser units will not cause an unreasonable loss of amenity to the adjoining sensitive use, as further discussed under the response to clause 9.5.1 (A4/P4).

As discussed in the Traffic Impact Assessment prepared by Ratio Consulting, all anticipated staff parking can be accommodated on site, while the remaining patient/ visitor parking demand can be accommodated within the nearby public on-street parking spaces without causing any unreasonable impact to the surrounding area. The proposal is likely to generate fewer than 20 vehicle movements per day.

The performance criteria (P4) are satisfied.

## 4.4 Clause 9.5.1 Non-dwelling development

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### Acceptable Solutions

### Performance Criteria

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#### Objective

That all non-dwelling development:

- (a) is compatible with the character, siting, apparent form, scale, bulk, massing and proportion of residential development; and
  - (b) does not cause an unreasonable loss of amenity on adjoining residential properties.
- 

#### A1

A building that is not a dwelling, excluding for General Retail and Hire, Food Services, garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is:

- (a) if the frontage is a primary frontage, not less than 3m, or if the setback from the primary frontage is less than 3.0m, not less than the setback, from the primary frontage, of any existing dwelling on the site;
  - (b) if the frontage is not a primary frontage, not less than 2m, or if the setback from the primary frontage is less than 2.0m, not less than the setback, from the primary frontage, of any existing dwelling on the site; or
  - (c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining properties on the same street.
- 

#### P1

A building that is not a dwelling, excluding for General Retail and Hire, or Food Services, must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.

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#### Planner Response

The subject site is on a corner site with Clare Street being the primary frontage. The extension on the northern elevation towards the Clare Street frontage will be set back further than the existing built form, which has a setback of approximately 1.9 m.

The proposed access ramp will extend practically up to the title boundary with a setback from Hildern Street of less than 10 cm. The access ramp is required for DDA compliance, enabling accessibility of the building. The new staircase entry will also have a reduced setback of approximately 0.9 m from Hildern Street. It is noted that the existing building already has a setback of less than 2 m to Hildern Street (approximately 1.8 m). The reduced setback is required to achieve accessible entry ways from Hildern Street.

Assessment against the performance criteria is required.

Properties on Hildern Street have varying building setbacks. Similarly reduced setbacks include the garage at the adjacent property at 14 Clare Street. Directly opposite the Hildern Street frontage is Relationships Australia's Family Relationship Centre, which is compatible with the proposed entry way to the Tasmanian Lung Centre as two adjacent Business and Professional Services uses.

Furthermore, siting the new entry way towards Hildern Street creates a more sympathetic outcome for the local heritage precinct, which centres around Clare Street as the listed component street.

Overall, the proposed setbacks will be compatible with the streetscape as the primary building setbacks will be retained and the reduced setbacks to accommodate the proposed entry way will not detract from the amenity of Hildern Street. New landscaping at the Hildern Street frontage associated with the entry way will improve streetscape amenity.

The performance criteria (P1) are satisfied.

---

**A2**

A building that is not a dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:

- (a) be contained within a building envelope (refer to Figures 9.1, 9.2 and 9.3) determined by:
  - (i) a distance equal to the frontage setback of 3m, or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and
  - (ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side or rear boundaries to a building height of not more than 9.5m above existing ground level; and
- (b) only have a setback within 1.5m of a side or rear boundary if the building:
  - (i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or
  - (ii) does not exceed a total length of 9m or one-third of the length of the side or rear boundary (whichever is lesser).

**P2**

The siting and scale of a building must:

- (a) not cause an unreasonable loss of amenity having regard to:
  - (i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;
  - (ii) overshadowing the private open space of a dwelling on an adjoining property;
  - (iii) overshadowing of an adjoining vacant property; and
  - (iv) visual impacts caused by the apparent scale, bulk or proportions of the building when viewed from an adjoining property; and
- (b) provide separation between buildings on adjoining properties that is consistent with that existing on established properties in the area.

---

**Planner Response**

The maximum building height above natural ground level will be unchanged.

As the site is a corner site, there is no rear boundary. The proposed gas bottle and rubbish enclosure and relocated air conditioner condensers will have setbacks of less than 1.5 m from the side boundaries. Each of these structures will not exceed a total length of 9 m. The rest of the built form along the side boundary is existing

The acceptable solution (A2) is met.

---

**A3**

A building that is not a dwelling, must have:

- (a) a site coverage of not more than 65% (excluding eaves up to 0.6m); and
- (b) a site area of which not less than 15% is free from impervious surfaces.

**P3**

A building that is not a dwelling must have:

- (a) site coverage consistent with that on established properties in the area; and
- (b) a reasonable space for the planting of gardens and landscaping.

---

**Planner Response**

The site coverage is 33.5%.

25.5% of the site will be free from impervious surfaces.

The acceptable solution (A3) is met.

---

**A5**

Outdoor storage areas, for a building that is not a dwelling including waste storage must not:

- (a) be visible from any road or public open space adjoining the site; and
- (b) encroach upon parking areas, driveways or landscaped areas.

**P5**

Outdoor storage areas, for a building that is not a dwelling, must be located or screened to minimise their impact on views into the site from any roads or public open space adjoining the site, having regard to:

- (a) the nature of the use;
- (b) the type of goods, materials or waste to be stored;
- (c) the topography of the site; and
- (d) any screening proposed.

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**Planner Response**

There is one proposed outdoor storage area enclosing the gas bottle and rubbish bins. It will be visible from the Hildern Street frontage and therefore requires assessment against the performance criteria.

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The outdoor storage area increases amenity by providing screening for gas bottle infrastructure and rubbish bins. The storage area will not encroach upon parking areas, driveways or landscaped areas. While located in the car parking area, the storage area will be at the property boundary and will not encroach on the formalised car parking arrangement.

The performance criteria (P5) are satisfied.

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**A6**

Air extraction, pumping, refrigeration systems or compressors, for a building that is not a dwelling, must have a setback not less than 10m from a property containing a sensitive use.

**P6**

Air conditioning, air extraction, pumping, heating or refrigeration systems or compressors, for a building that is not a dwelling, within 10m of the boundary of a property containing a sensitive use must be designed, located, baffled or insulated to not cause an unreasonable loss of amenity, having regard to:

- (a) the characteristics and frequency of any emissions generated;
  - (b) the nature of the proposed use;
  - (c) the topography of the site and location of the sensitive use; and
  - (d) any mitigation measures proposed.
- 

**Planner Response**

The proposal includes two relocated air conditioning condenser units on the eastern elevation, approximately 0.9 m from the shared boundary with a property containing a sensitive use (dwelling at 14 Clare Street). The proposal therefore requires assessment against the performance criteria.

The air conditioning condenser units will not cause an unreasonable loss of amenity to the adjoining sensitive use as they will have a setback of approximately 15 m from the dwelling and will be separated from the dwelling by an existing vegetated buffer along the boundary at 14 Clare Street. The condenser units will be typical air conditioner condenser units and will not create unreasonable noise or other emissions.

There are two existing air conditioning condenser units on the eastern elevation to be retained. The two condenser units to be relocated to the eastern elevation will have the same setback as the existing condenser units.

The performance criteria (P6) are satisfied.

# 5 Specific Area Plan assessment

## 5.1 Application of the SAP

16 Clare Street is subject to the Royal Hobart Hospital Helipad Airspace Specific Area Plan (HOB-S10.0) (the SAP).

## 5.2 Applicable standards

Clause	Applicability
<b>Development Standards for Buildings and Works</b>	
HOB-S10.7.1 Building height	A1/P1 Applicable.

## 5.3 Clause HOB-S10.7.1 Building height

This clause is in addition to the Inner Residential Zone building height standards.

Acceptable Solutions	Performance Criteria
<b>Objective</b> That buildings do not interfere with safe aircraft operations in the vicinity of the Royal Hobart Hospital helipad.	
<b>A1</b> Building height, including minor protrusions, masts or aerials must be no more than: (a) 64.5m AHD if within the inner area; or (b) 100m AHD if within the outer area.	<b>P1</b> Buildings that exceed the specified height must not create an obstruction or hazard for the operation of aircraft, having regard to any advice from the Civil Aviation Safety Authority, the Department of Health and the helipad operator.
<b>Planner Response</b> The subject site is within the outer area. The maximum building height of the existing building will not be increased. The building height does not exceed 100 m AHD. The acceptable solution (A1) is met.	

# 6 Code assessment

## 6.1 Applicable codes

The relevant codes against which this proposal requires consideration are:

- C2.0 Parking and Sustainable Transport Code
- C3.0 Road and Railway Assets Code
- C6.0 Local Historic Heritage Code

## 6.2 Parking and Sustainable Transport Code

### 6.2.1 Application of the code

The Parking and Sustainable Transport Code applies to all use and development.

### 6.2.2 Applicable standards

Clause	Applicability	
<b>Use Standards</b>		
C2.5.1 Car parking numbers	A1/P1	Applicable.
C2.5.2 Bicycle parking numbers	A1/P1	Applicable.
C2.5.3 Motorcycle parking numbers	A1/P1	Applicable.
C2.5.4 Loading bays	A1/P1	Not applicable pursuant to clause C2.2.3.
C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone	A1/P1	Applicable.
<b>Development Standards for Buildings and Works</b>		
C2.6.1 Construction of parking areas	A1/P1	Applicable.
C2.6.2 Design and layout of parking areas	A1/P1	Applicable.
C2.6.3 Number of accesses for vehicles	A1/P1	Applicable.
	A2/P2	Not applicable. Not within the Central Business Zone or in a pedestrian priority street.

Clause		Applicability
C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone	A1/P1	Not applicable. Not within the General Business Zone or Central Business Zone.
C2.6.5 Pedestrian access	A1/P1	Applicable.
C2.6.6 Loading bays	A1/P1	Not applicable. No loading bay proposed.
	A2/P2	Not applicable. No loading bay proposed.
C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	A1/P1	Not applicable. Not within the General Business Zone or Central Business Zone.
	A2/P2	Not applicable. Not within the General Business Zone or Central Business Zone.
C2.6.8 Siting of parking and turning areas	A1/P1	Applicable.
	A2/P2	Not applicable. Not within the Central Business Zone.

### Parking Precinct Plan

C2.7.1 Parking precinct plan	A1/P1	Not applicable. No relevant parking precinct plan.
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## 6.2.3 Clause C2.5.1 Car parking numbers

### Acceptable Solutions

### Performance Criteria

#### Objective

That an appropriate level of car parking spaces are provided to meet the needs of the use.

#### A1

The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:

- (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;
- (b) the site is contained within a parking precinct plan and subject to Clause C2.7;
- (c) the site is subject to Clause C2.5.5; or
- (d) it relates to an intensification of an existing use or development or a change of use where:
  - (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or
  - (ii) the number of on-site car parking spaces for the existing use or development specified in Table

#### P1.1

The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:

- (a) the availability of off-street public car parking spaces within reasonable walking distance of the site;
- (b) the ability of multiple users to share spaces because of:
  - (i) variations in car parking demand over time; or
  - (ii) efficiencies gained by consolidation of car parking spaces;
- (c) the availability and frequency of public transport within reasonable walking distance of the site;
- (d) the availability and frequency of other transport alternatives;
- (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping;
- (f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity;
- (g) the effect on streetscape; and

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C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:

$$N = A + (C - B)$$

N = Number of on-site car parking spaces required

A = Number of existing on site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.

(h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.

#### **P1.2**

The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:

- (a) the nature and intensity of the use and car parking required;
- (b) the size of the dwelling and the number of bedrooms; and
- (c) the pattern of parking in the surrounding area.

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#### **Planner Response**

The proposal is subject to Clause 2.5.5.

The acceptable solution (A1) is met.

### **6.2.4 Clause C2.5.2 Bicycle parking numbers**

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#### **Acceptable Solutions**

#### **Performance Criteria**

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#### **Objective**

That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.

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#### **A1**

Bicycle parking spaces must:

- (a) be provided on the site or within 50m of the site; and
- (b) be no less than the number specified in Table C2.1.

#### **P1**

Bicycle parking spaces must be provided to meet the reasonable needs of the use, having regard to:

- (a) the likely number of users of the site and their opportunities and likely need to travel by bicycle; and
- (b) the availability and accessibility of existing and any planned parking facilities for bicycles in the surrounding area.

---

#### **Planner Response**

Table C2.1 requires 2 bicycle parking spaces per 8 practitioners. There will be up to five staff onsite comprised of three practitioners and two support staff. Based on three practitioners, approximately one bicycle parking space is required.

Bicycle parking will be provided onsite.

The acceptable solution (A1) is met.

## 6.2.5 Clause C2.5.3 Motorcycle parking numbers

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### Acceptable Solutions

### Performance Criteria

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#### Objective

That the appropriate level of motorcycle parking is provided to meet the needs of the use.

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#### A1

The number of on-site motorcycle parking spaces for all uses must:

- (a) be no less than the number specified in Table C2.4; and
- (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained.

#### P1

Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:

- (a) the nature of the proposed use and development;
  - (b) the topography of the site;
  - (c) the location of existing buildings on the site;
  - (d) any constraints imposed by existing development; and
  - (e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area.
- 

#### Planner Response

Table C2.4 generally requires one motorcycle parking space for every 20 car parking spaces for a use, after the first 20 spaces.

As the proposal is subject to P1 of clause C2.5.5, there is no set car parking requirement. With reference to Table C2.1 of the planning scheme, the proposed development would be required to provide 12 car parking spaces (based on three practitioners) in any other case.

Accordingly, no motorcycle parking space is required to be provided.

The acceptable solution (A1) is met.

## 6.2.6 Clause C.2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone

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### Acceptable Solutions

### Performance Criteria

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#### Objective

To:

- (a) facilitate the reuse of existing non-residential buildings within the General Residential Zone and Inner Residential Zone; and
  - (b) to not cause an unreasonable impact on residential amenity by the car parking generated by that reuse.
- 

#### A1

Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, on-site car parking is not required for:

- (a) Food Services uses up to 100m<sup>2</sup> floor area or 30 seats, whichever is the greater; and
- (b) General Retail and Hire uses up to 100m<sup>2</sup> floor area, provided the use complies with the hours of operation specified in the relevant Acceptable Solution for the relevant zone.

#### P1

Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, the number of on-site car parking spaces must be sufficient to meet the reasonable needs of users and must not cause an unreasonable impact on residential amenity, having regard to:

- (a) car parking demand generated by the proposed use during its proposed hours of operation;
- (b) the availability of on-street and public car parking in the surrounding area;

- 
- (c) the availability and frequency of public transport within a 400m walking distance of the site;
  - (d) the availability and likely use of other modes of transport;
  - (e) the availability and suitability of alternative arrangements for car parking provision;
  - (f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;
  - (g) any car parking deficiency or surplus associated with the existing use of the land;
  - (h) any relevant parking plan for the area adopted by council;
  - (i) any existing on-street car parking restrictions; and
  - (j) the proportion of residential properties without off-street parking within a 100m radius of the subject site.
- 

### **Planner Response**

The proposal is not for Food Services or General Retail and Hire. Assessment against the performance criteria is required. The Traffic Impact Assessment prepared by Ratio Consultants provides a detailed assessment against the performance criteria, as follows:

### **Car Parking Demand**

Based on information provided by the applicant, appointment times generally range from 15 minutes to 1 hour. The proposed development will typically operate on weekdays between 8:30 am and 4:30 pm. It is understood that most patients attend both a lung function testing appointment and a respiratory physician appointment during the same visit with appointment times ranging from 15 minutes to up to one hour in duration.

The maximum car parking demand would likely comprise:

- Up to five staff parking spaces comprising:
  - 1 x respiratory physician
  - 2 x lung function scientist
  - 2 x support staff (receptionist or administrative staff)
- Up to six patient parking spaces based on:
  - 3 x patients currently being seen
  - 3 x patients in the waiting room

It is noted that the proposal would typically see around 17 appointments per day on average, which means that back-to-back appointments will not occur all of the time and the maximum patient parking demand of 6 spaces is likely to be experienced infrequently.

Noting that all staff parking would be accommodated on-site, the proposal may generate an overflow of up to six car parking spaces on-street for a duration typically less than one hour.

### **Publicly Available On-Street Parking**

Unrestricted on-street parking is available along the site's western frontage on Hildern Street. Short-term, time-restricted parking (1P and 2P) is available along the site's northern frontage on Clare Street, applicable between 8:00 am and 6:00 pm, Monday to Friday. All of this parking would be suitable for patients given the expected duration of appointments.

Additional unrestricted on-street parking is available on the opposite side of Clare Street and on both sides on Seymour Street. A review of recent Nearmap aerial imagery indicates that these spaces are moderately utilised depending on the time of day, with more than six on street spaces generally available for patient and visitor parking within close proximity to the subject site.

It is also noted that the area surrounding the site is primarily residential in nature. The parking demands of residential uses typically peak in the evening and overnight, outside of the operating hours of the proposed use, such that reliance on on-street parking promotes efficiencies in the utilisation of public parking.

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## Public Transport

The site is well serviced by public transport, with the nearest bus stop conveniently located within 70 m of the site, as outlined in Section 2.3 of this report, and the Glenorchy to Hobart City “Turn up and GO” route located within a 400 m walking distance. These services provide employees and visitors with an alternative to private vehicle use, thereby potentially reducing reliance on on-site staff parking use or on-street parking for patients.

## Other Modes of Transport

While dedicated on road bicycle facilities are not formally provided in the surrounding streets of the subject, Clare Street is identified as a ‘bike friendly’ road given its local road nature and predominantly residential and associated speed. Bicycle parking will also be provided on site to ensure that this is a viable mode of transport for those that choose to cycle. Footpaths are provided along the site frontages along Hildern Street and Clare Street, with dedicated pedestrian access provided separate from the vehicular access. It is therefore considered that alternative transport modes to private car are available and convenient for those who choose to use them.

## Summary

Based on the above assessment, the proposed parking supply of 6 spaces is considered sufficient to meet the reasonable needs of the use. All anticipated staff parking can be accommodated on site, while the remaining patient/ visitor parking demand can be accommodated within the nearby public on-street parking spaces without causing any unreasonable impact to the surrounding area.

Given that patient appointments are likely managed on an appointment-only basis, patients will be advised that no on-site parking is available and will therefore be expected to make alternative travel arrangements.

The performance criteria (P1) are satisfied.

## 6.2.7 Clause C2.6.1 Construction of parking areas

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### Acceptable Solutions

### Performance Criteria

#### Objective

That parking areas are constructed to an appropriate standard.

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#### A1

All parking, access ways, manoeuvring and circulation spaces must:

- (a) be constructed with a durable all weather pavement;
- (b) be drained to the public stormwater system, or contain stormwater on the site; and
- (c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.

#### P1

All parking, access ways, manoeuvring and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to:

- (a) the nature of the use;
- (b) the topography of the land;
- (c) the drainage system available;
- (a) the likelihood of transporting sediment or debris from the site onto a road or public place;
- (b) the likelihood of generating dust; and
- (c) the nature of the proposed surfacing.

---

#### Planner Response

The proposal includes upgrades to the existing car park to formalise car parking arrangements. The car parking area will be asphalt, as existing, and will continue to be drained to the public stormwater system.

The acceptable solution (A1) is met.

## 6.2.8 Clause C2.6.2 Design and layout of parking areas

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### Acceptable Solutions

### Performance Criteria

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## Objective

That parking areas are designed and laid out to provide convenient, safe and efficient parking.

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### A1.1

Parking, access ways, manoeuvring and circulation spaces must either:

- (a) comply with the following:
  - (i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;
  - (ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;
  - (iii) have an access width not less than the requirements in Table C2.2;
  - (iv) have car parking space dimensions which satisfy the requirements in Table C2.3;
  - (v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;
  - (vi) have a vertical clearance of not less than 2.1m above the parking surface level; and
  - (vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or
- (b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6.

### P1

- (a) All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:
  - (b) the characteristics of the site;
  - (c) the proposed slope, dimensions and layout;
  - (d) useability in all weather conditions;
  - (e) vehicle and pedestrian traffic safety;
  - (f) the nature and use of the development;
  - (g) the expected number and type of vehicles;
  - (h) the likely use of the parking areas by persons with a disability;
  - (i) the nature of traffic in the surrounding area;
  - (j) the proposed means of parking delineation; and
  - (k) the provisions of Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking and AS 2890.2 -2002 Parking facilities, Part 2: Off- -street commercial vehicle facilities.

### A1.2

Parking spaces provided for use by persons with a disability must satisfy the following:

- (a) be located as close as practicable to the main entry point to the building;
- (b) be incorporated into the overall car park design; and
- (c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities.<sup>1</sup>

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## Planner Response

The site is highly constrained with respect to the dimensions of the car parking area. To maximise the availability of staff parking on the site, the car park has been designed to meet the performance criteria. The Traffic Impact Assessment, prepared by Ratio Consultants, provides a detailed assessment against P1 as follows.

Staff parking space dimensions comply with AS 2890.1 for User Class 1 (i.e. staff parking) with a length of 5.4 metres and a width of 2.4 metres, and the accessible space complies with the dimensions set out in AS 2890.6, including adjacent shared area for entering and exiting the vehicle.

Parking opposite the carpark is restricted due to the presence of "No Parking" signs, which means access to and from the carpark will be uninhibited by vehicles parked on-street. The existing carpark will be regraded as shown on the Ground Floor Plan to coordinate the existing slope on Hildern Street with the normal requirements of carparks. While there may be some minor nonconformance with respect to the slope, this is necessary due to the existing gradients. No change to the existing footpath levels is proposed.

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Given that the carpark is proposed to be used by staff only (and patients requiring accessible parking) the number of vehicle movements is limited. It is expected that each staff parking space would generate two movements per day (one entry and one exit) plus occasional use of the accessible parking space. This level of traffic is very low and, given that Hildern Street is a low-volume cul-de-sac, is unlikely to result in any impact to traffic operation. The use of staff parking would be managed by the centre to minimise the amount of potential conflict.

There is sufficient sight distance to the carpark for the prevailing vehicle speeds.

Based on the above assessment, it is considered that the proposed carpark provides convenient, safe and efficient parking for staff, and people requiring an accessible parking space.

The performance criteria (P1) are satisfied.

## 6.2.9 Clause C2.6.3 Number of accesses for vehicles

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### Acceptable Solutions

### Performance Criteria

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#### Objective

That:

- (a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;
  - (b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and
  - (c) the number of accesses minimise impacts on the streetscape.
- 

#### A1

The number of accesses provided for each frontage must:

- (a) be no more than 1; or
- (b) no more than the existing number of accesses,
- (c) whichever is the greater.

#### P1

The number of accesses for each frontage must be minimised, having regard to:

- (a) any loss of on-street parking; and
  - (b) pedestrian safety and amenity;
  - (c) traffic safety;
  - (d) residential amenity on adjoining land; and
  - (e) the impact on the streetscape.
- 

#### Planner Response

There will be one vehicle access to the site, at the Hildern Street frontage.

The acceptable solution (A1) is met.

## 6.2.10 Clause C2.6.5 Pedestrian access

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### Acceptable Solutions

### Performance Criteria

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#### Objective

That pedestrian access within parking areas is provided in a safe and convenient manner.

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#### A1.1

Uses that require 10 or more car parking spaces must:

- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
  - (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or
  - (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and

#### P1

Safe and convenient pedestrian access must be provided within parking areas, having regard to:

- (a) the characteristics of the site;
- (b) the nature of the use;
- (c) the number of parking spaces;
- (d) the frequency of vehicle movements;
- (e) the needs of persons with a disability;
- (f) the location and number of footpath crossings;
- (g) vehicle and pedestrian traffic safety;

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(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.

(h) the location of any access ways or parking aisles; and

(i) any protective devices proposed for pedestrian safety.

### A1.2

In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.

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#### Planner Response

Pedestrian movement within the surrounding area is facilitated by footpaths along Clare Street and Hildern Street, as well as an internal footpath through the site. Access to the site is also supported by a DDA-compliant access ramp with a gradient of 1:14 and subject to the provision of an appropriate slip resistant surface as specified under AS2890.6, providing convenient connection from the DDA car space to the site.

The acceptable solution (A1) is met.

## 6.2.11 Clause C2.6.8 Siting of parking and turning areas

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### Acceptable Solutions

### Performance Criteria

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#### Objective

That the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.

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#### A1

Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas must be located behind the building line of buildings, excluding if a parking area is already provided in front of the building line.

#### P1

Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas, may be located in front of the building line where this is the only practical solution and does not cause an unreasonable loss of amenity to adjoining properties, having regard to:

- (a) topographical or other site constraints;
- (b) availability of space behind the building line;
- (c) availability of space for vehicle access to the side or rear of the property;
- (d) the gradient between the front and the rear of existing or proposed buildings;
- (e) the length of access or shared access required to service the car parking;
- (f) the location of the access driveway at least 2.5m from a window of a habitable room of a dwelling;
- (g) the visual impact of the vehicle parking and access on the site;
- (h) the streetscape character and amenity;
- (i) the nature of the zone in which the site is located and its preferred uses; and
- (j) opportunities for passive surveillance of the road.

---

#### Planner Response

The car parking area will be in its existing location, behind the existing building.

The acceptable solution (A1) is met.

## 6.3 Road and Railway Assets Code

### 6.3.1 Application of the code

The Road and Railway Assets Code applies to a use or development that:

- (a) will increase the amount of vehicular traffic or the number of movements of vehicles longer than 5.5m using an existing vehicle crossing or private level crossing;
- (b) will require a new vehicle crossing, junction or level crossing; or
- (c) involves a subdivision or habitable building within a road or railway attenuation area if for a sensitive use.

The proposal has potential to increase the amount of vehicular traffic using the existing vehicle crossing onto Hildern Street.

### 6.3.2 Applicable standards

Clause		Applicability
<b>Use Standards</b>		
C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction	A1/P1	Applicable.
<b>Development Standards for Buildings and Works</b>		
C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area	A1/P1	Not applicable. No sensitive use proposed.
<b>Development Standards for Subdivision</b>		
C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area	A1/P1	Not applicable. No subdivision proposed.

### 6.3.3 Clause C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

Acceptable Solutions	Performance Criteria
<b>Objective</b>	
To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.	
<b>A1.1</b> For a category 1 road or a limited access road, vehicular traffic to and from the site will not require: (a) a new junction; (b) a new vehicle crossing; or (c) a new level crossing.	<b>P1</b> Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to: (a) any increase in traffic caused by the use; (b) the nature of the traffic generated by the use; (c) the nature of the road; (d) the speed limit and traffic flow of the road;
<b>A1.2</b>	

---

For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.

- (e) any alternative access to a road;
- (f) the need for the use;
- (g) any traffic impact assessment; and
- (h) any advice received from the rail or road authority.

### A1.3

For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.

### A1.4

Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:

- (a) the amounts in Table C3.1; or
- (b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road.

### A1.5

Vehicular traffic must be able to enter and leave a major road in a forward direction.

---

#### Planner Response

Hildern Street is not a category 1 road or limited access road. A1.1 is not applicable.

No new junction, vehicle crossing or level crossing is proposed. A1.2 is not applicable.

The proposal does not affect the rail network. A1.3 is not applicable.

For vehicles up to 5.5 m long, Table C3.1 allows an acceptable increase of 20% or 40 vehicle movements per day using an existing vehicle crossing, whichever is greater. The proposal is likely to generate fewer than 20 vehicle movements per day.

Hildern Street is not a major road. A1.5 is not applicable.

The acceptable solution (A1) is met.

## 6.4 Local Historic Heritage Code

### 6.4.1 Application of the code

The Local Historic Heritage Code applies to development on land within a local heritage precinct. The subject site is within the Clare Street local heritage precinct (HOB-C6.2.47). The Local Historic Heritage Code applies. While not recognised as a heritage place on the local or state register, the site is recognised as a 'contributory' site in the heritage precinct.

### 6.4.2 Applicable standards

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Clause	Applicability
<b>Development Standards for Local Heritage Places</b>	
C6.6 Development Standards for Local Heritage Places	Not applicable. Site is not a local heritage place.

---

#### Development Standards for Local Heritage Precincts and Local Historic Landscape Precincts

Clause		Applicability
C6.7.1 Demolition within a local heritage precinct	A1/P1	Applicable.
C6.7.2 Demolition within a local historic landscape precinct	A1/P1	Not applicable. Not within a local historic landscape precinct.
C6.7.3 Buildings and works, excluding demolition	A1/P1	Applicable.
	A2/P2	Applicable.

#### Development Standards for Places or Precincts of Archaeological Potential

C6.8.1 Building and Works	Not applicable. Not within a place or precinct of archaeological potential.
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#### Significant Trees

C6.9 Significant Trees	Not applicable. No significant trees affected.
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#### Development Standards for Subdivision

C6.10 Development Standards for Subdivision	Not applicable. No subdivision proposed.
---	--

### 6.4.3 Clause C6.7.1 Demolition within a local heritage precinct

#### Acceptable Solutions

#### Performance Criteria

#### Objective

That demolition within a local heritage precinct does not have an unacceptable impact on the local historic heritage significance of the precinct.

#### A1

Within a local heritage precinct, demolition of a building, works or fabric, including trees, fences, walls and outbuildings must:

- (a) not be on a local heritage place;
- (b) not be visible from any road or public open space; and
- (c) not involve a value, feature or characteristic specifically part of a precinct listed in the relevant Local Provisions Schedule.

#### P1

Within a local heritage precinct, demolition of a building, works or fabric, including trees, fences, walls and outbuildings, must not cause an unacceptable impact on the local historic heritage significance of the local heritage precinct as identified in the relevant Local Provisions Schedule, having regard to:

- (a) the physical condition of the building, works, structure or trees;
- (b) the extent and rate of deterioration of the building, works, structure or trees;
- (c) the safety of the building, works, structure or trees;
- (d) the streetscape in which the building, works, structure or trees is located;
- (e) the special or unique contribution that the building, works, structure or trees makes to the streetscape or townscape values of the local heritage precinct identified in the relevant Local Provisions Schedule;
- (f) any options to reduce or mitigate deterioration;
- (g) whether demolition is a reasonable option to secure the long-term future of a building, works or structure; and
- (h) any economic considerations.

---

## Planner Response

The site is not a local heritage place. However, proposed demolition will be visible from both Clare Street and Hildern Street. The proposal requires assessment against the performance criteria. The proposal will not cause an unreasonable impact to the heritage values of the local heritage precinct as follows:

- The original form of the house will generally be retained. All unpainted brickwork, and all unpainted and unrendered masonry will be retained, which are recognised as contributory elements. While the existing roof structure will be retained, the roof sheeting and flashing will be replaced with metal roof sheeting in a dark blue colour. On the upper storey, existing shingles and weatherboard cladding will be replaced with vertical board and batten cladding in an off-white colour. The replacement of building finishes is sympathetic to heritage values as a contemporary representation of timber cladding and will not unreasonably impact the contribution that the site makes to the streetscape.
- There will be minor demolition to the original building as the entrances will be moved. On the Clare Street frontage, the eastern existing entry door will be removed, and the opening will be retained to accommodate the proposed addition. The ramp at the Clare Street frontage associated with the existing entry will also be removed. This entry way and ramp is understood to be a later addition and does not contribute to heritage values at the site.
- Demolition includes all frontage fences and gates, which are understood to be later additions to the site. This will allow for new landscaping that contributes to streetscape amenity. Security bars will also be removed from windows and are also understood to be later additions.
- Several trees/vegetation will be removed to allow for the car park upgrade and new landscaping. There are no significant trees or vegetation at the site. A retaining wall in the car parking area will be demolished, which is not understood to have heritage value.

The performance criteria (P1) are satisfied.

## 6.4.4 Clause C6.7.3 Buildings and works, excluding demolition

---

### Acceptable Solutions

### Performance Criteria

---

#### Objective

That development within a local heritage precinct or a local historic landscape precinct is sympathetic to the character of that particular precinct.

---

#### A1

Within a local heritage precinct or local historic landscape precinct, building and works, excluding demolition, must:

- (a) not be on a local heritage place;
- (b) not be visible from any road or public open space; and
- (c) not involve a value, feature or characteristic specifically part of a local heritage precinct or local historic landscape precinct listed in the relevant Local Provisions Schedule.

#### P1.1

Within a local heritage precinct, design and siting of buildings and works, excluding demolition, must be compatible with the local heritage precinct, except if a local heritage place of an architectural style different from that characterising the precinct, having regard to:

- (a) the streetscape or townscape values identified in the local historic heritage significance of the local heritage precinct, as identified in the relevant Local Provisions Schedule;
- (b) the character and appearance of the surrounding area;
- (c) the height and bulk of other buildings in the surrounding area;
- (d) the setbacks of other buildings in the surrounding area; and
- (e) any relevant design criteria or conservation policies for the local heritage precinct, as identified in the relevant Local Provisions Schedule.

#### P1.2

---

Within a local heritage precinct, extensions to existing buildings must be compatible with the local heritage precinct, having regard to:

- (a) the streetscape or townscape values identified in the local historic heritage significance of the local heritage precinct, as identified in the relevant Local Provisions Schedule;
- (b) the character and appearance of the surrounding area;
- (c) the height and bulk of other buildings in the surrounding area;
- (d) the setbacks of other buildings in the surrounding area; and
- (e) any relevant design criteria or conservation policies for the local heritage precinct, as identified in the relevant Local Provisions Schedule.

### **P1.3**

Within a local historic landscape precinct, design and siting of buildings and works, excluding demolition, must be compatible with the local historic landscape precinct, having regard to:

- (a) the landscape values identified in the statement of local historic heritage significance for the local historic landscape precinct, as identified in the relevant Local Provisions Schedule; and
- (b) any relevant design criteria or conservation policies for the local historic landscape precinct, as identified in the relevant Local Provisions Schedule.

---

### **Planner Response**

The site is not a local heritage place. However, proposed works will be visible from both Clare Street and Hildern Street. The proposal requires assessment against the performance criteria.

#### **P1.1**

The proposal involves alterations to an existing building, and as such siting and orientation will be generally unchanged.

Unpainted brickwork will be retained on the ground level, which is recognised as a contributory element. The upper storey will have a new cladding finish but will be painted in traditional off-white colour. The existing roofing will be replaced with metal roof sheeting in a dark blue colour. Materials and colours will be sympathetic to the heritage precinct.

The original entrance stair will be retained in form but will become a planter which softens the facade and forms part of the landscaping. The new entrance ramp will be built as a feature element to accentuate the entrance so that new visitors will be able to clearly see the ramp and the route to the front door. These will read like landscaping elements.

The new entrance door will be built in place of an existing window. The existing bricks will be neatly cut below the existing window to form a flush sill threshold for the entrance door. On the rear of the house, some existing openings will be closed off and new openings created.

The uniformity of the streetscape is unchanged by the alterations to the building.

#### **P1.2**

Proposed additions to the external face of the building are compatible with the heritage precinct in height, bulk and setback. The materials are compatible and where different will be carefully selected to create a feature entrance in keeping with the purpose of the building. The proposed additions are minor and will be within the existing building envelope. They will be visually subservient when viewed from any road or public space.

#### **P1.3**

The site is not within a local historic landscape precinct. P1.3 is not applicable.

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The performance criteria (P1) are satisfied.

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**A2**

Within a local heritage precinct, new front fences and gates must be designed and constructed to match the existing original fences on the site.

**P2**

Within a local heritage precinct, new front fences and gates must be compatible with the local heritage precinct, having regard to:

- (a) the streetscape or townscape values identified in the local historic heritage significance of the local heritage precinct, as identified in the relevant Local Provisions Schedule;
  - (b) height, form, style and materials of the proposed fence; and
  - (c) the style, characteristics and setbacks of fences and gates in the surrounding area.
- 

**Planner Response**

The proposal includes a new gate at the Clare Street frontage. It is understood that the existing fencing and gates are not strictly original to the site, and the acceptable solution cannot be met.

Notwithstanding, the gate is designed to match the existing retained boundary fence and will not result in any incompatibility with the local heritage precinct. The proposed gate is set back to the building line and is not a typical 'front gate' that would be closer to the frontage.

The performance criteria (P1) are satisfied.

# 7 Conclusion

This planning permit application seeks approval for alterations and additions to 16 Clare Street, New Town, to accommodate the Tasmanian Lung Centre medical clinic. The proposed works will create a medical practice that is accessible, cohesive and welcoming for both patients and staff.

An assessment against all relevant standards is outlined in Sections 4 to 6 of this report. A total of 22 standards within the planning scheme are applicable to the proposal. The relevant standards and whether the proposal complies with the acceptable solution or relies on the performance criteria is outlined in Table 2.

Table 2 Summary of the applicable standards and whether the Project relies on the AS or PC

Clause/standard	AS or PC
Inner Residential Zone	
9.3.1 Discretionary uses (A1/P1)	AS
9.3.1 Discretionary uses (A2/P2)	AS
9.3.1 Discretionary uses (A3/P3)	AS
9.3.1 Discretionary uses (A4/P4)	PC
9.5.1 Non-dwelling development (A1/P1)	PC
9.5.1 Non-dwelling development (A2/P2)	AS
9.5.1 Non-dwelling development (A3/P3)	AS
9.5.1 Non-dwelling development (A5/P5)	PC
9.5.1 Non-dwelling development (A6/P6)	PC
Royal Hobart Hospital Helipad Airspace Specific Area Plan	
HOB-S10.7.1 Building height	AS
Parking and Sustainable Transport Code	
C2.5.1 Car parking numbers (A1/P1)	AS
C2.5.2 Bicycle parking numbers (A1/P1)	AS
C2.5.3 Motorcycle parking numbers (A1/P1)	AS

Clause/standard	AS or PC
C.2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone (A1/P1)	PC
C2.6.1 Construction of parking areas (A1/P1)	AS
C2.6.2 Design and layout of parking areas (A1/P1)	PC
C2.6.3 Number of accesses for vehicles (A1/P1)	AS
C2.6.5 Pedestrian access (A1/P1)	AS
C2.6.8 Siting of parking and turning areas (A1/P1)	AS
Road and Railway Assets Code	
C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction	AS
Local Historic Heritage Code	
C6.7.1 Demolition within a local heritage precinct (A1/P1)	PC
C6.7.3 Buildings and works, excluding demolition (A1/P1)	PC
C6.7.3 Buildings and works, excluding demolition (A2/P2)	PC

Overall, the proposal is considered acceptable under the applicable planning scheme standards and should be approved subject to conditions.

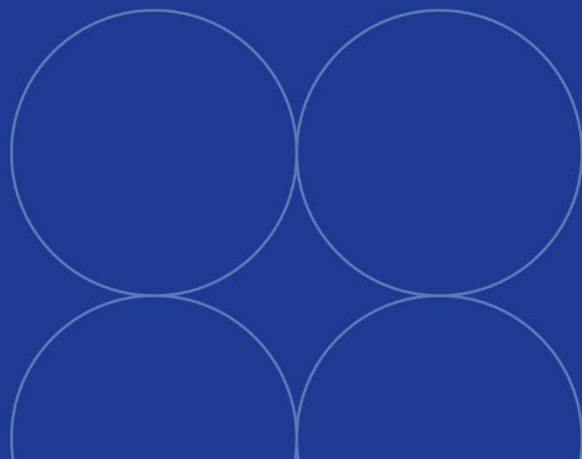
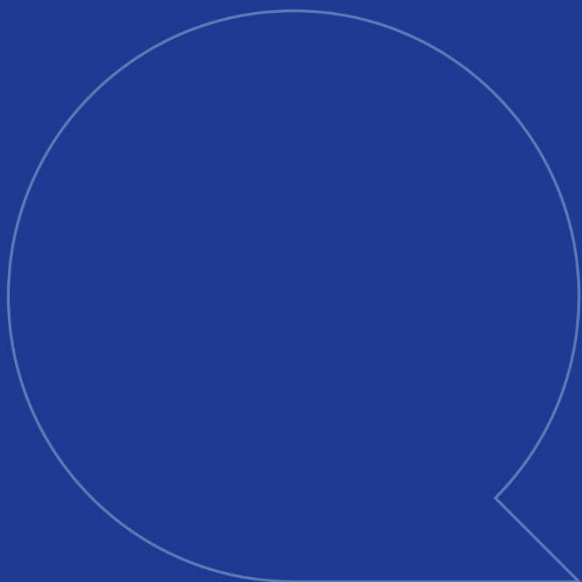
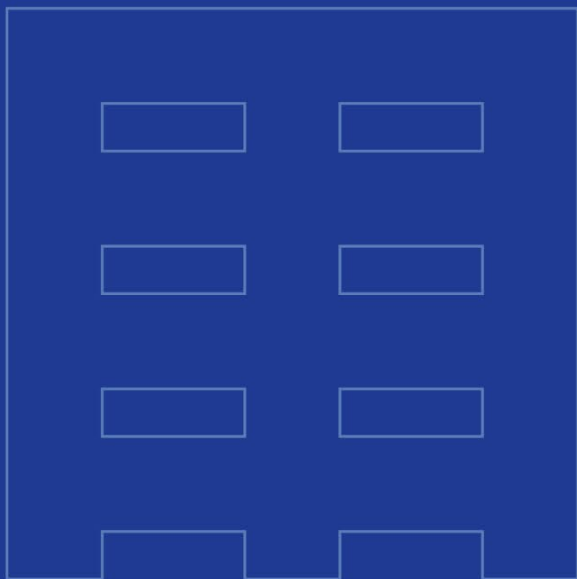
# Appendix A Title documentation

# Appendix B Proposal plans

# Appendix C Traffic Impact Assessment

Era Advisory  
Level 1, 125A Elizabeth St  
Hobart 7000

(03) 6165 0443  
[enquiries@era-advisory.com.au](mailto:enquiries@era-advisory.com.au)  
[era-advisory.com.au](http://era-advisory.com.au)



era

19 March 2026  
Reference: 2526-036

Mary McNeill  
Development Appraisal Planner  
City of Hobart

Via PlanBuild

Dear Mary,

**PLANNA-HOB-2025-4862 16 Clare Street New Town**  
**Response to request for additional information**

Era Advisory continues to act on behalf of the Tasmanian Lung Centre in relation to the proposed development at 16 Clare Street, New Town.

A response to the additional information request issued on 10 March is provided in the table below.

Note: The amended plans (attached) include a minor change to the west elevation. All three windows on the upper-storey are now proposed to be replaced with longer windows (see drawing A203-DA-04).

Request	Response
<p><u>HER - RFI 3 - Landscaping Plan</u> Provide a landscaping plan showing retained and proposed plantings.</p>	<p>For landscaping identified for removal, see A102-DA-04 together with the attached landscaping plant palette prepared by Loci Landscapes.</p> <p>For proposed landscaping, see the landscaping plant palette together with A101-DA-04 which identifies the location of new landscaping.</p>
<p><u>ENG - C2.6.2 - Parking and Sustainable Transport Code - Design and layout of parking areas</u> To satisfy the Tasmanian Planning Scheme - State Planning Provisions clause C2.6.2, please provide scaled and dimensioned drawings prepared by a suitably qualified expert to address all aspects of the acceptable solution or performance criteria. Design drawings to include;</p> <ul style="list-style-type: none"><li>Plan view showing the gradients (expressed as a percentage) for the full driveway's finished surface level in both longitudinal and cross sectional directions;</li></ul>	<p>Please find attached civil drawings prepared by Fysh Design.</p> <p>It is noted that the shared area adjacent to the accessible car parking space is marginally smaller than the AS/NZS 2890.6:2009 standard. This is a result of changes to the parking layout that were required through the General Manager Consent process. Notwithstanding, the traffic impact assessment confirms that the area provides sufficient space for full door opening and for safe entry to and exit from the vehicle.</p>

---

## Request

## Response

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- Plan view showing gradients (expressed as a percentage) of all on-site parking areas/spaces in both longitudinal and cross sectional directions;
- Plan view showing gradients (expressed as a percentage) of all on-site parking areas/spaces/shared areas for parking spaces for persons with a disability, in both longitudinal and cross sectional directions: and
- Plan view clearly showing the outline and location of the of the shared area at one end of the carparking space in accordance with AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities, such that the shared area is fully contained within the subject property.

---

### ENG - Traffic Impact Assessment

Please submit an amended Traffic Impact Assessment to be consistent with the current proposal and not in draft form.

Attached is an amended traffic impact assessment that is consistent with the current proposal and not in draft form.

---

### ENGrFi1 - Roads - Driveway

Driveways to AS

1. Show swept path of vehicles, at driveway crossover within Hildern Street highway reservation in accordance with AS/NZS 2890.1. Show swept-path movements for every car parking space (Specially for accessible parking slot), including forward/reverse entry and exit. When demonstrating the swept path for any individual parking bay, assume all other bays are occupied.

Attached is a traffic impact assessment and civil drawings which include swept path movements for each car parking space.

---

### ENGrFi2 - Roads - Use of Road Reserve

Please provide amended plans demonstrating that the new landscaping extends up to the edge of the crossover within the road reserve area on Hilden Street.

The new landscaping does not extend to the edge of the crossover within the road reserve area on Hildern Street. This is to provide for a paved link between the parking area and the footpath. Users of the car park would otherwise be required to walk over the vehicle crossover to reach the primary pedestrian entrance.

---

### INFsw 1 - Protection of council Infrastructure - Stormwater

Please provide evidence demonstrating that the kerb connection on Hildern St which is to be removed is redundant, eg the sketch of drainage connections and investigation findings uploaded on 2/2/26 under the GMC.

Find attached the requested evidence (stormwater report and drainage plan) as lodged to Council at the General Manager Consent stage.

I trust that the information detailed above is sufficient and that the proposal can proceed to advertising.

Yours sincerely,

Grace Elliott  
Planner

*Attachments*

- Amended proposal plans*
- Landscape plan – planting palette*
- Civil drawings*
- Amended traffic impact assessment*
- Stormwater report and drainage plan*

# TASMANIAN LUNG CENTRE

## ALTERATIONS AND ADDITIONS

CUMULUS RESPECTFULLY ACKNOWLEDGES THE FIRST PEOPLES OF AUSTRALIA, THEIR ELDERS PAST, PRESENT AND EMERGING, WHO WERE AND ARE THE KEEPERS OF THEIR CULTURAL AND SPIRITUAL KNOWLEDGE AND TRADITIONS, AND THE TRADITIONAL CUSTODIANS OF THE LAND ON WHICH WE LIVE AND WORK.

### ARCHITECT/BSP

ARCHITECT / ACCREDITED DESIGNER  
KEITH WESTBROOK

ACCREDITATION N°  
482182300

ARCHITECT ADDRESS  
SUITE 2, LEVEL 2, 147 MACQUARIE STREET HOBART, TAS 7000  
+61(3) 6231 4841

### CLIENT/OWNER/PRINCIPAL

CLIENT NAME  
BETH MCDONALD & DAVID FENG

CLIENT ADDRESS  
GLEESON HOUSE CALVARY, 49 AUGUSTA ROAD LENAH VALLEY  
TASMANIA 7008

### PROJECT INFORMATION

PROJECT N°  
J25022

PROJECT NAME  
TASMANIAN LUNG CENTRE  
ALTERATIONS AND ADDITIONS

PROJECT ADDRESS  
16 CLARE STREET  
NEW TOWN  
TASMANIA 7008

### DETAILS

NCC CLASSIFICATION	CLASS 5
CONSTRUCTION TYPE	TYPE C
TITLE REFERENCE	1/45950
DESIGN WIND SPEED	REFER ENG
SOIL CLASS	REFER ENG
CLIMATE ZONE	7
BAL RATING	N/A
ALPINE AREA	N/A
CORROSION LEVEL	C2
OTHER HAZARDS	N/A



PERSPECTIVE VIEW OF PROJECT



LOCATION PLAN

### SITE AREA

AREA (m2)	NOTES
-----------	-------

### COVERAGE

FLOOR	Measured Area
	161
	61
	222

### EXTERNAL PAVED AREA

DESCRIPTION	AREA
POS	2.19
ENTRY WALKWAY	31.43
CAR PARK	139.58
ACCESS RAMP	14.10
---	---

### COVERAGE

161m<sup>2</sup>/479,7m<sup>2</sup> = 33,5%

### IMPERVIOUS SURFACES

172,82m<sup>2</sup>/479,7m<sup>2</sup> = 36%

### PERVIOUS SURFACES

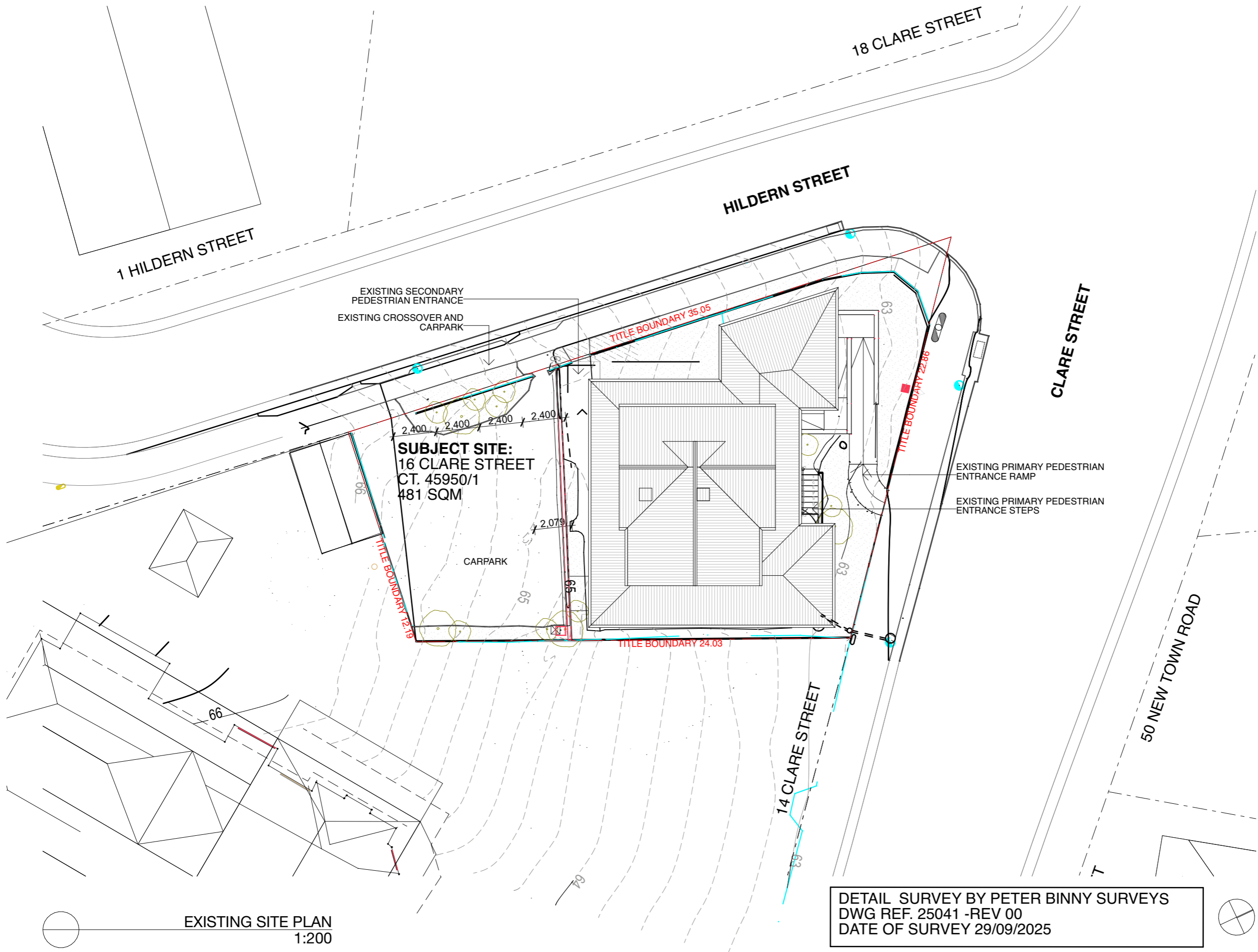
122,54/479,7m<sup>2</sup> = 25,5%

### DRAWING LIST

No.		REV	DATE
A001	COVER PAGE	DA 04	
A100	SITE PLAN - EXISTING	DA 04	
A101	SITE PLAN - PROPOSED	DA 04	
A102	DECONSTRUCTION PLAN - GROUND LEVEL	DA 04	
A103	DECONSTRUCTION PLAN - LEVEL 1	DA 04	
A104	DECONSTRUCTION PLAN - ROOF	DA 04	
A105	PLAN - GROUND LEVEL	DA 04	
A106	PLAN - LEVEL 1	DA 04	
A107	PLAN - ROOF	DA 04	
A200	DECONSTRUCTION ELEVATIONS 01	DA 04	
A201	DECONSTRUCTION ELEVATIONS 02	DA 04	
A202	ELEVATIONS 01	DA 04	
A203	ELEVATIONS 02	DA 04	
A300	MATERIALITY	DA 04	

REV	DATE	PURPOSE	PROJECT STAGE	PROJECT N°	DATE
01		100% CLIENT PRESENTATION	DESIGN DEVELOPMENT	J25022	15/3/2026
04		DEVELOPMENT APPLICATION	SCALE: 1:221.76, 1:2000	DRAWING TITLE: COVER PAGE	
DA-01		DEVELOPMENT APPLICATION	ORIGINAL SIZE: A3	DRAWING N°: A001- DA 04	
DA-02	5/2/2026	DESIGN DEVELOPMENT	ARCHITECT: KEITH WESTBROOK, 482182300	REVISION: DA 04	
DA 04		BUILDING APPLICATION	DRAWN BY: GR, KF	CHECKED BY: KW	

MU  
CU LUS<sup>EST 2011</sup>



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
**16 CLARE STREET  
NEW TOWN  
TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK,  
482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:200**

ORIGINAL SIZE  
**A3**

PROJECT N°  
**J25022**

DATE  
**15/3/2026**

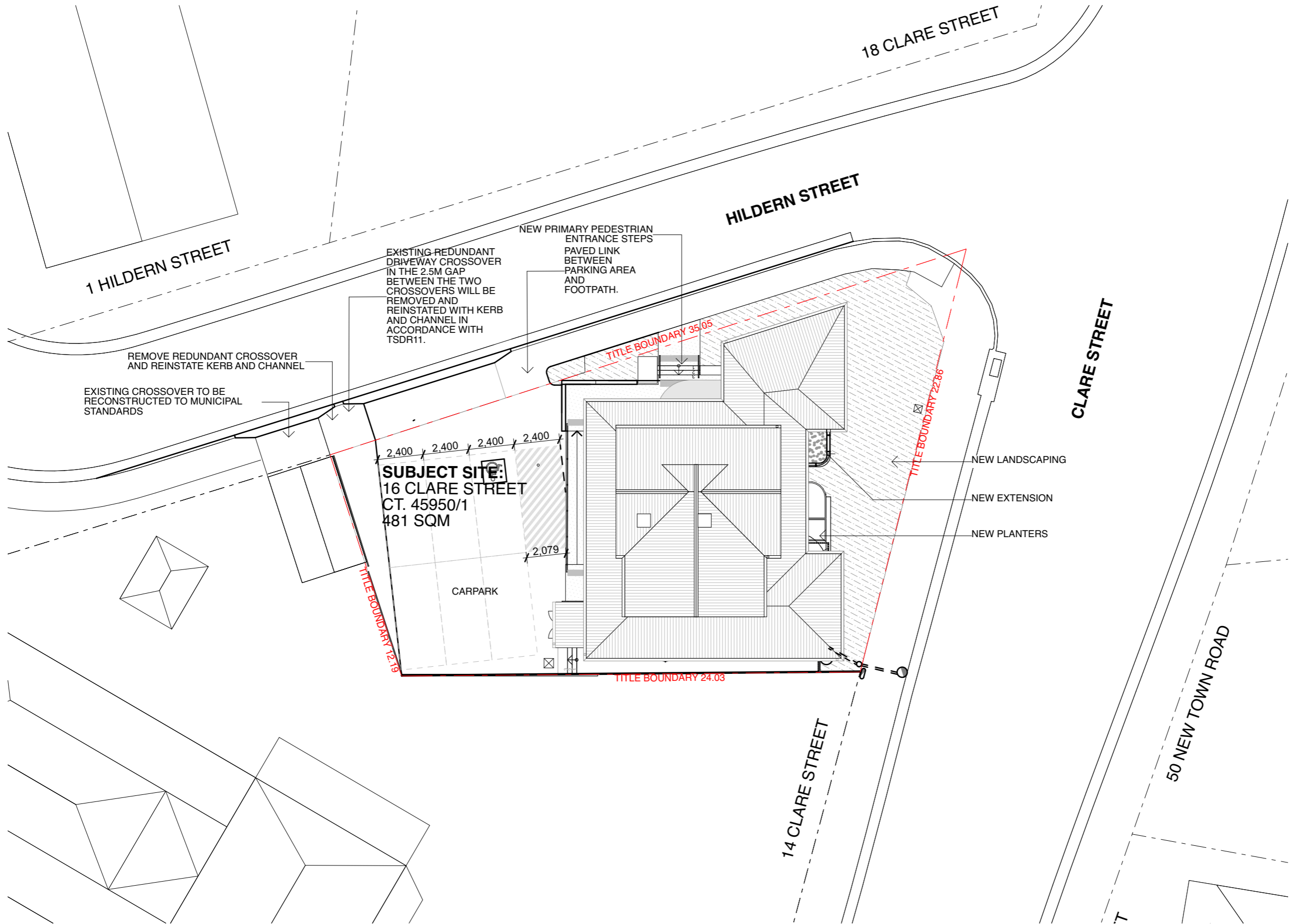
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**SITE PLAN - EXISTING**

DRAWING N° REVISION

**A100- DA 04**

DETAIL SURVEY BY PETER BINNY SURVEYS  
DWG REF. 25041 -REV 00  
DATE OF SURVEY 29/09/2025

EXISTING SITE PLAN  
1:200



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
INFO@CUMULUS.STUDIO  
SUITE 2, LEVEL 2, 147 MACQUARIE STREET HOBART,  
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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
16 CLARE STREET  
NEW TOWN  
TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
DESIGN DEVELOPMENT

ARCHITECT  
KEITH WESTBROOK,  
482182300

DRAWN BY  
GR, KF

CHECKED BY  
KW

SCALE  
1:200

ORIGINAL SIZE  
A3

PROJECT N°  
J25022

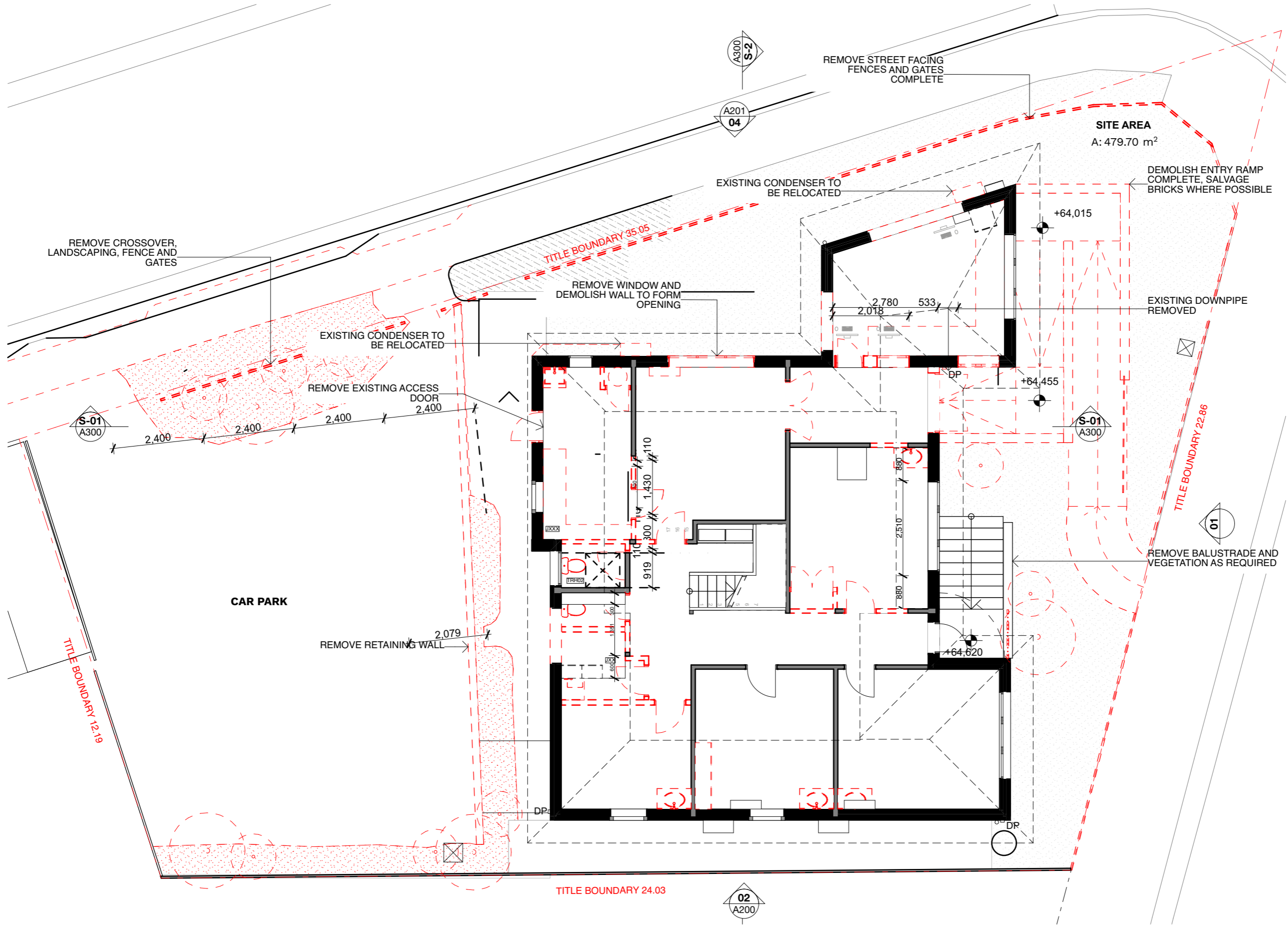
DATE  
15/3/2026

DRAWING TITLE  
**SITE PLAN - PROPOSED**

DRAWING N° REVISION

**A101- DA 04**

NEW SITE PLAN  
1:200



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
 16 CLARE STREET  
 NEW TOWN  
 TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
 DESIGN DEVELOPMENT

ARCHITECT  
 KEITH WESTBROOK,  
 482182300

DRAWN BY  
 GR, KF

CHECKED BY  
 KW

SCALE  
 1:100

ORIGINAL SIZE  
 A3

PROJECT N°  
 J25022

DATE  
 15/3/2026

DRAWING TITLE  
**DECONSTRUCTION PLAN - GROUND LEVEL**

DRAWING N°  
 A102-

REVISION  
 DA 04

DECONSTRUCTION GROUND FLOOR PLAN  
 1:100

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
 16 CLARE STREET  
 NEW TOWN  
 TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
 DESIGN DEVELOPMENT

ARCHITECT  
 KEITH WESTBROOK,  
 482182300

DRAWN BY  
 GR, KF

CHECKED BY  
 KW

SCALE  
 1:100

ORIGINAL SIZE  
 A3

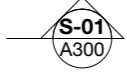
PROJECT N°  
 J25022

DATE  
 15/3/2026

DRAWING TITLE  
**DECONSTRUCTION PLAN - LEVEL 1**

DRAWING N°  
**A103-**

REVISION  
**DA 04**



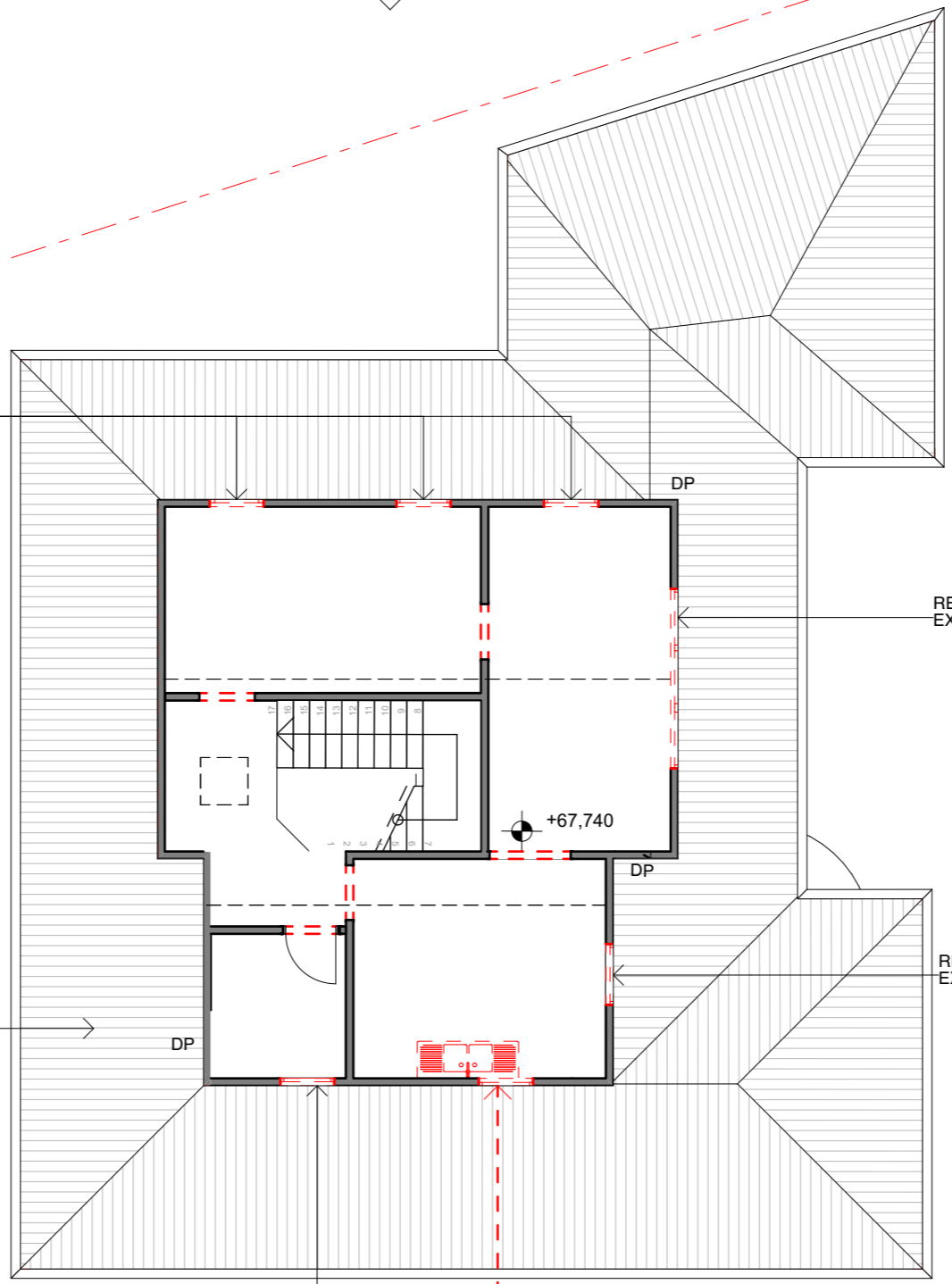
REMOVE / REPLACE EXISTING WINDOWS

REMOVE / REPLACE EXISTING WINDOW

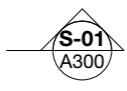
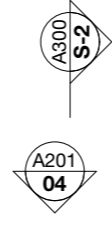
REMOVE / REPLACE EXISTING WINDOW

REMOVE / REPLACE EXISTING WINDOW

RETAIN & PAINT EXISTING ROOF SHEETING, FLASHINGS AND ASSOCIATED ROOF PLUMBING, RETAIN EXISTING ROOF STRUCTURE



DECONSTRUCTION FIRST FLOOR PLAN  
 1:100



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
 16 CLARE STREET  
 NEW TOWN  
 TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
 DESIGN DEVELOPMENT

ARCHITECT  
 KEITH WESTBROOK,  
 482182300  
 DRAWN BY  
 GR, KF  
 CHECKED BY  
 KW

SCALE  
 1:100  
 ORIGINAL SIZE  
 A3

PROJECT N°  
 J25022  
 DATE  
 15/3/2026

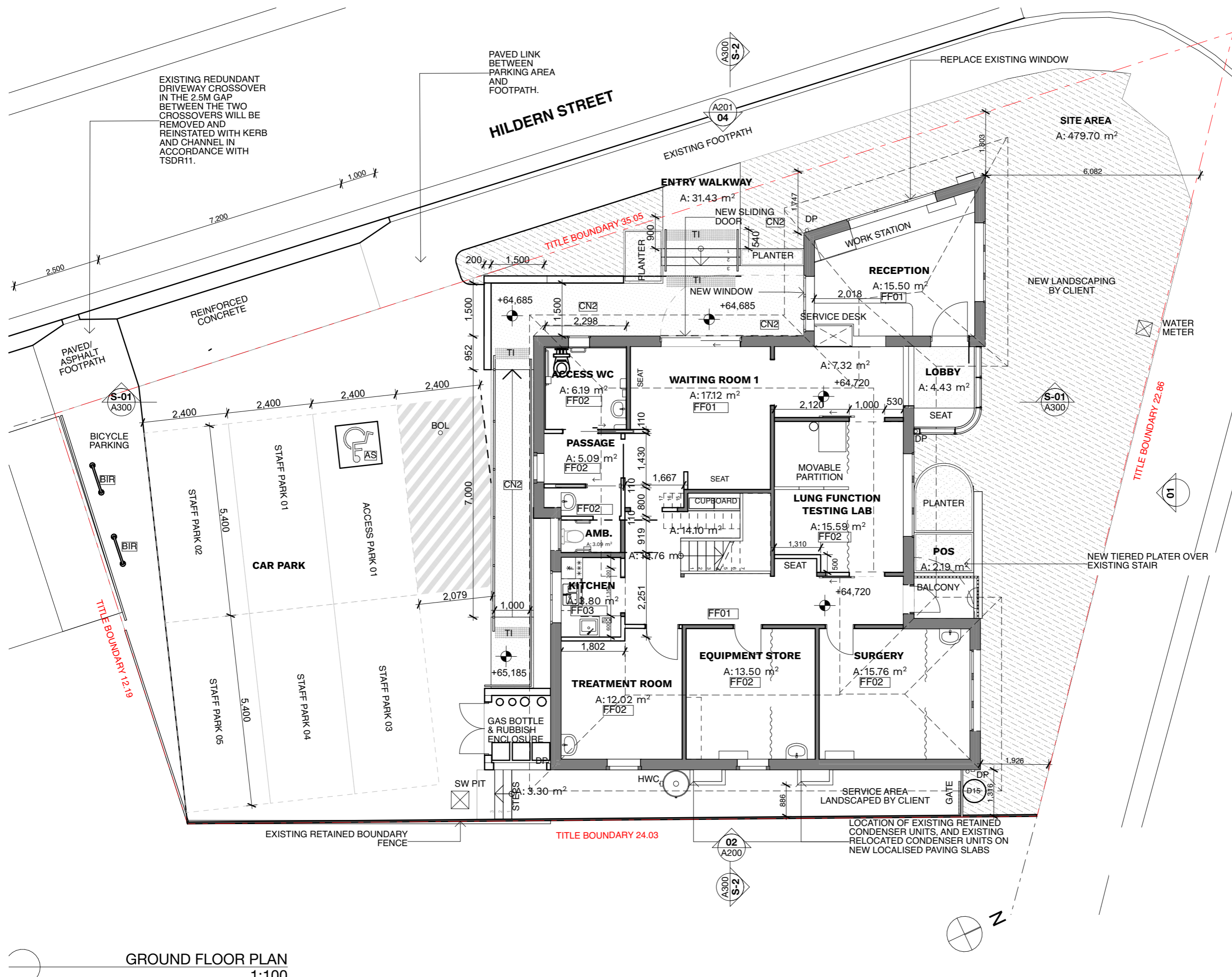
DRAWING TITLE  
**DECONSTRUCTION PLAN - ROOF**

DRAWING N°  
**A104-**  
 REVISION  
**DA 04**

REMOVE / REPLACE EXISTING SKYLIGHTS

PAINT EXISTING ROOF SHEETING AND REPLACE FLASHINGS GUTTERS & DOWNPIPES. EXISTING ROOF STRUCTURE.

DECONSTRUCTION ROOF PLAN  
 1:100



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
INFO@CUMULUS.STUDIO  
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**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
16 CLARE STREET  
NEW TOWN  
TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
DESIGN DEVELOPMENT

ARCHITECT  
KEITH WESTBROOK,  
482182300

DRAWN BY  
GR, KF

CHECKED BY  
KW

SCALE  
1:100

ORIGINAL SIZE  
A3

PROJECT N°  
J25022

DATE  
15/3/2026

DRAWING TITLE  
**PLAN - GROUND LEVEL**

DRAWING N°

REVISION

**A105- DA 04**

GROUND FLOOR PLAN  
1:100

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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PROJECT ADDRESS  
 16 CLARE STREET  
 NEW TOWN  
 TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
 DESIGN DEVELOPMENT

ARCHITECT  
 KEITH WESTBROOK,  
 482182300

DRAWN BY  
 GR, KF

CHECKED BY  
 KW

SCALE  
 1:100

ORIGINAL SIZE  
 A3

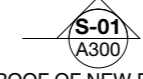
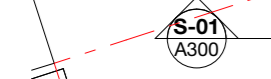
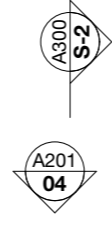
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 J25022

DATE  
 15/3/2026

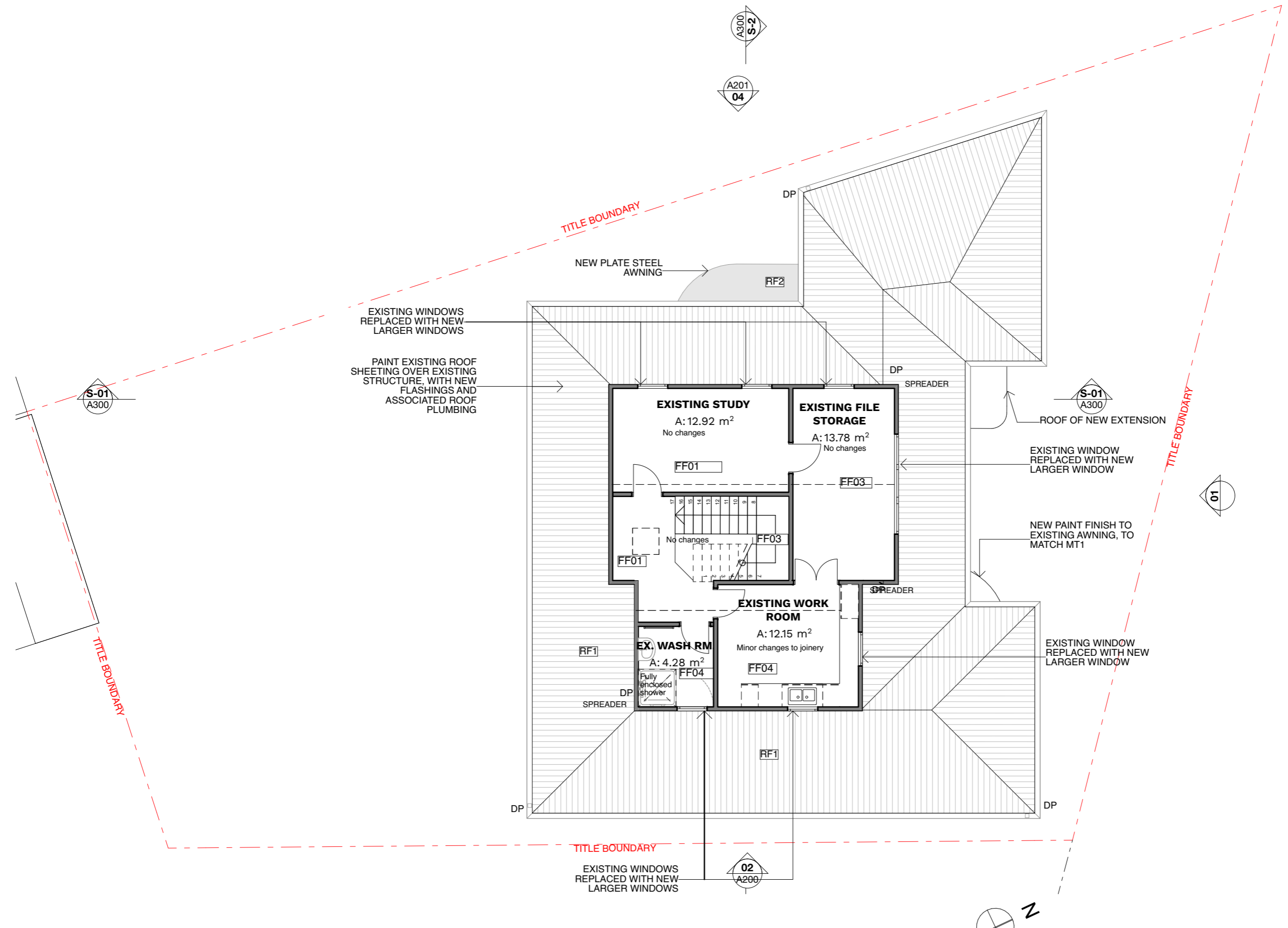
DRAWING TITLE  
**PLAN - LEVEL 1**

DRAWING N°  
**A106-**

REVISION  
**DA 04**



FIRST FLOOR PLAN  
 1:100



EXISTING WINDOWS REPLACED WITH NEW LARGER WINDOWS

PAINT EXISTING ROOF SHEETING OVER EXISTING STRUCTURE, WITH NEW FLASHINGS AND ASSOCIATED ROOF PLUMBING

NEW PLATE STEEL AWNING

**EXISTING STUDY**  
 A: 12.92 m<sup>2</sup>  
 No changes

**EXISTING FILE STORAGE**  
 A: 13.78 m<sup>2</sup>  
 No changes

**EXISTING WORK ROOM**  
 A: 12.15 m<sup>2</sup>  
 Minor changes to joinery

**EX. WASH RM**  
 A: 4.28 m<sup>2</sup>  
 Fully enclosed shower

ROOF OF NEW EXTENSION

EXISTING WINDOW REPLACED WITH NEW LARGER WINDOW

NEW PAINT FINISH TO EXISTING AWNING, TO MATCH MT1

EXISTING WINDOW REPLACED WITH NEW LARGER WINDOW

EXISTING WINDOWS REPLACED WITH NEW LARGER WINDOWS

TITLE BOUNDARY

TITLE BOUNDARY

TITLE BOUNDARY

TITLE BOUNDARY

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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 INFO@CUMULUS.STUDIO  
 SUITE 2, LEVEL 2, 147 MACQUARIE STREET HOBART,  
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**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
 16 CLARE STREET  
 NEW TOWN  
 TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
 DESIGN DEVELOPMENT

ARCHITECT  
 KEITH WESTBROOK,  
 482182300  
 DRAWN BY  
 GR, KF  
 CHECKED BY  
 KW

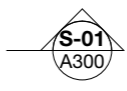
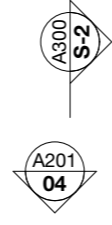
SCALE  
 1:100  
 ORIGINAL SIZE  
 A3

PROJECT N°  
 J25022  
 DATE  
 15/3/2026

DRAWING TITLE  
**PLAN - ROOF**

DRAWING N° REVISION

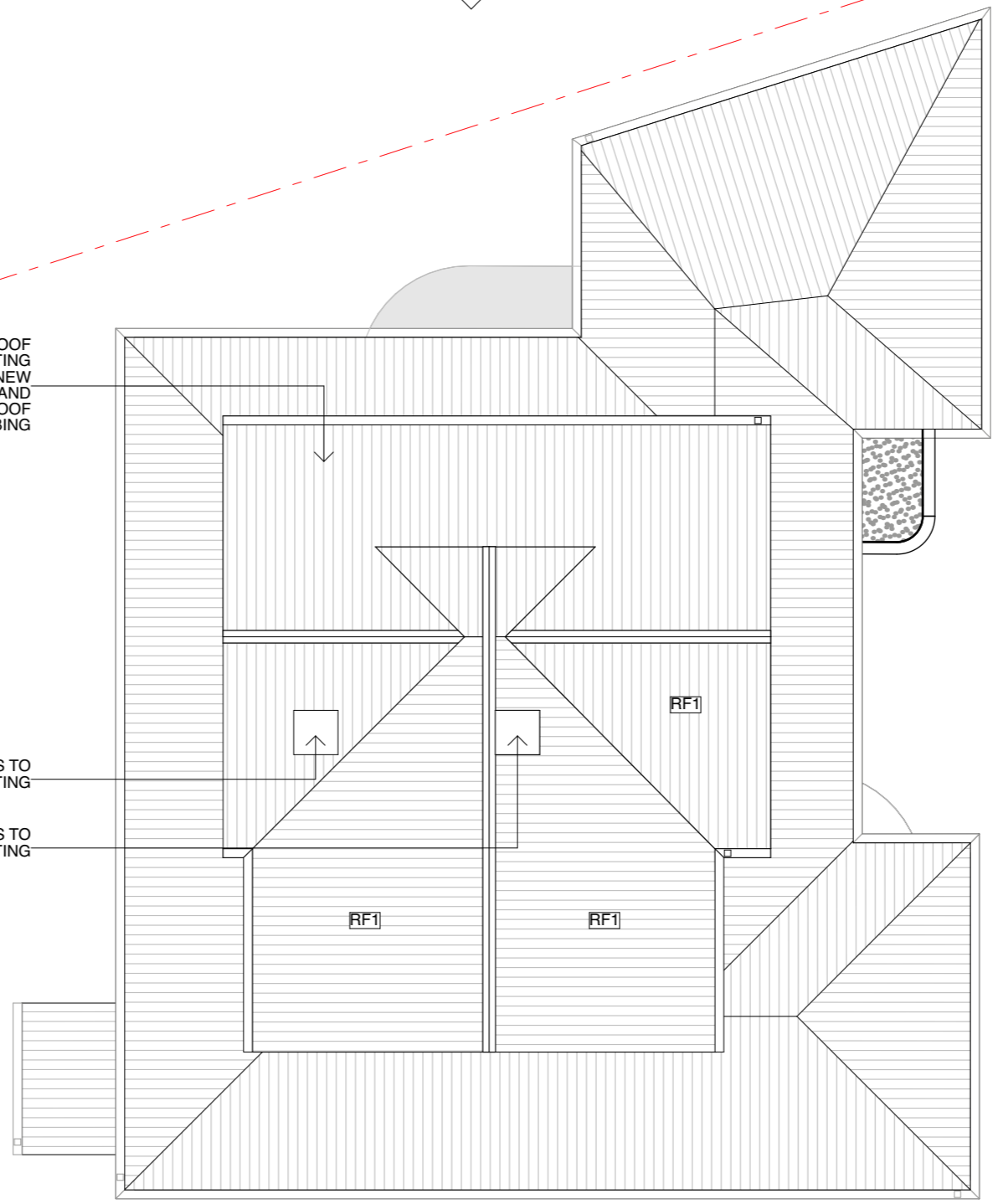
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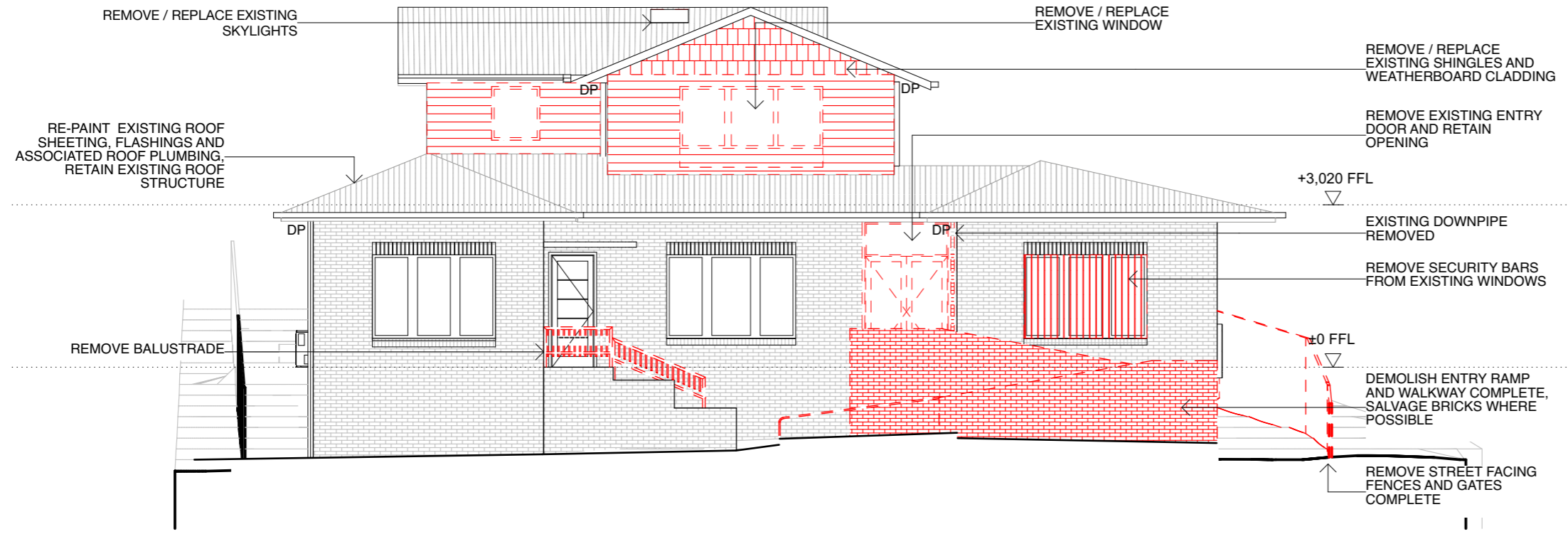
PAINT EXISTING ROOF SHEETING OVER EXISTING STRUCTURE, WITH NEW FLASHINGS AND ASSOCIATED ROOF PLUMBING


NEW SKYLIGHTS TO MATCH EXISTING

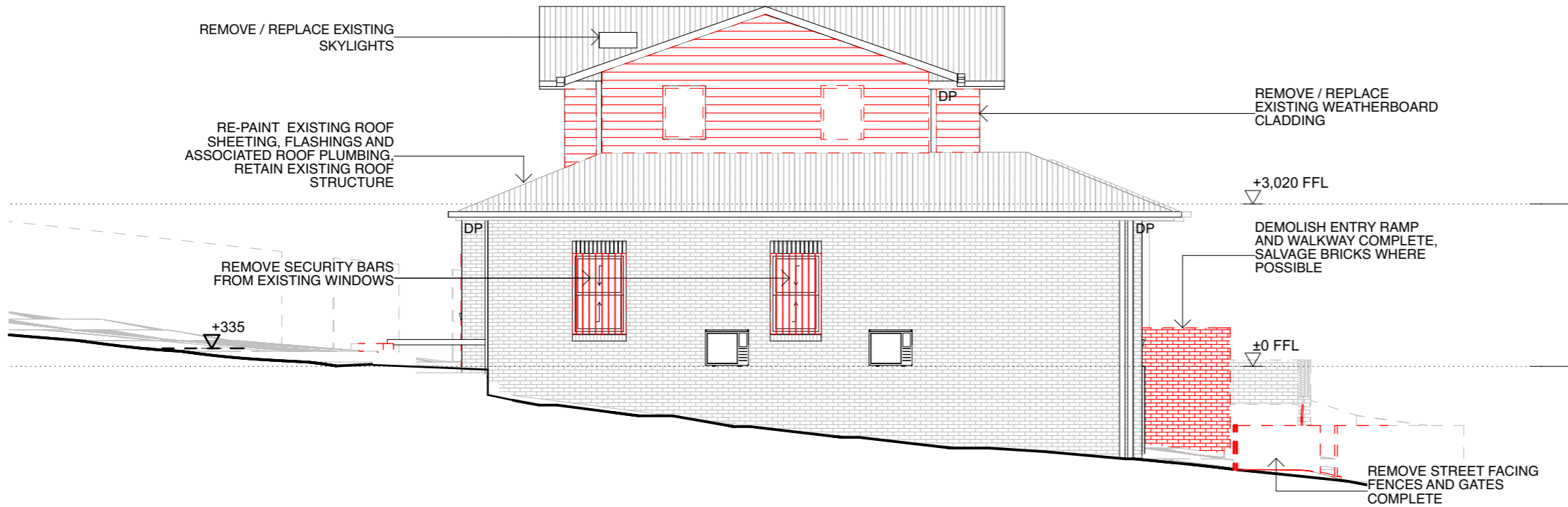
NEW SKYLIGHTS TO MATCH EXISTING



**ROOF PLAN**  
 1:100



 NORTH ELEVATION - DECONSTRUCTION  
1:100



 EAST ELEVATION - DECONSTRUCTION  
1:100

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
**16 CLARE STREET NEW TOWN TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK, 482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:100**

ORIGINAL SIZE  
**A3**

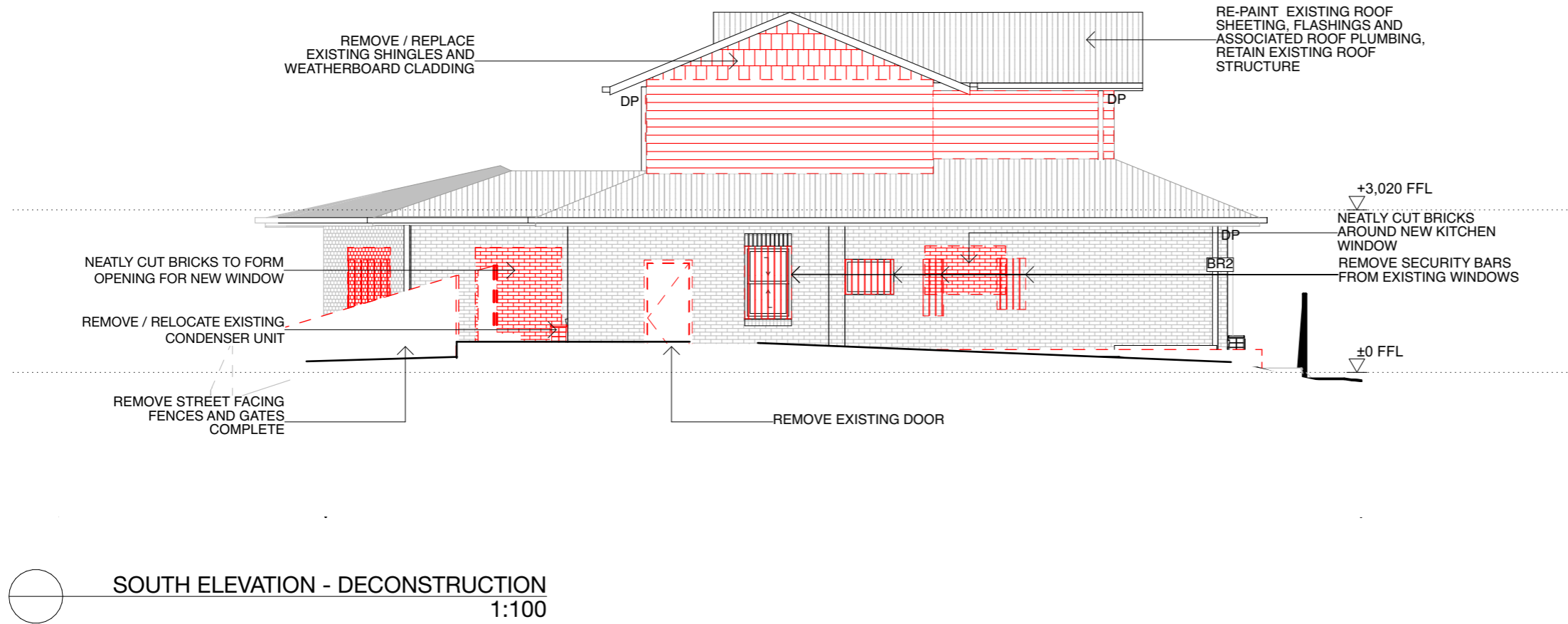
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**J25022**

DATE  
**15/3/2026**

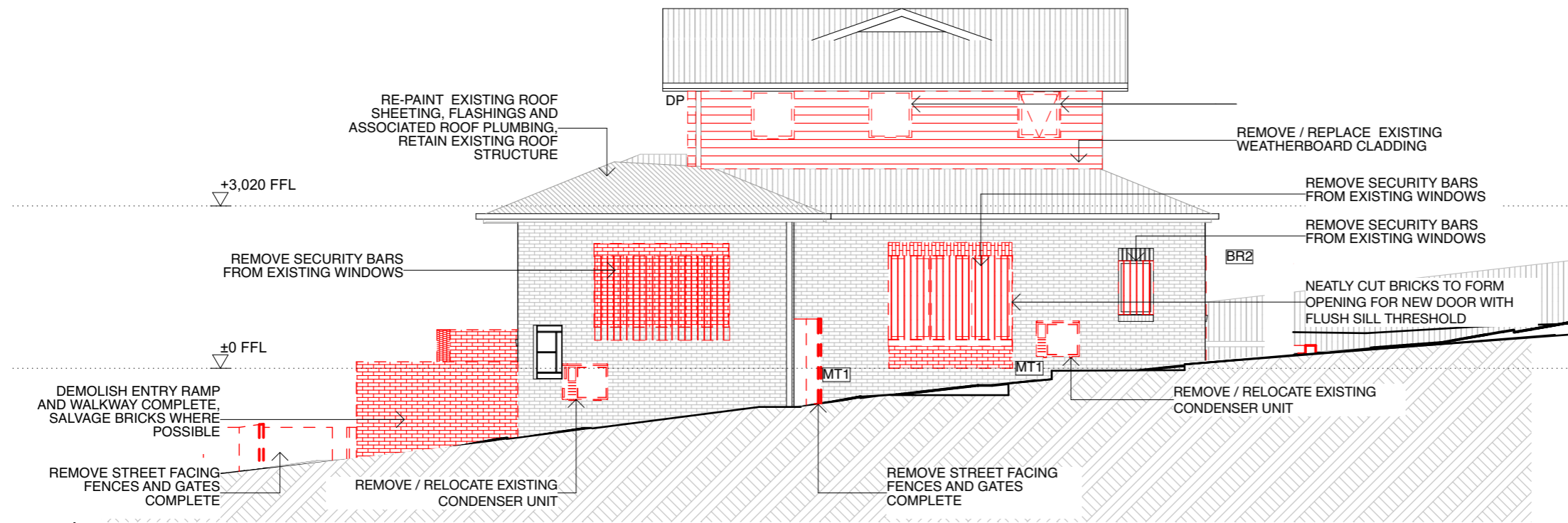
DRAWING TITLE  
**DECONSTRUCTION ELEVATIONS 01**

DRAWING N° REVISION

**A200- DA 04**



**SOUTH ELEVATION - DECONSTRUCTION**  
1:100



**WEST ELEVATION - DECONSTRUCTION**  
1:100

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
**16 CLARE STREET  
NEW TOWN  
TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK,  
482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:100**

ORIGINAL SIZE  
**A3**

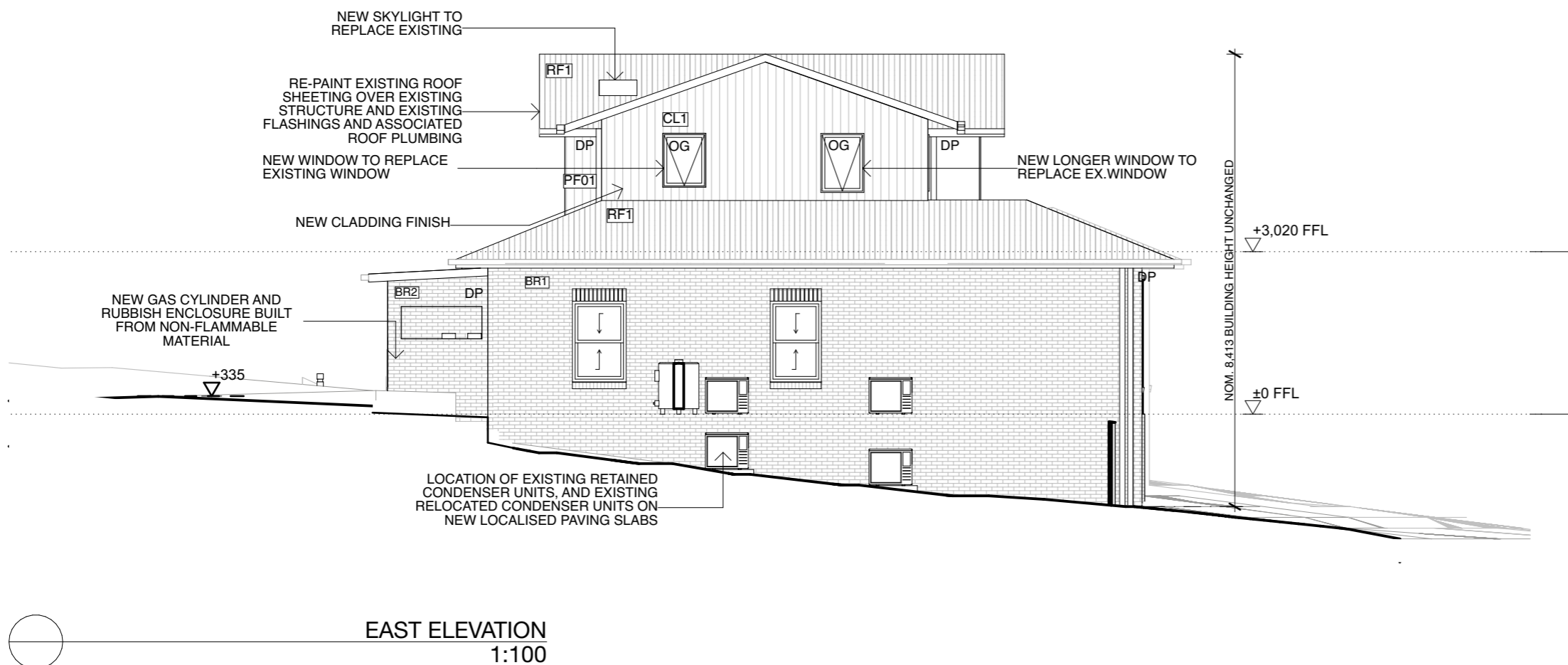
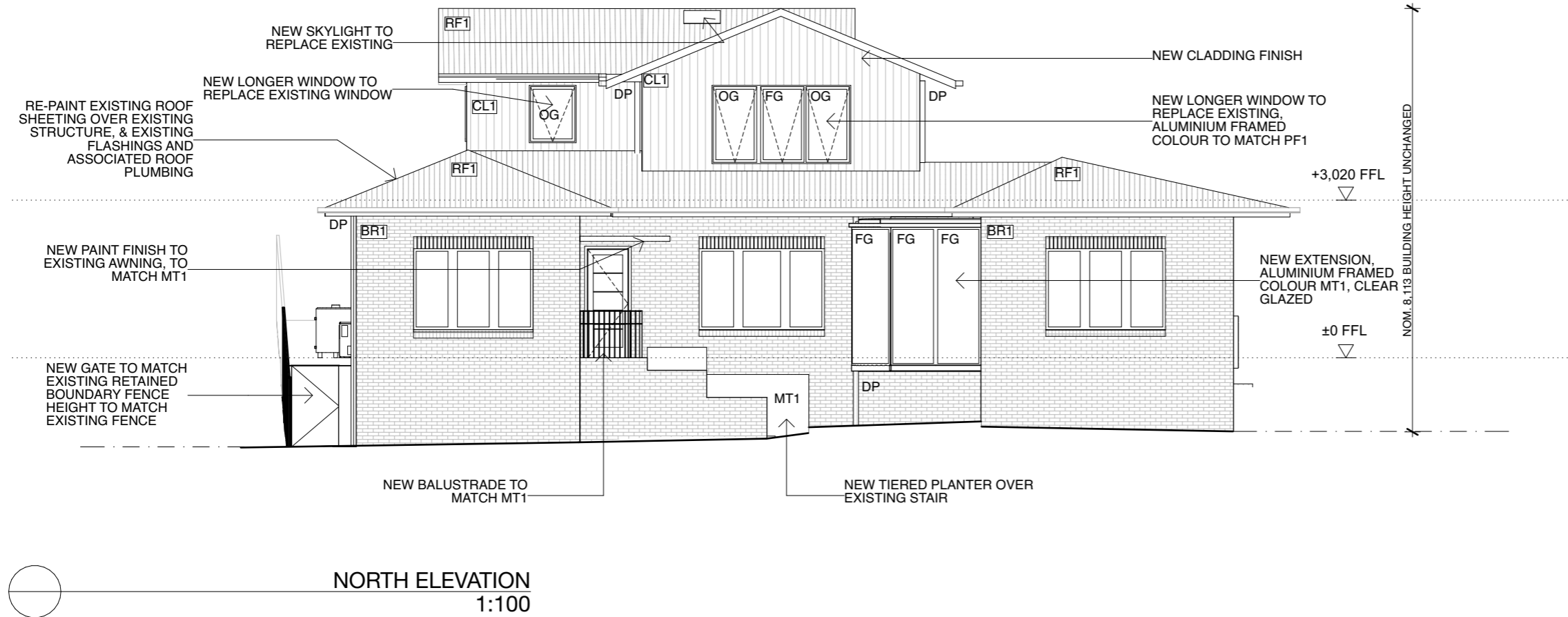
PROJECT N°  
**J25022**

DATE  
**15/3/2026**

DRAWING TITLE  
**DECONSTRUCTION ELEVATIONS 02**

DRAWING N° REVISION

**A201- DA 04**



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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NEW TOWN  
TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK,  
482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:100**

ORIGINAL SIZE  
**A3**

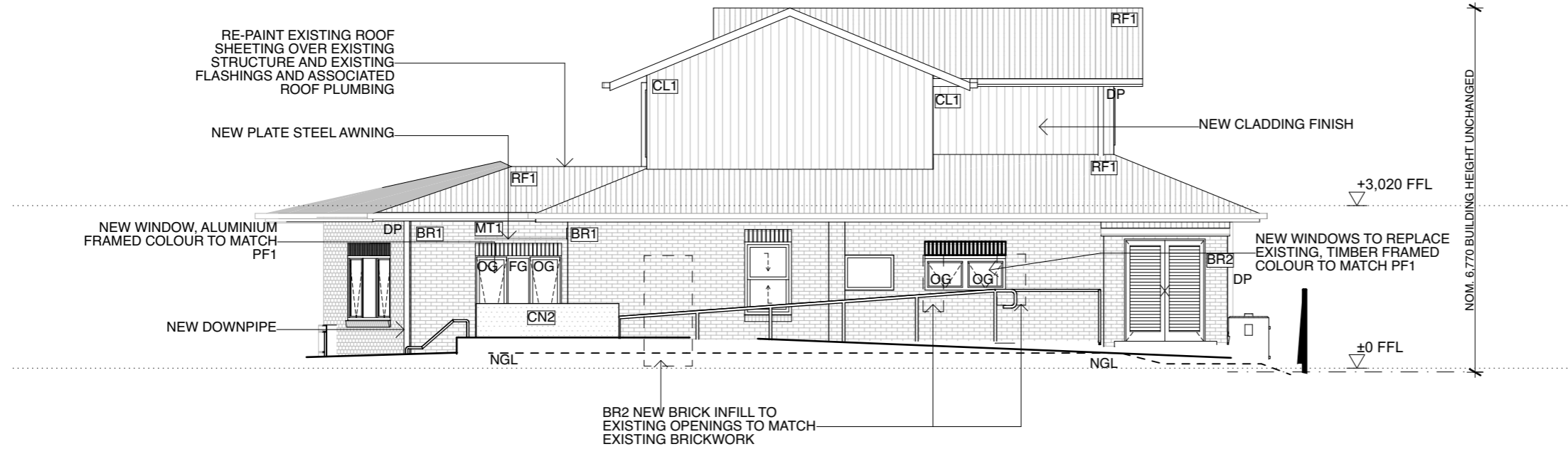
PROJECT N°  
**J25022**

DATE  
**15/3/2026**

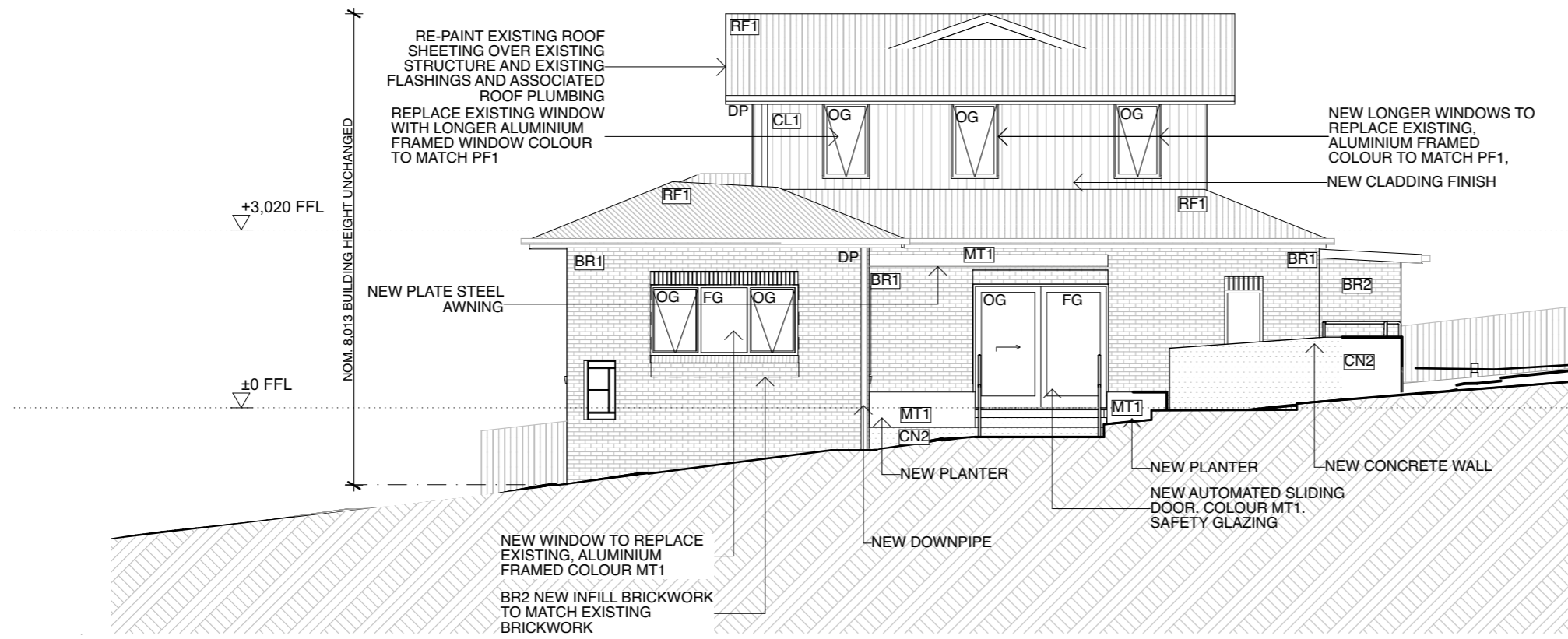
DRAWING TITLE  
**ELEVATIONS 01**

DRAWING N°  
**A202-**

REVISION  
**DA 04**



**SOUTH ELEVATION**  
1:100



**WEST ELEVATION**  
1:100

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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NEW TOWN  
TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
DESIGN DEVELOPMENT

ARCHITECT  
KEITH WESTBROOK,  
482182300

DRAWN BY  
GR, KF

CHECKED BY  
KW

SCALE  
1:100

ORIGINAL SIZE  
A3

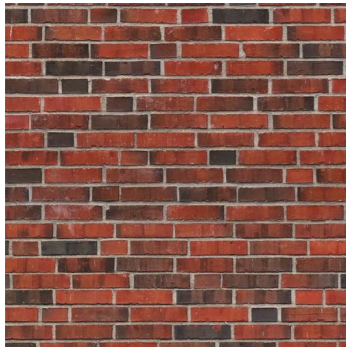
PROJECT N°  
J25022

DATE  
15/3/2026

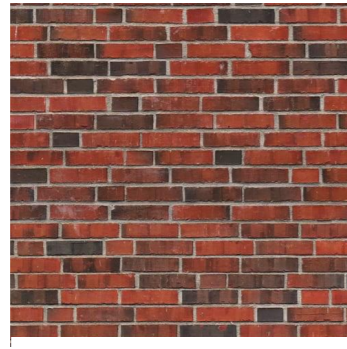
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**ELEVATIONS 02**

DRAWING N° REVISION

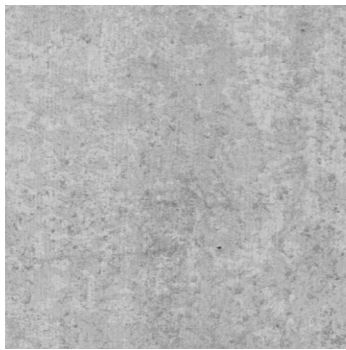
**A203- DA 04**



BR1 -  
EXISTING BRICK



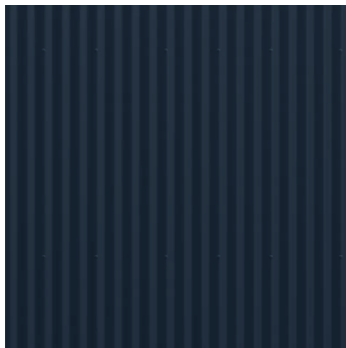
BR2 -  
NEW BRICK TO MATCH  
EXISTING BRICK



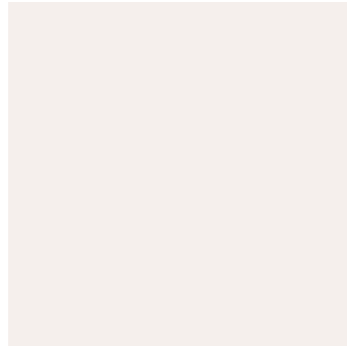
CN1 -  
NATURAL CONCRETE,  
SMOOTH FINISH



CN2 -  
CONCRETE WITH PINK OXIDE,  
TEXTURED FINISH



RF1 -  
RE-PAINT EXISTING ROOF  
SHEETING  
COLOUR: IRONSTONE



PF1 -  
PAINT FINISH TO WINDOW  
FRAMES TO MATCH CL1



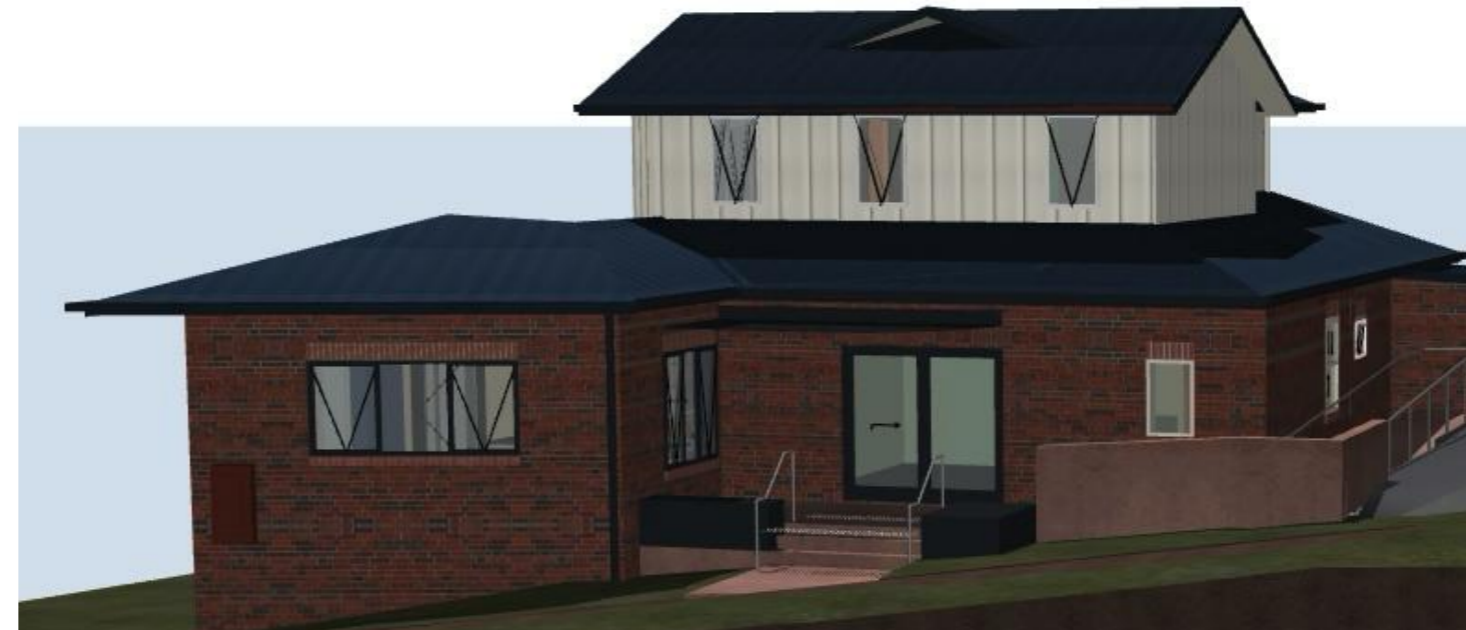
MT1 -  
METAL COLORBOND FINISH  
COLOUR -IRONSTONE TO  
MATCH ROOF



CL1 -  
VERTICAL BOARD AND  
BATTEN CLADDING, PAINT  
FINISH COLOUR OFF-WHITE



CLARE STREET PERSECTIVE



HILDERN STREET PERSPECTIVE

REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
**16 CLARE STREET  
NEW TOWN  
TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK,  
482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:144.78, 1:169.54**

ORIGINAL SIZE  
**A3**

PROJECT N°  
**J25022**

DATE  
**15/3/2026**

DRAWING TITLE  
**MATERIALITY**

DRAWING N°  
**A300-**

REVISION  
**DA 04**

	SPECIES	COMMON NAME
1	<i>Banksia marginata</i>	Silver banksia
2	<i>Brachyscome multifida</i>	Cut-leaf daisy
3	<i>Bulbine bulbosa</i>	Bulbine lily
4	<i>Correa alba</i>	White correa
5	<i>Dianella revoluta</i>	Blue flax-lily
6	<i>Dichondra repens</i>	Kidney weed
7	<i>Diplarena moraea</i>	White flag iris
8	<i>Dodonea viscosa</i>	Hopbush
9	<i>Leptospermum lanigerum</i>	Woolly teatree
10	<i>Olearia phlogopappa</i>	Dusty daisy-bush
11	<i>Poa labillardieri</i>	Silver tussock-grass
12	<i>Stylidium graminifolium</i>	Trigger plant



All vegetation marked for removal and retention as per architects deconstruction plan (A102-DA-01).

Identified species marked for removal are as follows:  
*Agonis flexuosa* - Peppermint willow (mainland species)  
*Polygala dalmaisiana* - Sweet pea shrub (exotic)  
*Grevillea sp.* (mainland cultivar)

**GENERAL NOTES**

1. Drawings to be read in conjunction with details, specification, and relevant consultant documents
2. Do not scale drawings. Refer to figured dimensions only
3. Drawings to be printed in colour
4. Discrepancies to be reported immediately to Loci Landscapes
5. Locate and protect all underground services prior to any excavation. Make good all damage to existing works caused by the activity of these works.

**TASMANIAN LUNG CENTRE**

**Suggested Plant Palette**

DRAWN	BB
CHECKED	JP
SCALE	N/A
DWG NO.	<b>Sht-5</b>
PLOT DATE	12/03/2026

**CIVIL / HYDRAULIC DRAWINGS**  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**  
**16 CLARE STREET NEW TOWN TASMANIA 7008**

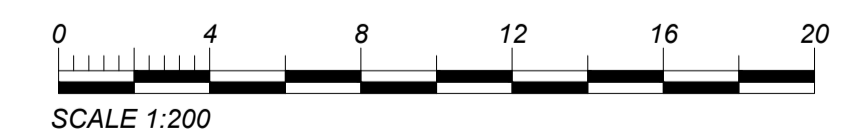
**DRAWING SCHEDULE**

SHEET	DRAWING TITLE	REV	DATE
C01	TITLE & OVERALL PLAN	1	13/03/2026
C02	NOTES & LEGEND	0	24/02/2026
C03	OVERALL LAYOUT PLAN	0	24/02/2026
C04	DETAILED CIVIL AND DRAINAGE LAYOUT 1 OF 2	1	13/03/2026
C05	DETAILED CIVIL AND DRAINAGE LAYOUT 2 OF 2	1	13/03/2026
C06	SEWER AND WATER LAYOUT	0	24/02/2026
C07	DRIVEWAY LONG SECTIONS	0	24/02/2026
C08	DRIVEWAY CROSS SECTIONS	0	24/02/2026
C09	CONSTRUCTION DETAILS 1	0	24/02/2026
C10	TURN PATHS	0	24/02/2026



**OVERALL PLAN**  
 SCALE 1:200 (mm) (A1)

**WARNING**  
 BEWARE OF UNDERGROUND SERVICES  
 THE LOCATION OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND THE EXACT POSITION SHOULD BE PROVEN ON SITE. NO GUARANTEE IS GIVEN THAT ALL SERVICES ARE SHOWN



1	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	CF	13/03/2026	BASE SURVEY SUPPLIED BY BINNY SURVEYS SURVEYED ON: 29/09/2025 HORIZONTAL DATUM: MGA2020 AHD 83 GRID: GDA2020, ZONE 55 LEVEL DATUM: AHD		FYSH DESIGN UNIT 4, 160 BUNGANA WAY CAMBRIDGE TAS  PH: 0414 149 394  ACCREDITATION: BSD LICENCE NO. 479819732	NORTH 	CIVIL/HYDRAULIC DRAWINGS  CLIENT: TASMANIAN LUNG CENTRE 16 CLARE STREET NEW TOWN TASMANIA 7008  DRAWING TITLE TITLE AND OVERALL PLAN	DESIGNED CF  PROJECT CKD-CIV-192	DRAWN CF  SHEET NO. C01	SCALE AS NOTED  REVISION 1
0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	CF	24/02/2026								
REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE						



18 CLARE STREET  
NEW TOWN TAS  
7008 REF 235518/1

EXISTING STORMWATER ROAD SIDE ENTRY PIT

CAUTION: EXISTING POWER POLE

EXISTING CO DN150 SEWER MAIN ID A637697

EXISTING CCL DN100 WATER MAIN ID A376668

HILDERN STREET

KERB / GUTTER ROAD EDGE

FOOTPATH

BOUNDARY

NEW DRIVEWAY TO EXTEND UP TO KERB SEPERATOR FROM PROPOSED ACCESS RAMP, AS PER CUMULUS LEVELS AND DETAILS

EXISTING CROSSOVER TO BE REMOVED

EXISTING TYPE KC KERB AND CHANNEL TO BE NEATLY SAWCUT, REMOVED AND REPLACED WITH NEW MIN. 7.20m WIDE DOUBLE CROSSOVER AS PER LGAT TSD-R09 AT DEVELOPERS COST

AREA OF DISTURBANCE SUCH AS FOOTPATH AND CONCRETE KERB AND BITUMEN TO BE REINSTATED AND MADE GOOD TO LGAT TSD-R08 - R09 STANDARDS AND COUNCILS SATISFACTION

EXISTING BLOCKED, REDUNDANT STORMWATER KERB CONNECTION TO BE CAPPED AND SEALED

1 HILDERN ST NEW TOWN TAS 7008 REF 74149/1

CROSSFALL ACROSS NEW FOOTPATH TO BE MAX 2.0% AS PER LONGSECTION AND NOTE 3. OF LGAT TSD-R11. EXISTING FOOTPATH TO BE REMOVED AND REPLACED APPROX. 4.0m EITHER SIDE TO ALLOW FOR GRADUAL TRANSITION OF CROSSFALL

GENERAL LAYOUT PLAN  
SCALE 1:100 (mm)

KERB / GUTTER ROAD EDGE

FOOTPATH

BOUNDARY

EXISTING REDUNDANT DRIVEWAY WITHIN NEW CROSSOVER GAP TO BE REMOVED AND REPLACED WITH TYPE KC KERB AND CHANNEL AS PER LGAT TSD-R09 AT DEVELOPERS COST

16 CLARE STREET  
NEW TOWN TAS  
7008 REF 45950/1

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT AND 150mm WIDE STRIP DRAIN

PROPOSED CARPARK

PARKING 1 CENTRELINE

STAFF PARK 01

STAFF PARK 02

STAFF PARK 03

STAFF PARK 04

STAFF PARK 05

BOUNDARY

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT

EXISTING TELSTRA PIT

EXISTING ROAD SIDE STORMWATER SIDE ENTRY PIT

EXISTING DN90 PVC REDUNDANT STORMWATER KERB CONNECTION TO BE CAPPED AND SEALED

EXISTING WATER METER

EXISTING CO DN150 SEWER MAIN ID A637697

EXISTING CCL DN100 WATER MAIN ID A376668

CLARE STREET

BOUNDARY

EXISTING STORMWATER LINE / KERB CONNECTION NOW REDUNDANT DUE TO EXISTING DOWNPIPE BEING DIVERTED AS SHOWN

EXISTING DOWNPIPE TO BE RECONNECTED TO NEW STORMWATER LINE AS SHOWN. CONTRACTOR TO ALLOW TO LOCATE

EXISTING 75x250 RHS KERB CONNECTION TO REMAIN AS MAIN PROPERTY STORMWATER KERB CONNECTION

CAUTION: EXISTING SEWER IO

EXISTING TELSTRA PIT

DN100 DWV SN6 STORMWATER @ MIN 1.0% GRADE

EXISTING CONCRETE TO BE SAWCUT AND NEW SEWER AND STORMWATER LINES TRENCHED MIN 1.0M AWAY FROM BUILDING AND FOOTINGS

RAMP LEVELS AND GRADE AS PER CUMULUS ARCHITECTURAL DRAWINGS

14 CLARE STREET  
NEW TOWN TAS  
7008 REF 124547/1

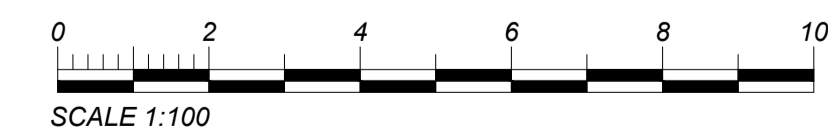
DN100 DWV SN6 STORMWATER @ MIN 1.0% GRADE

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT AND 150mm WIDE STRIP DRAIN

REFER TO C10 DRAWING FOR CAR PARK LAYOUT AND DIMENSIONS AS PER AS2890.1

DN100 DWV SN6 STORMWATER @ MIN 1.0% GRADE TO DOWNSTREAM PIT

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT

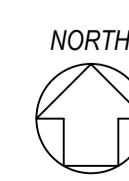


NOT FOR CONSTRUCTION

REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
1	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	13/03/2026	CF		
0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	24/02/2026	CF		



FYSH DESIGN  
UNIT 4, 160 BUNGANA WAY  
CAMBRIDGE TAS  
PH: 0414 149 394  
ACCREDITATION: BSD LICENCE NO. 479819732



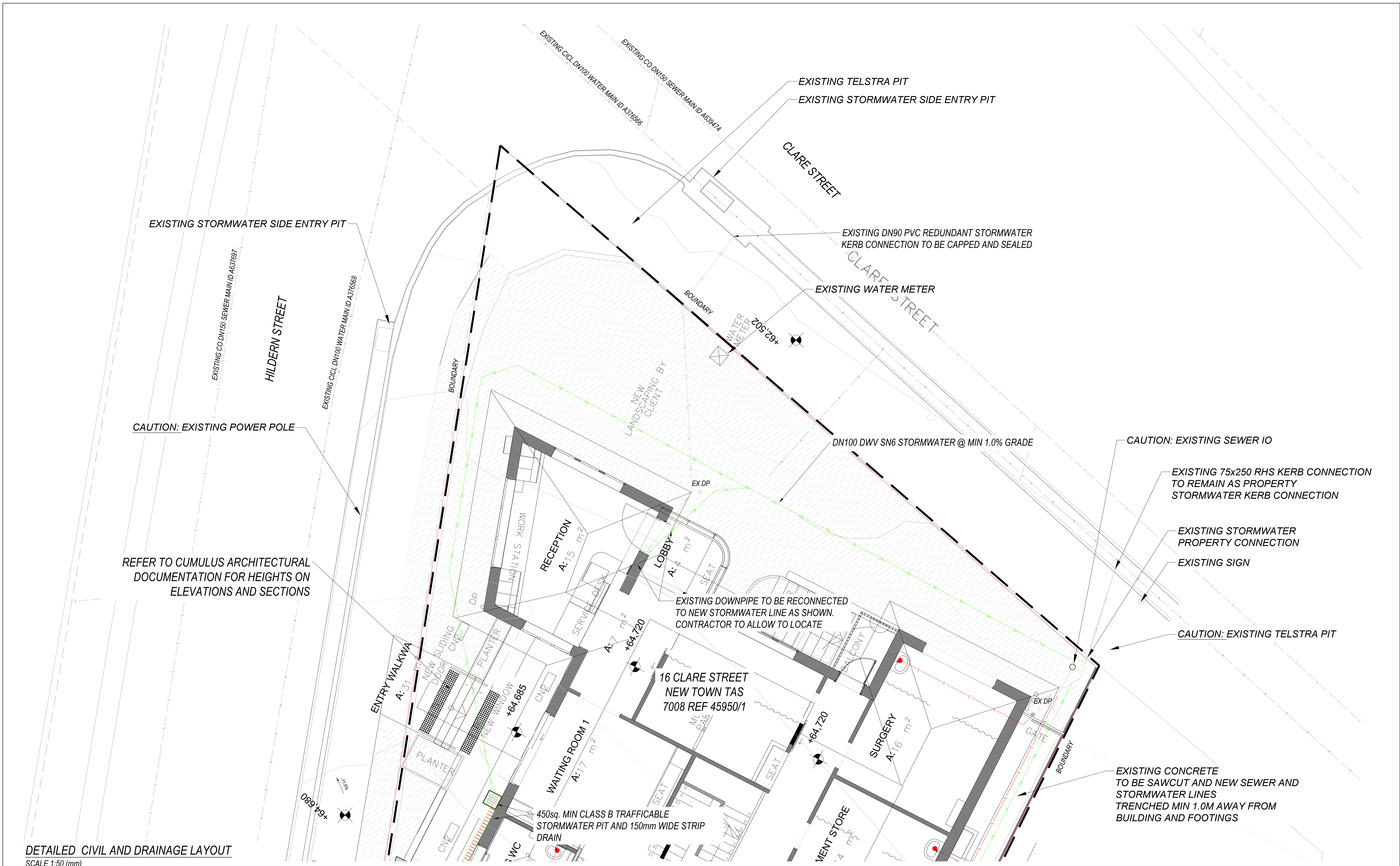
NORTH

CIVIL/HYDRAULIC DRAWINGS  
CLIENT: TASMANIAN LUNG CENTRE  
16 CLARE STREET NEW TOWN TASMANIA 7008  
DRAWING TITLE  
OVERALL LAYOUT PLAN

DESIGNED  
CF  
PROJECT  
CKD-CIV-192

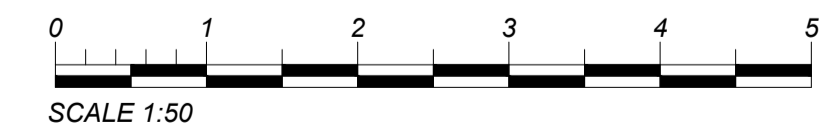
DRAWN  
CF  
SHEET NO.  
C03

SCALE  
AS NOTED  
REVISION  
1



DETAILED CIVIL AND DRAINAGE LAYOUT  
SCALE 1:50 (mm)

**NOT FOR CONSTRUCTION**



REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	24/02/2026			

FYSH DESIGN  
UNIT 4, 160 BUNGANA WAY  
CAMBRIDGE TAS  
PH: 0414 149 394  
ACCREDITATION: BSD LICENCE NO. 479819732

NORTH

CIVIL/HYDRAULIC DRAWINGS  
CLIENT: TASMANIAN LUNG CENTRE  
16 CLARE STREET NEW TOWN TASMANIA 7008  
DRAWING TITLE  
DETAILED CIVIL AND DRAINAGE LAYOUT 1 OF 2

DESIGNED	DRAWN	SCALE
CF	CF	AS NOTED
PROJECT	SHEET NO.	REVISION
CKD-CIV-192	C04	0



REFER TO CUMULUS ARCHITECTURAL DOCUMENTATION FOR HEIGHTS ON ELEVATIONS AND SECTIONS

"NO PARK" SIGN NEW LOCATION  
MIN 150mm HIGH BARRIER KERB & EDGE BEAM THICKENING ON SLAB TO ACT AS MINOR RETAINING AND SEPARATION FROM RAMP

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT

RAMP LEVELS AND GRADE AS PER CUMULUS ARCHITECTURAL DRAWINGS

PROPOSED 7.2m LONG CROSSOVER AS PER LGAT TSD-R09-v3

"NO PARK" SIGN TO BE RELOCATED 10m NORTH

EXISTING CO DN150 SEWER MAIN ID A637697

HILDERN STREET

EXISTING C/D DN100 WATER MAIN ID A376588

EXISTING BLOCKED, REDUNDANT STORMWATER KERB CONNECTION TO BE CAPPED AND SEALED

SHARED SPACE AS PER AS2890.6 2009

PARKING 1 DWAY CENTRELINE  
PROPOSED 6 x 2.4m x 5.40m CARPARKS REFER TO DRAWING C10 FOR TURNPATHS AND DIMENSIONS

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT AND 150mm WIDE STRIP DRAIN

DN100 DWV SN6 STORMWATER @ MIN 1.0% GRADE TO DOWNSTREAM PIT

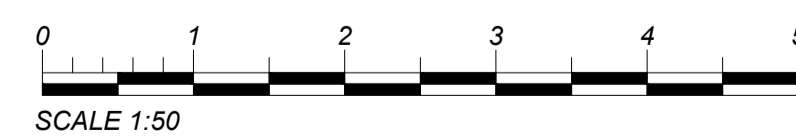
NEW DRIVEWAY TO EXTEND UP TO KERB SEPARATOR FROM PROPOSED ACCESS RAMP, AS PER CUMULUS LEVELS AND DETAILS REFER TO CROSS SECTIONS ON DRAWING C08 FOR FURTHER DETAILS

450sq. MIN CLASS B TRAFFICABLE STORMWATER PIT

REFER TO C10 DRAWING FOR CAR PARK LAYOUT AND DIMENSIONS AS PER AS2890.1

**DETAILED CIVIL AND DRAINAGE LAYOUT**  
SCALE 1:50 (mm)

**NOT FOR CONSTRUCTION**



REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
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0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	24/02/2026	CF		

FYSH DESIGN  
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CAMBRIDGE TAS  
PH: 0414 149 394  
ACCREDITATION: BSD LICENCE NO. 479819732

NORTH

CIVIL/HYDRAULIC DRAWINGS  
CLIENT: TASMANIAN LUNG CENTRE  
16 CLARE STREET NEW TOWN TASMANIA 7008  
DRAWING TITLE  
DETAILED CIVIL AND DRAINAGE LAYOUT 2 OF 2

DESIGNED  
CF  
PROJECT  
CKD-CIV-192

DRAWN  
CF  
SHEET NO.  
C05

SCALE  
AS NOTED

REVISION  
1

CAUTION: EXISTING POWER POLE

CAUTION: EXISTING SEWER IO

EXISTING TELSTRA PIT

EXISTING CO DN150 SEWER MAIN ID A637697

EXISTING CI CL DN100 WATER MAIN ID A376568

EXISTING CO DN100 SEWER MAIN ID A639474

EXISTING CI CL DN100 WATER MAIN ID A376568

HILDERN STREET

CLARE STREET

16 CLARE STREET  
NEW TOWN TAS  
7008 REF 45950/1

EXISTING INTERNAL DN100 SEWER LINE AND ORG

HAND BASIN TO PRIME ORG  
NEW DN100 DWV SN6 SEWER LINE  
TO CONNECT TO EXISTING  
SEWER LINE AS SHOWN, CONTRACTOR TO ALLOW TO  
CONFIRM EXACT LOCATION, CONDITION AND DEPTH  
OF EXISTING LINE BEFORE COMMENCING WORKS

DN100 DWV SN6 SEWER @ MIN 1.65% GRADE

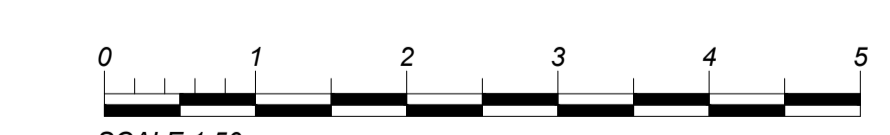
DN50 UPSTREAM VENT TO ATMOSPHERE

TRAPPED SAFE TRAY / TUNDISH FOR HWC  
IN ACCORDANCE WITH AS3500.2:2021

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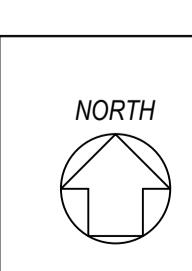
EXISTING CONCRETE  
TO BE SAWCUT AND NEW SEWER AND STORMWATER LINES  
TRENCHED MIN 1.0M AWAY FROM BUILDING AND FOOTINGS

DETAILED SEWER AND WATER LAYOUT  
SCALE 1:50 (mm)  
**NOT FOR CONSTRUCTION**



REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	24/02/2026	CF		

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UNIT 4, 160 BUNGANA WAY  
CAMBRIDGE TAS  
PH: 0414 149 394  
ACCREDITATION: BSD LICENCE NO. 479819732



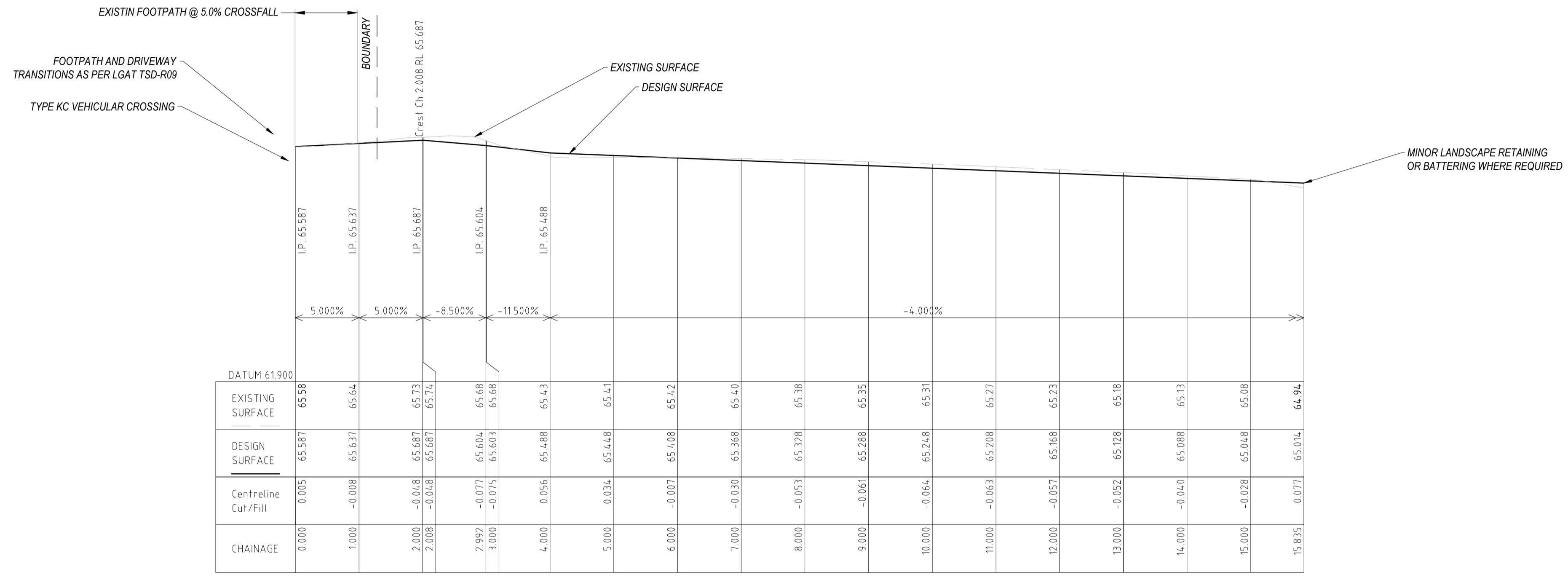
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CLIENT: TASMANIAN LUNG CENTRE  
16 CLARE STREET NEW TOWN TASMANIA 7008  
DRAWING TITLE  
SEWER AND WATER LAYOUT

DESIGNED  
CF  
PROJECT  
CKD-CIV-192

DRAWN  
CF  
SHEET NO.  
C06

SCALE  
AS NOTED

REVISION  
0



MC01 LONGITUDINAL SECTION  
HORIZONTAL 1:50 VERTICAL 1:50

**PARKING 1 DRIVEWAY LONG SECTION**  
HORIZ SCALE 1:50 (mm)  
VERT SCALE 1:50 (mm)

**NOT FOR CONSTRUCTION**



0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	CF	24/02/2026																	
REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE															

FYSH DESIGN  
UNIT 4, 160 BUNGANA WAY  
CAMBRIDGE TAS  
PH: 0414 149 394  
ACCREDITATION: BSD LICENCE NO. 479819732

CIVIL/HYDRAULIC DRAWINGS  
CLIENT: TASMANIAN LUNG CENTRE  
16 CLARE STREET NEW TOWN TASMANIA 7008  
DRAWING TITLE  
DRIVEWAY LONG SECTIONS

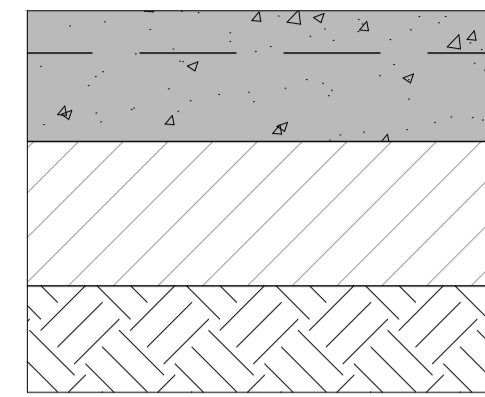
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PROJECT  
CKD-CIV-192

DRAWN  
CF  
SHEET NO.  
C07

SCALE  
AS NOTED

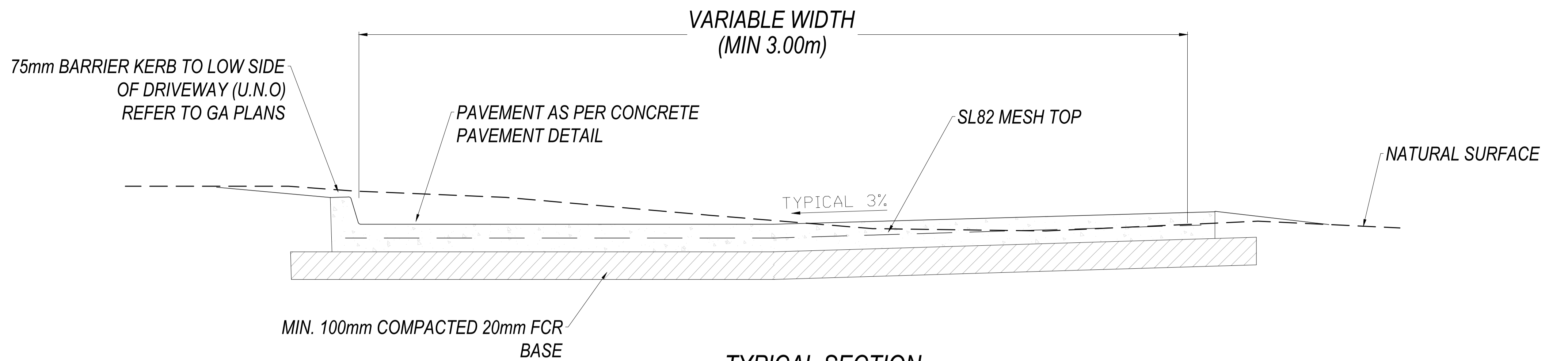
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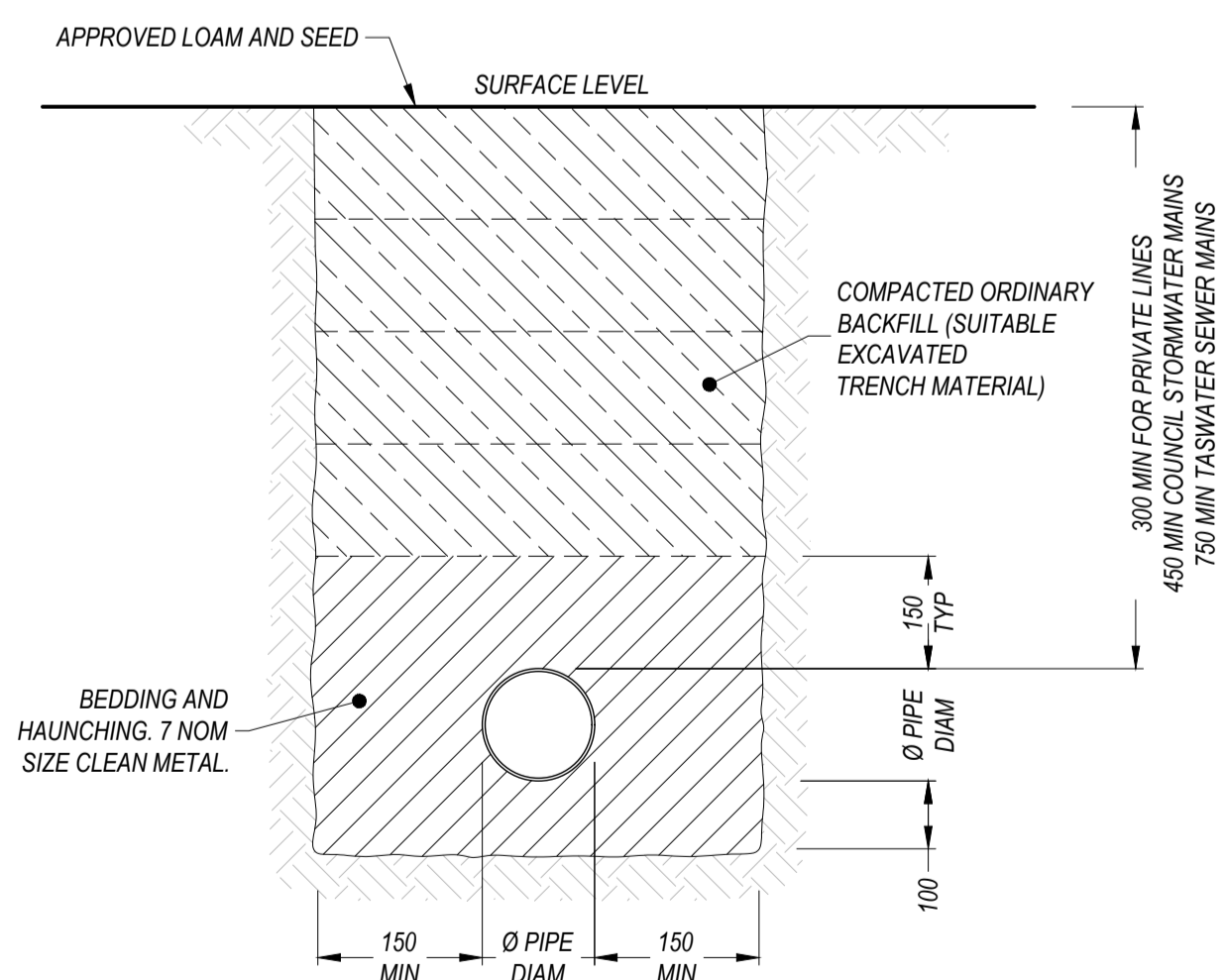


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 40mm SAWCUTS AT MAX. 4.0m CRS  
 100mm 20mm FCR  
 SUBGRADE

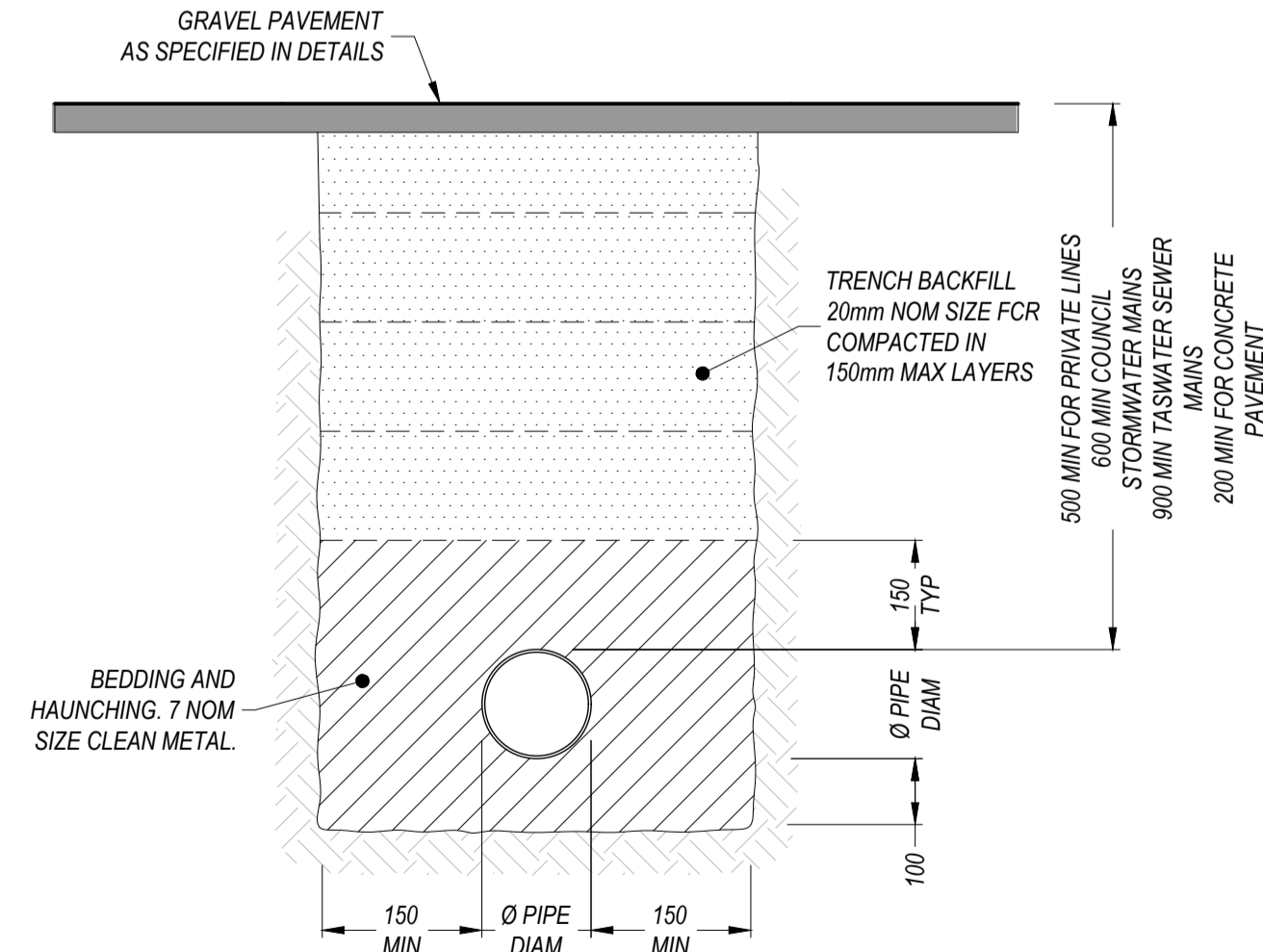
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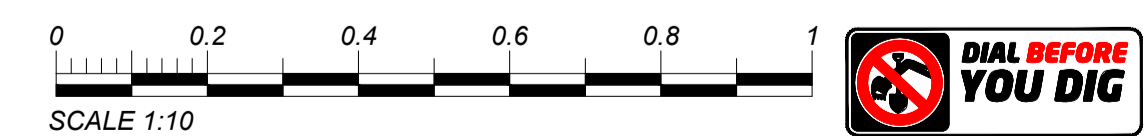
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 SCALE 1:10 (A1)



**TYPICAL CROSS SECTION - NON TRAFFICABLE**  
 SCALE 1:10 (A1)



**TYPICAL CROSS SECTION - TRAFFICABLE**  
 SCALE 1:10 (A1)



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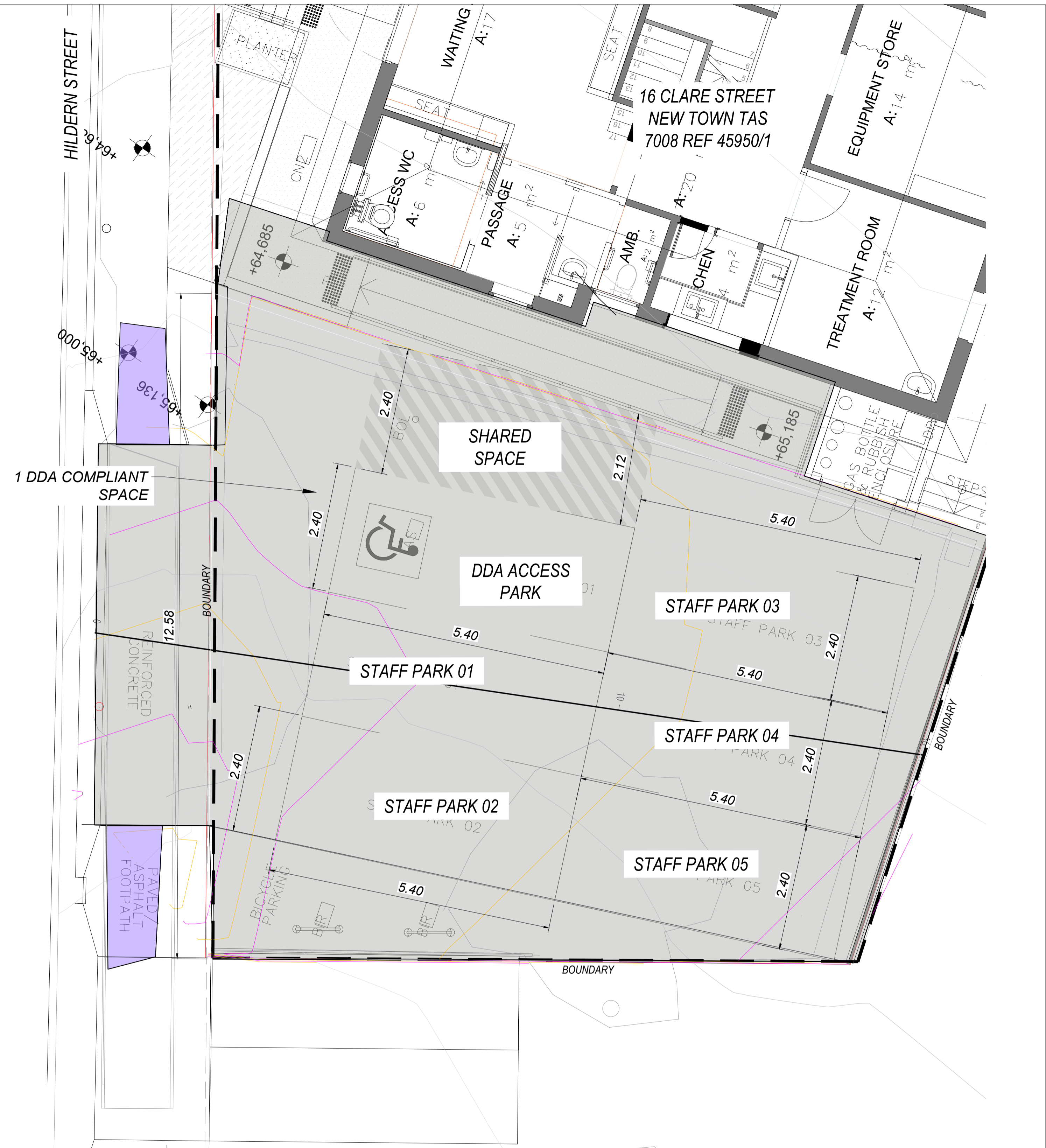
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 UNIT 4, 160 BUNGANA WAY  
 CAMBRIDGE TAS  
 PH: 0414 149 394  
 ACCREDITATION: BSD LICENCE NO. 479819732

CIVIL/HYDRAULIC DRAWINGS		SCALE AS NOTED
CLIENT: TASMANIAN LUNG CENTRE 16 CLARE STREET NEW TOWN TASMANIA 7008		
DESIGNED CF	DRAWN CF	
PROJECT CKD-CIV-192	SHEET NO. C09	REVISION 0



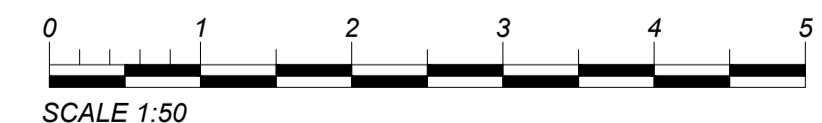
**CARPARK TURN PATHS**  
SCALE 1:50 (mm)

**B85 VEHICLE (8m RADIUS) (2004)**  
 OVERALL LENGTH 4.910m  
 OVERALL WIDTH 1.870m  
 OVERALL BODY HEIGHT 1.421m  
 MIN BODY GROUND CLEARANCE 0.159m  
 TRACK WIDTH 1.770m  
 LOCK TO LOCK TIME 4.00s  
 CURB TO CURB TURNING RADIUS 8.000m



**CARPARK LAYOUT**  
SCALE 1:50 (mm)

**NOT FOR CONSTRUCTION**



REV	DESCRIPTION	DATE	REV	DESCRIPTION	DATE
0	FOR DEVELOPMENT APPROVAL / RFI RESPONSE	24/02/2026	CF		



FYSH DESIGN  
 UNIT 4, 160 BUNGANA WAY  
 CAMBRIDGE TAS  
 PH: 0414 149 394  
 ACCREDITATION: BSD LICENCE NO. 479819732



CIVIL/HYDRAULIC DRAWINGS  
 CLIENT: TASMANIAN LUNG CENTRE  
 16 CLARE STREET NEW TOWN TASMANIA 7008  
 DRAWING TITLE  
 TURN PATH REQUIREMENTS

DESIGNED CF  
 PROJECT CKD-CIV-192  
 DRAWN CF  
 SHEET NO. C10

SCALE AS NOTED  
 REVISION 0

Client  
Tasmanian Lung Centre

Date  
17 March 2026

# Transport Impact Assessment

## Tasmanian Lung Centre

16 Clare Street, New Town

Planning

Transport

Urban Design

Waste Management

Landscape Architecture

Civil Engineering

ratio.com.au

# ratio:

**Project**  
16 Clare Street, New Town

**Prepared for**  
Tasmanian Lung Centre

**Our reference**  
23690T-REP01

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Directory path      <https://ratioconsultants1.sharepoint.com/sites/23690T/Shared Documents/Work/Reports/23690T-REP01-F03.docx>

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Version	Date	Issue	Prepared by	Checked by
F01	5/12/2025	Final Report	R. Tan	M. Petrusma
F02	11/12/2025	Amended Final	R. Tan	M. Petrusma
F03	17/03/2026	Minor Update	R. Tan	M. Petrusma

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#### Acknowledgement of Country

We acknowledge the Traditional Owners of the land we work, live and travel on, and appreciate the rich cultures of the Aboriginal and Torres Strait Islander Peoples and their enduring connection to country.

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# 1. Introduction

## 1.1. Background

Ratio Consultants were engaged by Cumulus Studio on behalf of the applicant (Tasmanian Lung Centre) to prepare a Transport Impact Assessment for a proposed medical centre addressed at 16 Clare Street in New Town.

## 1.2. Purpose and Structure of this Report

The purpose of this report is to identify and assess the potential traffic and transport impacts of the proposed development, and to address the relevant provisions of the *Tasmanian Planning Scheme – Hobart*.

This report is structured as outlined below:

- **Section 1:** Introduction
- **Section 2:** Assessment of existing transport conditions on the site and key streets surrounding the site
- **Section 3:** A description of the proposed development
- **Section 4:** Assessment of site access arrangements and carpark layout
- **Section 5:** Assessment of parking supply
- **Section 6:** Assessment of the potential traffic and transport impacts of the proposal
- **Section 7:** A review of the proposal against the relevant provisions of the Planning Scheme
- **Section 8:** Conclusions

## 1.3. References

The following documents and resources have been referred to during the preparation of this report:

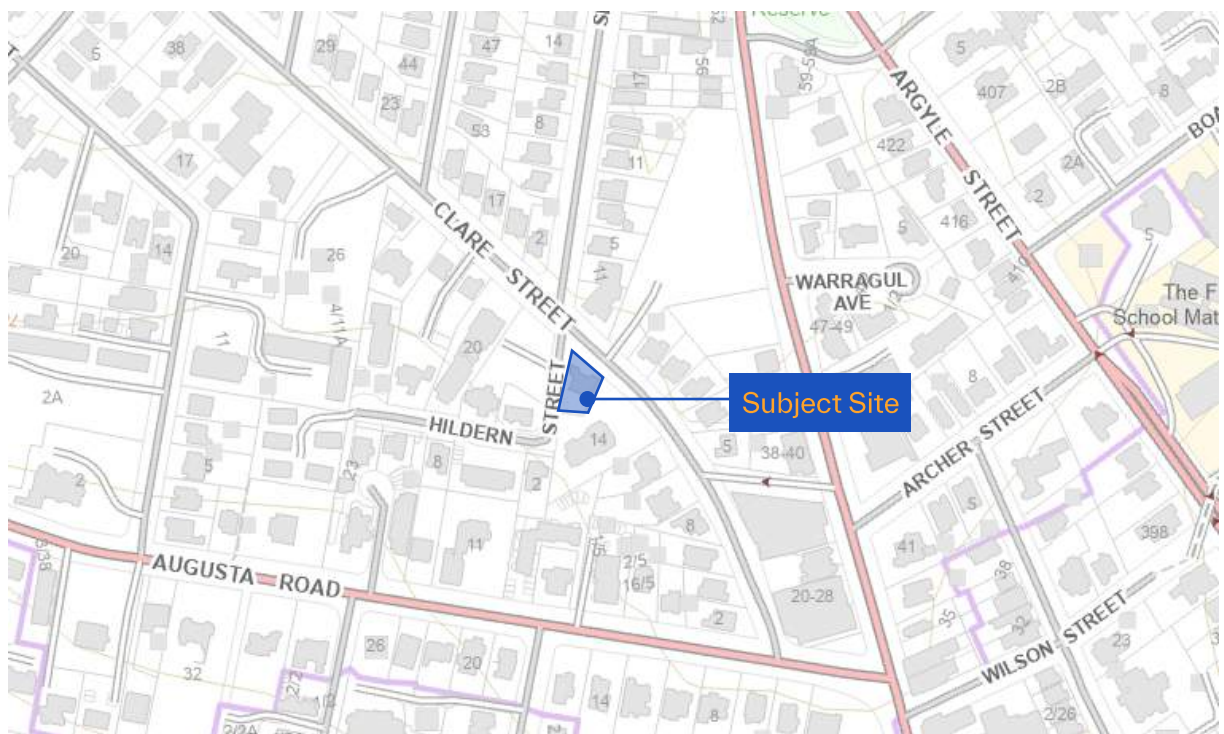
- Plans supplied by Cumulus on 11 December 2025
- Tasmanian State Planning Provisions
- Hobart Local Provisions Schedules
- Australian/New Zealand Standard AS/NZS 2890.1, *Parking facilities – Part 1: Off-street car parking*, 2004
- National Construction Code (NCC) 2022
- Transport for NSW, *Guide to Transport Impact Assessment*, 2024

# 2. Existing Conditions

## 2.1. Location and Environment

The subject site is located within the land parcel addressed at 16 Clare Street in New Town, on the corner of Hildern Street and Clare Street. The subject site location is shown in Figure 2-1.

Figure 2-1: Subject site location



Source: LISTMap © State of Tasmania (maps.thelist.tas.gov.au)

The site is located within the Inner Residential Zone as defined by the Planning Scheme. The area to the west is predominantly residential, and the site is located in close proximity to the Urban Mixed Use zone to the east which includes local shops and businesses. Immediately opposite the site on Hildern Street is the Family Relationships Centre.

An aerial view of the site is provided in Figure 2-2.

Figure 2-2: Aerial view of the existing site



Source: Nearmap Imagery © 2024 Nearmap, HERE (dated 16 October 2025)

The subject site is irregularly shaped, with a northern frontage to Clare Street of approximately 23 metres and a western frontage to Hildern Street of approximately 35 metres for an overall site area of approximately 481 square metres.

## 2.2. Road Network

**Clare Street** is a council managed collector road which essentially runs in a northwest-southeast alignment between Augusta Road to the south and Montagu Street to the north.

In the vicinity of the site, Clare Street has a sealed carriageway width of approximately 11 metres, accommodating two-way traffic movements and parallel parking on both sides of the road. Parking along the immediate northern frontage of the site to Clare Street on the northbound side is subject to a mix of 1P and 2P restrictions between 8am to 6pm (Monday to Friday), while parking on the southbound side is mostly unrestricted.

Clare Street is subject to a default speed limit of 50km/h and constructed footpath is provided on both sides on the road.

A typical view along Clare Street in the vicinity of the site is shown in Figure 2-3.

Figure 2-3: Clare Street Facing Southeast



Source: google.com/maps (image dated: May 2024)

**Hildern Street** is a Council-managed local road forming an L-shaped cul-de-sac. Near the site, Hildern Street has a sealed carriageway approximately 5.5 metres wide, accommodating two-way traffic movements with unrestricted parallel kerbside parking permitted on one side of the road only (southbound side). “No Parking” signs are present opposite the site, and on both sides of the road through the bend. Constructed footpaths are provided on both sides of the road.

A default urban speed limit of 50 km/h applies to Hildern Street. A view along Hildern Street looking south is shown in Figure 2-4.

Figure 2-4: Hildern Street Facing South



Source: google.com/maps (image dated: May 2024)

## 2.3. Sustainable Transport

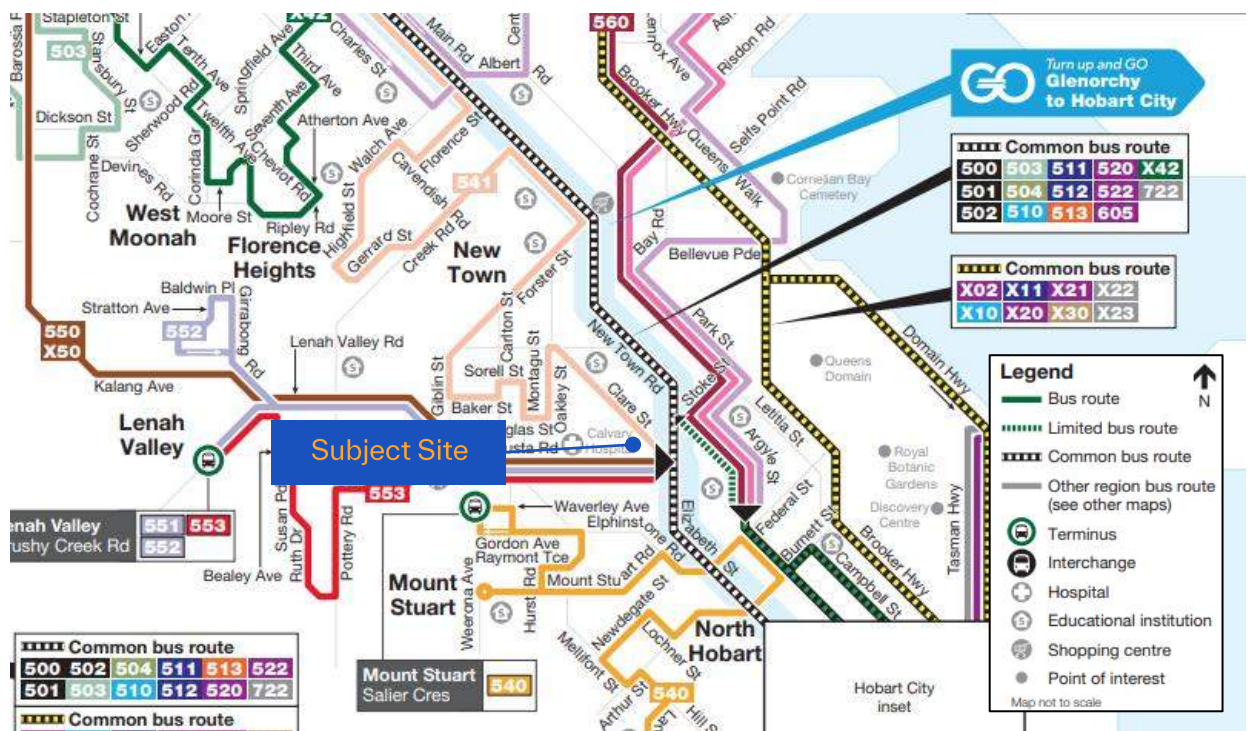
### Public Transport

The proposed development has convenient access to public transport services, with several bus routes operating in the vicinity of the site, as shown in Figure 2-5. The nearest bus stop is located approximately 60 metres northwest of the site along Clare Street, or less than a one-minute walk. This stop (Clare Street/Seymour Street) is serviced by Route 541, providing connections towards Metro Springfield, with the opposite-side stop servicing trips towards Hobart City. Bus services along Clare Street generally operate at hourly intervals between approximately 9:06am and 4:06pm on both weekdays and weekends.

The site is also located within around 320 metres of the Glenorchy to Hobart City 'Turn up and GO' route along New Town Road, which operates services at typically 10-minute intervals, as well as three routes along Augusta Road with stops immediately adjacent to the Clare Street intersection.

The site is considered to be highly public transport accessible, with ready access to Hobart, Glenorchy and surrounding suburbs.

Figure 2-5: Excerpt of Hobart North Network Map



Source: Metro Tas

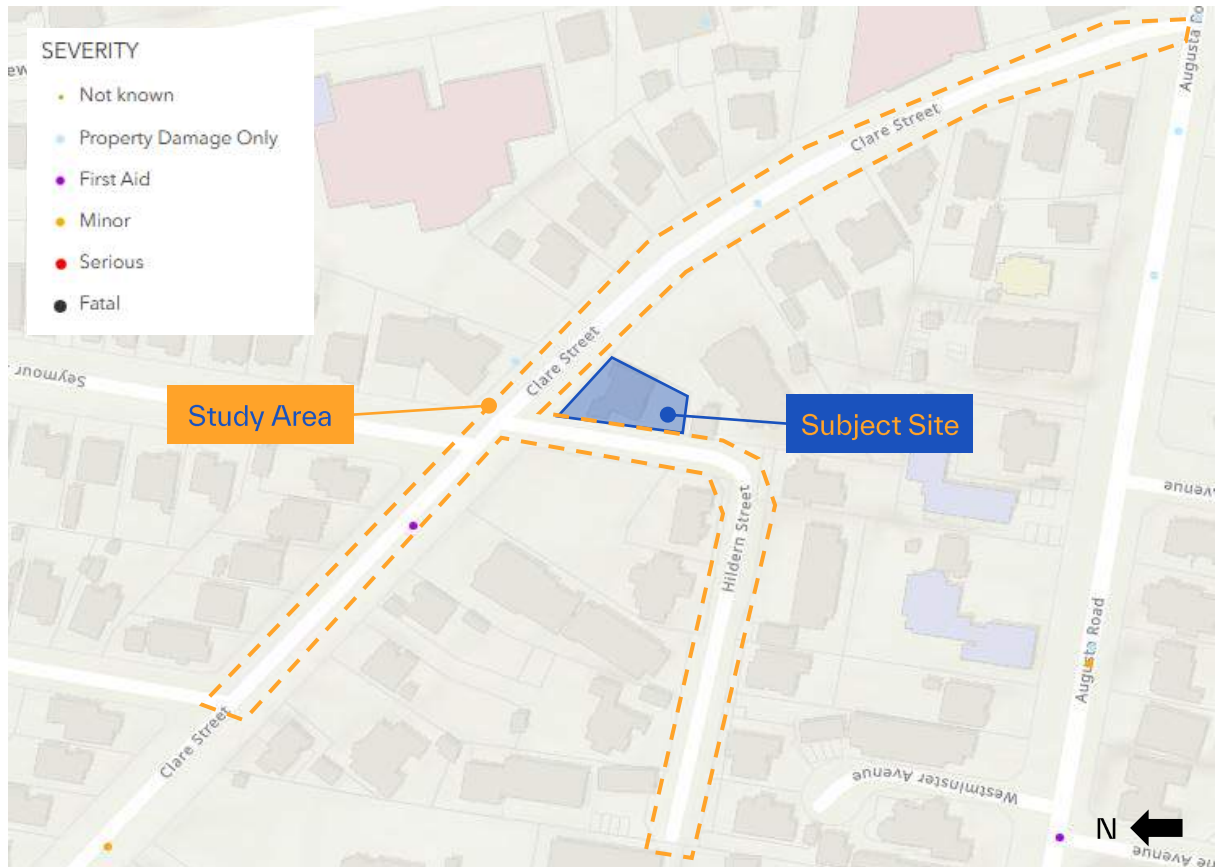
### Walking and Cycling Facilities

While no dedicated on-road cycling facilities are provided in the immediate vicinity of the site, constructed footpaths are available along both sides of Clare Street and Hildern Street, providing good pedestrian connectivity along the site frontages.

## 2.4. Crash History

The most recent five-year crash history has been obtained from the Department of State Growth in the vicinity of the subject site, for the study area shown in Figure 2-6, covering the most recent 5-year time period from 1 October 2020 to 1 October 2025.

Figure 2-6: Study area for crashes surrounding the subject site



Source: Department of State Growth

A total of three crashes were recorded within the study area, with no casualty crashes reported. All crashes occurred more than two years ago. Of these, two resulted in property damage only, and one involved a first-aid injury crash that occurred approximately 40 metres northwest of Hildern Street along Clare Street, associated with a U-turn manoeuvre.

Accordingly, the crash history does not indicate any specific road safety deficiencies that are likely to be exacerbated by the proposed development.

# 3. The Proposal

## 3.1. Overview

The proposed development involves the conversion of an existing residential dwelling at 16 Clare Street in New Town to a medical centre.

More specifically, the development is proposed to comprise the following land use yield:

- Two consulting rooms
- Lung function testing lab

Based on information provided by the permit applicant, the proposed operational details of the site are understood to be as follows:

- **Number of practitioners:** Typically 2 practitioners on site at any one time (maximum 3)
- **Operating periods:** Monday to Friday
- **Appointment spacing:** on average 17 appointments per day, each varies between 15 minutes to one hour in duration.

A summary of the proposed transport infrastructure is provided in Table 3-1.

Table 3-1: Summary of Proposed Transport Infrastructure

Transport Element	Description
Vehicular Access	Access to the site is provided via a proposed 7.2-metre-wide crossover from Hildern Street, directly serving the on-site parking spaces
Pedestrian Access	A new entry walkway connecting the site to the existing footpath along Hildern Street
Car Parking	A total of six (6) car parking spaces (inclusive of 1 DDA space and 5 staff-only spaces)
Bicycle Parking	Up to 4 x bicycle parking spaces

An excerpt of the site plan is shown in Figure 3-1, also attached to Appendix A.



# 4. Site Access

## 4.1. Access Arrangements

Clause 2.6.3-A1 of the Planning Scheme states:

*“A1 The number of accesses provided for each frontage must:*

- (a) be no more than 1; or*
- (b) no more than the existing number of accesses, whichever is the greater”*

Access to the car park is provided via an existing entry on Hildern Street, located at the southwestern corner of the site, which will be widened to 7.2 metres to accommodate the full width of the car park. A separation of 2.5 metres (including wings) would be available between the proposed widened crossover, and the neighbouring crossover for the shed at 14 Clare Street.

Accordingly, the proposal is considered to comply with the acceptable solution under Clause 2.6.3–A1, with no more than one or the existing number of accesses for the Hildern Street frontage.

Figure 4-1: Proposed Site Access



## 4.2. Sight Distance Assessment

The available sight distance at the site access has been assessed against the requirements of Australian/New Zealand Standard AS/NZS 2890.1, *parking facilities – Part 1: Off-street car parking* (2004).

Sight distance at access driveway is measured from a point 2.5 metres back from the edge of the frontage road and increases with the design speed. The subject site is located near the T-junction of Clare Street and Hildern Street, with a 90-degree bend along Hildern Street immediately south of the site. Given the sharp bend and the turning movements at an intersection, vehicles are likely to be approaching the access at a lower speed than the default 50 km/h speed limit. Therefore, it is considered appropriate to assess the sight distance requirement at a reduced speed of 40 km/h, corresponding to a minimum sight distance requirement of 35 metres.

Given that there is no single consolidated access point to the car park, the sight lines have been assessed from each edge of the car park access, as shown in Figure 4-2.

Figure 4-2: Sight Distance Assessment



Based on the sight distance assessment, sufficient visibility is available from the site access, with an observed distance of approximately 30 metres. Although this is slightly below the requirement of 35 metres, it is considered acceptable given that vehicles are realistically likely to travel at a lower speed of around 30km/h or less given the road geometry. Accordingly, the available sight distance is considered appropriate.

### 4.3. Pedestrian Access and Layout

Clause C2.6.5-A1.1 & A1.2 of the Planning Scheme states:

“A1.1 Uses that require 10 or more car parking spaces must:

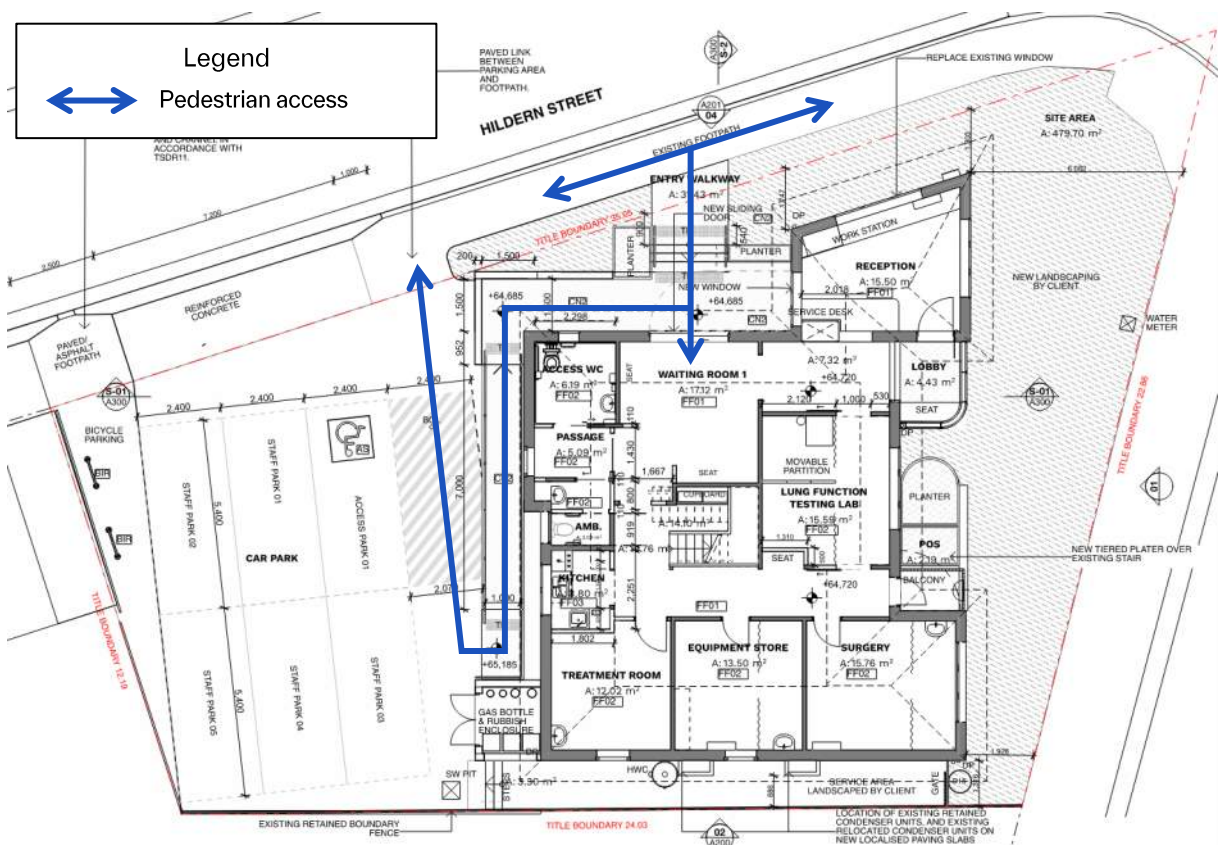
- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
  - (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or
  - (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and
- (b) be signed and line marked at points where pedestrians cross access ways or parking aisles

A1.2 In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.”

Pedestrian movement within the surrounding area is facilitated by footpaths along Clare Street and Hildern Street, with direct access to the building via stairs, as shown in Figure 4-3. Access to the site is also supported by a DDA-compliant access ramp with a gradient of 1:14 and subject to the provision of an appropriate slip resistant surface as specified under AS2890.6, providing convenient connection from the accessible car space to the site.

Accordingly, the proposal is considered to satisfy the acceptable solution of Clause C2.6.5-A1.1 and A1.2 of the Planning Scheme.

Figure 4-3: Proposed Pedestrian Access Network



## 4.4. Construction of Parking Area

Clause 2.6.1-A1 of the Planning Scheme states:

*“All parking, access ways, manoeuvring and circulation spaces must:*

- (a) be constructed with a durable all weather pavement;*
- (b) be drained to the public stormwater system, or contain stormwater on the site; and*
- (c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.”*

The car parking area will be asphalt, consistent with the existing carpark, and therefore complies with Clause C2.6.1-A1 of the Planning Scheme.

## 4.5. Design and Layout of Parking Areas

The proposed car parking area provides five staff parking spaces and one accessible parking space in a tandem arrangement, accessed directly via Hildern Street. The proposal does not comply with the acceptable solution and relies on performance criteria of Clause C2.6.2-P1 which states:

*“All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:*

- (a) the characteristics of the site;*
- (b) the proposed slope, dimensions and layout;*
- (c) useability in all weather conditions;*
- (d) vehicle and pedestrian traffic safety;*
- (e) the nature and use of the development;*
- (f) the expected number and type of vehicles;*
- (g) the likely use of the parking areas by persons with a disability;*
- (h) the nature of traffic in the surrounding area;*
- (i) the proposed means of parking delineation; and*
- (j) the provisions of Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking and AS 2890.2 -2002 Parking facilities, Part 2: Off-street commercial vehicle facilities.*

The site is highly constrained with respect to the dimensions of the car parking area, which necessitates the proposed tandem solution in order to maximise the availability of staff parking on the site, and reduce reliance on on-street parking in the surrounding area.

Staff parking space dimensions comply with AS 2890.1 for User Class 1 (i.e. staff parking) with a length of 5.4 metres and a width of 2.4 metres, and the accessible space complies with the dimensions set out in AS 2890.6. While the adjacent shared area is slightly substandard, it provides sufficient space for full door opening and allows for entering and exiting the vehicle.

Parking opposite the carpark is restricted due to the presence of “No Parking” signs, which means access to and from the carpark will be uninhibited by vehicles parked on-street. Swept path analysis demonstrating efficient use of the carpark is provided in Appendix B.

The existing carpark will be regraded to coordinate the existing slope on Hildern Street with the normal requirements of carparks. The concrete apron between the car parking spaces and the property boundary will provide the necessary grade transitions. While there may be some

minor nonconformance with respect to the slope, this is necessary due to the existing gradients. No change to the existing footpath levels is proposed.

It is expected that each staff parking space would generate two movements per day (one entry and one exit) plus occasional use of the accessible parking space in the order of a few movements per week. This level of traffic is very low and, given that Hildern Street is a low-volume cul-de-sac, is unlikely to result in any impact to traffic operation. The use of staff parking would be managed by the centre to minimise the amount of potential conflict, including managing shift start and finish times to ensure rear car parking spaces can be used when required.

There is sufficient sight distance to the carpark for the prevailing vehicle speeds.

Based on the above assessment, it is considered that the proposed carpark provides convenient, safe and efficient parking for staff, and people requiring an accessible parking space and therefore is consistent with the performance criteria of Clause C2.6.2-P1.

# 5. Parking Assessment

## 5.1. Car Parking Assessment

Given the proposed development is classified as a Business and Professional Service use, and is located within the Inner Residential Zone, Clause C2.5.5 of the Planning Scheme will apply.

The acceptable solution states:

*“A1 Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, on-site car parking is not required for:*

- (a) Food Services uses up to 100m<sup>2</sup> floor area or 30 seats, whichever is the greater; and*
- (b) General Retail and Hire uses up to 100m<sup>2</sup> floor area,*

*provided the use complies with the hours of operation specified in the relevant Acceptable Solution for the relevant zone.”*

Given that the proposed development does not fall into either of the two categories above, it relies on performance criteria which is as follows:

*“P1 Within existing non-residential buildings in the General Residential Zone and Inner Residential Zone, the number of on-site car parking spaces must be sufficient to meet the reasonable needs of users and must not cause an unreasonable impact on residential amenity, having regard to:*

- (a) car parking demand generated by the proposed use during its proposed hours of operation;*
- (b) the availability of on-street and public car parking in the surrounding area;*
- (c) the availability and frequency of public transport within a 400m walking distance of the site;*
- (d) the availability and likely use of other modes of transport;*
- (e) the availability and suitability of alternative arrangements for car parking provision;*
- (f) any reduction in car parking demand due to the sharing of car parking spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces;*
- (g) any car parking deficiency or surplus associated with the existing use of the land;*
- (h) any relevant parking plan for the area adopted by council;*
- (i) any existing on-street car parking restrictions; and*
- (j) the proportion of residential properties without off-street parking within a 100m radius of the subject site”*

The relevant factors are addressed in the following sections:

### **(a) Car Parking Demand**

Based on information provided by the applicant, appointment times generally range from 15 minutes to 1 hour. The proposed development will typically operate on weekdays between 8:30 am and 4:30 pm. It is understood that most patients attend both a lung function testing

appointment and a respiratory physician appointment during the same visit with appointment times ranging from 15 minutes to up to one hour in duration.

The maximum car parking demand would likely comprise:

- Up to five (5) staff parking spaces comprising
  - 1 x full time respiratory physician
  - 2 x lung function scientist
  - 2 x support staff (receptionist or administrative staff)
- Up to six (6) patient parking spaces based on
  - 3 x patients currently being seen
  - 3 x patients in the waiting room

It is noted that the proposal would typically see around 17 appointments per day on average, which means that back-to-back appointments will not occur all of the time and the maximum patient parking demand of 6 spaces is likely to be experience relatively infrequently.

Noting that all staff parking would be accommodated on-site, the proposal may generate an overflow of up to six (6) car parking spaces on-street for a duration typically less than one hour.

#### **(b) Publicly Available On-Street Parking**

Unrestricted on-street parking is available along the site's western frontage on Hildern Street. Short-term, time-restricted parking (1P and 2P) is available along the site's northern frontage on Clare Street, applicable between 8:00 am and 6:00 pm, Monday to Friday. All of this parking would be suitable for patients given the expected duration of appointments.

Additional unrestricted on-street parking is available on the opposite side of Clare Street and on both sides on Seymour Street. A review of recent Nearmap aerial imagery indicates that these spaces are moderately utilised depending on the time of day, with more than six on-street spaces generally available for patient and visitor parking within close proximity to the subject site.

It is also noted that the area surrounding the site is primarily residential in nature. The parking demands of residential uses typically peak in the evening and overnight, outside of the operating hours of the proposed use, such that reliance on on-street parking promotes efficiencies in the utilisation of public parking.

#### **(c) Public Transport**

The site is well serviced by public transport, with the nearest bus stop conveniently located within 70 metres of the site, as outlined in Section 2.3 of this report, and the Glenorchy to Hobart City "Turn up and GO" route located within a 400 metre walking distance. These services provide employees and visitors with an alternative to private vehicle use, thereby potentially reducing reliance on on-site staff parking use or on-street parking for patients.

#### **(d) Other Modes of Transport**

While dedicated on road bicycle facilities are not formally provided in the surrounding streets of the subject, Clare Street is identified as a 'bike friendly' road given its local road nature and predominantly residential and associated speed. Bicycle parking will also be provided on site to ensure that this is a viable mode of transport for those that choose to cycle.

Footpaths are provided along the site frontages along Hildern Street and Clare Street, with dedicated pedestrian access provided separate from the vehicular access. It is therefore considered that alternative transport modes to private car are available and convenient for those who choose to use them.

### **Summary**

Based on the above assessment, the proposed parking supply is considered sufficient to meet the reasonable needs of the use. All anticipated staff parking can be accommodated on site, while the remaining patient/ visitor parking demand can be accommodated within the nearby public on-street parking spaces without causing any unreasonable impact to the surrounding area.

Given that patient appointments are likely managed on an appointment-only basis, patients will be advised that no on-site parking is available and will therefore be expected to make alternative travel arrangements.

The proposal is therefore consistent with the performance criteria C2.5.5-P1.

## **5.2. Bicycle Parking Spaces**

Clause C2.5.2-A1 of the Planning Scheme states:

*“Bicycle parking spaces must:*

- (a) be provided on the site or within 50m of the site;*
- (b) be no less than the number specified in Table C2.1”.*

Under Table C2.1 of the Planning Scheme, the proposed development is required to provide one (1) bicycle parking space, based on the specified rate of providing 2 spaces for each 8 practitioners.

Bicycle parking will be provided in the carpark. The specific bicycle parking product to be installed, and its design, will be confirmed during detailed design of the car parking area.

## **5.3. Motorcycle Parking**

Clause C2.5.3 of the Planning Scheme states:

*“The number of on-site motorcycle parking spaces for all uses must:*

- (a) be no less than the number specified in Table C2.4; and*
- (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained.”*

Table C2.4 generally requires one motorcycle parking space for every 20 car parking spaces for a use, after the first 20 spaces. With reference to Table C2.1 of the Planning Scheme, the proposed development would be required to provide 12 car parking spaces (based on three practitioners). Accordingly, no motorcycle parking space is required to be provided.

# 6. Traffic Impacts

## 6.1. Expected Trip Generation

A total of six (6) parking spaces are provided on site, comprising five (5) spaces allocated to staff and one (1) accessible (DDA) space available for use by patients or visitors. Traffic activity associated with the development will be primarily generated by staff movements, which are expected to result in only minor traffic volumes. Assuming the DDA space is occupied concurrently with peak staff movements, the development is estimated to generate a maximum of **six (6) vehicle trips** during each of the AM and PM peak periods.

## 6.2. Assessment of Clause C3.5.1

The acceptable solution of Clause C3.5.1 states:

*“A1.1 For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:*

- (a) a new junction;*
- (b) a new vehicle crossing; or*
- (c) a new level crossing.*

*A1.2 For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.*

*A1.3 For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.*

*A1.4 Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:*

- (a) the amounts in Table C3.1; or*
- (b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road.*

*A1.5 Vehicular traffic must be able to enter and leave a major road in a forward direction.”*

Given that Hildern Street is not a Category 1 road, a limited access road or a major road, A1.1 and A1.5 are not relevant. Similarly, there is no new vehicle crossing proposed (rather the existing crossing will be modified) and A1.2 is not relevant.

Table C3.1 permits an increase in daily traffic of up to 40 vehicle movements per day. The proposal is likely to generate fewer than 20 vehicle movements per day via the existing (modified) site access and therefore complies with A1.4.

### 6.3. Impact to Road Safety

No significant detrimental road safety impacts are anticipated due to the proposed development. This is based on the following:

- The proposed traffic generation of 6 vehicle trips during the AM and PM peak hour on Hildern Street is considered a minor increase in traffic and can be accommodated by the surrounding road network.
- The crash history does not suggest that there are any specific road safety deficiencies in the vicinity of the site that might be exacerbated by the proposed development.
- There is sufficient sight distance at the proposed new site egress point on Hildern Street in accordance with normal requirements, including for both pedestrians and vehicles.

# 7. Planning Scheme Assessment

The proposed development has been assessed against the relevant, traffic and transport related provisions of the *Parking and Sustainable Transport Code* and the *Road and Railway Assets Code*. A summary of compliance is provided in the following table.

Clause	Response	Reference
C2.5.1 Car parking numbers	Assessed under C2.5.5	-
C2.5.2 Bicycle parking numbers	Complies with A1	Section 5.2
C2.5.3 Motorcycle parking numbers	Complies with A1	Section 5.3
C2.5.4 Loading bays	Not applicable	-
C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone	Consistent with P1	Section 5.1
C2.6.1 Construction of parking areas	Complies with A1	Section 4.4
C2.6.2 Design and layout of parking areas	Consistent with P1	Section 4.5
C2.6.3 Number of accesses for vehicles	Complies with A1	Section 4.1
C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone	Not assessed	-
C2.6.5 Pedestrian access	Complies with A1.1 and A1.2	Section 4.3
C2.6.6 Loading bays	Not applicable	-
C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	Not applicable	-
C2.6.8 Siting of parking and turning areas	Not assessed	-
C2.7 Parking Precinct Plan	Not applicable	-
C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction	Complies with A1.4	Section 6.2

# 8. Conclusion

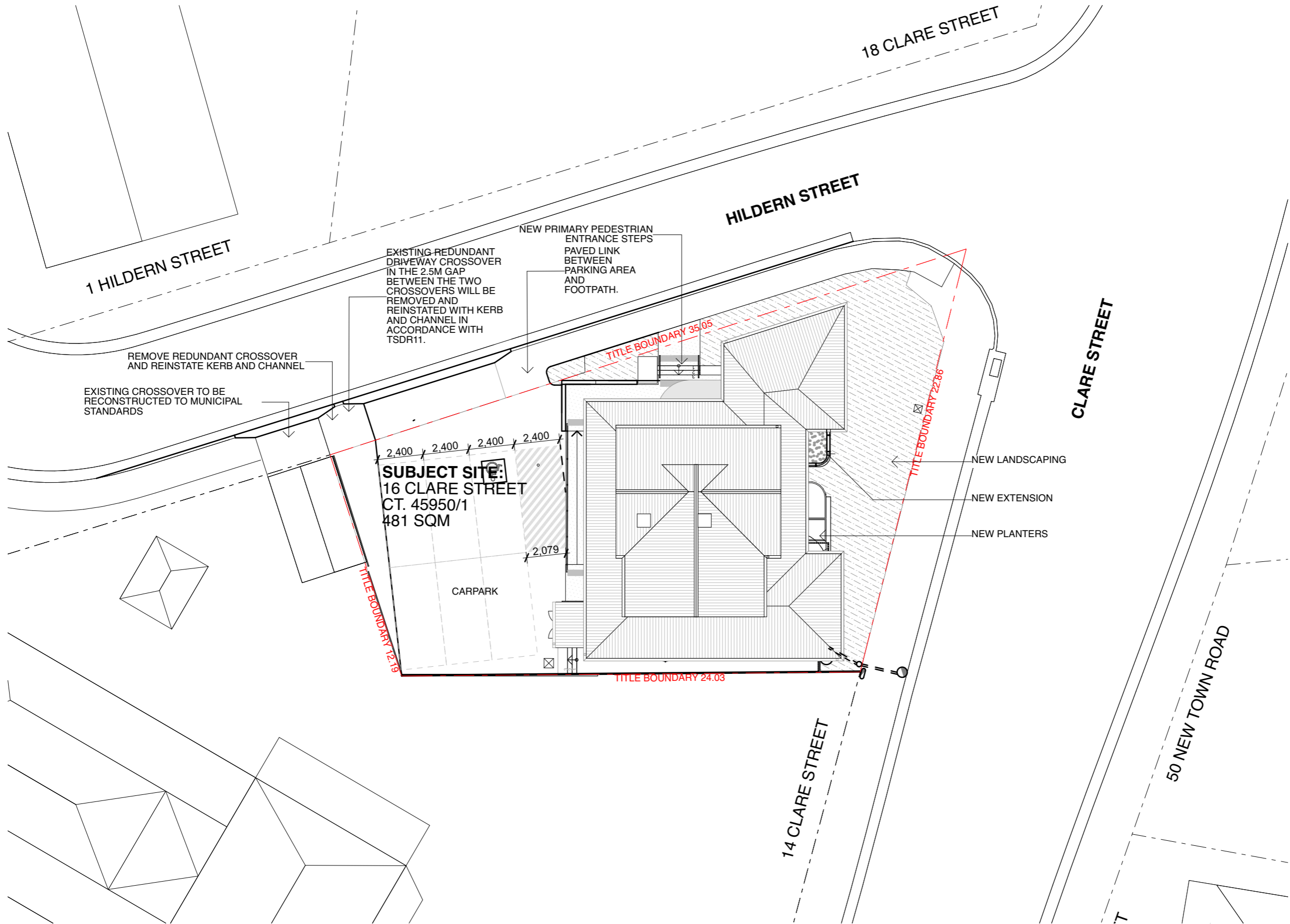
This Transport Impact Assessment report has assessed the potential traffic and transport impacts of the proposed Tasmanian Lung Centre at 16 Clare Street in New Town.

The key findings are as follows:

- The proposed development provides 6 car parking spaces for staff and people requiring an accessible parking space. The resulting maximum overflow parking is up to 6 patient vehicles. This is considered acceptable for the following reasons:
  - Sufficient on-street parking is available in the surrounding area, including both short-term (time-restricted) and unrestricted parking opportunities suitable for patient/visitor use.
  - The Tasmanian Lung Centre is expected to operate on an appointment basis, typical of the specialist clinic, with patient arrivals distributed throughout the day. Patients are also likely to be informed of the limited on-site parking and encouraged to utilise nearby on-street parking.
  - The site benefits from convenient access to nearby public transport services, offering a viable alternative to private vehicle use.
  - The total estimated maximum parking demand of eleven (11) spaces can be appropriately accommodated, with five (5) staff spaces provided on site and the remaining six (6) patient spaces able to be comfortably accommodated within available on-street parking near the site.
- The proposed car park layout has been designed generally in accordance with the relevant Australian Standards. The tandem arrangement is considered acceptable given the proposed use of parking (staff parking) which can be managed by the centre, the low frequency of movements into and out of the carpark, and the nature of Hildern Road.
- Travel to the site by alternative means (walking, cycling and by public transport) is reasonably well supported by proposed on site facilities and the external transport network.
- Four (4) bicycle parking spaces are provided, in excess of the Planning Scheme requirement.
- There is sufficient sight distance at the proposed access point for the prevailing vehicle speeds on Hildern Street.
- The proposed development is expected attract up to 6 vehicle movements per hour during the site peak AM and PM period

Based on the findings of this report, the proposal is not expected to create significant adverse traffic and parking impacts and therefore is supported on traffic and transport grounds.

# Appendix A Development Plans



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
INFO@CUMULUS.STUDIO  
SUITE 2, LEVEL 2, 147 MACQUARIE STREET HOBART,  
TAS 7000 +61(0) 8251 4841

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
**16 CLARE STREET  
NEW TOWN  
TASMANIA 7008**

PROJECT NORTH

PROJECT STAGE  
**DESIGN DEVELOPMENT**

ARCHITECT  
**KEITH WESTBROOK,  
482182300**

DRAWN BY  
**GR, KF**

CHECKED BY  
**KW**

SCALE  
**1:200**

ORIGINAL SIZE  
**A3**

PROJECT N°  
**J25022**

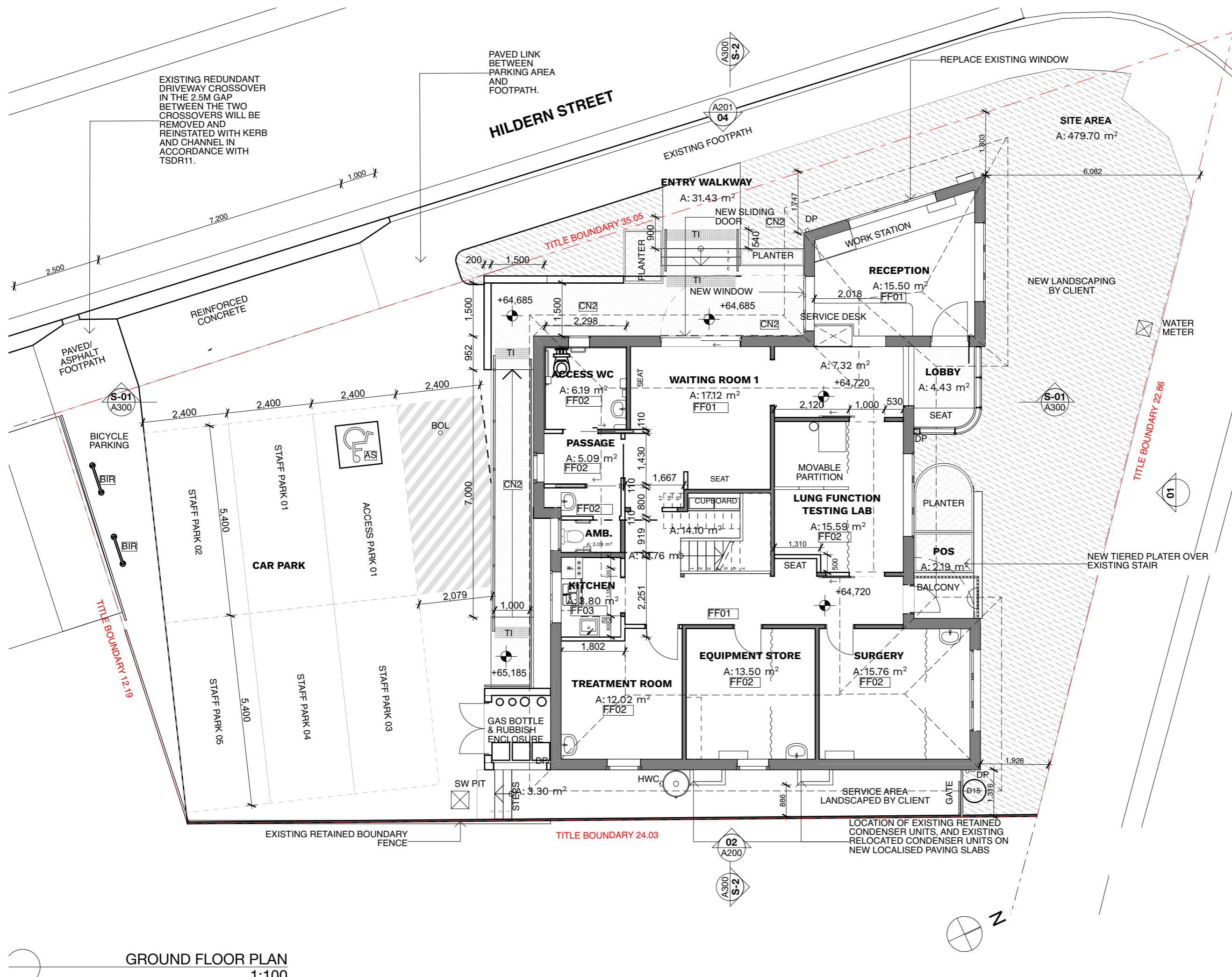
DATE  
**15/3/2026**

DRAWING TITLE  
**SITE PLAN - PROPOSED**

DRAWING N° REVISION

**A101- DA 04**

NEW SITE PLAN  
1:200



REV	DATE	PURPOSE
DA 04		BUILDING APPLICATION

**NOT FOR CONSTRUCTION**

CUMULUS STUDIO PTY LTD  
INFO@CUMULUS.STUDIO  
SUITE 2, LEVEL 2, 147 MACQUARIE STREET HOBART,  
TAS 7000 +61(0) 8231 4841

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PROJECT NAME  
**TASMANIAN LUNG CENTRE ALTERATIONS AND ADDITIONS**

PROJECT ADDRESS  
16 CLARE STREET  
NEW TOWN  
TASMANIA 7008

PROJECT NORTH

PROJECT STAGE  
DESIGN DEVELOPMENT

ARCHITECT  
KEITH WESTBROOK,  
482182300

DRAWN BY  
GR, KF

CHECKED BY  
KW

SCALE  
1:100

ORIGINAL SIZE  
A3

PROJECT N°  
J25022

DATE  
15/3/2026

DRAWING TITLE  
**PLAN - GROUND LEVEL**

DRAWING N°

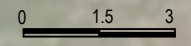
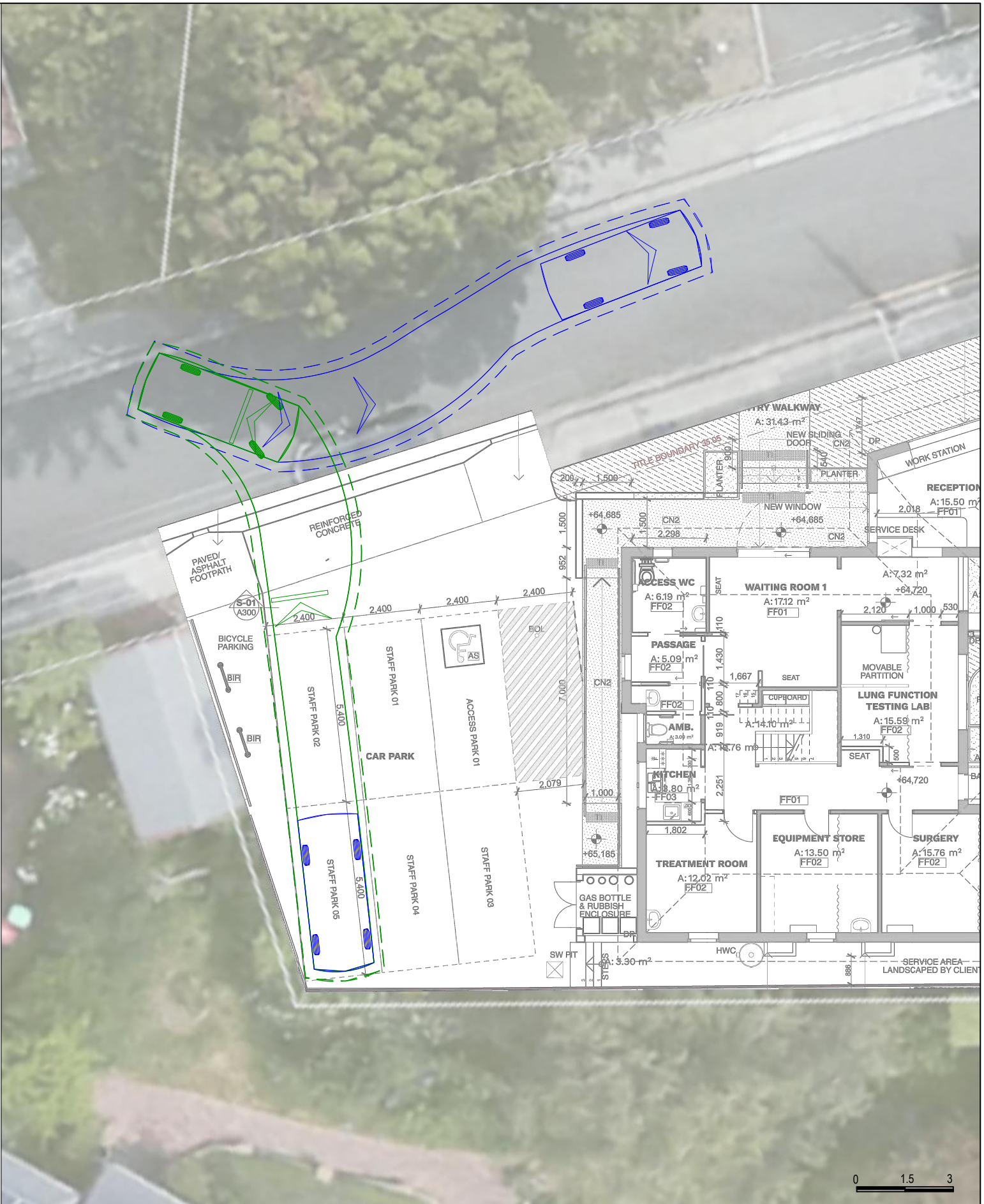
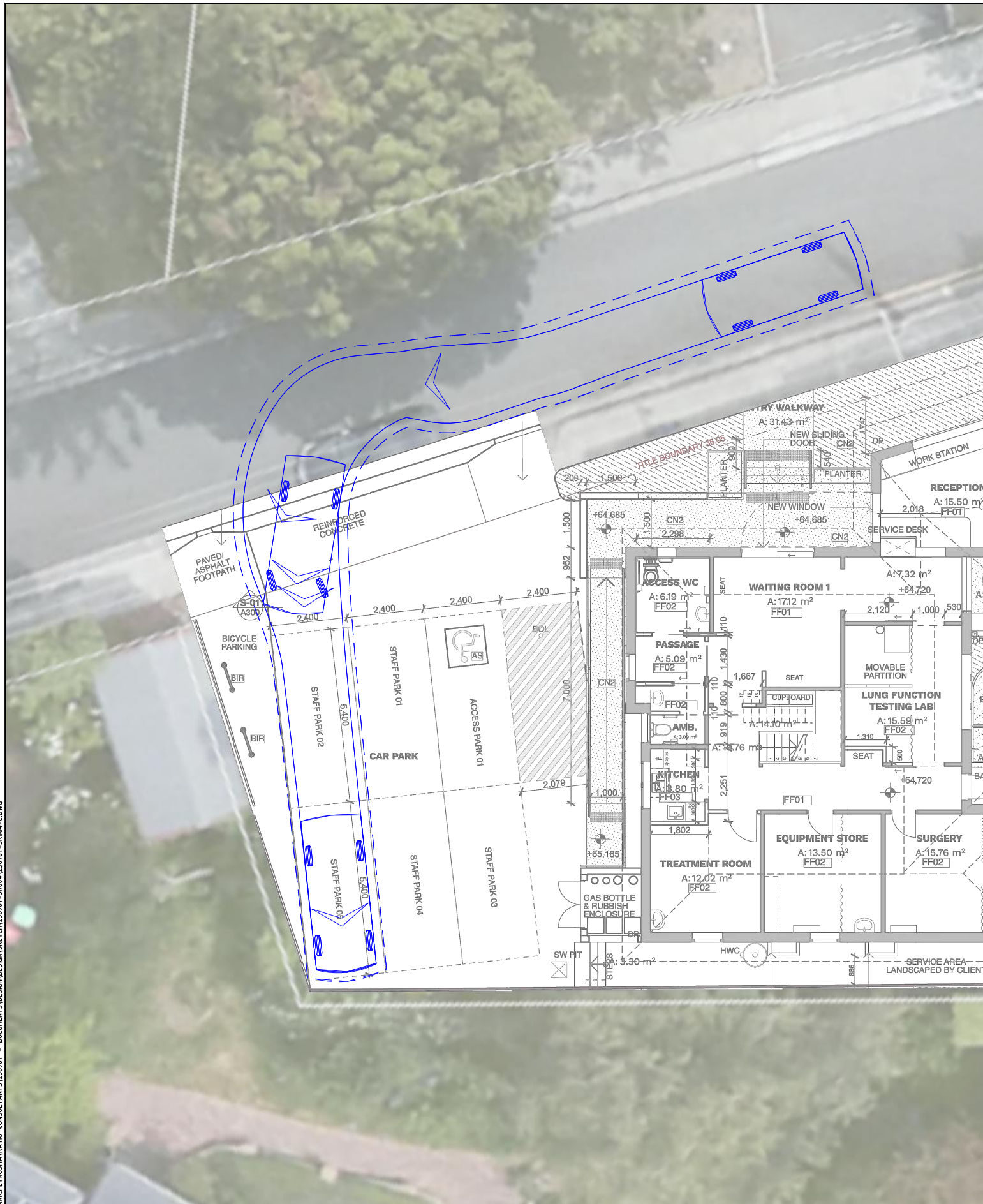
REVISION

**A105- DA 04**

GROUND FLOOR PLAN  
1:100

# Appendix B Swept Path Assessment

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**ratio:**

RATIO CONSULTANTS PTY LTD  
 ABN 005 422 104  
 LEVEL 5, 65 DOVER STREET  
 CREMORNE, VICTORIA 3121  
 TELEPHONE (03)9429 3111  
 FACSIMILE (03)9429 3011

**B85 Vehicle (AS/NZS2890.1:2004)**

VEHICLE ENVELOPE (FORWARD)  
 300mm CLEARANCE (FORWARD)  
 VEHICLE ENVELOPE (REVERSE)  
 300mm CLEARANCE (REVERSE)

Overall Length 4.910m  
 Overall Width 1.870m  
 Overall Body Height 1.421m  
 Min Body Ground Clearance 0.159m  
 Track Width 1.770m  
 Lock to Lock Time 4.00 sec  
 Curb to Curb Turning Radius 5.80m

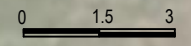
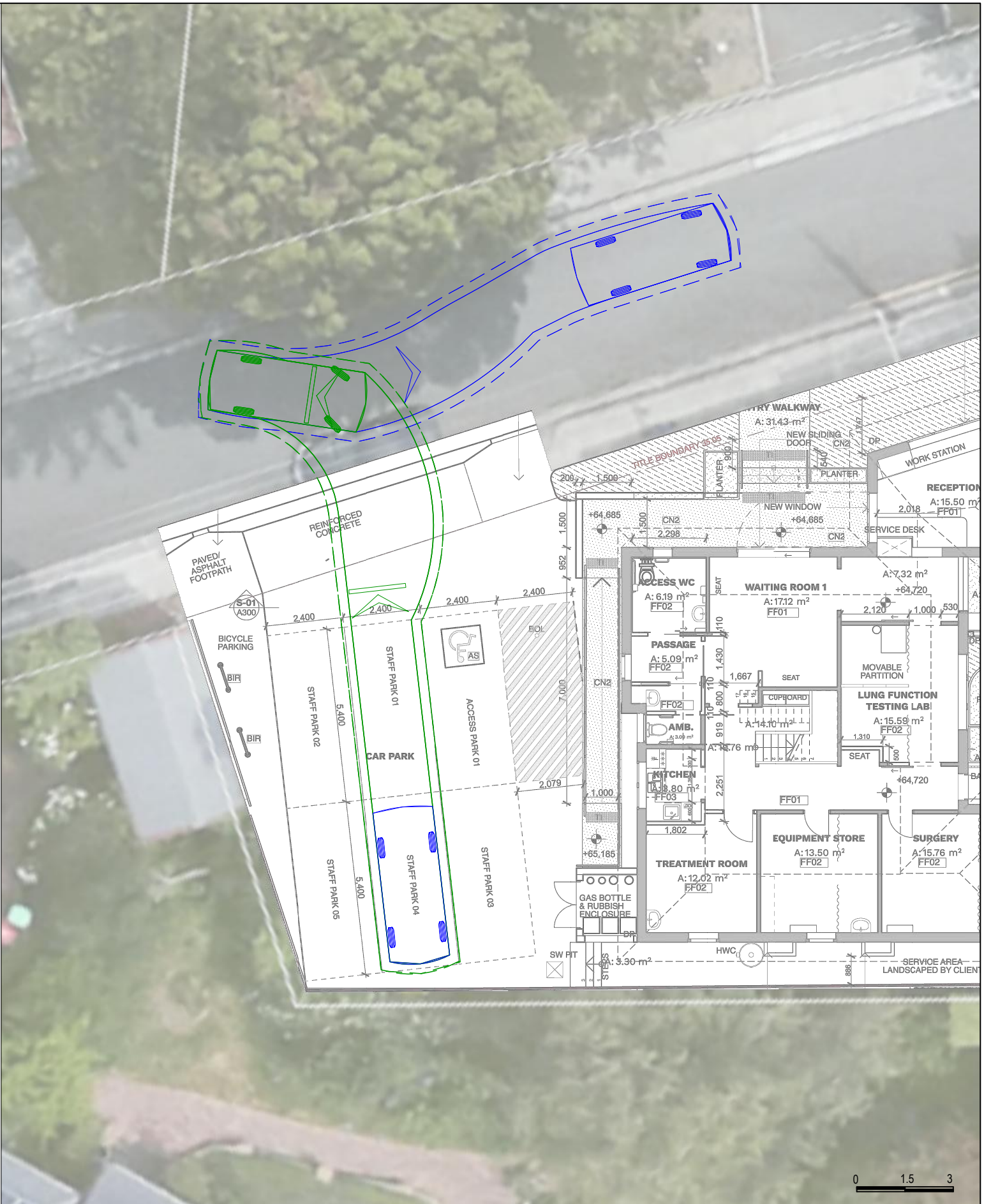
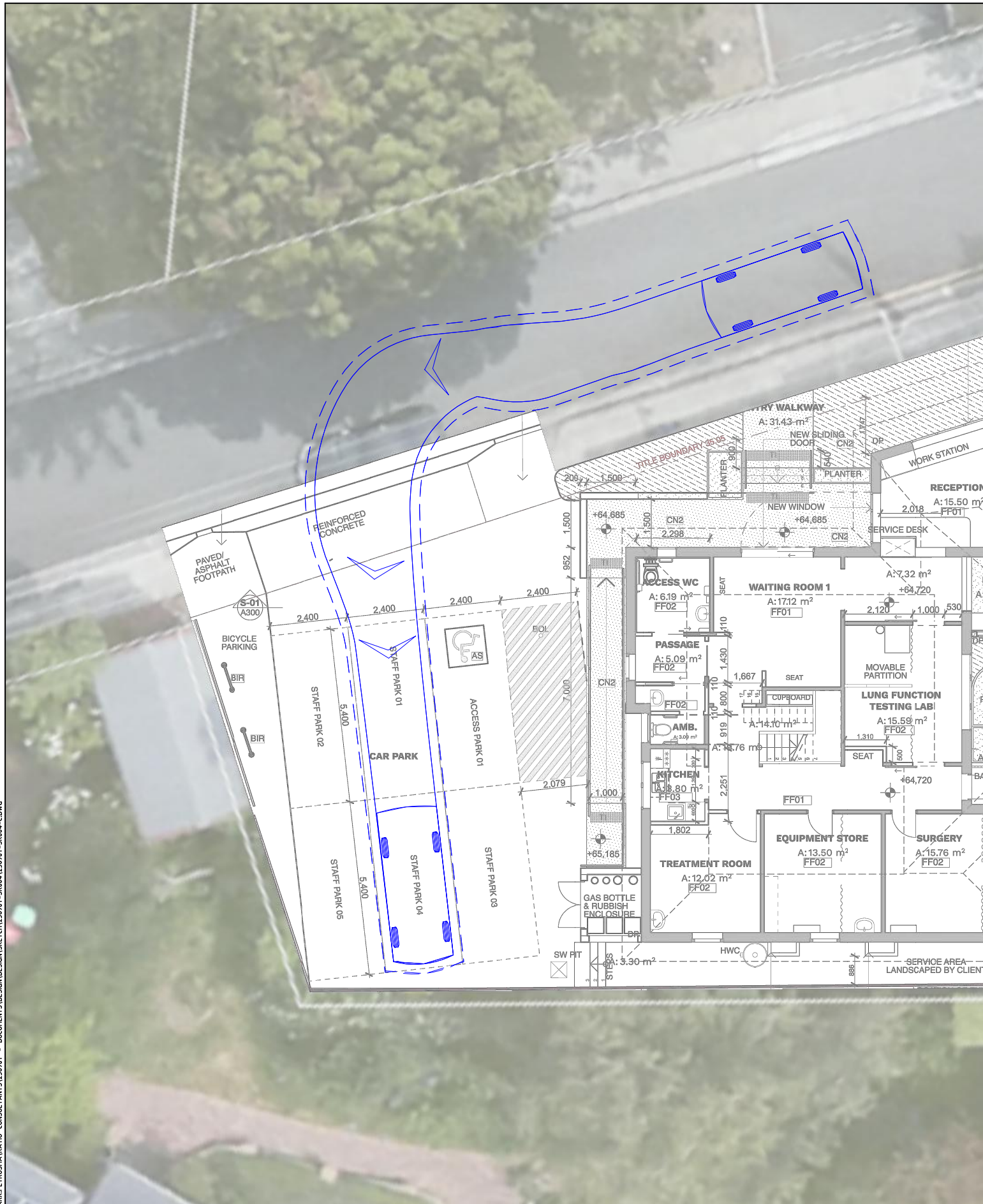
**TASMANIAN LUNG CENTRE**  
**16 CLARE STREET, NEW TOWN**  
**SWEPT PATH ASSESSMENT**

NOTE:  
 1) Base Plan Supplied By Cumulus  
 2) Maximum Design Speed 5km/h

RATIO REFERENCE 23690T-SK004-C	SHEET No. 1 of 4	PREPARED BY M.P.	SCALE 1:150 @ A3	DATE 17/03/2026
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**ratio:**

RATIO CONSULTANTS PTY LTD  
 ABN 005 422 104  
 LEVEL 5, 65 DOVER STREET  
 CREMORNE, VICTORIA 3121  
 TELEPHONE (03)9429 3111  
 FACSIMILE (03)9429 3011

**B85 Vehicle (AS/NZS2890.1:2004)**

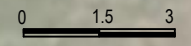
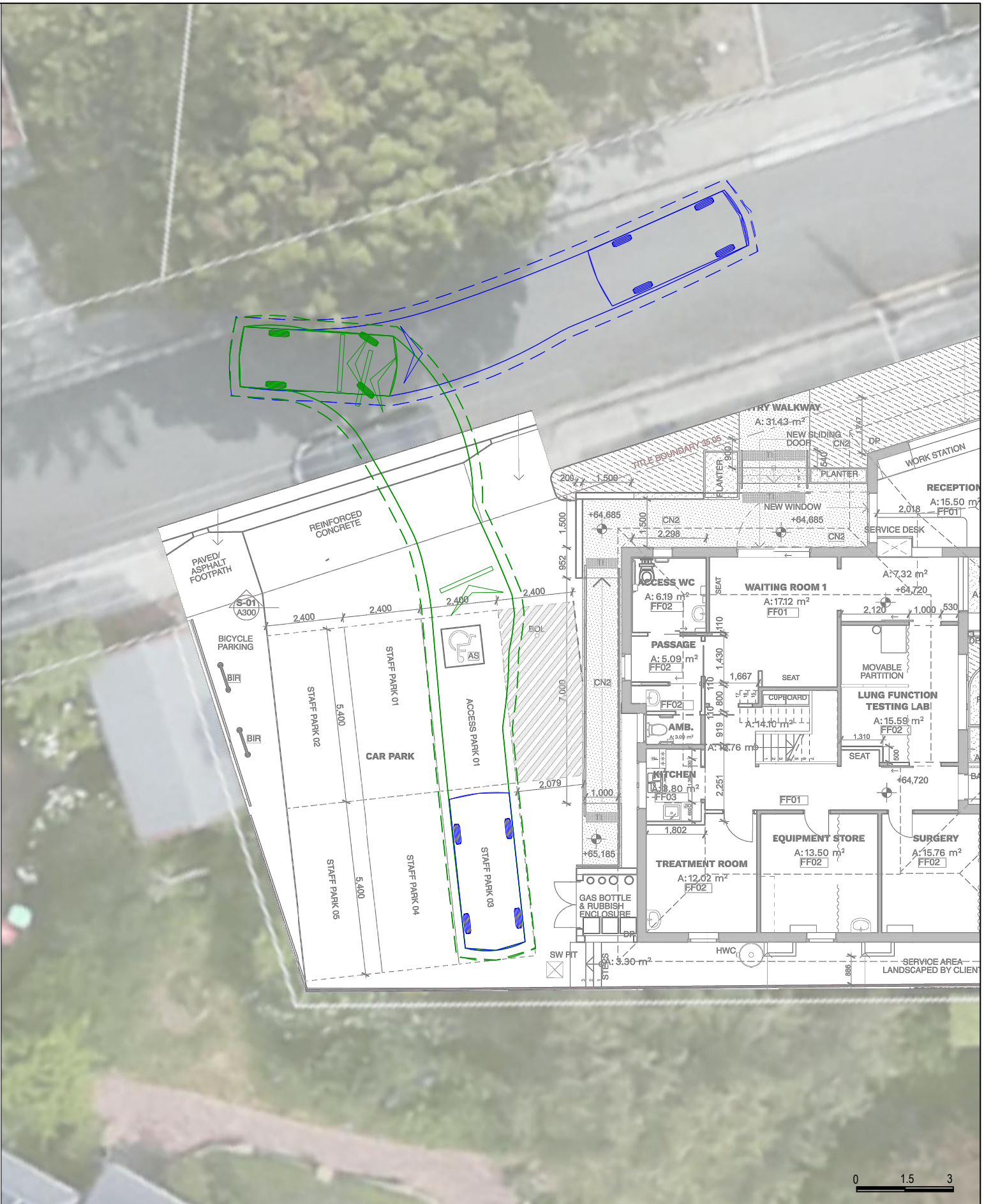
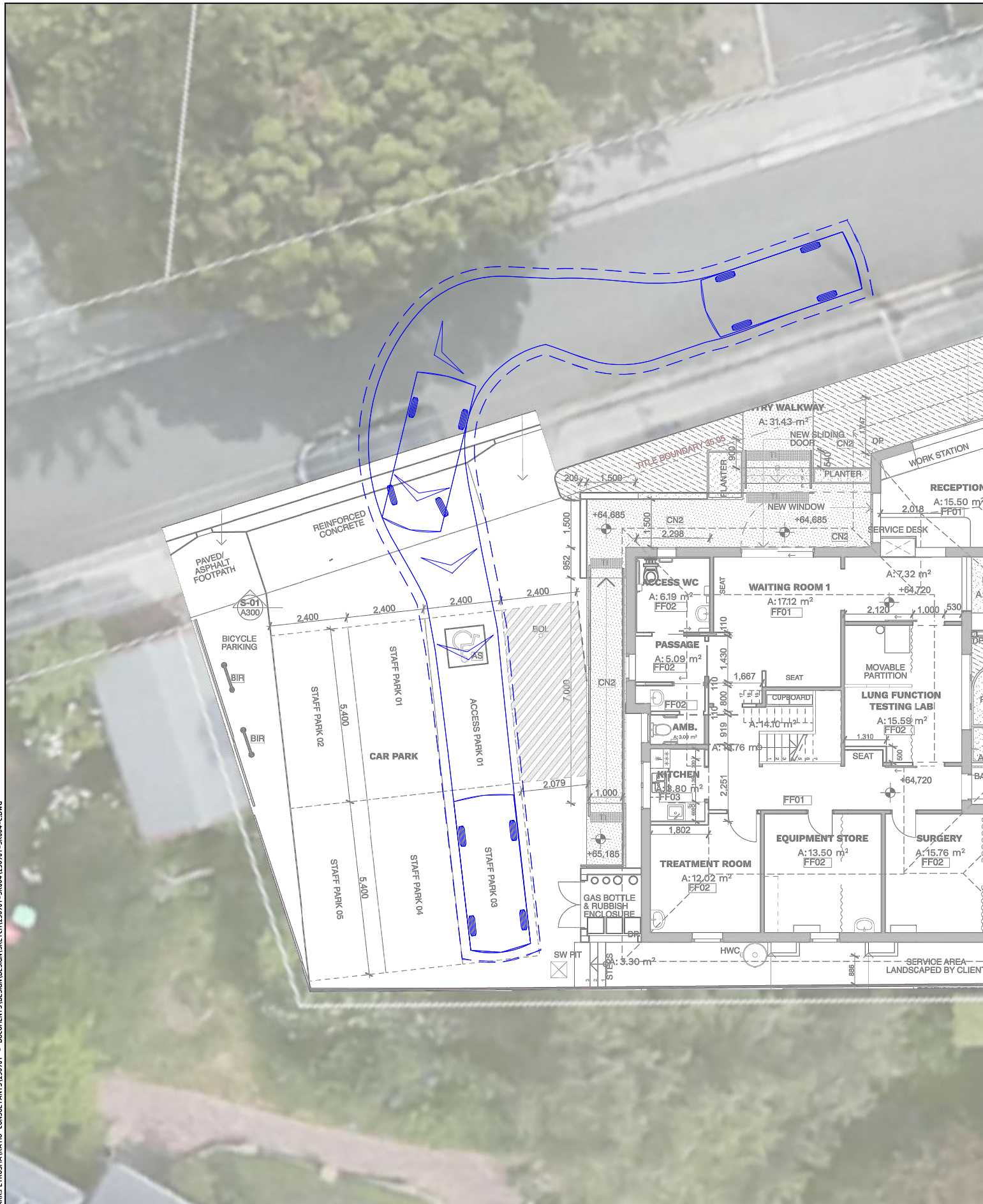
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 300mm CLEARANCE (FORWARD)  
 VEHICLE ENVELOPE (REVERSE)  
 300mm CLEARANCE (REVERSE)

**TASMANIAN LUNG CENTRE**  
**16 CLARE STREET, NEW TOWN**  
**SWEPT PATH ASSESSMENT**

NOTE:  
 1) Base Plan Supplied By Cumulus  
 2) Maximum Design Speed 5km/h

RATIO REFERENCE 23690T-SK004-C	SHEET No. 2 of 4	PREPARED BY M.P.	SCALE 1:150 @ A3	DATE 17/03/2026
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**ratio:**  
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 ABN 005 422 104  
 LEVEL 5, 65 DOVER STREET  
 CREMORNE, VICTORIA 3121  
 TELEPHONE (03)9429 3111  
 FACSIMILE (03)9429 3011

**B85 Vehicle (AS/NZS2890.1:2004)**

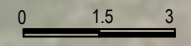
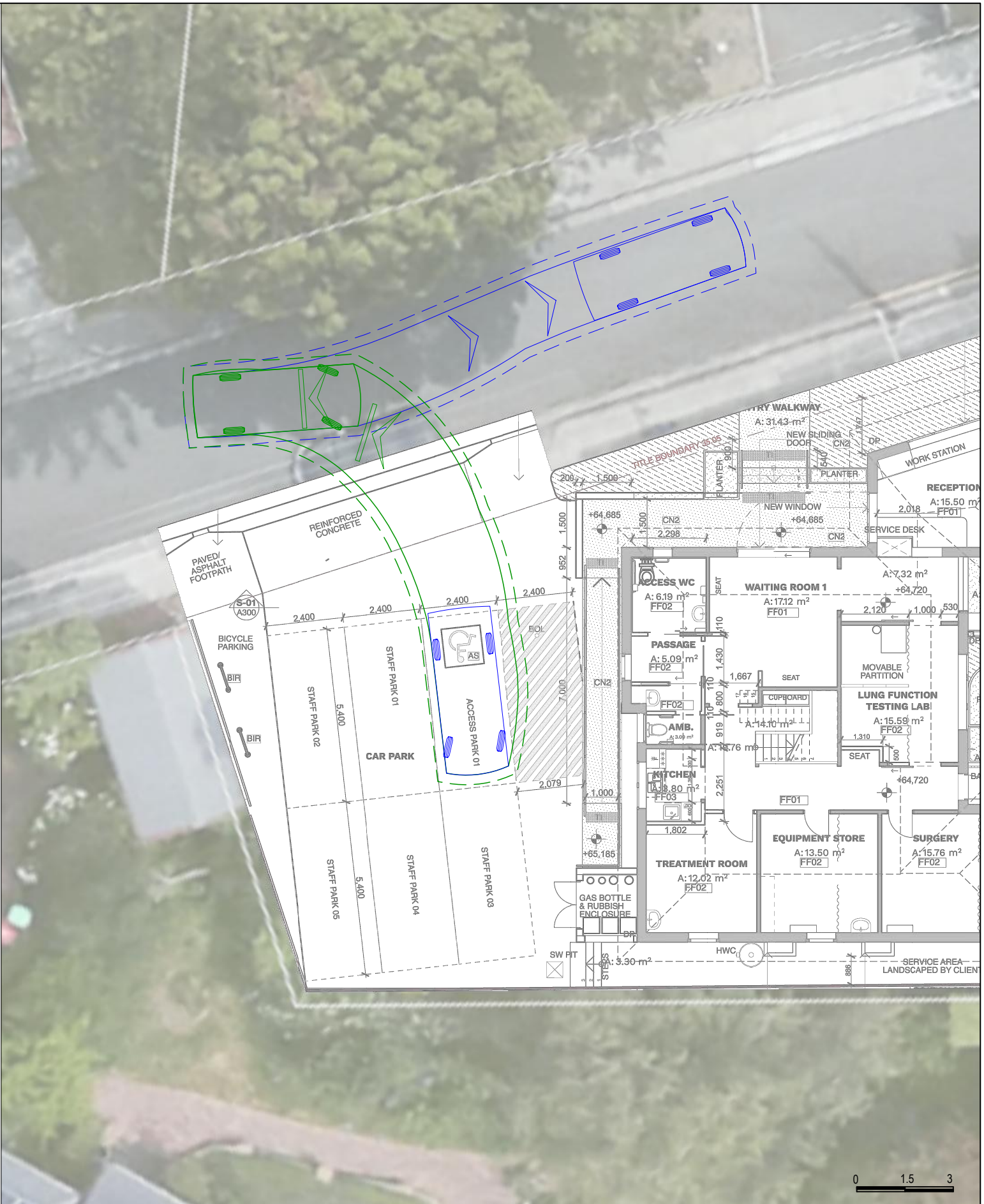
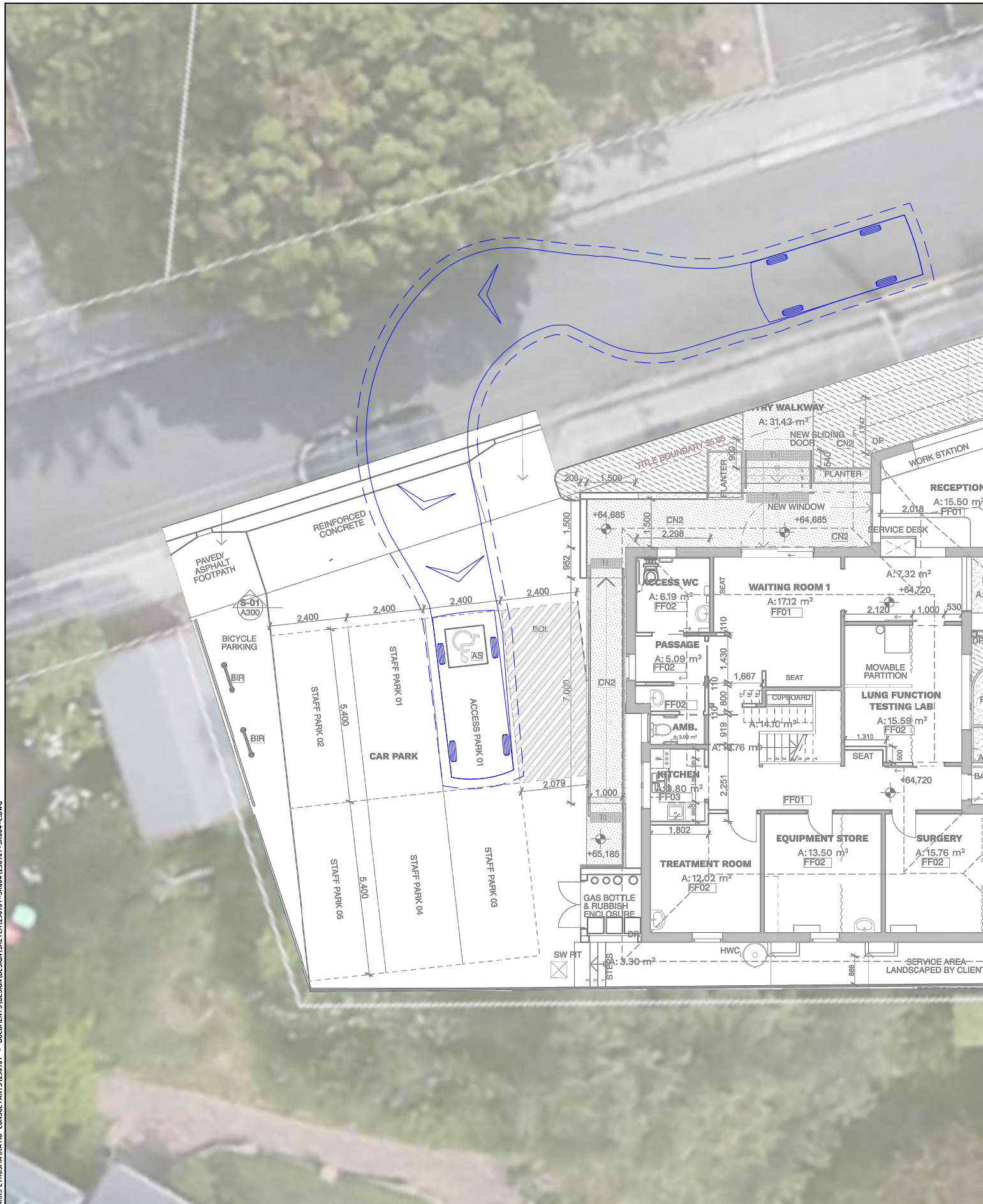
	<b>VEHICLE ENVELOPE (FORWARD)</b>
	300mm CLEARANCE (FORWARD)
	<b>VEHICLE ENVELOPE (REVERSE)</b>
	300mm CLEARANCE (REVERSE)
Overall Length	4.910m
Overall Width	1.870m
Overall Body Height	1.421m
Min Body Ground Clearance	0.159m
Track Width	1.770m
Lock to Lock Time	4.00 sec
Curb to Curb Turning Radius	5.80m

**TASMANIAN LUNG CENTRE**  
**16 CLARE STREET, NEW TOWN**  
**SWEPT PATH ASSESSMENT**

NOTE:  
 1) Base Plan Supplied By Cumulus  
 2) Maximum Design Speed 5km/h

RATIO REFERENCE 23690T-SK004-C	SHEET No. 3 of 4	PREPARED BY M.P.	SCALE 1:150 @ A3	DATE 17/03/2026
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**ratio:**

RATIO CONSULTANTS PTY LTD  
 ABN 005 422 104  
 LEVEL 5, 65 DOVER STREET  
 CREMORNE, VICTORIA 3121  
 TELEPHONE (03)9429 3111  
 FACSIMILE (03)9429 3011

**B99 Vehicle (AS/NZS2890.1:2004)**

Overall Length	5.200m
Overall Width	1.940m
Overall Body Height	2.200m
Min Body Ground Clearance	0.312m
Track Width	1.840m
Lock to Lock Time	4.00 sec
Curb to Curb Turning Radius	6.30m

VEHICLE ENVELOPE (FORWARD)  
 300mm CLEARANCE (FORWARD)  
 VEHICLE ENVELOPE (REVERSE)  
 300mm CLEARANCE (REVERSE)

**TASMANIAN LUNG CENTRE**  
**16 CLARE STREET, NEW TOWN**  
**SWEPT PATH ASSESSMENT**

NOTE:  
 1) Base Plan Supplied By Cumulus  
 2) Maximum Design Speed 5km/h

RATIO REFERENCE 23690T-SK004-C	SHEET No. 4 of 4	PREPARED BY M.P.	SCALE 1:150@A3	DATE 17/03/2026
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# NALLY PLUMBING AND GAS

## TAX INVOICE

Gumbo Trust	<b>Site</b> 16 Clare Street, New Town	<b>Invoice Date</b> 02 February 2026	Nally Plumbing & Gas PO Box 4 Blackmans Bay TAS 7052
	<b>Job No</b> JB21539	<b>Due Date</b> 16 February 2026	Ph: 0400 134 260 E: admin@npgtas.com.au Lic: 145 2861
	<b>Job Address</b> 16 Clare Street, New Town, Tasmania, 7008, Australia	<b>Invoice No</b> INV-21467	
	<b>Reference</b> Drains	<b>ABN</b> 35 634 401 648	

Please attend site to carry out Storm Water investigations as requested.

Description	Quantity	Unit Price	Amount
Attended site to determine the function of the stormwater kerb and gutter connections. - We used our drain cleaning equipment to clear out tree root blockages from the two Clare Street side kerb and gutter connections. Once clear, we viewed both drains with the camera and confirmed both are connecting to the house stormwater system. We also cleaned out the stormwater pit behind the fence closest to Clare Street as it was full of dirt and roots. - No neighbouring properties were found to be entering their stormwater system. - We tried to view the connection next to the driveway on Hildern Street, approximately 100mm into the drain we hit a wall of concrete. Tapping on the concrete echoed through the sign pole next to it. This drain is not in use. - 21 Jan 2026 2:30 hrs Second Plumber. - 21 Jan 2026 2:30 hrs	5	125.00	625.00
Drain cleaning equipment fee.	1	150.00	150.00
Drain camera equipment fee	1	150.00	150.00
Consumables	1	12.50	12.50
Subtotal			937.50
Total GST			93.75
<b>Total AUD</b>			<b>1,031.25</b>

*Thank you for your business.*

**Payment Details:**

National Australia Bank  
Nally Plumbing Pty Ltd  
BSB: 087 007  
Account number: 9017 51848

Please use the above invoice number as your payment reference.

---

**PAYMENT ADVICE**

Nally Plumbing & Gas  
PO Box 4  
Blackmans Bay TAS 7052

Ph: 0400 134 260  
E: admin@npgtas.com.au  
Lic: 145 2861

**Customer**  
**Invoice No**  
**Amount Due**  
**Due Date**  
**Amount Enclosed**

Gumbo Trust  
INV-21467  
**1,031.25**  
16 February 2026

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SIRREE I

RCP

N 1731

76

DD11386

DA11284

None



P-O

P-O

P-O

300MM

300MM

...Town ... 500mm ... Map