

PLANNING APPLICATION

Status:

Reference

PLN-HOB-2025-0610

Address

37 WILLIAM COOPER DR NEW TOWN TAS
7008

Titles

173156/28

Before you start

Before you start your application, you will need to know if you require planning approval or not.

If you are unsure if you require a permit, use the [PlanBuild Tasmania Enquiry Service](#) to lodge a request for advice from the relevant Council.

Once your application has been submitted the Council will review your application. If payment has not been made, you will be sent a request for the payment of application fees via PlanBuild Tasmania.

Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification containing a task to complete.

Pre-Application Advice

Have you spoken with anyone at Council about this application?

Yes - enter details below

No - continue to the next section

If yes, provide the name of the person you contacted

Applicant

Name	Email	Phone	Address	Involvement
Personal Information Removed				

Owners

Name	Email Address	Address
Personal Information Removed		

Certificate(s) of Title

Selected Titles

Total Area: 1535m²

173156/28

Owner Notification

Are you the sole owner of the land?

Yes - continue to the next section

No - answer question below

If no, have you notified all owners, joint or part owners of your intention to submit this application?

Yes - enter owner details below

No - you must notify all owners before proceeding with this application

List all owners, joint or part owners as recorded on the Title documents notified:

Xavier Koumos

Enter the date that the last owner, joint or part owner was notified

23/11/2025

Declaration

I declare that all land owners, joint or part owners have been notified of this planning application.

Crown Land Consent

Is Crown Land involved in the proposed use or development?

Yes - complete question below

No - continue to the next section - see further information below

Unsure

If yes, has written Crown Land consent been obtained?

Yes - upload written consent

No - application will not be progressed until consent has been provided

General Manager Consent

Is Council-owned or administered land involved in the proposed use or development?

Yes - complete question below

No - continue to the next section

Unsure

If yes, has written consent been obtained from the Council General Manager?

Yes - upload written consent

No - application will not be progressed until consent has been provided

Proposed Use or Development

What is the reason for your planning application?

I want to change how the property is used

I want to use the property for visitor accommodation

I want to subdivide

I want to undertake a new development or alteration

I want to do a minor boundary adjustment

I want to put up a sign(s)

I want to demolish

I want to do works only

Other

If your application is to subdivide, please enter the number of proposed lots.

0

If your application is for signage, please enter the number of signs.

0

Is the property a Tasmanian Heritage Listed Property?

Yes

No

Is the application for an EPA Activity under the Environmental Management and Pollution Control Act 1994?

Yes

No

Unsure

Is the proposed use or development permitted or discretionary?

Permitted

Discretionary

Unsure if permitted or discretionary

Provide a full description of the proposed use or development

Proposed 4 dwellings to existing vacant allotment

Will the proposed use or development involve a road reserve?

Yes - complete the section below

No - continue to the next section

Unsure

If yes, enter the address(es) or locations below:

37 William Cooper Drive

If yes, how will the road reserve be affected?

The driveway in the road reserve area will be widened either side in 2 very small patches of driveway totaling 2 sq.m.. The roll-over kerb and footpath do not require any changes.

This should not require crown consent or GM consent given the very small areas.

Value of Works

What is the estimated value of the works?

950000

Supporting Documents

Version	Document Date	Document Type	Description	Prepared By
1	23 Nov 2025	Property Title Document	TitleDocuments-37WilliamCooperDr-231125.pdf	Mr Kevin Roberts
1	23 Nov 2025	Architectural Plans	Architectural Plans	Mr Kevin Roberts
1	23 Nov 2025	Planning Assessment Report	Planning Submission	Mr Kevin Roberts
1	23 Nov 2025	Accessibility Report	Traffic Investigation Assessment	Mr Kevin Roberts
1	23 Nov 2025	Plans	Land Surveyor drawing	Mr Kevin Roberts
1	23 Nov 2025	Plans	Land Survey Plan 2	Mr Kevin Roberts

Next steps

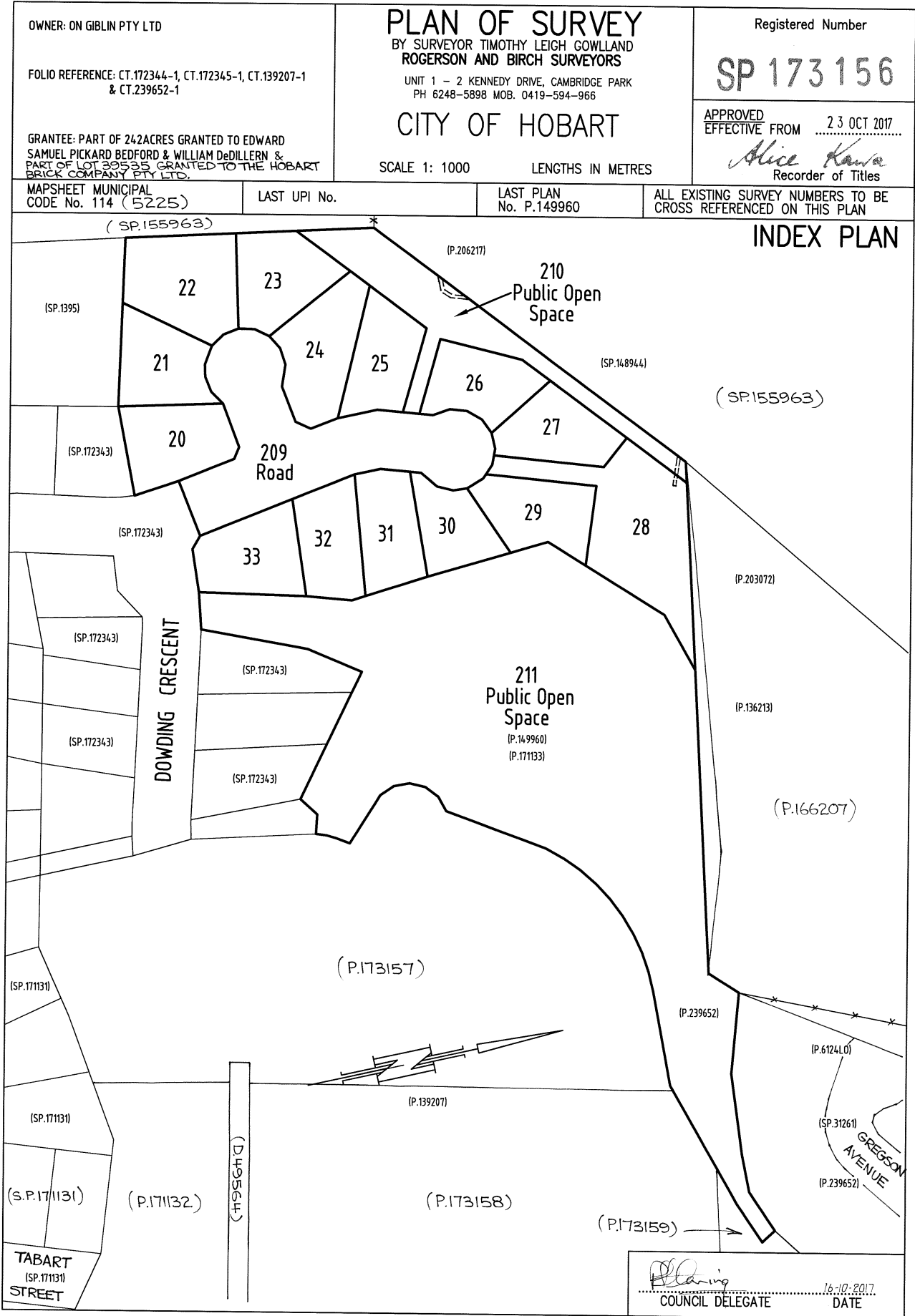
When you have completed all the necessary fields and attached all required documents to support your application, click on the green 'Save & Submit' button at the top right of this form.

Once submitted, the Council will review your application. A request for the payment of application fees will be sent to you via PlanBuild Tasmania.

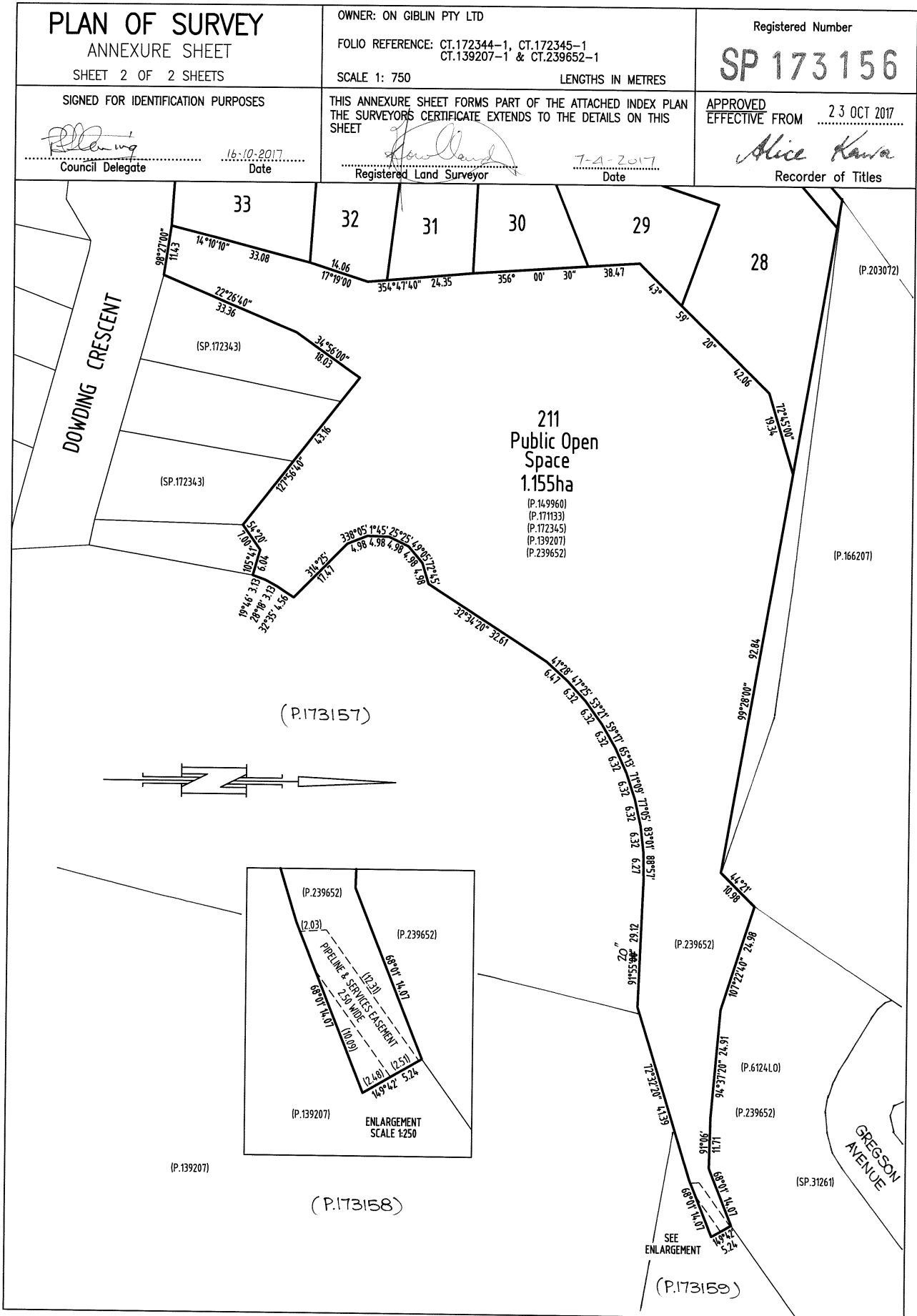
Once the fees have been paid and the Council is satisfied with the information provided, the application will be assessed and you will be notified of the outcome.

If further action is required to assess your application you will receive an email notification from PlanBuild which will tell you what you need to provide to continue the application.

Form published: 14/05/2025 15:58



Alaning
COUNCIL DELEGATE 16.10.2017
DATE



SEARCH OF TORRENS TITLE

VOLUME 173156	FOLIO 28
EDITION 7	DATE OF ISSUE 21-Oct-2025

SEARCH DATE : 23-Nov-2025

SEARCH TIME : 01.51 PM

DESCRIPTION OF LAND

City of HOBART

Lot 28 on Sealed Plan [173156](#)

Derivation : Part of 242 Acres Gtd. to Edward Samuel Pickard
Bedford & William DeGillern

Prior CT [172344/1](#)

SCHEDULE 1

[N281425](#) TRANSFER to XAVIER JON KOUMOS Registered
21-Oct-2025 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

[SP173156](#) EASEMENTS in Schedule of Easements

[SP173156](#) COVENANTS in Schedule of Easements

[SP173156](#) FENCING PROVISION in Schedule of Easements

[E105547](#) BURDENING EASEMENT: a drainage easement in favour of
Hobart City Council over the land marked Drainage
Easement 'B' 3.00 wide on Sealed Plan [173156](#)
Registered 23-Oct-2017 at 12.03 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 173156

PAGE 1 OF 7 PAGE/S

EASEMENTS AND PROFITS

Each lot on the plan is together with:

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

EASEMENTS



Lots 22, 28 and 211 on the Plan are subject to a Pipeline and Services Easement (as defined) in gross in favour of the Tasmanian Water & Sewerage Corporation Pty Ltd over the land marked PIPELINE AND SERVICES EASEMENT 2.50 WIDE on the Plan.

Lots 20, 21, 22, 29, 30, 31, 32 and 33 on the Plan are subject to a Pipeline and Services Easement (as defined) in gross in favour of the Tasmanian Water & Sewerage Corporation Pty Ltd over the land marked PIPELINE AND SERVICES & DRAINAGE EASEMENT 3.00 WIDE on the Plan.

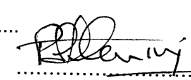
Lots 20, 21, 22, 29, 30, 31, 32 and 33 on the Plan are subject to a Drainage Easement (as defined) in gross in favour of the Hobart City Council over the land marked PIPELINE AND SERVICES & DRAINAGE EASEMENT 3.00 WIDE on the Plan.

Lots 28 and 210 on the Plan are subject to an Electricity Infrastructure Easement (as defined in Transfer of Easement C494182) in gross in favour of Aurora Energy Pty Ltd over the land marked ELECTRICITY INFRASTRUCTURE EASEMENT (C.494182) on the Plan.

Lots 22, 23 and 210 on the Plan are subject to a right for Walter Ashton Jones his heirs and assigns to lay down drain pipes over the land marked DRAINAGE EASEMENT 0.61 WIDE on the Plan subject to a right

Director  Director/Secretary 

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: ON GIBLIN PTY LTD FOLIO REF: 172344-1, 172345-1, 139207-1, 239652-1 SOLICITOR & REFERENCE: PAGE SEAGER (DAS 151616)	PLAN SEALED BY: HOBART CITY COUNCIL DATE: 16-10-2017  REF NO. 521-11 & STR-17-22 Council Delegate
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 173156</p>
<p>SUBDIVIDER: ON GIBLIN PTY LTD</p> <p>FOLIO REFERENCE: 172344-1, 172345-1, 139207-1, 239652-1</p>	

of use for the said land ^{within}~~with~~ described to have a common use of the said drain pipes and also paying one half of the expenses incurred in laying down and cleansing inspecting or mending the said drain pipes as more fully set out in Conveyance 15/2106.

Lots 20 to 33 inclusive, 209 and 210 on the Plan and that part of Lot 211 on the Plan that was formerly comprised within Folio of the Register Volume 172344 Folio 1 are together with a right of carriageway over the land marked RIGHT OF WAY_A (SP.148944) on the Plan.

(PRIVATE)

FENCING PROVISION

In respect of the Lots shown on the Plan, the Vendor (On Giblin Pty Ltd (ACN 602 471 518)) shall not be required to fence.

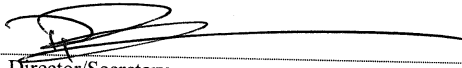
~~That part of lot 211 on the Plan that was formerly comprised within Folio of the Register Volume 172345 Folio 1 is subject to a fencing condition that the vendor ((The Hobart Brick Company Pty. Ltd.) shall not be required to fence.~~

COVENANTS

The owners of each lot on the Plan (excluding Lots 209, 210 and 211 on the Plan) covenant with the Vendor (On Giblin Pty Ltd (ACN 602 471 518)) and the owners for the time being of every other lot shown on the Plan to the intent that the burden of these covenants may run with and bind the covenantors' lot and every part thereof and that the benefit thereof may be annexed to and devolve with each and every part of every other lot shown on the Plan to observe the following stipulations:-

1. Not to park or permit to be parked any caravan or similar vehicle on such lot for the use as permanent or semi-permanent residential accommodation.
2. Not to erect or permit to be erected on such lot or any part thereof any kit home, removable or relocated dwellings or buildings of whatsoever nature.

Director 


Director/Secretary

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 173 156</p>
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- The Vendor reserves the right to sell any part of the unsold land shown on the said survey plan freed and exempt from any one more of the restrictive covenants and conditions or to waive or alter any of such restrictive covenants as to any land not transferred.

The owners of lots 28 to 33 inclusive on the Plan covenant with the Vendor (On Giblin Pty Ltd (ACN 602 471 518)) and the owners for the time being of every other lot shown on the Plan to the intent that the burden of these covenants may run with and bind the covenantors' lot and every part thereof and that the benefit thereof may be annexed to and devolve with each and every part of every other lot shown on the Plan to observe the following stipulations:-

- Not to erect any building on such lot within the area shown as ABCDEFGHIJKLMA on the Plan.

DEFINITIONS

“Drainage Easement” means a right of drainage (including the right of construction of drains) for Hobart City Council with which the right shall be capable of enjoyment for the purpose of carrying away stormwater and other surplus water from any land over or under the land herein indicated as the land over which the right is to subsist, and through all sewers and drains which may hereafter be made or passing under, through, and along the last-mentioned land and the right for Hobart City Council and its employees, agents and contractors from time to time and at all times hereafter if it or they should think fit to enter into and upon the last-mentioned land and to inspect, repair, cleanse, and amend any such sewer or drain without doing unnecessary damage to the said land.

“Pipeline and Services Easement” means:


THE FULL RIGHT AND LIBERTY for TasWater at all times to:

- enter and remain upon the Easement Land with or without employees, contractors, agents and all other persons duly authorised by it and with or without machinery, vehicles, plant and equipment;

Director



Director/Secretary



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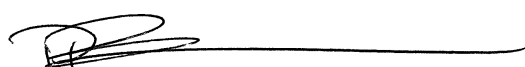
<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 4 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 173 156</p>
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- (2) investigate, take soil, rock and other samples, survey, open and break up and excavate the Easement Land for any purpose or activity that TasWater is authorised to do or undertake;
- (3) install, retain, operate, modify, relocate, maintain, inspect, cleanse and repair the Infrastructure;
- (4) remove and replace the Infrastructure;
- (5) run and pass sewage, water and electricity through and along the Infrastructure;
- (6) do all works reasonably required in connection with such activities or as may be authorised or required by any law:
 - (1) without doing unnecessary damage to the Easement Land; and
 - (2) leaving the Easement Land in a clean and tidy condition; and
- (7) if the Easement Land is not directly accessible from a highway, then for the purpose of undertaking any of the preceding activities TasWater may with or without employees, contractors, agents and all other persons authorised by it, and with or without machinery, vehicles, plant and equipment enter the Lot from the highway at any then existing vehicle entry and cross the Lot to the Easement Land; and
- (8) use the Easement Land as a right of carriageway for the purpose of undertaking any of the preceding purposes on other land, TasWater reinstating any damage that it causes in doing so to any boundary fence of the Lot.

PROVIDED ALWAYS THAT:

- (1) The registered proprietors of the Lot in the folio of the Register (“the Owner”) must not without the written consent of TasWater first had and obtained and only in compliance with any conditions which form the consent:
 - (a) alter, excavate, plough, drill or otherwise penetrate the ground level of the Easement Land;
 - (b) install, erect or plant any building, structure, fence, pit, well, footing, pipeline, paving, tree, shrub or other object on or in the Easement Land;
 - (c) remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land;

Director 


Director/Secretary

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- (d) do any thing which will or might damage or contribute to damage to any of the Infrastructure on or in the Easement Land;
 - (e) in any way prevent or interfere with the proper exercise and benefit of the Easement Land by TasWater or its employees, contractors, agents and all other persons duly authorised by it; or
 - (f) permit or allow any action which the Owner must not do or acquiesce in that action.
- (2) TasWater is not required to fence any part of the Easement Land.
- (3) The Owner may erect a fence across the Easement Land at the boundaries of the Lot.
- (4) The Owner may erect a gate across any part of the Easement Land subject to these conditions:
- (a) the Owner must provide TasWater with a key to any lock which would prevent the opening of the gate; and
 - (b) if the Owner does not provide TasWater with that key or the key provided does not fit the lock, TasWater may cut the lock from the gate.
- (5) If the Owner causes damage to any of the Infrastructure, the Owner is liable for the actual cost to TasWater of the repair of the Infrastructure damaged.
- (6) If the Owner fails to comply with any of the preceding conditions, without forfeiting any right of action, damages or otherwise against the Owner, TasWater may:
- (a) reinstate the ground level of the Easement Land; or
 - (b) remove from the Easement Land any building, structure, pit, well, footing, pipeline, paving, tree, shrub or other object; or
 - (c) replace any thing that supported, protected or covered the Infrastructure.

In this definition of Pipeline and Services Easement:

“**Easement Land**” means the land which is subject to this easement.

“**Infrastructure**” means infrastructure owned or for which TasWater is responsible and includes but is not limited to:

- (a) sewer pipes and water pipes and associated valves;
- (b) telemetry and monitoring devices;

Director



Director/Secretary



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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 6 OF 7 PAGES</p>	<p>Registered Number</p> <p>SP 173 156</p>
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- (c) inspection and access pits;
- (d) power poles and lines, electrical wires, electrical cables and other conducting media (excluding telemetry and monitoring devices);
- (e) markers or signs indicating the location of the Easement Land, the Infrastructure or any warnings or restrictions with respect to the Easement Land or the Infrastructure;
- (f) any thing reasonably required to support, protect or cover any of the Infrastructure;
- (g) any other infrastructure whether of a similar nature or not to the preceding which is reasonably required for the piping of sewage or water, or the running of electricity, through the Easement Land or monitoring or managing that activity; and
- (h) where the context permits, any part of the Infrastructure.

“**Owner**” means the registered proprietors of the Lot in the folio of the Register from time to time.

“**TasWater**” means Tasmanian Water & Sewerage Corporation Pty Ltd.

Director



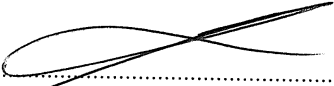
Director/Secretary



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ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 7 OF 7 PAGES	Registered Number SP 173156
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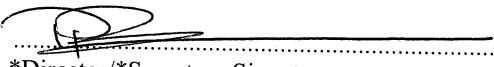
EXECUTED by ON GIBLIN PTY LTD (ACN 602 471)
 518), as registered proprietor of the land comprised in)
 Folio of the Register Volume 172344 Folio 1, Volume)
 172345 Folio 1, Volume 139207 Folio 1 and Volume)
 239652 Folio 1 pursuant to section 127 of the Corporations)
 Act 2001 by:



 Director Signature

RONALD AULBEN BROOKS

 Director Full Name (print)




 *Director/*Secretary Signature

PAUL ROBERT BROOKS

 *Director/*Secretary Full Name (print)

(*please strike out inapplicable *if Sole Director/Secretary write 'Sole')

Director 
 Director/Secretary

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Planning Submission

Proposed 4 Dwellings

at

37 William Cooper Drive
New Town
Tasmania

for

MK Constructions Pty. Ltd.

Tas Building Design Pty Ltd
P.O.Box 2018
Howrah 7018
e. tas.buildingdesign@bigpond.com
p. 0408 882 283

November 2025.

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 - b.) Dwelling density/plot ratio
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 - e.) Private open spaces
 - f.) Colour scheme/finishes
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 - h.) Lighting
 - i.) Landscaping
 - j.) Waste Management/Waste Collections
5. Impact to neighbouring sites
6. Site Servicing
7. Access and Parking
8. Conclusion

attachments

Tas Building Design Pty Ltd

sk01 – Site Plan
sk02 - Site Drainage Plan
sk03 - Site Landscape Plan
sk04 – Site Turning Plan
sk05 – Soil & Water Management Plan
sk09-Sk12 – Floor Plans
sk13–sk16 Elevations

Land Survey – Rogerson and Birch Land Surveyors
Certificate of Title documents
TIA – Hubble Traffic

1. - Introduction

The 37 William Cooper Drive, New Town property is zoned General Residential under the Tas Planning Scheme. The proposed development submitted is for the proposed four new strata title dwellings with three double storey dwellings having 3 bedrooms, 2 bathrooms with double lock up garages and one single level 2 bedroom and 2 bathroom dwelling.

2. – Site Location

The proposed four dwelling development site address is located at 37 William Cooper Drive, New Town on one title. The development is located at the end/north side of William Cooper Drive . The land gently falls to the east aspect and is within a new homes area. The lot abuts existing residential homes and HCC reserve.

3.- Design statement

The proposed development is designed for a strata title development and is aimed to provide infill development with general low maintenance dwellings. The dwellings are located within a new homes area and are a short drive to all local services.

Careful design allows for comfortable access to the new dwelling for pedestrians and vehicles. The existing access driveway is located on the southern side of the property to an internal allotment. The dwellings are placed with a centre driveway stepping down the site.

Both Proposed Dwellings have a mix of external finishes with generally first floor colorbond vertical nail-strip cladding to be in a darker monument/night sky colour with barestone or similar cladding below. The entry areas will have a small area of timber cladding. All dwellings have low level colorbond skillion roofs.

Living spaces are located to the north for all dwellings with access to the north facing courtyards. Placement of windows and planting will be used to give privacy and hide overlooking of open spaces to each dwelling and the surrounding properties.

4. – Proposed Development Facts

a.) Strata Title outline

The property at 37 William Cooper Drive, New Town consists of one title (CT. 173156/28). If approved, the development would be built as a strata title development.

b.) Dwelling density/plot ratio

The General Residential Zone allows for multiple residential with a minimum of 325 sq.m. per dwelling site. The site area is 1535 sq.m. The plot ratio allows for a maximum 4 dwellings for the site under the General Residential zone requirements. We are applying for 4 dwellings for this application which calculates out to 383.75 sq.m. per dwelling site. Without the driveway land area of 200 sq.m., the size is 333.75 sq.m. p/dwelling site.

c.) Height

The maximum height for this development is Proposed Dwelling 3 at 6640mm.

d.) Setbacks to boundaries/infrastructure

All dwellings comply to the building envelope for side and rear boundaries. Dwellings 1 & 3 are setback 4.5 metres as per Tas Planning Scheme 8.4.2 A3 (a) + Figure 3 for internal allotments.

The site has been surveyed by both Rogerson and Birch and underground asset services to locate all TasNetworks infrastructure. This is an important setback requirement for the project. Various communications have occurred with TasNetworks and have agreed in principle to the proposed development. We understand that HCC will need to contact TasNetworks via the correct channels to confirm the setbacks agreed.

e.) Private open spaces

Private open spaces are located facing on the north side for all Proposed Dwellings and comply to the Tas Planning Scheme requirements.

f.) Colour scheme/finishes

The proposed development will provide non-reflective colours to blend into the existing landscape and neighbouring properties. Selection of wall colours and colorbond roofing provide the main finishes. Fencing, driveway and landscaping provide a secondary colour scheme adding variety of colour and finishes separation.

g.) Site levels

Generally the site falls slightly to the east and have level garage parking for Dwellings 1,2 & 3 with Dwelling 4 having two external car spaces.

h.) Lighting

Parking and vehicle circulation will have external lighting covering these areas. This lighting will enable safe access for vehicles and pedestrians alike without light transferring into adjoining properties.

i.) Landscaping

Landscaping within the development will be low maintenance including shrubs, plantings, trees, fencing, driveway and courtyards. Refer to Landscape Plan for further information.

j.) Waste Management/Waste Collections

The bin placement/waste collection on collection day will be located in front of the property 33 William Cooper Drive and kept in each private courtyard area as indicated on the drawings. Refer TIA for more descriptive information.

5. – Impact to neighbouring sites

The property is within a new but established homes area. The impact to the existing neighbouring sites is low-impact and the development complies to the planning scheme requirements. The last remaining allotment to the end of the street provides important urban infill to the New Town suburb.

6. – Site servicing

The existing 100mm dia sewer is connected at the lower rear of the site and will service the new dwellings.

There is an existing 20 (ID) water connection shown to the front of the property which is to be upgraded to a 25 (ID) water connection. An existing 150mm dia stormwater main located also of the property.

Detention will be in the form of slow release outlets to stormwater tanks to all four new dwellings and stormstack pits to the driveway. Any detention calculations will be designed at time of the main building and plumbing application. Any RFI requirements for the planning application for final hydraulic design will stall the timeline of the project.

Power and NBN are close to the front boundary.

7. – Access and parking

The proposed development complies to the access and parking requirements for the site. Ten car spaces are being provided – 2 per dwelling plus 2 visitor spaces. Please refer to the attached TIA.

8. – Conclusion

The proposal for the four new strata dwellings will be a quality residential development for the new Town suburb. The location in a newer homes area and the General Residential Zone allows the for this type of important infill development.

The proposed development would be a valuable addition to the Hobart City Council municipality.



TRAFFIC IMPACT ASSESSMENT

Hubble Traffic

NOVEMBER 2025

RESIDENTIAL UNIT DEVELOPMENT 37 WILLIAM COOPER DRIVE, NEW TOWN

Disclaimer: This report has been prepared based on and in reliance upon the information provided to Hubble Traffic Pty Ltd by the client and gathered by Hubble Traffic Pty Ltd during the preparation of the report. Whilst all reasonable skill, care and diligence has been used in preparation of the report, Hubble Traffic Pty Ltd take no responsibility for errors or omissions arising from misstatements by third parties.

This report has been prepared specifically for the exclusive use of the client named in the report and to the extent necessary, Hubble Traffic Pty Ltd disclaim responsibility for any loss or damage occasioned by use of or reliance upon this report, or the date produced herein, by any third party.

Version	Date	Reason for Issue
Draft	November2025	Draft issued for client feedback
Final	November 2025	Final issued

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1. Introduction

Tas Building Design Pty Ltd has engaged Hubble Traffic on behalf of the developer, to prepare an independent Traffic Impact Assessment for a proposed residential unit development at 37 William Cooper Drive, New Town.

This report has considered the amount of traffic the development is likely to generate and how these additional traffic movements will integrate into the surrounding road network. The development has been assessed against the Tasmanian Planning Scheme C2 Parking and Sustainable Transport Code, C3 Road and Railway Assets Code, and the Australian Standard 2890.1:2004 (the Standard).

This report has been prepared to satisfy the requirements of Austroads, Guide to Traffic Management Part 12: Traffic Impacts of Developments, 2019, and referred to the following information and resources:

- Tasmanian Planning Scheme (Hobart)
- Transport for NSW (TfNSW) Guide to Transport Impact Assessment version 1.1
- Australian Standards AS2890 parts 1, 2 and 6
- Austroads series of Traffic Management and Road Design
 - Part 4: Intersection and crossings, General
 - Part 4a: Unsignalised and Signalised Intersections
 - Part 12: Traffic Impacts of Development
- Department of State Growth
- Autoturn Online vehicle turning software
- LIST – Land Information Database

2. Site Description

The development site at 37 William Cooper Drive is an internal lot situated at the end of a cul-de-sac, accessed via an unsealed right-of-way with an existing concrete crossover. This large parcel of land is cleared of vegetation and is located on the side of a hill.

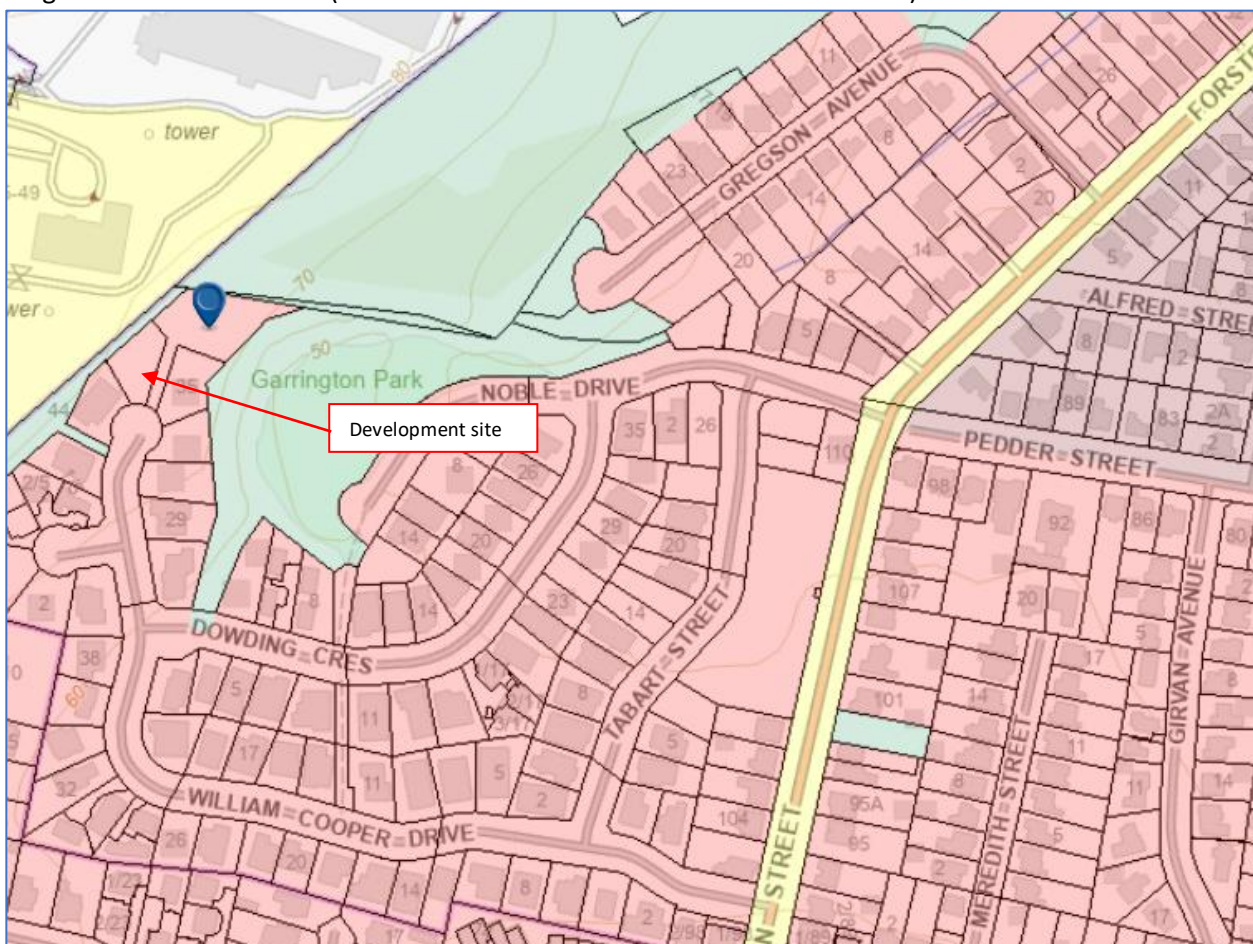
The site is part of an existing residential subdivision comprising approximately 130 residences, with limited vacant lots. Access to the site is available via Williams Cooper Drive and Noble Drive, both of which intersect with Giblin Street.

The surrounding road network consists of sealed urban streets with footpaths, providing direct connectivity to Giblin Street, a local collector road that connects south to Augusta Road and north to Forster Street, which in turn connects to New Town Road.

The site is conveniently located near bus services, New Town Shopping Centre, Augusta Road shops, and local schools.

According to the Land Information System Tasmania (LIST), the site is zoned as general residential.

Diagram 2.0 – Site location (extract from the LIST land information database)



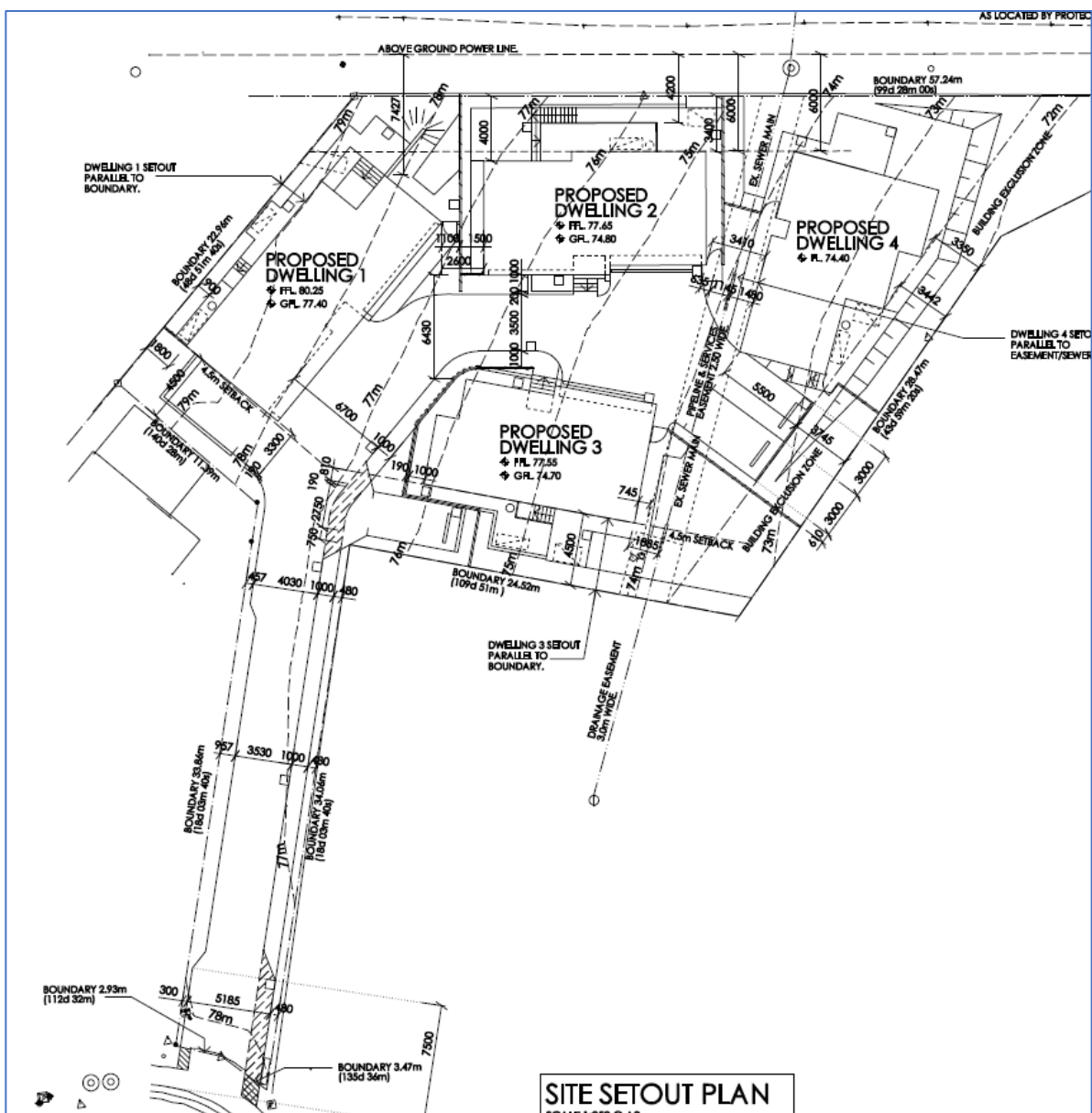
3. Development proposal

The proposal involves the construction of four residential units comprising of:

- three two-storey three bedroom units, each with a double garage,
- one single storey two bedroom unit, with two open parking spaces,
- two visitor parking spaces, located either side of the site entrance.

A concrete driveway will be provided within the right of way and will operate as a single traffic lane with passing bays at either end. The driveway will be designed with appropriate camber and drainage infrastructure, connecting to William Cooper Drive. A dedicated pedestrian pathway will run along one side of the driveway to provide safe and convenient access.

Diagram 3.0 – Proposed development layout



4. Trip generation by this development

A trip in this report is defined as a one-way vehicular movement from one point to another, excluding the return journey, with a return trip to and from a land use counted as two trips.

To determine the likely number of trips generated by the development, observed traffic data has been utilised. Manual traffic surveys were conducted to capture vehicle movements during the weekday AM and PM peak periods. Given that traffic generated by the residential development relies on the two Giblin Street intersections, at William Cooper Drive and Noble Drive, surveys were undertaken at both locations. This approach ensures that all inbound and outbound trips associated with the development were captured, providing a robust and representative basis for estimating trip generation.

Table 4.0 – Number of trips during weekday morning and evening periods

Junction	Morning peak (90 minutes 7:30 to 9am)		Evening peak (90 minutes 4 to 5:30am)	
William Cooper Dr	41	15	24	42
Noble Drive	37	23	22	38
Total	78	38	46	80
Combined in and out	116		126	
Peak hour trips	77		84	

Analysis of the existing residential catchment, comprising 107 dwellings, indicates an average trip generation rate of 0.72 trips per dwelling during the AM peak hour and 0.78 trips per dwelling during the PM peak hour. Applying these rates to the proposed four additional residential units yields an estimated increase of approximately:

- 3 trips during the AM peak hour (4 dwellings × 0.72 trips/dwelling)
- 3 trips during the PM peak hour (4 dwellings × 0.78 trips/dwelling)

Assuming peak hour trips represent approximately 10% of total daily movements (an industry standard assumption consistent with Austroads guidance), the four additional dwellings are expected to generate approximately 30 daily trips.

5. Existing traffic conditions and road network

The development site is accessible via Noble Drive and William Cooper Drive, both of which intersect with Giblin Street. Giblin Street functions as a local collector road, providing connectivity southward to Augusta Road and northward to Forster Street. Forster Street subsequently links to New Town Road, a higher-order route within the regional network. The nearest arterial road is New Town Road, facilitating integration with the broader transport system.

5.1 William Cooper Drive characteristics

William Cooper Drive is a local residential street serving established and new residential developments. It extends approximately 480 metres west from Giblin Street, ending in a cul-de-sac at the proposed access point to the development site.

The road is built to urban standards with a sealed carriageway that accommodates two-way traffic flow, concrete kerb and channel, grass verges, concrete footpaths on both sides, and street lighting.

The residential subdivision is located on undulating terrain, with existing dwellings provided with off-street parking. The road comprises a combination of straight segments and curved alignments, responding to the site's topographical constraints. Vertical grades are present, particularly approaching the cul-de-sac, reflecting the natural slope of the land.

The road intersects Giblin Street at a 90-degree angle, with priority reinforced by a give way sign and holding line. The road has a sealed pavement width of approximately 7.5 metres between kerb faces, with outstands located midway along a relatively straight section of road, operating as traffic calming devices.

The road operates under the urban default 50 km/h speed limit.

Photograph 5.1 – William Cooper Drive construction standard – at junction of Giblin Street



5.2 Noble Drive characteristics

Noble Drive provides an alternative route between the development site and Giblin Street. It is a local residential street with similar urban standards to William Cooper Drive and operates with a default 50 km/h speed limit. This road also intersects Giblin Street at 90 degrees, with priority reinforced by give way signage.

Photograph 5.2 – Junction of Giblin Street and Noble Drive



5.3 Giblin Street characteristics

Giblin Street originates in Lenah Valley and extends approximately 1.3 kilometres before transitioning into Forster Street just beyond its intersection with Noble Drive. The corridor is predominantly residential in character, with direct property access and on-street parking available along much of its length.

Within the surrounding road network, Giblin Street functions as a collector road, facilitating the movement of traffic from adjacent local streets and providing a key connection between Augusta Road and New Town Road. Signalised intersections at Giblin Street/New Town Road and Giblin Street/Augusta Road, support efficient traffic operations and contribute to a satisfactory level of service for all road users.

The centre of the road is delineated by two spaced line markings, which includes a section where trees run down the middle of the road. On the southbound side is a marked bicycle lane that incorporates a marked parking lane.

The road is signposted with the default urban speed limit of 50 km/h.

Photograph 5.3 — Typical cross section of Giblin Street



5.4 Sight distance leaving William Cooper Drive and Noble Drive

Based on the layout of the surrounding local road network, all traffic generated from the development will need to turn at the intersections of William Cooper Drive and Giblin Street or Noble Drive and Giblin Street.

It is important that drivers have appropriate sight distance. Safe Intersection Sight Distance (SISD) is the highest sight distance parameter for an intersection, with the Austroads Guide to Road Design specifying that SISD for a 50 km/h speed environment is 90 metres, based on a driver reaction time of 1.5 seconds and an observation time of three seconds.

Sight distance was assessed with the driver's eye height at 1.1 metres and an approaching vehicle height of 1.2 metres. For both intersections the available sight distance exceeds 90 metres.

Photograph 5.4A – William Cooper Drive, Available sight distance to the Right



Photograph 5.4B – William Cooper Drive, Available sight distance to the left



Photograph 5.4C – Noble Drive, Available sight distance to the right



Photograph 5.4C – Noble Drive, Available sight distance to the left



5.5 Summary of surrounding road network

The surrounding road network is built to appropriate urban standards and can accommodate the modest increase in residential traffic from the proposed development. William Cooper Drive, Noble Drive, and Giblin Street all have suitable carriageway widths and pedestrian infrastructure, appropriate for their respective traffic functions. A route inspection confirmed that all roads are in good condition, with no deficiencies observed that would hinder safe or efficient vehicle movement.

5.6 Traffic activity

To assess the traffic impact associated with the proposed development, it is essential to understand the prevailing traffic conditions on the surrounding road network. Manual traffic surveys were undertaken at the intersection of Giblin Street with William Cooper Drive and Noble Drive, capturing weekday peak periods when traffic volumes are typically at their highest.

Peak hour traffic volumes derived from the surveys are illustrated in the accompanying diagrams.

Survey results confirm that Giblin Street accommodates moderate traffic volumes, with an average of approximately 447 two-way vehicle movements during the AM peak hour and 543 two-way movements during the PM peak hour. These volumes are consistent with the functional role of an urban collector road and indicate that Giblin Street currently operates with a high level of service, providing efficient movement for through and local traffic.

In contrast, William Cooper Drive and Noble Drive are lightly trafficked local streets, with recorded two-way flows of:

- William Cooper Drive: 37 vehicles (AM peak), 44 vehicles (PM peak)
- Noble Drive: 40 vehicles (AM peak), 41 vehicles (PM peak)

These volumes reflect the expected performance of local urban streets, offering a high level of service for residential access.

Diagram 5.6A – Morning Peak hour period

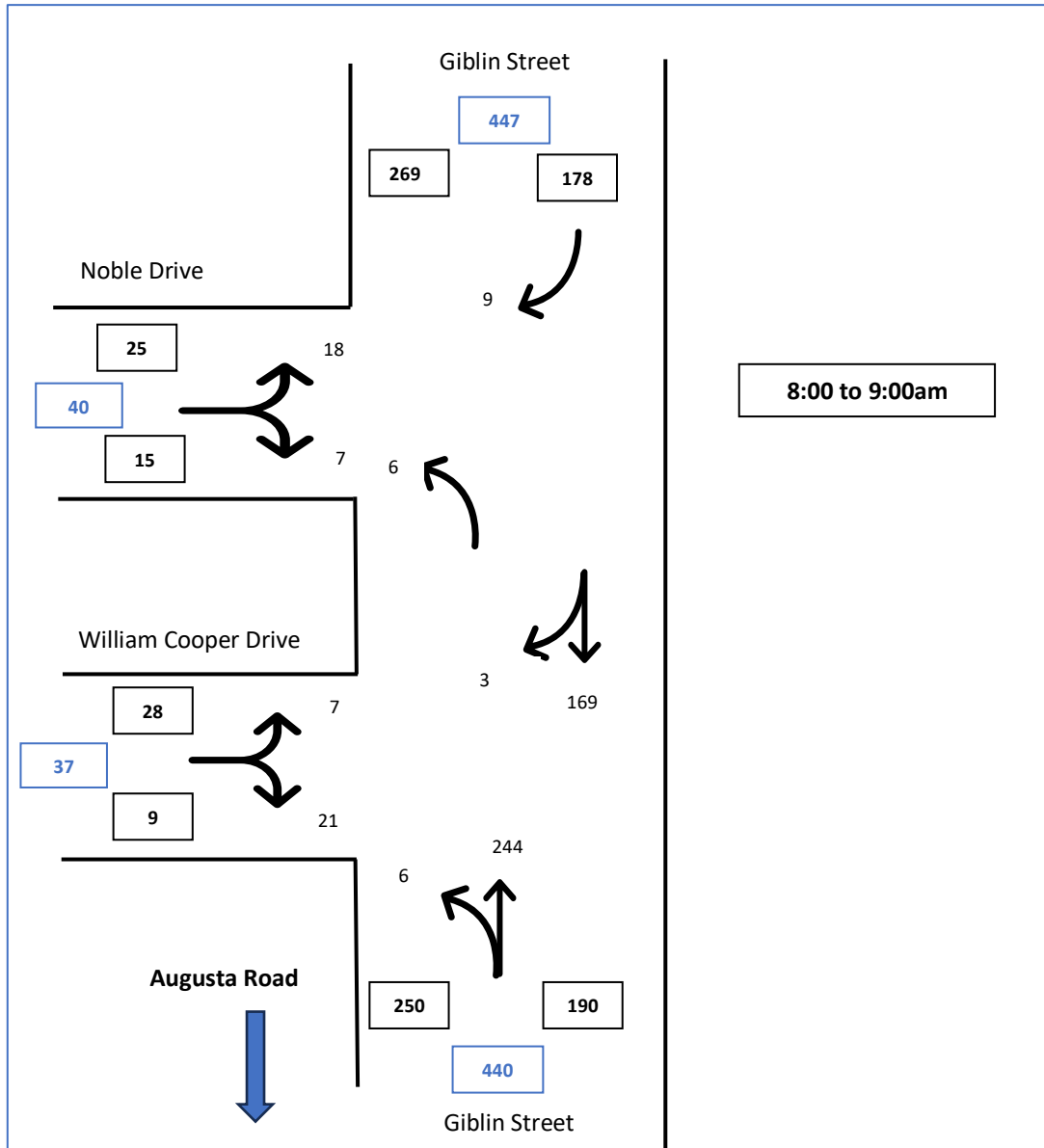
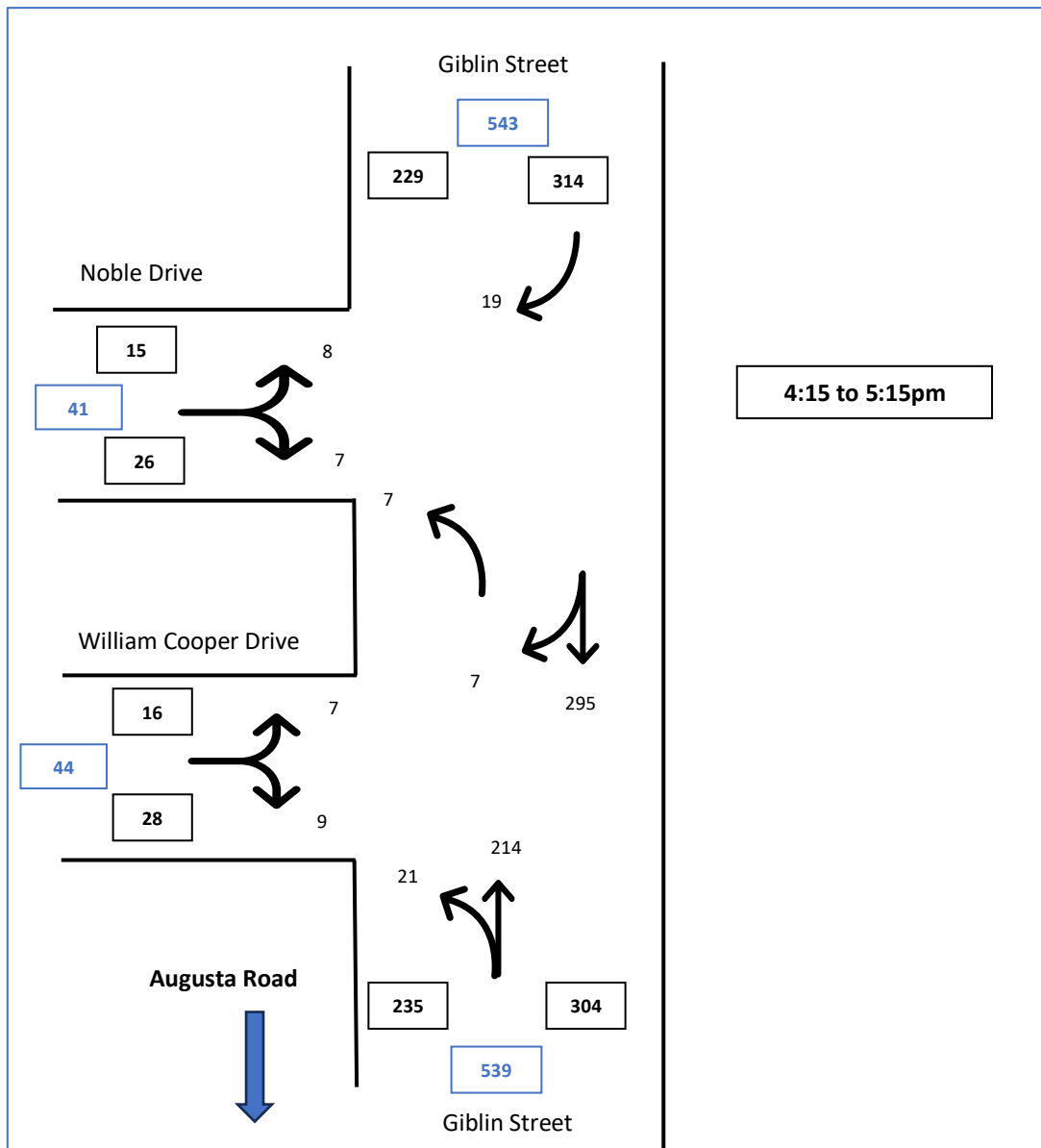


Diagram 5.6B – Evening Peak hour period



5.7 Public bus service

The development site benefits from close proximity to bus stops located on Gilbert Street and Augusta Road. These stops are well serviced by Metro Tasmania, providing direct connections to Hobart and surrounding areas.

5.8 Traffic safety

A review of the State Growth crash database for the five most recently completed calendar years, identified one reported crash on William Cooper Drive on 4 February 2023 at 4:15pm. This crash involved a vehicle emerging from a driveway, which resulted in property damage only. No crashes were reported on Dowding Crescent, Nobel Drive, or at the intersections of Giblin Street with Nobel Drive and William Cooper Drive.

This crash history does not pose a constraint to the proposed development, and the expected minor increase in traffic volumes is unlikely to significantly change the crash risk along the surrounding road network.

6. Impact from traffic generated by this development

The four proposed units will generate about three trips in each peak hour, equating to approximately 30 daily trips. These vehicles have two routes to connect between the development site and Giblin Street, either William Cooper Drive or Noble Drive.

With two-way traffic flows on William Cooper Drive and Noble Drive less than 50 vehicles per hour during weekday peak periods, the additional three peak-hour trips represent only a minor increase and are well within the traffic capacity of the local streets. This level of traffic increase is insignificant in operational terms and will not affect residential amenity or safety.

Similarly, with Giblin Street functioning as a collector road and carrying around 450 vehicles in the morning peak and 540 in the evening peak, the additional three trips from the development represent a very small traffic increase. Giblin street Traffic volumes remain within acceptable tolerances, providing for free-flowing conditions. The presence of traffic signals at each end of Giblin Street further regulates movements and ensures efficient operation, meaning the development traffic is unlikely to alter efficiency, or the level of service on the surrounding network.

SIDRA modelling confirms that the additional trips will not adversely affect traffic efficiency. The two Giblin Street junctions accommodate similar traffic flows, and modelling of the William Cooper Drive junction demonstrates that it operates at the highest level of performance, Level of Service (LOS) A, during both the morning and evening peak.

The junction records a Degree of Saturation below 0.16, meaning it is operating at only 16% of its theoretical traffic capacity. This clearly indicates spare capacity within the network. The average delay for vehicles exiting William Cooper Drive is less than 9.2 seconds, reflecting minimal delay and the absence of traffic queues. The modelling results are illustrated in the table below.

Overall, the SIDRA analysis demonstrates that both Giblin Street junctions have sufficient capacity to accommodate the modest traffic increase without compromising the performance of other road users or the surrounding network.

Traffic modelling is available in Appendix A.

Table 6.0 – Summary of traffic modelling of Giblin Street and William Copper Drive

Peak period	Total vehicles	DOS	Average delay	Worst Delay	LOS	Max queue length
Morning	474	0.135	0.6 secs	8.2 seconds	A	1 metre
Evening	582	0.159	0.5 secs	9.2 seconds	A	0.5metre

7. Development layout and internal road arrangements

7.1 Existing vehicular access

The existing crossover is located at the end of the cul-de-sac and extends across the full 15-metre width of the property's right of way. This design meets standard requirements for urban street crossovers and is suitable for two-way vehicle movements. The width allows enough space for vehicles to enter and exit the site safely and easily, ensuring good access for residents and visitors.

Photograph 7.1 – Existing vehicular access



7.2 Site distance at the existing access

Sight distance at the proposed residential access has been assessed in accordance with The Standard for a residential property operating with four units. The desirable sight distance for a 50 km/h speed environment is 69 metres, with the minimum sight distance being 45 metres, as illustrated in the extract below.

Extract 7.2 – Sight distance for a residential property

Frontage road speed (Note 4) km/h	Distance (Y) along frontage road m		
	Access driveways other than domestic (Note 5)		Domestic property access (Note 6)
	Desirable 5 s gap	Minimum SSD	
40	55	35	30
50	69	45	40
60	83	65	55
70	97	85	70
80	111	105	95
90	125	130	Use values from 2 nd and 3 rd columns
100	139	160	
110	153	190	

A site inspection confirms that the available sight distance from the proposed access point exceeds 70 metres, which is sufficient to support safe and efficient vehicle movements. This level of visibility enables motorists to enter and exit the property without adversely affecting other road users, consistent with the performance expectations for local residential streets.

The available sight distance from the property driveway is illustrated in the photograph below.

Photograph 7.2 – Available sight distance



7.5 Gradients of parking spaces

The majority of parking spaces will have grades that comply with Section 2.4.6 of the Standard and shall not exceed five percent. The two open parking spaces allocated to Unit 4 will have a grade of 5.92 percent.

7.6 Car parking manoeuvrability

Although the design is constrained, a minimum manoeuvring depth of 5.8 metres is provided behind all parking spaces, consistent with The Standard for domestic residential use. Section B4.8 of The Standard allows concessions for residential and domestic parking layouts where space is limited, recognising that such developments typically have a low turnover and that users are generally prepared to accept some inconvenience when entering or leaving spaces.

In this case, vehicles accessing Units 2 and 3 may be required to undertake a three-point turn, while vehicles reversing from the open spaces allocated to Unit 4 may experience minor inconvenience. These movements remain acceptable under Section B4.8, which anticipates such outcomes in constrained residential environments.

Vehicle swept path analysis has been undertaken, confirming a B85 design vehicle can enter and exit all parking spaces without conflict. Swept path outputs are provided in Appendix B for reference and include 0.3 metre clearance (green line).

7.7 Other parking requirements

With the development providing ten on-site car parking spaces, dedicated motorcycle parking spaces are not required. Similarly, the residential units do not need to provide bicycle or accessible parking.

7.9 Internal driveway design and gradient

The four proposed units will be accessed via a single internal driveway that terminates at Unit 4. In response to the site's natural topography, the driveway has been designed with a consistent downward grade, with specific attention given to minimising slope through the manoeuvring areas adjacent to parking spaces.

The grade across the two visitor parking bays will be maintained at around 1.3%, providing compliant and accessible conditions. Adjacent to the double garage at Unit 1, the grade increases to 5.43%, while between the garage openings of Units 1 and 2 - 3, the vertical grade steepens to 18%. This design approach allows the grade adjacent to the garage openings of Units 2 and 3 to be moderated back to 5.95%, supporting safe and functional access while maintaining compliance with AS2890.1 for residential driveways.

The grade of the two open parking spaces will be 5.92%. To achieve this grade, a retaining wall and safety barrier behind the spaces will be necessary.

The following table summarises the proposed driveway gradients, with chainage measured from the property boundary at the existing crossover. The maximum vertical grade will not exceed 18%, which is within the acceptable threshold for residential driveways. Changes in grade have been carefully managed to remain within tolerable limits, allowing for adequate ground clearance.

Table 7.9 – Internal driveway gradients and change in grade

Chainage	Distance	Grade	Change in grade	Adjacent to parking spaces	Comment
0 to 21.43m	21.43m	-4.85%			
21.43 to 40.29m	18.86m	-1.33%	3.52%	Visitor spaces	Sag
40.29 to 46.74m	6.45m	-5.43%	4.1%	Garage unit 1	Crest
46.74 to 51.39m	4.65m	-18.28	12.85%		Crest
51.39 to 55.92m	4.53m	-17.66%	0.62%		Sag
55.92 to 65.17m	9.25m	-5.95%	11.71%	Garages unit 2,3	Sag
65.17 to 72.47m	6.76m	-5.92%	0.03%	Parking space unit 4	

7.10 Pedestrian access

The internal pedestrian pathway connecting the public footpath along William Cooper Drive to the residential units has been designed to provide safe and legible access, while accommodating vehicular movements and the required passing bay at the driveway entrance. For the initial seven metres, the pathway will operate at the same level as the driveway and be delineated by pavement markings. Beyond this point, a dedicated one-metre-wide concrete pathway will be provided along the eastern side of the right of way, extending to the entrance of Unit 3. Where the pathway crosses a visitor parking space, it will again operate at the same level as the driveway, with clear pavement markings to maintain pedestrian priority.

Where practicable, the pathway will be separated from the driveway by a kerb, enhancing pedestrian safety and definition. Suitable ramps will be incorporated where the pathway interfaces with the driveway to provide accessibility.

Beyond Unit 3, there is insufficient width to continue the separated pathway. This area will operate as a shared arrangement between pedestrians and vehicles. Given the low volume of vehicle movements and likely operating speeds below 10 km/h, this arrangement is unlikely to compromise pedestrian safety. This shared arrangement is considered safe and appropriate for the scale and function of the development.

7.11 Access for emergency vehicles

According to Tasmanian Fire Brigade, their largest pump vehicle is of similar size to a medium rigid vehicle. The driveway has sufficient width to accommodate a medium rigid vehicle to enter and reach all units. It is noted that this vehicle will need to reverse out of the driveway, which is acceptable given the likelihood of this movement being very low.

7.12 Waste collection

Due to the constrained width of the right-of-way, on-site collection cannot be accommodated. External collection is proposed, with sufficient kerb length available (approximately 12 metres) along the frontage of property No. 33 to accommodate the required bins. The impact on the adjoining property is considered minimal, as the arrangement involves four bins placed for collection once per week and eight bins placed every second week for recycling.

Each dwelling unit will be responsible for transporting its bin to the street frontage. Bins will be positioned on the footpath adjacent to the driveway of property No. 33 for collection by the service provider.

This arrangement ensures adequate provision for waste management while minimising disruption to neighbouring properties and maintaining compliance with council collection requirements.

7.13 Turning around area

The two visitor parking spaces are located near the driveway entrance, with sufficient manoeuvring depth to enable vehicles to enter and depart in a forward-driving direction. To prevent non-resident vehicles from entering the constrained section at the end of the driveway and attempting a five-point turn, a sign stating “Residents only beyond this point” will be installed immediately beyond the visitor spaces. This arrangement provides safe and efficient visitor parking, maintains clear circulation for residents, and avoids unnecessary reversing movements within the limited driveway layout.

All visitor and residential vehicles will be able to enter and leave the development in a forward-driving direction, consistent with the requirements of the Tasmanian Planning Scheme.

8. Planning scheme

8.1 C2.0 Parking and Sustainable Transport Code

C2.5.1 Car parking numbers

The development site is providing a total of 10 on-site car parking spaces, meeting the planning scheme acceptable solution, and minimising the risk of overflow parking. Each of the units will be provided with two dedicated parking spaces, plus two visitor parking spaces, as the site is considered an internal lot, and the location of the access is at the end of a cul-de-sac.

C2.5.2 Bicycle parking numbers

Table C2.1 of the planning scheme prescribes that a residential use does not require bicycle parking spaces.

C2.5.3 Motorcycle parking numbers

Table C2.4 of the planning scheme prescribes that where this use requires less than 20 parking space, motorcycle parking spaces are not required.

C2.5.4 Loading bays

Not applicable for a residential development.

C2.6. Development standards

C2.6.1 Construction of parking areas.	The parking areas and internal driveway will be a concrete surface, with the driveway operating with a one-way camber to direct surface water to the concrete kerb, which will flow into an approved stormwater drainage system. The design complies with the acceptable solution A1.
C2.6.2 Design and layout of parking areas.	The internal layout and parking areas have been designed to comply with both the Australian Standard 2890.1:2004 for user class 1A, and the planning scheme, to ensure vehicles can easily enter, manoeuvre, and leave in a forward-driving direction. Vehicle swept path software verified there will be sufficient manoeuvring areas adjacent to all the parking spaces to enable vehicles to enter and leave the spaces efficiently. Uncovered parking spaces located adjacent to a vertical obstruction greater than 150 millimetres in height, will have an additional 0.3 metres width on either side to assist with manoeuvring. The parking spaces will be located on a gradient less than five percent. The

	width of the driveway will comply with dimensions specified in the planning scheme table C2.3, based on the number of car parking spaces the driveway serves, and the single lane section will be supported with appropriate passing bays. Where two or more parking spaces are located together, they will be delineated by line markings, and supported with wheel stops where appropriate. Overall, the parking spaces and internal layout complies with the acceptable solution A1.1 (a) and (b).
C2.6.3 Number of accesses for vehicles.	The development will operate with the existing vehicular access onto Williams Cooper Drive, and this complies with the acceptable solution A1 (a) and (b).
C2.6.4 Lighting of parking areas within the general business zone and central business zone	Sufficient lighting will be provided to light the parking spaces, driveway, and pedestrian pathways to the acceptable standards.
C2.6.5 Pedestrian access.	An internal pedestrian pathway will provide safe access from William Cooper Drive to the residential units, designed with regard to the width of the right of way and the need to accommodate passing vehicles. The pathway will extend to Unit 3, beyond which insufficient width requires pedestrians to share the driveway. This shared arrangement is considered safe, as vehicle volumes are expected to be low and operating speeds below 10 km/h. The pathway will be one metre wide with a concrete surface and, where practicable, separated from the driveway by kerbing to assist stormwater drainage and enhance pedestrian safety. At two locations, the visitor parking space and the passing bay near the driveway entrance, the pathway will operate at driveway level, with pavement markings to maintain pedestrian priority. Kerb ramps will be provided to ensure accessibility at these interfaces.
C2.6.6 Loading bays.	Not required for a residential development.
C2.6.7 Bicycle parking and storage facilities	Not required for a residential development.
C2.6.8 Siting of parking and turning areas.	Not applicable for a residential development.

8.2 C3.0 Road and Railway Assets Code

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

As the site is currently vacant, the proposed development will increase the use of the existing access by more than 20 percent or generate over 40 vehicle movements per day. Consequently, it will need to be assessed against the performance criteria P1.

Traffic analysis confirms the additional movements will not adversely affect the safety or efficiency of the surrounding road network. Vehicles will be able to enter and exit the site safely, without conflict or delay to other road users.

Performance criteria	Assessment
To ensure that the safety and efficiency of roads is not reduced by the creation of a new access and junctions.	
a) Any increase in the traffic caused by the use;	The four residential units is predicted to generate 30 daily trips, with three of these trips likely to occur during the morning and evening peak periods.
b) The nature and frequency of the traffic generated by the use;	The residential units are expected to generate light vehicles less than 5.5 metres in length. These types of vehicles are associated with urban residential living, have good manoeuvrability, and are compatible with the existing vehicles using the surrounding road network.
c) The nature of the road;	Williams Cooper Drive is a no-through residential road, built to an urban standard, and has sufficient width to accommodate two-way traffic movements. The surrounding road network is of a suitable urban road standard to accommodate the minor increase in traffic flow. There is sufficient sight distance at the existing vehicular access, to enable vehicles to enter and leave the development site in a safe and efficient manner.
d) The speed limit and traffic flow of the road;	The urban default 50 km/h speed limit applies to Williams Cooper Drive, Noble Drive, and Giblin Street. Recent manual traffic surveys found that the local residential roads are lightly trafficked, with less than 50 two-way vehicles in the peak periods. While the local collector road is moderately trafficked, with less than 550 two-way vehicles along Giblin Street. Traffic analysis of the surrounding road network, including traffic modelling indicates there is sufficient spare traffic capacity to absorb the increase in traffic, without causing adverse traffic impact, or a reduction in traffic flow. The increase in traffic is also not expected to adversely impact residential amenity along the local residential roads.
e) Any alternative access;	None.
f) The need for the access or junction;	Urban infill in established urban areas is an excellent method to increase the supply of housing, while optimising the current infrastructure and community facilities.
g) Any traffic impact assessment; and	An independent traffic assessment has found there no reason for this development not to proceed.

h) Any written advice received from the road authority.	Aware of none.
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9. Conclusion


From a traffic engineering and road safety perspective, the additional traffic generated by the proposed development is not expected to create any adverse safety, amenity, or efficiency issues.

- The volume of traffic is low and can be readily absorbed within the surrounding road network without impacting other users.
- The existing vehicular access provides safe and efficient movements, supported by a passing bay at the driveway entrance and appropriate sight distances.
- The single-lane driveway, supplemented with passing bays, will deliver a suitable level of service for residents, while the provision of sufficient on-site parking spaces eliminates the risk of parking overflow.
- Pedestrian access is accommodated by a dedicated pathway where practicable, and a shared zone acceptable given the low traffic volumes and operating speeds.
- Swept path analysis confirms that B85 design vehicles can access all parking spaces, with manoeuvring consistent with Section B4.8 of AS2890.1.
- Driveway gradients remain within tolerable limits, with all vehicles able to enter and leave the site in a forward-driving direction


This Traffic Impact Assessment found no reason for this development not to proceed.

10. Appendix A – Traffic modelling results

Morning peak hour

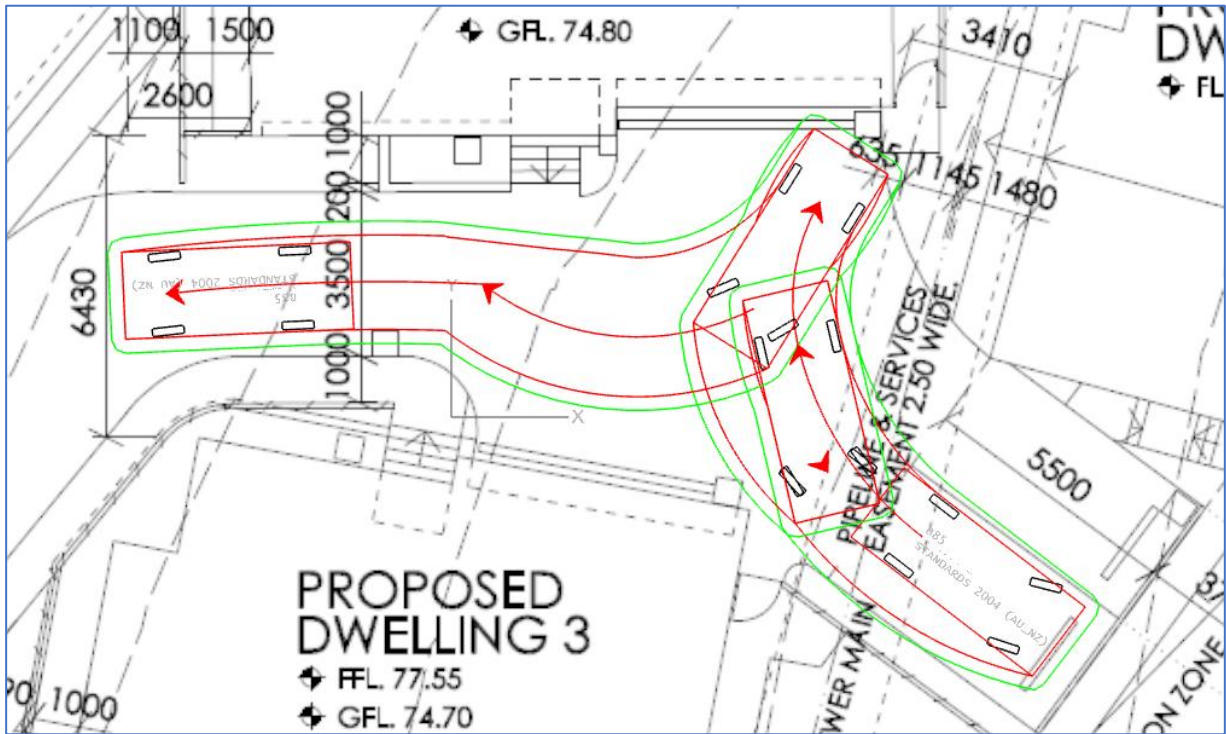
MOVEMENT SUMMARY								
 Site: 101 [William Cooper and Giblin - Existing morning]								
New Site								
Site Category: (None)								
Giveaway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Giblin Street								
1	L2	6	0.0	0.135	5.6	LOS A	0.0	0.0
2	T1	257	0.0	0.135	0.0	LOS A	0.0	0.0
Approach		263	0.0	0.135	0.1	NA	0.0	0.0
North: Giblin Street								
8	T1	178	0.0	0.091	0.0	LOS A	0.0	0.0
9	R2	3	0.0	0.002	6.2	LOS A	0.0	0.1
Approach		181	0.0	0.091	0.1	NA	0.0	0.1
West: William Cooper Drive								
10	L2	7	0.0	0.037	6.3	LOS A	0.1	1.0
12	R2	22	0.0	0.037	8.2	LOS A	0.1	1.0
Approach		29	0.0	0.037	7.8	LOS A	0.1	1.0
All Vehicles		474	0.0	0.135	0.6	NA	0.1	1.0

Evening peak hour

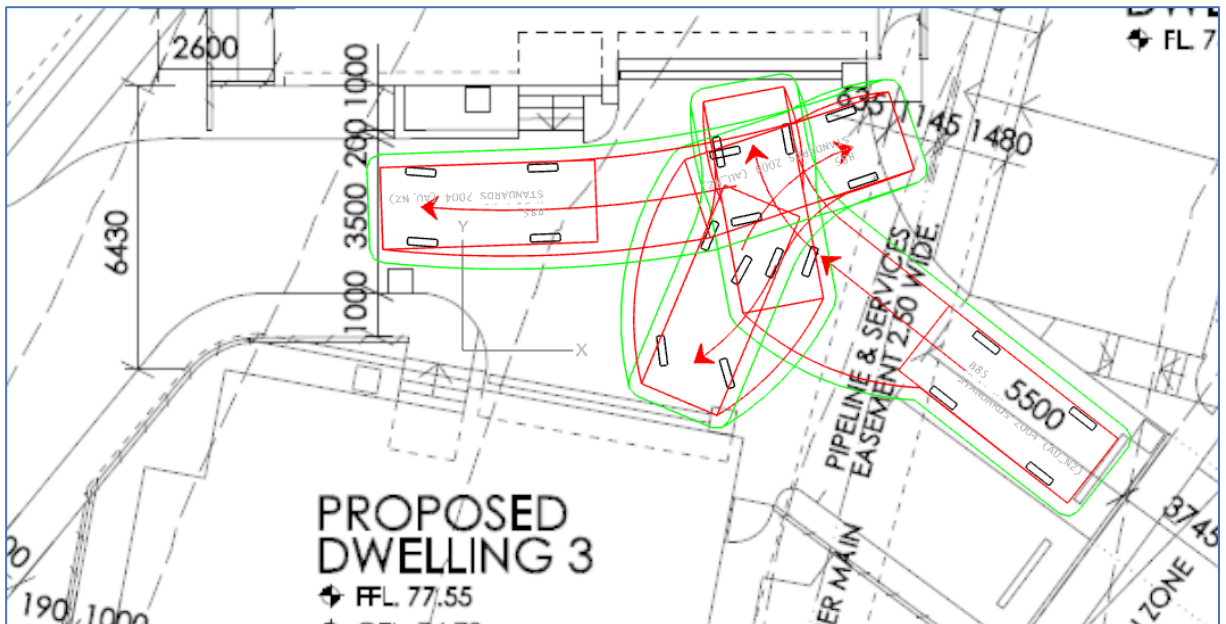
MOVEMENT SUMMARY								
 Site: 101 [William Cooper and Giblin - Existing evening]								
New Site								
Site Category: (None)								
Giveaway / Yield (Two-Way)								
Movement Performance - Vehicles								
Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Giblin Street								
1	L2	22	0.0	0.127	5.6	LOS A	0.0	0.0
2	T1	225	0.0	0.127	0.0	LOS A	0.0	0.0
Approach		247	0.0	0.127	0.5	NA	0.0	0.0
North: Giblin Street								
8	T1	311	0.0	0.159	0.0	LOS A	0.0	0.0
9	R2	7	0.0	0.005	6.2	LOS A	0.0	0.2
Approach		318	0.0	0.159	0.2	NA	0.0	0.2
West: William Cooper Drive								
10	L2	7	0.0	0.021	6.2	LOS A	0.1	0.5
12	R2	9	0.0	0.021	9.2	LOS A	0.1	0.5
Approach		17	0.0	0.021	7.9	LOS A	0.1	0.5
All Vehicles		582	0.0	0.159	0.5	NA	0.1	0.5

11. Appendix B – Swept path diagrams

Swept path of B85 vehicle leaving the first open space allocated to unit 4

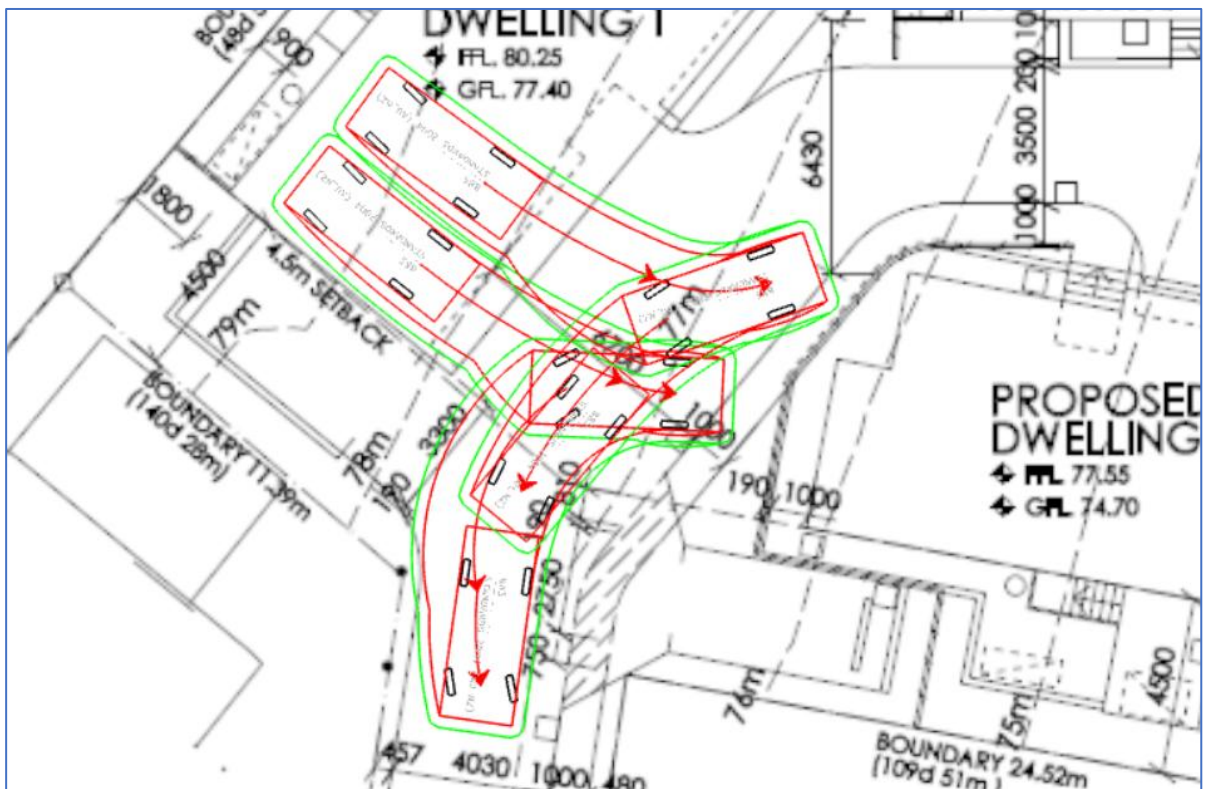


Swept path of B85 vehicle leaving the second open space allocated to unit 4

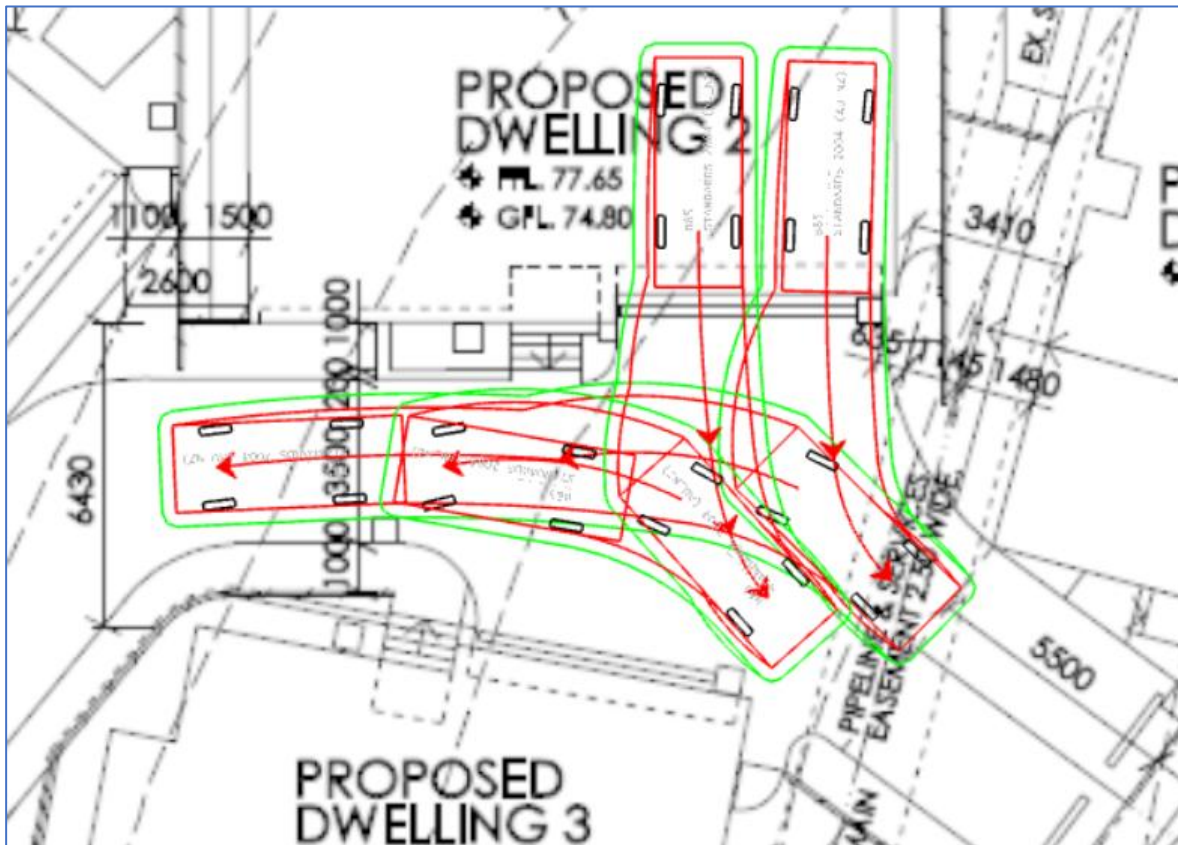




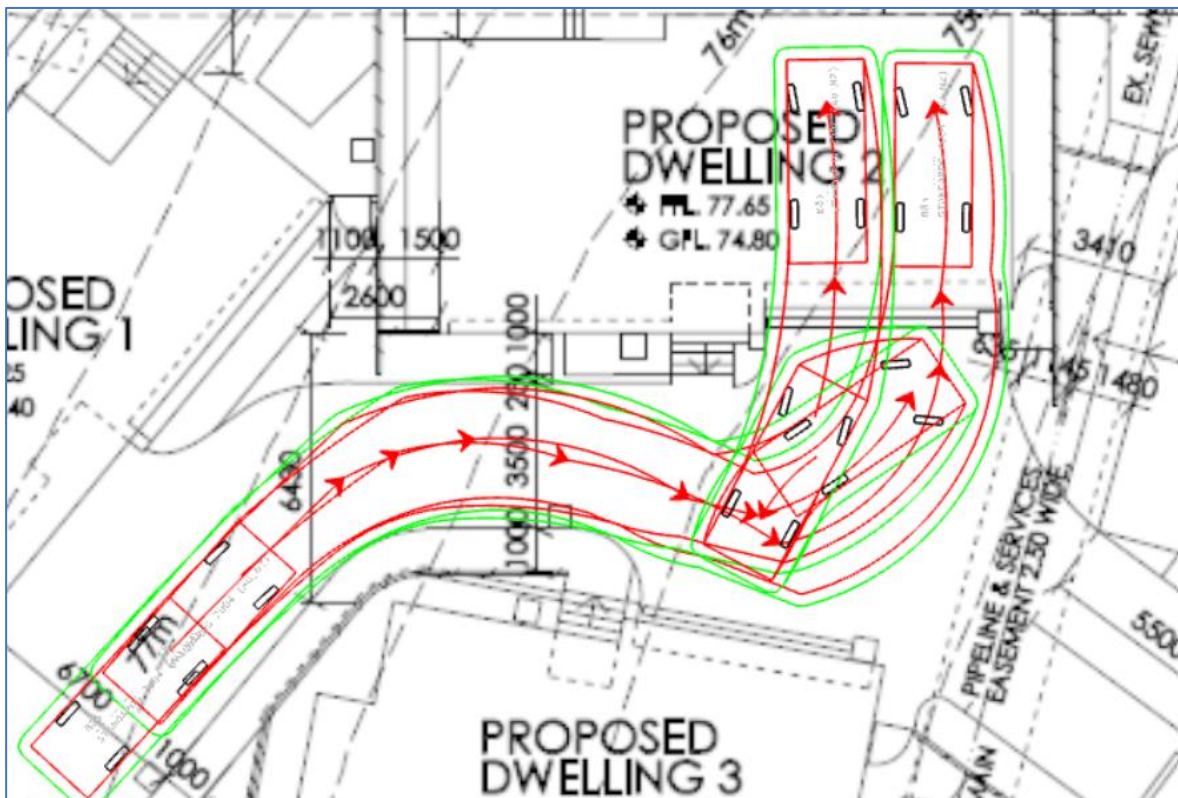
Vehicles leaving unit 1 double garage



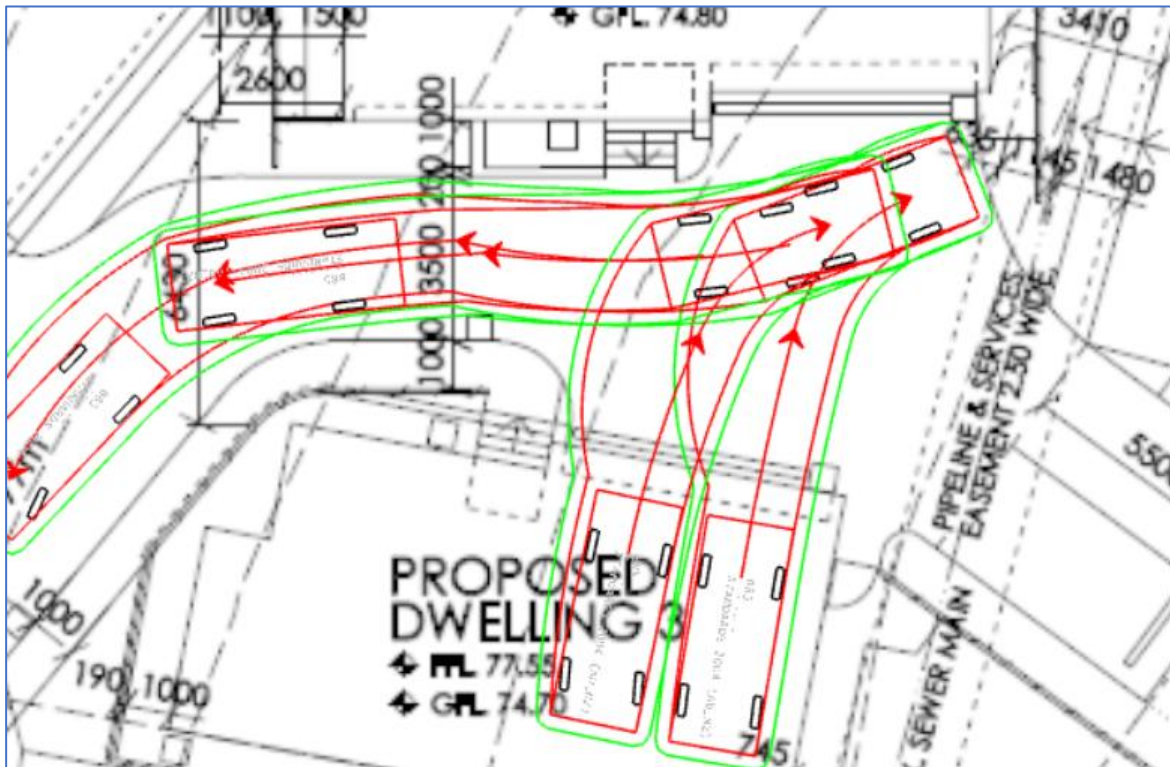
Swept path of B85 vehicle leaving the double garage from unit 2



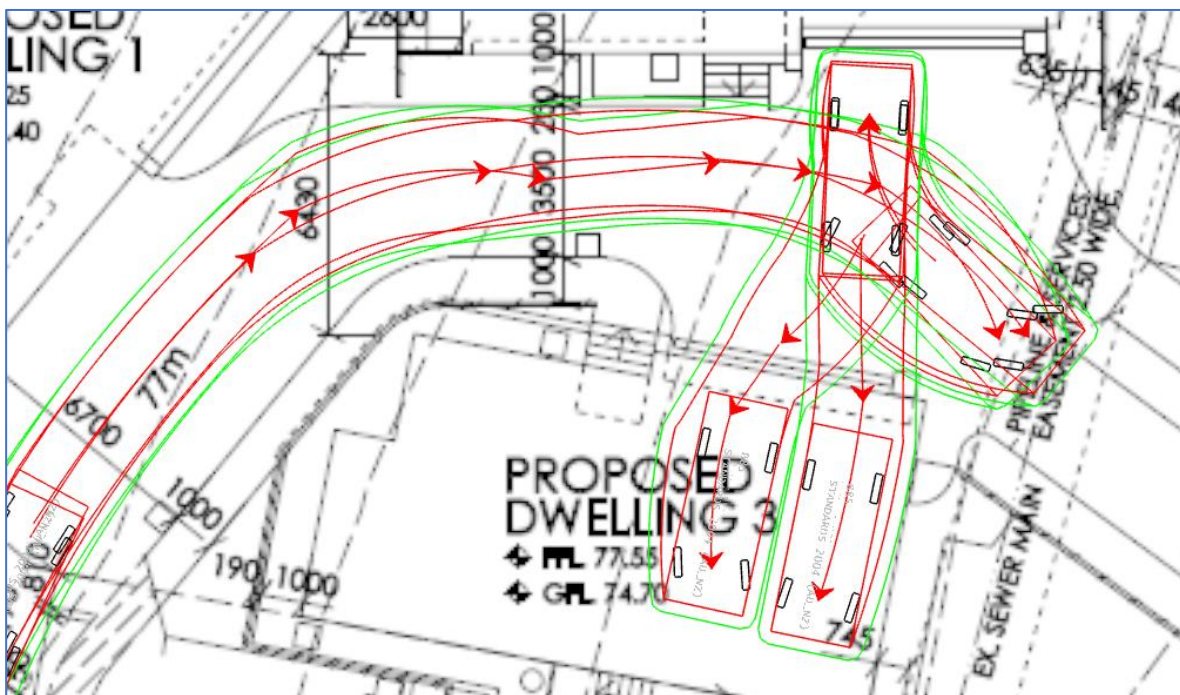
Swept path of B85 vehicle entering unit 2 double garage



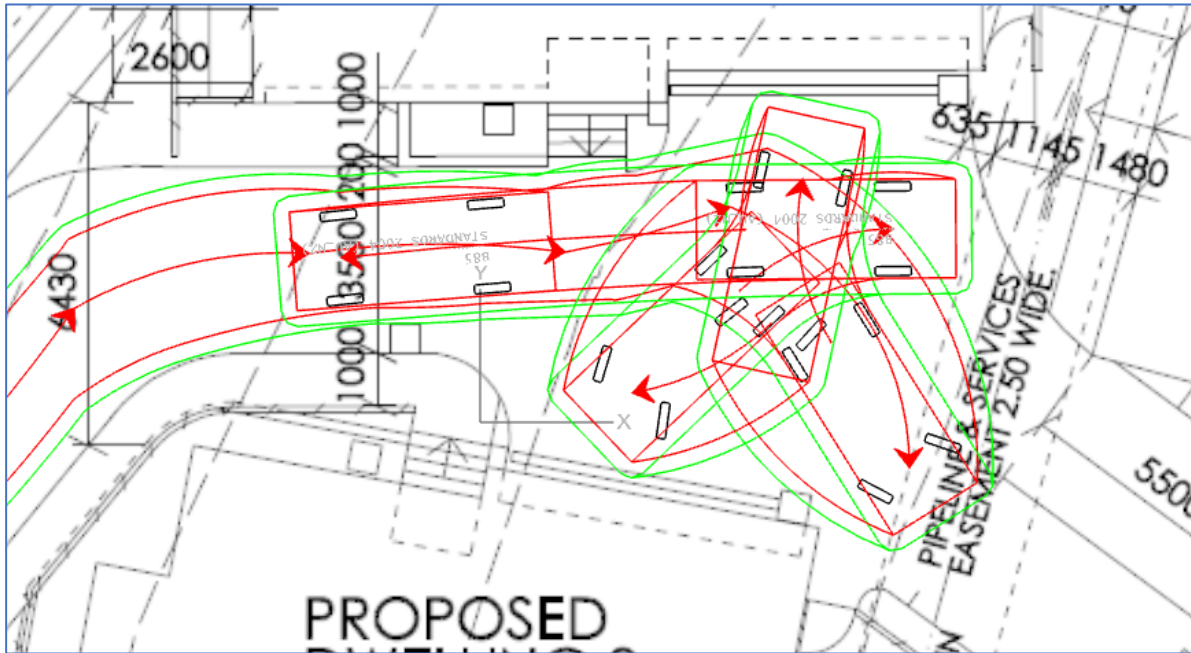
Swept path of B85 vehicle leaving the double garage from unit 3



Swept path of B85 vehicles entering unit 3 double garage



Swept path of B85 vehicles turning around



REQUEST FOR INFORMATION

Reference

PLANNA-HOB-2025-4878

Application Reference

PLN-HOB-2025-0610

Address

37 WILLIAM COOPER DR NEW TOWN TAS 7008

Titles

173156/28

Information Requested

#	Reason	Origin
1	<p>Sw1 - Stormwater</p> <p>To allow the appropriate conditioning of the permit under the Scheme, and assess likely compliance with Council's <i>Stormwater Management Policy for Development</i> and the <i>Urban Drainage Act 2013</i>; please provide:</p> <ul style="list-style-type: none"> Please confirm all impervious surfaces and detention tank outlets can drain to the existing lot connection via gravity, taking internal pipe fall and minimum clearance from the sewer main into consideration. Clarify if a pump is required for any subsoil drains etc. Should any impervious surfaces not be able to drain via gravity, discuss how overflow (in the case of larger storms or pump failure) will be safely managed, noting the mapped landslip hazard and proximity to the quarry edge. <p><i>Advice:</i></p> <ul style="list-style-type: none"> Please note "top-up" detention and treatment is required for the site, as the proposed impervious site coverage exceeds that assumed at time of subdivision (developed C=0.65, pre-dev C=0.4, tc of catchment =19min). Full details, including supporting calculations for all 5% AEP events, long-sections and maintenance plans, will be required at detailed design stage. Council does not support onsite stormwater disposal on this site. Any proposed onsite disposal must be supported by a full geotechnical assessment. Council refers the applicant to Council's Policy available from our website. Council notes later design alterations can require planning amendments, and recommends the applicant addresses these concerns at this stage. At Planning stage, Council seeks enough information to demonstrate a solution is feasible. Full engineering design and supporting calculations will be required at detailed design stage via the Condition Endorsement Process if not provided at this stage. 	LUPAA 1993

Response comments

Response to Request for Further Information

Dear HCC,

Please find attached Arete Engineering stormwater report.

Please note that the construction drawings will be updated noting the 1000 ltr detention tanks to each dwelling.

Regards,

Kevin Roberts

TAS BUILDING DESIGN

p. 0408882283

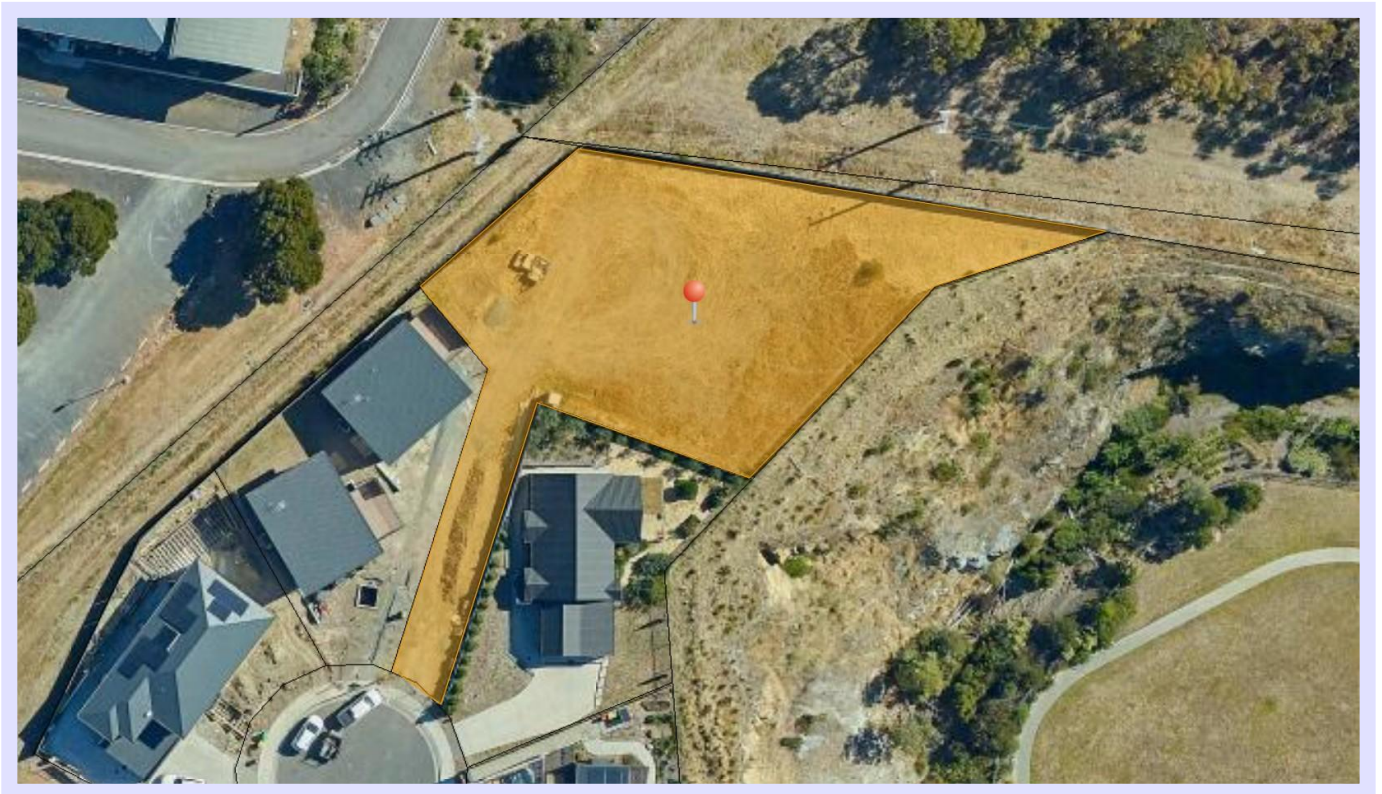
Documents

Version	Document Date	Document Type	Description	Prepared By
1	5 Jan 2026	Stormwater Report	Stormwater report	Mr Kevin Roberts

Submitted on 12/01/2026

STORMWATER MANAGEMENT REPORT

PROPOSED 4 DWELLING DEVELOPMENT AT 37 WILLIAM COOPER DRIVE NEW TOWN



DOCUMENT DETAILS

Prepared For	Prepared By	Date	Arete Project Number
Tas Building Design Pty. Ltd.	R. Moon	23 rd December 2025	2025-C02-PR01

DOCUMENT HISTORY

Revision	Date	Description	Prepared By	Reviewed By
D1	23/12/2025	For Development Approval	R. Moon	D. Morley

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1 SCOPE OF REPORT

Arete Engineering has been engaged by Tas Building Design Pty. Ltd. to prepare a stormwater management report for the proposed development of four dwellings at 37 William Cooper Drive, New Town.

Further information was requested by Hobart City Council in December 2025. The content of the request for further information (RFI) relevant to the scope of this report is as follows:

Sw1 – Stormwater

To allow the appropriate conditioning of the permit under the Scheme, and assess likely compliance with Council's *Stormwater Management Policy for Development* and the *Urban Drainage Act 2013*, please provide:

- Please confirm all impervious surfaces and detention tank outlets can drain to the existing lot connection via gravity, taking internal pipe fall and minimum clearance from the sewer main into consideration.
- Clarify if a pump is required for any subsoil drains etc. Should any impervious surfaces not be able to drain via gravity, discuss how overflowing (in the case of larger storms or pump failure) will be safely managed, noting the mapped landslip hazard and proximity to the quarry edge.

Advice:

- Please note “top-up” detention and treatment is required for the site, as the proposed impervious site coverage exceeds that assumed at time of subdivision (developed C = 0.65, pre-dev C = 0.4, tc of catchment = 19 min). Full details, including supporting calculations for all 5% AEP events, long-sections and maintenance plans, will be required at detailed design stage.
- Council does not support onsite stormwater disposal on this site. Any proposed onsite disposal must be supported by a full geotechnical assessment.
- Council refers the applicant to Council's Policy available from our website.
- Council notes later design alterations can require planning amendments, and recommends the applicant addresses these concerns at this stage. At planning stage, Council seeks enough information to demonstrate a solution is feasible. Full engineering design and supporting calculations will be required at detailed design stage via the Condition Endorsement Process if not provided at this stage.

Section 2 of this report addresses the requirement for top-up on site detention. Section 3 addresses the requirement for top-up stormwater treatment. Section 4 demonstrates that all impervious areas can drain to the connection via gravity and that no pumps are required.

2 ON-SITE DETENTION MODEL

2.1 MODELLING APPROACH

Autodesk SSA was used to model the pre-development and post-development runoff from the site and to model detention storages to reduce runoff quantities. The required volumes of the on-site detention devices were calculated to ensure sufficient storage without overflow or surcharging for all storm durations. The modified rational method was selected as the hydrological model for the analysis.

2.2 MODEL INPUTS

2.2.1 CATCHMENT AREAS

The total catchment area for the site was determined from the site detail survey provided. Proposed post-development catchment areas were determined from site plans prepared by Tas Building Design Pty. Ltd.

Tables 2.1 and 2.2 summarise the catchment characteristics assumed for the on-site detention model.

Table 2.1: Pre-Development Catchment Summary

Catchment	Area (m ²)	Runoff Coefficient 'C'	Time of Concentration (mins)
Entire site	1534.64	0.65	9

Table 2.2: Post-Development Catchment Summary

Catchment	Area (m ²)	Runoff Coefficient 'C'	Time of Concentration (mins)
Roofed areas	392.38	1.0	5
Driveway paved areas	446.9	0.9	5
Other paved areas	179.68	0.9	5
Garden and lawns	515.68	0.25	5

Runoff Coefficients

Runoff coefficients were determined in accordance with AS/NZS 3500.3 2021 and the City of Hobart Stormwater Management Policy for Development.

Coefficients of 1.0 and 0.9 were adopted for impervious roof and paved areas respectively as per clause 5.4.6 of AS/NZS 3500.3 2021.

A coefficient of 0.65 was applied to the entire site in the pre-development condition as advised in the HCC Stormwater RFI to account for the stormwater detention infrastructure provided at the time of subdivision.

A coefficient of 0.25 was applied to lawn and landscaped garden areas in accordance with table 5.4.6(B) of AS/NZS 3500.3 2021.

Times of Concentration

A Time of Concentration (TOC) of 5 minutes was assumed for all post-development catchments as per the 'Standard inlet time' method provided in the Queensland Urban Drainage Manual 2017 (QUDM 2017).

A TOC of 9 minutes was applied to the pre-development site. This time of concentration was determined in accordance with Figure 4.4 of QUDM 2017 (reproduced in Figure 2.1 below).

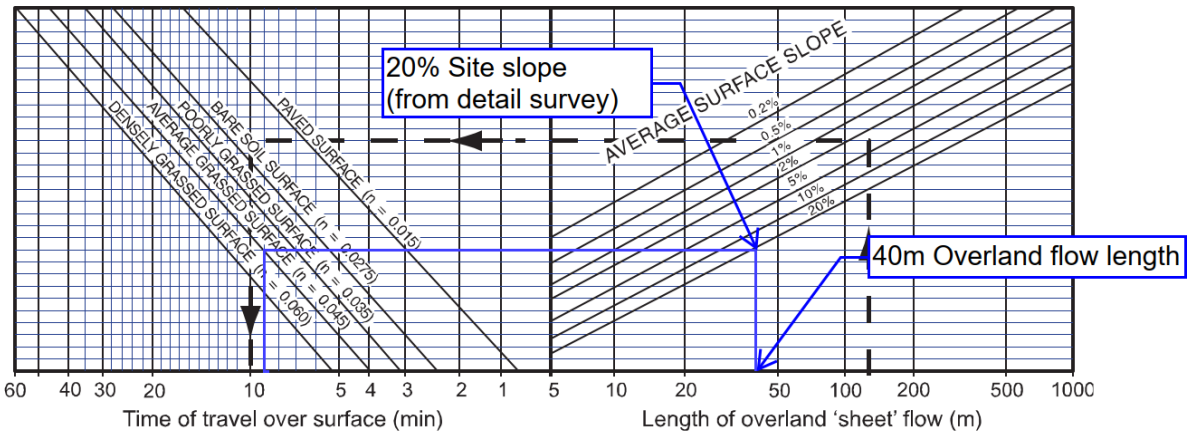


Figure 2.1: Overland sheet flow times (shallow sheet flow only) (QUDM 2017, ARR 1977)

It is noted that the 19-minute time of concentration referenced in the HCC RFI advice refers to the time of concentration for the detention model prepared for the entire subdivision. This 19-minute TOC is therefore not applicable in modelling the single lot addressed by this report in either the pre- or post-development conditions.

2.2.2 RAINFALL

Rainfall data for the subject site was sourced from the Bureau of Meteorology Design Rainfalls website (<http://www.bom.gov.au/water/designRainfalls/revised-ifd/?multipoint>). Storm durations between 5 minutes and 30 minutes were selected for use in the model, as this range of storm durations commonly includes the critical storm durations for both site runoff and detention tank volumes.

Table 2.3: Rainfall IFD Data – Hobart, Tasmania

Storm Duration (Min.)	5% AEP Rainfall Depth (mm/hr)
5	87.5
10	65.4
15	53.1
20	45.1
30	35.5

2.3 MODEL RESULTS

Due to the increase in impervious area, the post-development site condition produced runoff quantities higher than the permissible site discharge in the critical storm duration, and thus on-site detention was determined to be necessary. The duration producing the highest peak flow rate was taken to be the critical storm duration for each site condition.

Due to constraints of the rational method, storm durations shorter than the catchment time of concentration cannot be simulated.

Table 2.4 summarises the peak 5% AEP runoff for each site condition. Figure 2.2 shows the hydrograph for the critical 5% AEP storm of each site condition.

Table 2.4: Model Results Summary

Model Scenario	Peak 5% AEP Site Runoff (L/s)	Critical Storm Duration (mins)
Permissible Site Discharge (PSD)	18.79	9
Post-development unmitigated	26.14	5
Post-development with OSD	17.80	5

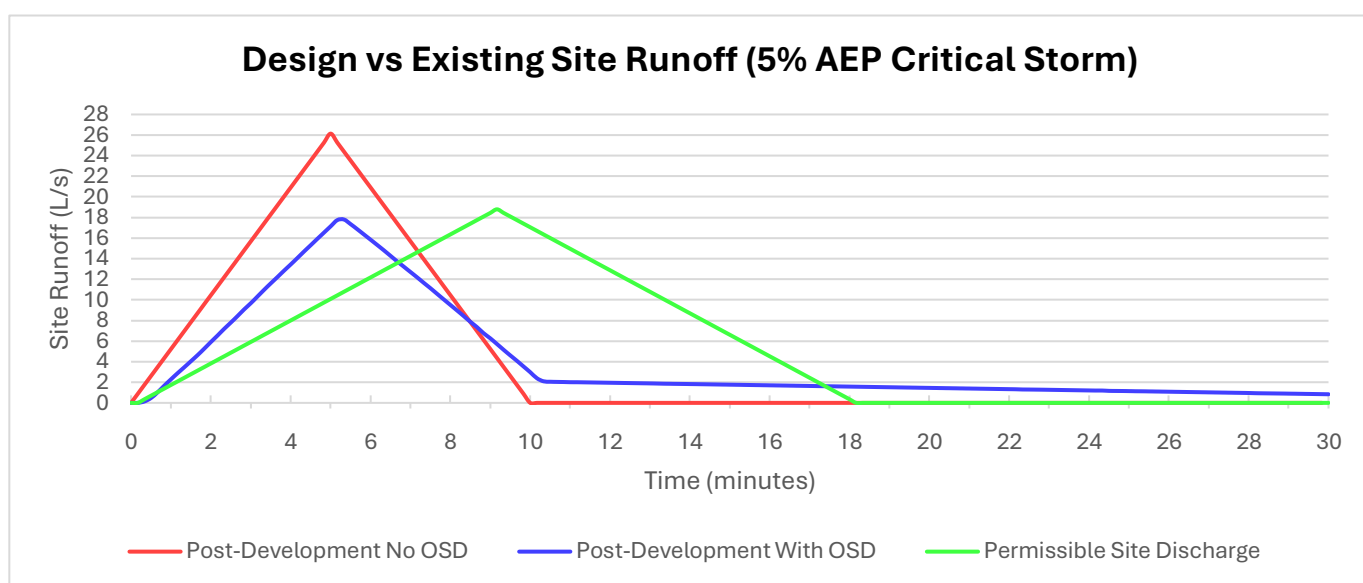


Figure 2.2: Design vs Existing Site Runoff Hydrograph (5% AEP Critical Storm)

The on-site detention system modelled in Autodesk SSA consisted of one rainwater detention tank connected to the roof catchment of each proposed dwelling. Table 2.5 details the required detention volume of each tank for the simulated 5% AEP storm durations.

Table 2.5: On-Site Detention Storage Volumes

Storm Duration (min)	5% AEP Maximum Volume (m ³)			
	Tank 1	Tank 2	Tank 3	Tank 4
5	0.59	0.53	0.48	0.50
10	0.83	0.73	0.67	0.70
15	0.93	0.81	0.74	0.77
20	0.96	0.84	0.76	0.80
30	0.95	0.82	0.75	0.78

2.4 PROPOSED DETENTION SYSTEM

The on-site detention model determined that a total of four rainwater detention tanks with a nominal detention capacity of 1000L each will be sufficient to reduce peak site runoff below the PSD. Each detention tank must receive flows from the entire roof catchment of the associated dwelling.

The critical specifications for the proposed on-site detention tanks are summarised in table 2.6.

Construction details and long sections for the proposed on-site detention tanks shall be provided at condition endorsement stage.

Table 2.6: On-site Detention System Specifications

Tank Description	Nom. 1000L Rainwater Tank
Minimum Detention Storage Volume (m ³)	0.96
Retention/Reuse Storage (m ³)	0
Outlet Orifice Diameter (mm)	20
Overflow Diameter (mm)	100
No. Identical Tanks	4

3 STORMWATER TREATMENT

Advice contained in the Hobart City Council stormwater RFI notes that ‘top-up’ stormwater treatment will be required as the impervious site coverage exceeds that assumed at the time of subdivision.

It is proposed to utilise proprietary primary and tertiary stormwater treatment devices (for example, pit baskets and up-flow filters) to achieve sufficient levels of ‘top-up’ treatment. The arrangement, details, required maintenance schedule and supporting MUSIC model for the proposed stormwater treatment system shall be provided at detailed design stage.

4 STORMWATER LOT SERVICING VIA GRAVITY

The proposed stormwater network shown on the Site Drainage Plan by Tas Building Design Pty. Ltd. provides adequate inlet pit and downpipe locations to collect and convey runoff from all impervious surfaces to the site stormwater connection. Preliminary calculations have found that the proposed surface levels of all impervious areas allow for runoff to drain to the stormwater connection via gravity. No pumped stormwater or on-site stormwater disposal systems are proposed or required.

Long sections of the proposed stormwater network will be provided at condition endorsement stage.

Invert Levels at Connection

The stormwater inlet pit near ‘Proposed Dwelling 4’ with a surface RL of 74.00 was found to be the primary constraint on stormwater invert levels at the stormwater property connection IO. Assuming a pit depth of 450mm a minimum grade of 1% for stormwater pipework, it was calculated that an invert level of 73.39 is achievable at the point of connection. The existing connection has an invert level of 73.06 and therefore has adequate depth to service the proposed development.

Sewer Main Crossing

The same inlet pit is the primary constraint on stormwater invert levels near the existing sewer main. Using the assumptions detailed above, a stormwater invert level of 73.44 is achievable at the sewer main crossing. The invert level of the sewer main at the crossing was calculated to be 72.89 therefore 390 mm of vertical clearance is available. This complies with the vertical clearance requirements of AS/NZS 3500.3 and table 5.4 of the Gravity Sewerage Code of Australia MRWA Edition.

Subsoil Drains

The lowest subsoil drains required to be installed on the site will be in the proximity of Proposed Dwelling 4. Assuming a depth to invert of 0.45m, these subsoil drains will be able to connect to the inlet pit discussed above and therefore will be able to drain via gravity.

5 RFI RESPONSE SUMMARY

Table 5.1: Stormwater RFI Response Summary

Sw1 - Stormwater

To allow the appropriate conditioning of the permit under the Scheme, and assess likely compliance with Council’s *Stormwater Management Policy for Development* and the *Urban Drainage Act 2013*, please provide:

Request	Response
Please confirm all impervious surfaces and detention tank outlets can drain to the existing lot connection via gravity, taking internal pipe fall and minimum clearance from the sewer main into consideration.	The floor levels of all dwellings and surface levels of all impervious surfaces allow for all associated drainage, including subsoil drains, to drain via gravity to the stormwater connection. Please refer to Section 4 of this report.
Clarify if a pump is required for any subsoil drains etc. Should any impervious surfaces not be able to drain via gravity, discuss how overflowing (in the case of larger storms or pump failure) will be safely managed, noting the mapped landslip hazard and proximity to the quarry edge	No pump is required, therefore overflowing concerns are not applicable. Please refer to Section 4 of this report.
Advice	Response
Please note “top-up” detention and treatment is required for the site, as the proposed impervious site coverage exceeds that assumed at time of subdivision (developed C = 0.65, pre-dev C = 0.4, tc of catchment = 19 min). Full details, including supporting calculations for all 5% AEP events, long-sections and maintenance plans, will be required at detailed design stage.	Top-up detention and treatment requirements addressed in sections 2 and 3 of this report.
Council does not support onsite stormwater disposal on this site. Any proposed onsite disposal must be supported by a full geotechnical assessment.	No onsite stormwater disposal required or proposed.

<p>Council refers the applicant to Council’s Policy available from our website.</p>	<p>Report has been prepared considering the Stormwater Management Policy for Development.</p>
<p>Council notes later design alterations can require planning amendments, and recommends the applicant addresses these concerns at this stage. At planning stage, Council seeks enough information to demonstrate a solution is feasible. Full engineering design and supporting calculations will be required at detailed design stage via the Condition Endorsement Process if not provided at this stage.</p>	<p>Sufficient information has been provided to demonstrate a solution is feasible.</p>

6 CONCLUSION

This stormwater management report demonstrates that the proposed development complies with the Hobart City Stormwater Management Policy for Development and the request for further information issued December 2025.

Please address any queries regarding this stormwater management plan to rmoon@areteengineering.com.au or call 0435 043 413.

Yours faithfully,



Ruben Moon
 Engineering Technician
 Arete Engineering

Reviewed and approved by:



David Morley BEng (Hons) MIEAust
 Director/Civil Engineer
 License No. 012807319

Company Insurance Details:
 Public Liability Policy No. 68U130436BPK
 Professional Indemnity Policy No. 202308-2054 BIA

APPENDIX A: MAINTENANCE PLANS

A1 STORMWATER DETENTION TANK MAINTENANCE

Activity	Frequency	Responsibility	Procedure
Outlets			
Inspect & remove any blockage of orifices	Six monthly	Owner	Remove grate & screen to inspect orifice. See plan for location of outlets
Check attachment of orifice plates to wall of chamber and/or pit (gaps less than 5 mm)	Annually	Maintenance Contractor	Remove grate and screen. Ensure plates are mounted securely, tighten fixings if required. Seal gaps as required.
Check orifice diameters are correct and retain sharp edges	Five yearly	Maintenance contractor	Compare diameter to design (see Work-as-Executed) and ensure edge is not pitted or damaged.
Inspect screen and clean	Six monthly	Owner	Remove grate(s) and screens if required to clean them.
Check attachment of screens to wall of chamber or pit	Annually	Maintenance Contractor	Remove grate(s) and screen(s). Ensure screen fixings are secure. Repair as required.
Check screen(s) for corrosion	Annually	Maintenance contractor	Remove grate(s) and examine screen(s) for rust or corrosion, especially at corners or welds.
Inspect walls (internal and external, if appropriate) for cracks or spalling	Annually	Maintenance contractor	Remove grate(s) to inspect internal walls. Repair as required. Clear vegetation from external walls if necessary and repair as required.
Inspect outlet sumps & remove any sediment/sludge	Six monthly	Owner	Remove grate(s) and screen(s). Remove sediment/sludge build-up and check orifices are clear.
Inspect grate(s) for damage or blockage	Six monthly	Owner	Check both sides of a grate for corrosion, (especially corners and welds) damage or blockage.
Inspect outlet pipe & remove any blockage	Six monthly	Maintenance contractor	Remove grate(s) and screen(s). Ventilate underground storage if present. Check orifices and remove any blockages in outlet pipe. Flush outlet pipe to confirm it drains freely. Check for sludge/debris on upstream side of return line.

Check step irons for corrosion	Annually	Maintenance contractor	Remove grate. Examine step irons and repair any corrosion or damage.
Detention Storage			
Inspect storage & remove any sediment/sludge	Six monthly	Owner	Remove grate(s) and screen(s) where required. Remove sediment/sludge build-up.
Inspect internal walls of storage (and external, if appropriate) for cracks, spalling or any other defects	Annually	Maintenance contractor	Remove grate(s) to inspect internal walls if required. Repair as required. Clear vegetation from internal and external walls if necessary and repair as required.
Inspect & remove any debris/litter/mulch etc blocking grates	Six monthly	Owner	Remove blockages from grate(s) and check if storage is blocked.
Inspect areas draining to the storage(s) & remove debris/mulch/ litter etc likely to block screens/grates	Six monthly	Owner	Remove debris and floatable material likely to be carried to grates.
Compare actual storage available with Work-as Executed plans.	Annually	Maintenance contractor	If volume loss is greater than 5%, arrange for reconstruction to replace the volume lost. Council to be notified of the proposal.

REQUEST FOR INFORMATION

Reference

PLANNA-HOB-2025-4785

Application Reference

PLN-HOB-2025-0610

Address

37 WILLIAM COOPER DR NEW TOWN TAS 7008

Titles

173156/28

Information Requested

#	Reason	Origin
1	Electricity Transmission Infrastructure Protection Code Please submit the written advice of the electricity entity setting out the entity's views of the proposed use or development.	
2	Electricity Transmission Infrastructure Protection Code Please submit an acoustic assessment prepared by a suitably qualified person that assesses whether the proposed use will comply with acceptable solution C4.5.1 A1 or P1 of the Code. <i>Advice: Please note that similar assessments have been conducted in the area by Noise Vibration Consulting.</i>	

Response comments

Response to Request for Further Information

Dear HCC,

Please find attached additional information including Acoustic Report for the Substation overlay requirement and TasNetworks Consent letter.

Could the application be progressed asap to advertisement. Thankyou.

Regards,

Kevin Roberts

TAS BUILDING DESIGN

P. 040882283

Documents

Version	Document Date	Document Type	Description	Prepared By
1	15 Jan 2026	Details	Acoustic Report	Mr Kevin Roberts
1	15 Jan 2026	Details	TasNetworks Consent Letter	Mr Kevin Roberts

Submitted on **23/01/2026**

16 December 2025

Mr Xavier Koumos
37 William Cooper Dr
Newtown TAS 7008

Dear Mr Koumos

RE Encroachment of TasNetworks' Electricity Infrastructure Easement at 37 William Cooper Drive, New Town Tasmania 7008; Certificate of Title Volume 173156 Folio 28.

We refer to a request from your building designer Kevin Roberts of Tas Building Design within correspondence dated 3 December 2025 regarding a request to encroach Tasmanian Networks Pty Ltd (**TasNetworks**) Electricity Infrastructure Easement (**Easement**) at 37 William Cooper Dr, New Town Tasmania 7008 (Certificate of Title Volume 173156 Folio 28) (**Property**).

The encroachment is detailed within **Attachment A** and shows the proposed deck within the TasNetworks' Easement.

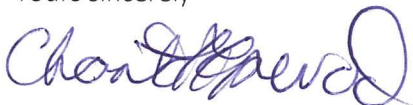
TasNetworks has considered your request and is agreeable on this occasion to allow the encroachment to remain as detailed in **Attachment A** on the basis the below conditions are complied with.

In allowing the encroachment, the landholder:

- acknowledges and unconditionally accepts that TasNetworks cannot be absolutely certain there will be no health and safety (or other) risks associated with the encroachment in the future and given this, TasNetworks reserves its rights in respect of any future claim that may be made in respect of the encroachment and considers that any future claim would be a liability that sits with the landholder;
- must at all times meet or exceed minimum statutory clearances as outlined in SAA AS7000;
- must at all times meet or exceed TasNetworks Distribution Standard Overhead Line Design Manual; and
- must at all times comply with WH&S Regulations;

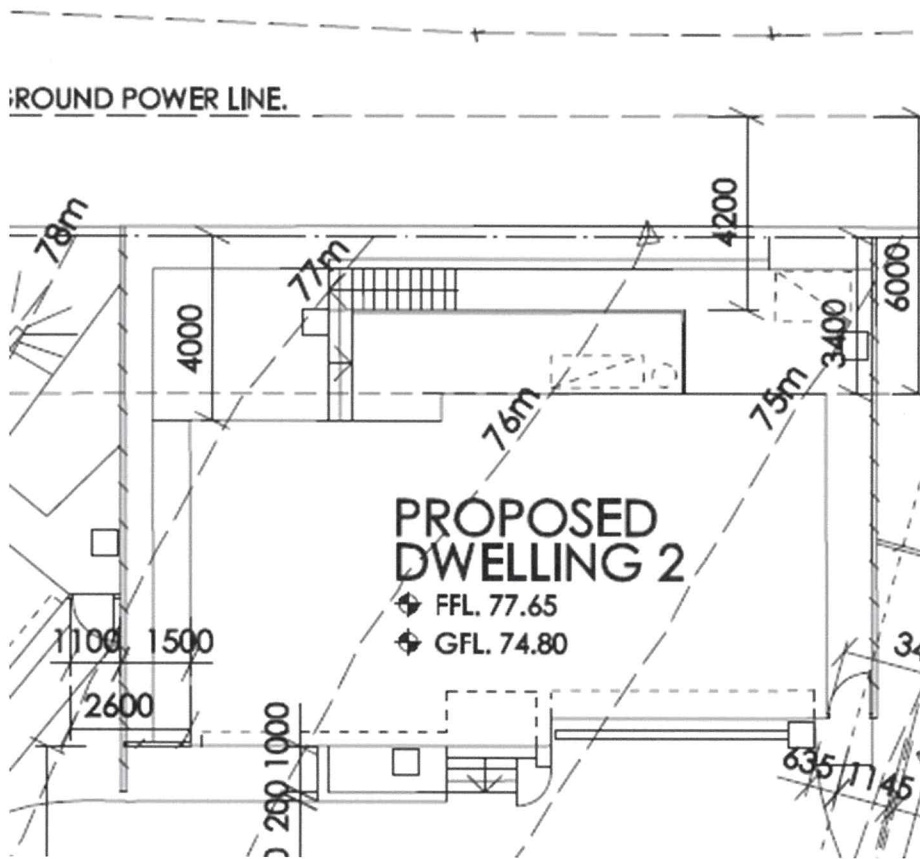
If you have any queries in relation to this letter, please do not hesitate to contact **Derryn Spinks** on **03 6324 7640** or via Email: Easement.surveys@tasnetworks.com.au

Yours sincerely



Chantal Hopwood

Executive Governance | Governance



37 William Cooper Drive

Substation Noise Impact Assessment



Ref: 1217-6 37 William Cooper Drive Substation NIA

15 January 2026

Executive Summary

A multi-residential development is proposed at 37 William Cooper Drive, New Town, with the entirety of the lot located within a substation facility buffer area under the Tasmanian Planning Scheme (the Scheme). As such, the development is required to comply with the *Electricity Transmission Infrastructure Protection Code (C4.0)* of the Scheme.

Noise measurements have been carried out on the subject site by NVC in April 2018 to quantify noise emissions from the substation, with the following key findings:

- Noise from the substation transformers were inaudible on site.
- The existing acoustic environment was controlled primarily by noise from traffic on surrounding roads, with some noise from building mechanical plant equipment audible.
 - Noise from the substation provided no meaningful contribution to the measured existing ambient (leq of 37 dBA) or background (L90 of 35 dBA) noise levels.
- Noise from the substation facility was measured to be 25 dBA_{adj} (inclusive of + 1 dB tonality adjustment).
- Predicted worst-case noise levels during the substation's peak period in July (winter) show that a worst-case noise level of 28 dBA_{adj} is expected on site.
 - Analysis of TasNetworks' Annual Substation Reports showed that there was a < 1% increase in load between 2018 and 2024, resulting in no meaningful increase in noise levels over this period.

A noise level of 28 dBA_{adj} is substantially below the 40 dBA criterion. Additionally, noise from the substation is comfortably below the measured background of 35 dBA, and thus is expected to be inaudible.

As such, the proposed multi-residential development at 37 William Cooper Drive, New Town, satisfies Clause C4.5.1-A1 of the Tasmanian Planning Scheme.

37 William Cooper Drive Substation Noise Impact Assessment

Prepared for:
TAS Building Design Pty Ltd
PO Box 2018
Howrah TAS 7018
Attention: Kevin Roberts

Prepared by:
NVC
1/95 Elizabeth Street
Hobart TAS 7000
(03) 6145 7700
jack@nvc.com.au

Document Control

Reference	Date	Author	Reviewed	Comments
1217-6 37 William Cooper Drive Substation NIA	15/01/26	J Parry	J Pitt	Issued

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1. INTRODUCTION

A multi-residential development is proposed at 37 William Cooper Drive, New Town, with the entirety of site located within a substation facility buffer area under the Tasmanian Planning Scheme (the Scheme). The development is therefore subject to the *Electricity Transmission Infrastructure Protection Code* of the Scheme. As such, NVC has been engaged by the developer to carry out a Noise Impact Assessment to determine likely compliance with the Scheme, with the methodology and findings summarised in this report completed by NVC in January 2026.

2. BACKGROUND

2.1. Site and Surroundings

The site is located at 37 William Cooper Drive, New Town, and is denoted by the solid white outline in Figure 2.1, below. The site is currently vacant and shares its southern boundary with existing residential dwellings. TasNetworks is located to the west and north of site, primarily comprising administrative buildings to the north (not shown in Figure 2.1) and a substation to the west (see Figure 2.1, below).

The whole of TasNetworks is located on a knoll, and thus is elevated compared to site. As such, the natural topographic screening created by the edge of this knoll (see dotted white line in Figure 2.1, below) results in no line of sight between the substation and site.

The blue overlay in Figure 2.1, below, shows the substation buffer area, with the entire site within this buffer area.



FIGURE 2.1: SITE AND SURROUNDING AREA

3. NOISE MEASUREMENTS

Attended noise measurements were made on site on the 10th April 2018 to quantify noise emissions from the substation. Measurements used a Svan Type 1 sound level meter, logging in A-weighted decibels with a *Fast* response time. Additionally, narrowband spectra were recorded so that substation noise could be clearly identified and differentiated from other noises.

Measurements were made on site, denoted location 1 in Figure 3.1, below.



FIGURE 3.1: MEASUREMENT LOCATION

The following observations were made during the noise measurements:

- Traffic was the main source of noise, perceived as being predominantly from Augusta Road and New Town Road.
 - Noise from the Brooker Highway was a consistent noise source and controlled the background.
- Noise from the substation transformers was inaudible at location 1.
 - Some mechanical plant noise perceived as possibly being from the administrative building was occasionally audible.
- On two occasions for only several seconds a clear 357 Hz tone was audible, perceived to be from the main TasNetworks administrative building.
- Noise due to insects and dogs was audible throughout the noise measurements.

2.2. Proposed Development

The proposed development comprises four residential dwellings, as shown in Figure 2.2, below, with a nominally 1.8 m tall timber fence located on the site's entire perimeter.

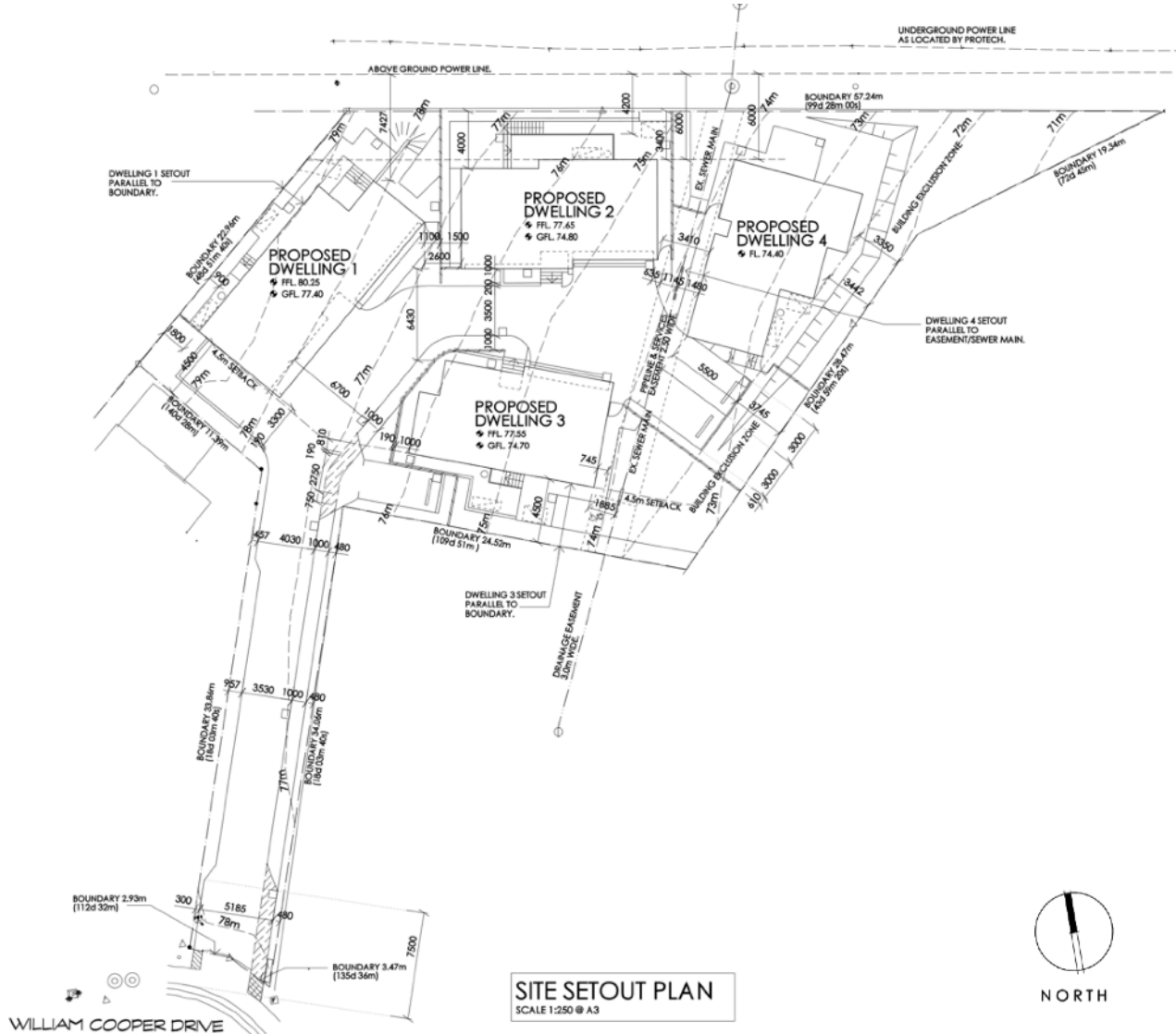


FIGURE 2.2: PROPOSED DEVELOPMENT - SITE PLAN

Analysis of the measured noise data shows that transformer noise was primarily characterised by noise in the 300 Hz range, with TasNetworks mechanical plant noise within the 290 Hz range. The remainder of the noise is a result of external noise sources (traffic, residential mechanical plant, general suburban noise, etc.). Therefore, noise from the substation in isolation can be determined and compared against the overall (Leq) and background (L90) noise levels, the results of which are presented in Table 3.1, below.

TABLE 3.1: SUMMARY OF MEASURED NOISE LEVELS

Location	Sound Pressure Level				
	TasNetworks Only			All Noise Sources	
	Transformer (dBA)	Mech Plant (dBA _{adj})	Total (dBA _{adj})	Overall (Leq) (dBA)	Background (L90) (dBA)
1	20	24	25	37	35

Table 3.1 clearly shows transformer noise to be a minor contribution that does not influence the overall noise level. This aligns with perception that the transformers were inaudible at location 1.

4. CRITERIA

The Tasmanian Planning Scheme contains, under Section C4.0, the *Electricity Transmission Infrastructure Protection Code*. Specifically relevant is Clause C4.5.1 which details criteria specific to sensitive use within a substation facility buffer area. The relevant parts of this clause have been reproduced below:

C4.5.1 Sensitive use within a substation facility buffer area

Objective:	That a sensitive use contained within a building and located within a substation facility buffer area is located and designed to not cause an unreasonable loss of amenity due to substation noise.	
Acceptable Solutions	Performance Criteria	
<p>A1 A sensitive use, excluding any non-habitable rooms, contained within a building and located within a substation facility buffer area must:</p> <p>(a) be for an existing sensitive use, provided the distance between the building and the substation facility is not reduced; or</p> <p>(b) not be exposed to substation noise emission higher than:</p> <p>(i) 55 dB(A) (LAeq) within the hours of 8.00am to 6.00pm;</p> <p>(ii) 5 dB(A) above the background (LA90) level or 40 dB(A) (LAeq), whichever is the lower, within the hours of 6.00pm to 8.00am; and</p> <p>(iii) 65 dB(A) (LAm_{ax}).</p> <p>Noise levels are to be averaged over a 15 minute interval.</p>	<p>P1 A sensitive use, excluding any non-habitable rooms, contained within a building and located within a substation facility buffer area must be appropriately located or designed to not cause unreasonable loss of amenity due to substation noise emission, having regard to:</p> <p>(a) the nature of the sensitive use;</p> <p>(b) proximity to the substation facility;</p> <p>(c) noise levels generated by the substation facility;</p> <p>(d) any existing buffers to noise impacts;</p> <p>(e) any mitigation measures proposed;</p> <p>(f) any written advice from a suitably qualified person; and</p> <p>(g) any advice from the electricity entity.</p>	

As shown above, the night time criterion is the tightest, and thus has been adopted for appropriate assessment criteria. Section 3, above, shows that the measured background (L90) on site (location 6) is nominally 35 dBA. The Scheme states the night time criterion is to be the lower of background (L90) + 5 dB (in this instance: 35 dBA + 5 dB = 40 dBA), or 40 dBA.

Therefore, the adopted project criterion is as follows:

40 dBA Leq(15-minute) Between 6PM to 8AM.

Satisfying the 40 dBA night time criterion will result in comfortable compliance with the 55 dBA day time criterion.

5. DISCUSSION

The following key points are relevant to the assessment:

- The adjusted noise level from the substation facility, as shown in Table 3.1, above, is 25 dBA_{adj}.
 - All noise levels have been assessed for tonality as required by the TAS Noise Measurement Procedures Manual¹, with a + 1 dB tonal adjustment required due to noise in the 92 and 290 Hz frequency ranges (resulting from building mechanical plant equipment, not transformers). This + 1 dB adjustment is reflected in the 25 dBA_{adj} noted above.
- Substation noise is primarily due to transformers, which have noise emissions that vary with load (primarily a result of seasonal temperature changes). The relationship between load and noise emissions is described in *Annex ZZ* of AS/NZS 60076.10:2009² (the Standard).
 - Noise measurements were carried out in April, with TasNetworks load profile information showing that the substation load during April is typically 38 MW, and typically increases to 57 MW during the July peak period. As such, using the relationship described in the Standard, an increase of nominally 3 dB is expected during peak periods compared to the measured noise levels.
 - Additionally, review of 2018 and 2024 Annual Substation Reports provided by TasNetworks³ shows that demand increased by < 1% over this duration. This results in no meaningful change in noise emissions from the substation facility.
- The substation noise levels in Table 3.1 are a combination of transformer noise and building mechanical plant noise, and thus to ensure a conservative approach, the total substation facility noise is taken to be nominally 28 dBA_{adj}.
 - The substation's noise levels are nominally 12 dB below the criterion, comfortably satisfying the criterion.
 - Noise levels from the substation are substantially below the measured background noise level, and thus the substation transformers are likely to be inaudible for the majority of the time.
- As shown in Figure 2.1, there are multiple existing residential dwellings that are nearer to the substation than site. As such, development for sensitive use on site will not impose additional constraint on the operation of the substation.

¹ 'Noise Measurement Procedures Manual', 2nd Ed., Department of Environment, Parks, Heritage and Arts, 2008

² 'AS/NZS 60076.10 - Power Transformers', Standards Australia, 2009

³ <https://www.tasnetworks.com.au/planning-and-projects/planning-our-network>

6. ASSESSMENT

Noise emissions from the substation are shown to comfortably satisfy the criterion, and are significantly below the measured background noise level. As such, noise from the substation facility is not expected to cause environmental nuisance to the proposed development at 37 William Cooper Drive, New Town.

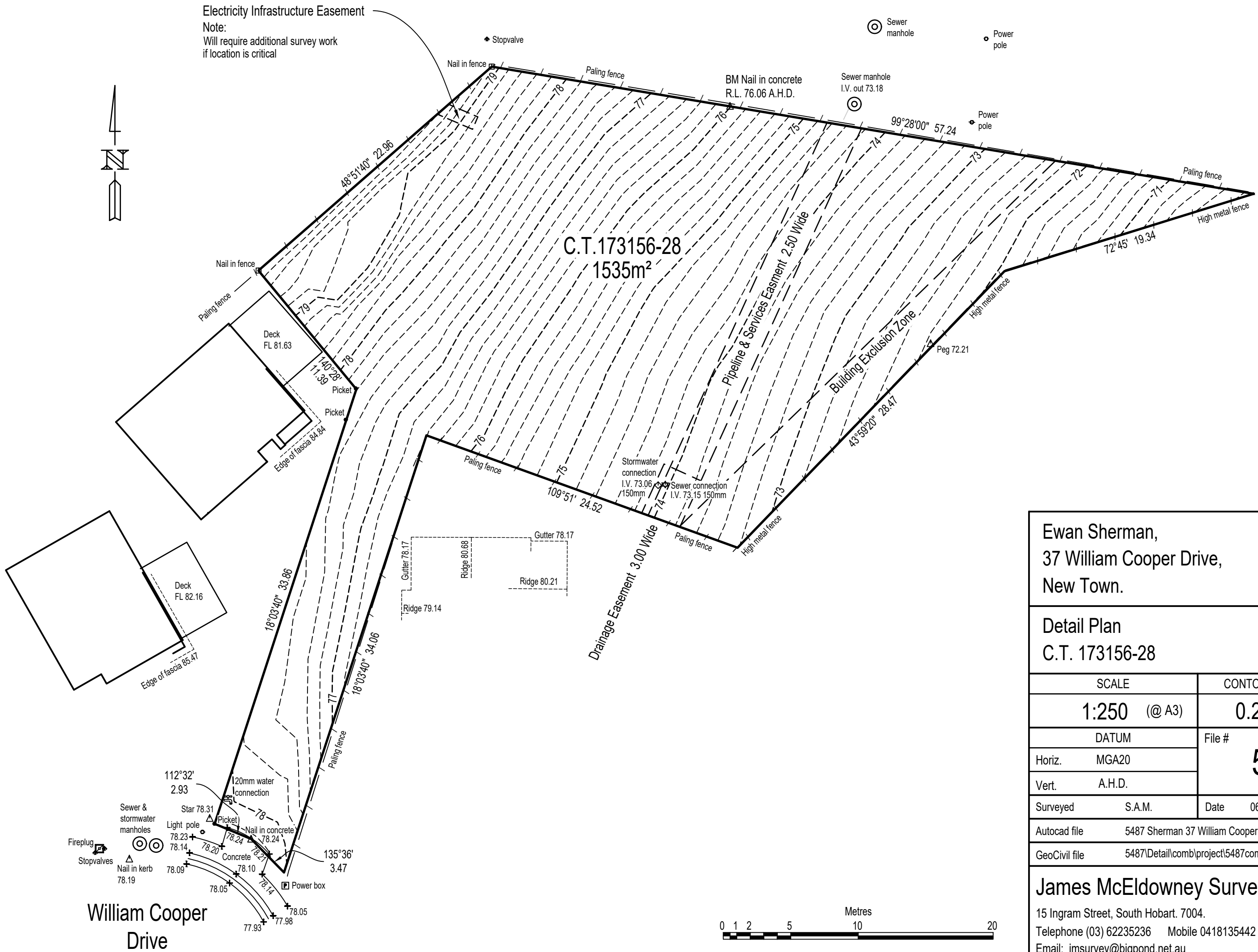
Therefore, the proposed development satisfies Clause C4.5.1-A1 of the Tasmanian Planning Scheme.

Appendix A – Acoustic Glossary

<i>Ambient Noise</i>	All noise associated with a measurement, and typically ignoring the particular noise under investigation. Typically measured as Leq and will usually comprise noise from many sources.
<i>Background Noise</i>	Background noise describes the underlying level of noise present in the ambient noise. It may be described as the average of the minimum noise levels measured, and is typically measured by the statistical L90 level.
<i>Decibel [dB]</i>	The scale used for describing sound. It is a logarithmic scale that uses a reference sound pressure of 20 µPa, or reference sound power of 10 ⁻¹² Watts.
<i>dBA</i>	A-weighted decibel. The human ear does not perform linearly and is better at hearing high frequency rather than low frequency sounds, ie. low frequency sound at the same dB level as a high frequency sound will be perceived as quieter. To replicate the human ear response a frequency weighting, denoted as an A-weighting, is applied to the sound. A sound measured in this way is then an A-weighted sound pressure level with units dBA. Practically all noise is measured using the A-weighting.
<i>Leq</i>	Energy averaged sound pressure level over a period of time, usually 10 to 15 minutes. Units of decibels, typically A weighted (LAeq). Because the decibel scale is a logarithmic ratio, the higher noise levels have far more sound energy, and therefore the Leq level tends to indicate an average which is strongly influenced by short-term, high level noise events. Many studies show that human reaction to level-varying sounds tends to relate closer to the LAeq noise level than any other descriptor.
<i>Frequency</i>	Frequency is synonymous with pitch and has the units of Hertz (Hz) or cycles per second. A bass drum produces a low frequency sound, and a small bell a high frequency sound. The frequency range for human hearing is approximately 30Hz to 16kHz.
<i>L10, L90...</i>	Ln is the sound pressure level that is exceeded for n% of the time. Hence the L10 describes the noisier events during the interval, and L90 the quieter events. The L90 is often used to describe the background level. A significant variation between the L10 and L90 would indicate an environment where there is a strong variation in noise levels, and the background is not the dominant source. As the variation between the L10 and L90 decreases, the background becomes a more dominant.
<i>Lmax</i>	The instantaneous maximum level using the time response and frequency weighting set for the meter (typically Fast response, A weighted).
<i>Inversion</i>	A condition typically occurring on clear, still nights which is characterised by the air near the ground being colder than air at higher altitudes. The increasing speed of sound with altitude bends the sound back towards the ground causing a focussing of the sound in a small area. The inversion effect can cause increases in noise levels of 5 to 10 dB with greater increases in exceptional circumstances.

Electricity Infrastructure Easement

Note:
Will require additional survey work
if location is critical

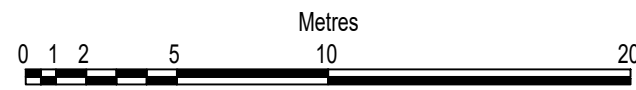


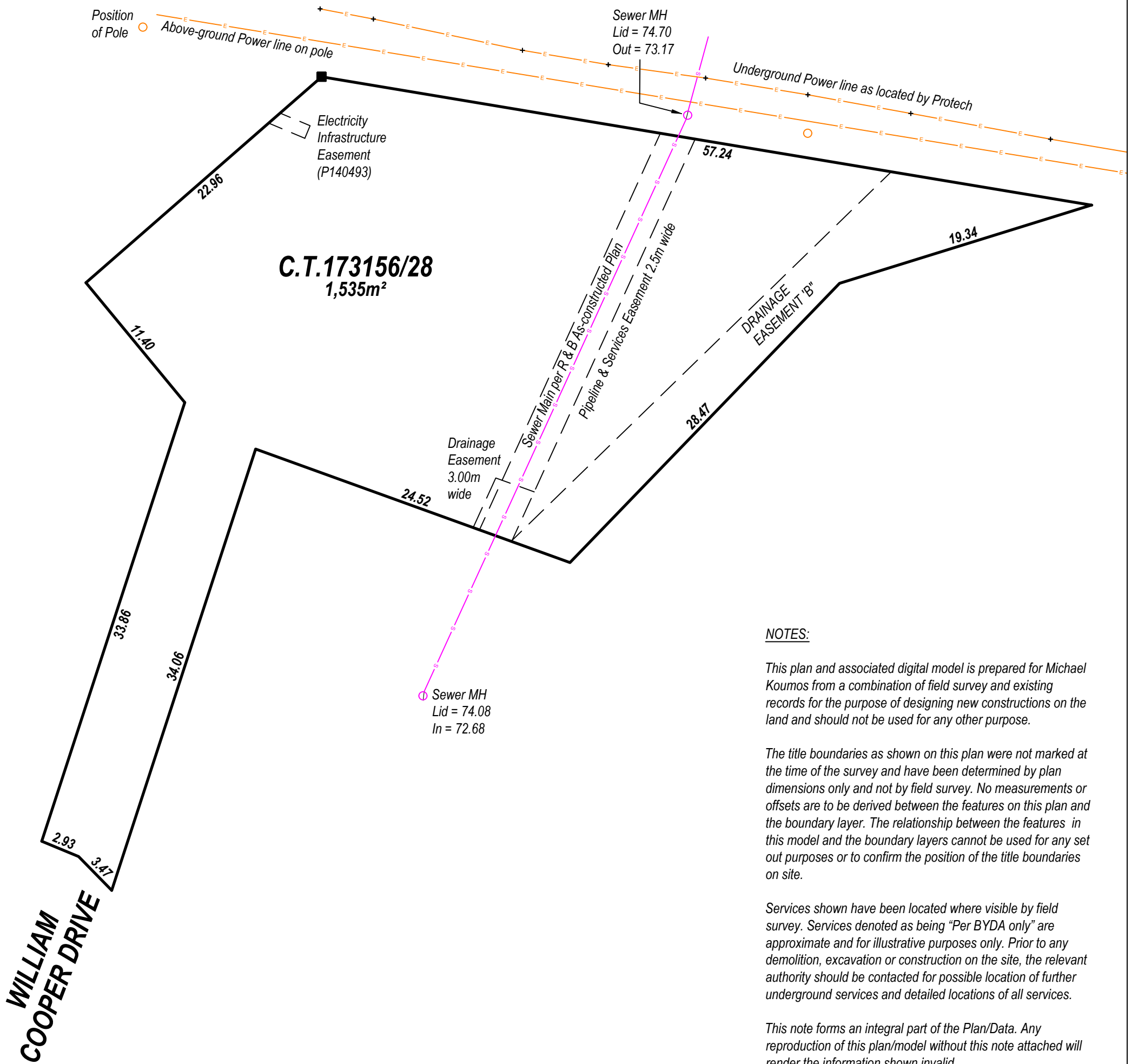
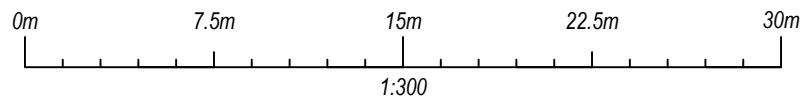
Ewan Sherman,
37 William Cooper Drive,
New Town.

Detail Plan
C.T. 173156-28

SCALE		CONTOUR INTERVAL	
1:250 (@ A3)		0.2 metre	
DATUM		File #	
Horiz.	MGA20	5487	
Vert.	A.H.D.		
Surveyed	S.A.M.	Date	06-05-21 & 09-02-2024
Autocad file	5487 Sherman 37 William Cooper Drive		
GeoCivil file	5487\Detail\comb\project\5487\comb		

James McEldowney Surveying
15 Ingram Street, South Hobart. 7004.
Telephone (03) 62235236 Mobile 0418135442
Email: jmsurvey@bigpond.net.au





NOTES:

This plan and associated digital model is prepared for Michael Koumos from a combination of field survey and existing records for the purpose of designing new constructions on the land and should not be used for any other purpose.

The title boundaries as shown on this plan were not marked at the time of the survey and have been determined by plan dimensions only and not by field survey. No measurements or offsets are to be derived between the features on this plan and the boundary layer. The relationship between the features in this model and the boundary layers cannot be used for any set out purposes or to confirm the position of the title boundaries on site.

Services shown have been located where visible by field survey. Services denoted as being "Per BYDA only" are approximate and for illustrative purposes only. Prior to any demolition, excavation or construction on the site, the relevant authority should be contacted for possible location of further underground services and detailed locations of all services.

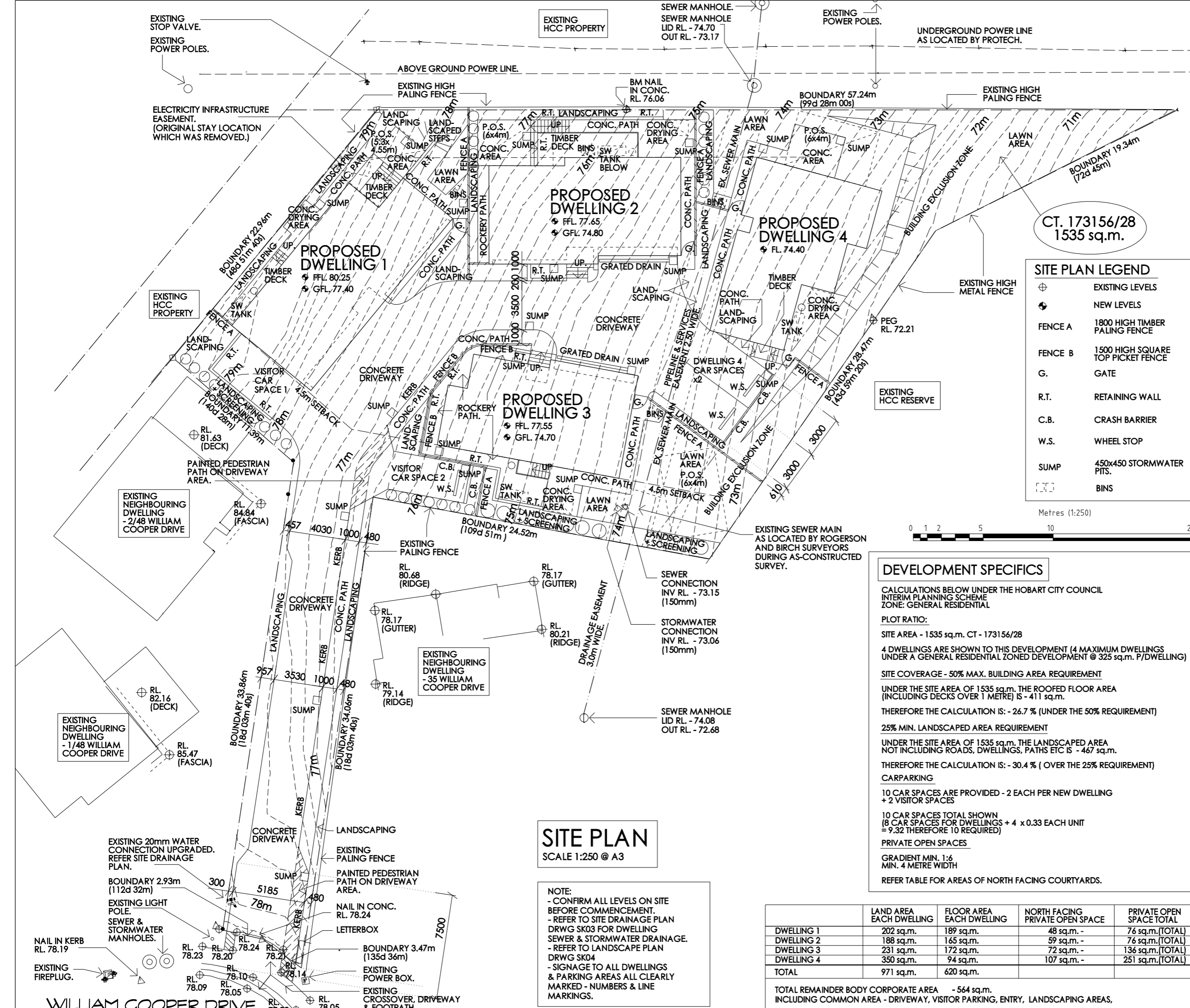
This note forms an integral part of the Plan/Data. Any reproduction of this plan/model without this note attached will render the information shown invalid.

HORIZONTAL DATUM is GDA2020, Coordinates are Plane Coordinate Origin: TBM Nail from McEldowney Detail E 523909.496 N 5254659.996 PER RTK GPS

E				
D				
C				
B				
A				
REV	AMENDMENTS	DRAWN	DATE	APPR.

Contour & Detail Plan
FOR: Michael Koumos
LOCATION: 37 William Cooper Drive, New Town

Date:	Contour interval:	Reference:
18-08-25	N/A	KOUMM06 1626801
Drawn:	Scale:	Bearing Datum: MGA2020 per
CF	1:300 (A3)	McEldowney Detail
Approved:	Title Reference:	Vertical Datum:
CF	C.T.173156/28	AHD83 per McEldowney Detail



CT. 173156/28
1535 sq.m.

SITE PLAN LEGEND

⊕	EXISTING LEVELS
⊕	NEW LEVELS
FENCE A	1800 HIGH TIMBER PALING FENCE
FENCE B	1500 HIGH SQUARE TOP PICKET FENCE
G.	GATE
R.T.	RETAINING WALL
C.B.	CRASH BARRIER
W.S.	WHEEL STOP
SUMP	450x450 STORMWATER PITS.
BINS	BINS



DEVELOPMENT SPECIFICS

CALCULATIONS BELOW UNDER THE HOBART CITY COUNCIL INTERIM PLANNING SCHEME
ZONE: GENERAL RESIDENTIAL

PLOT RATIO:
SITE AREA - 1535 sq.m. CT - 173156/28
4 DWELLINGS ARE SHOWN TO THIS DEVELOPMENT (4 MAXIMUM DWELLINGS UNDER A GENERAL RESIDENTIAL ZONED DEVELOPMENT @ 325 sq.m. P/DWELLING)

SITE COVERAGE - 50% MAX. BUILDING AREA REQUIREMENT
UNDER THE SITE AREA OF 1535 sq.m. THE ROOFED FLOOR AREA (INCLUDING DECKS OVER 1 METRE) IS - 411 sq.m.
THEREFORE THE CALCULATION IS: - 26.7% (UNDER THE 50% REQUIREMENT)

25% MIN. LANDSCAPED AREA REQUIREMENT
UNDER THE SITE AREA OF 1535 sq.m. THE LANDSCAPED AREA NOT INCLUDING ROADS, DWELLINGS, PATHS ETC IS - 467 sq.m.
THEREFORE THE CALCULATION IS: - 30.4% (OVER THE 25% REQUIREMENT)

CARPARKING
10 CAR SPACES ARE PROVIDED - 2 EACH PER NEW DWELLING + 2 VISITOR SPACES
10 CAR SPACES TOTAL SHOWN (8 CAR SPACES FOR DWELLINGS + 4 x 0.33 EACH UNIT = 9.32 THEREFORE 10 REQUIRED)

PRIVATE OPEN SPACES
GRADIENT MIN. 1:6
MIN. 4 METRE WIDTH
REFER TABLE FOR AREAS OF NORTH FACING COURTYARDS.

	LAND AREA EACH DWELLING	FLOOR AREA EACH DWELLING	NORTH FACING PRIVATE OPEN SPACE	PRIVATE OPEN SPACE TOTAL
DWELLING 1	202 sq.m.	189 sq.m.	48 sq.m. -	76 sq.m. (TOTAL)
DWELLING 2	188 sq.m.	165 sq.m.	59 sq.m. -	76 sq.m. (TOTAL)
DWELLING 3	231 sq.m.	172 sq.m.	72 sq.m. -	136 sq.m. (TOTAL)
DWELLING 4	350 sq.m.	94 sq.m.	107 sq.m. -	251 sq.m. (TOTAL)
TOTAL	971 sq.m.	620 sq.m.		

TOTAL REMAINDER BODY CORPORATE AREA - 564 sq.m.
INCLUDING COMMON AREA - DRIVEWAY, VISITOR PARKING, ENTRY, LANDSCAPING AREAS.

SITE PLAN
SCALE 1:250 @ A3

NOTE:
- CONFIRM ALL LEVELS ON SITE BEFORE COMMENCEMENT.
- REFER TO SITE DRAINAGE PLAN DRWG SK03 FOR DWELLING SEWER & STORMWATER DRAINAGE.
- REFER TO LANDSCAPE PLAN DRWG SK04
- SIGNAGE TO ALL DWELLINGS & PARKING AREAS ALL CLEARLY MARKED - NUMBERS & LINE MARKINGS.

creative practical design solutions

TAS BUILDING DESIGN PTY. LTD.

Kevin J. Roberts

m. 0408 882283
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cc 652

residential commercial

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Project and Address:
Proposed 4 No. Dwellings at 37 William Cooper Drive, New Town.

Client:
MK Constructions

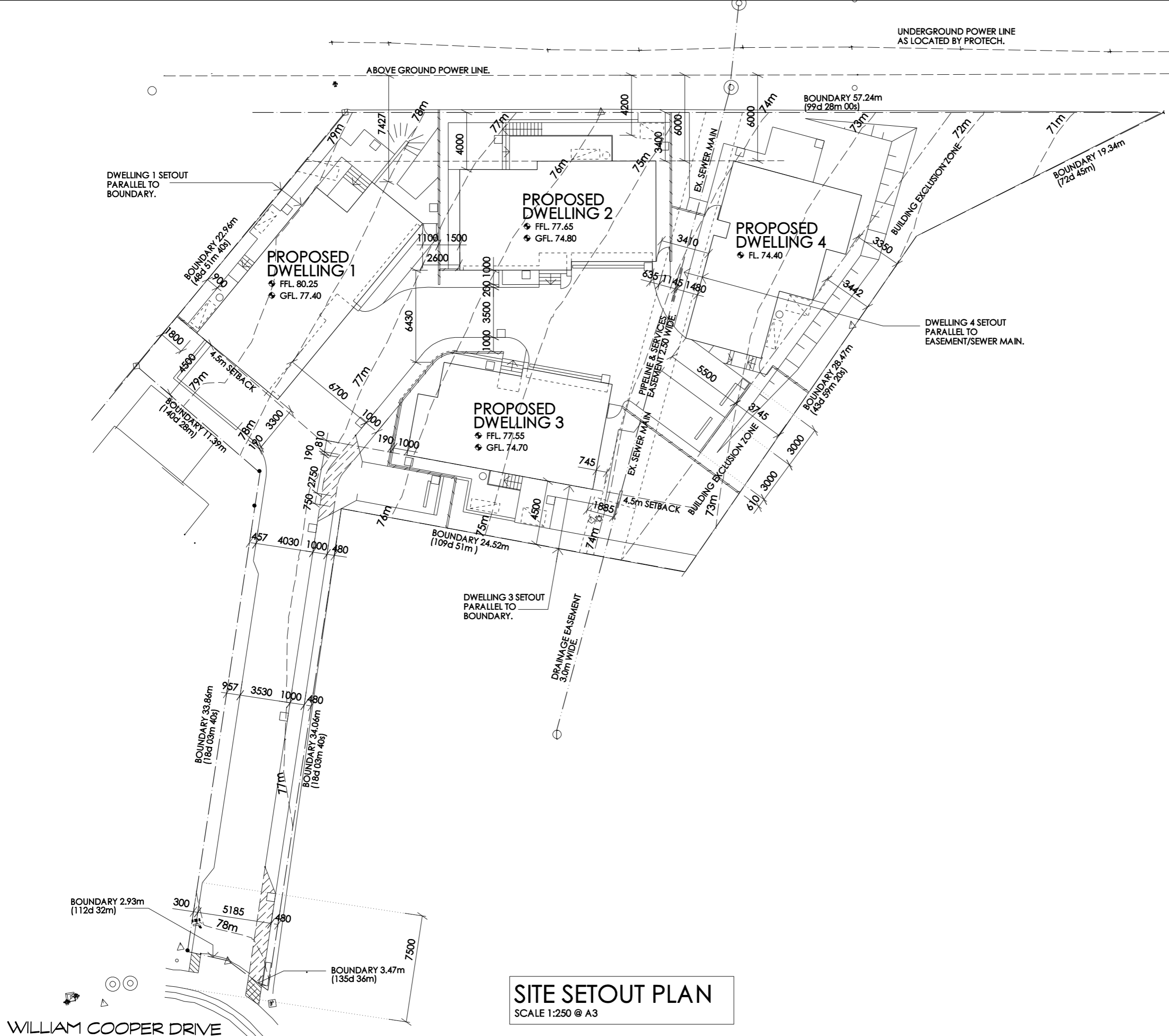
Drawing Title:
Site Plan

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

Plot Date: 19/11/2025 Drawn: KJR
Approval: preliminary Scale: 1:250 @ A3

Project No.: **25.18**
Drawing No.: **sk01** Rev. -

WILLIAM COOPER DRIVE



SITE SETOUT PLAN
SCALE 1:250 @ A3

WILLIAM COOPER DRIVE

UNDERGROUND POWER LINE
AS LOCATED BY PROTECH.

ABOVE GROUND POWER LINE.

DWELLING 1 SETOUT
PARALLEL TO
BOUNDARY.

PROPOSED DWELLING 1
◆ FFL. 80.25
◆ GFL. 77.40

PROPOSED DWELLING 2
◆ FFL. 77.65
◆ GFL. 74.80

PROPOSED DWELLING 3
◆ FFL. 77.55
◆ GFL. 74.70

PROPOSED DWELLING 4
◆ FL. 74.40

DWELLING 4 SETOUT
PARALLEL TO
EASEMENT/SEWER MAIN.

DWELLING 3 SETOUT
PARALLEL TO
BOUNDARY.

DRAINAGE EASEMENT
3.0m WIDE

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Project and Address:
**Proposed 4 No. Dwellings
at 37 William Cooper Drive,
New Town.**

Client:
MK Constructions

Drawing Title:
Site Setout Plan

Please note: - Verify all dimensions on site. Figured
dimensions take precedence over scale readings.

Plot Date: 19/11/2025 Drawn: KJR

Approval: preliminary Scale: 1:250 @ A3



Project No.:
25.18

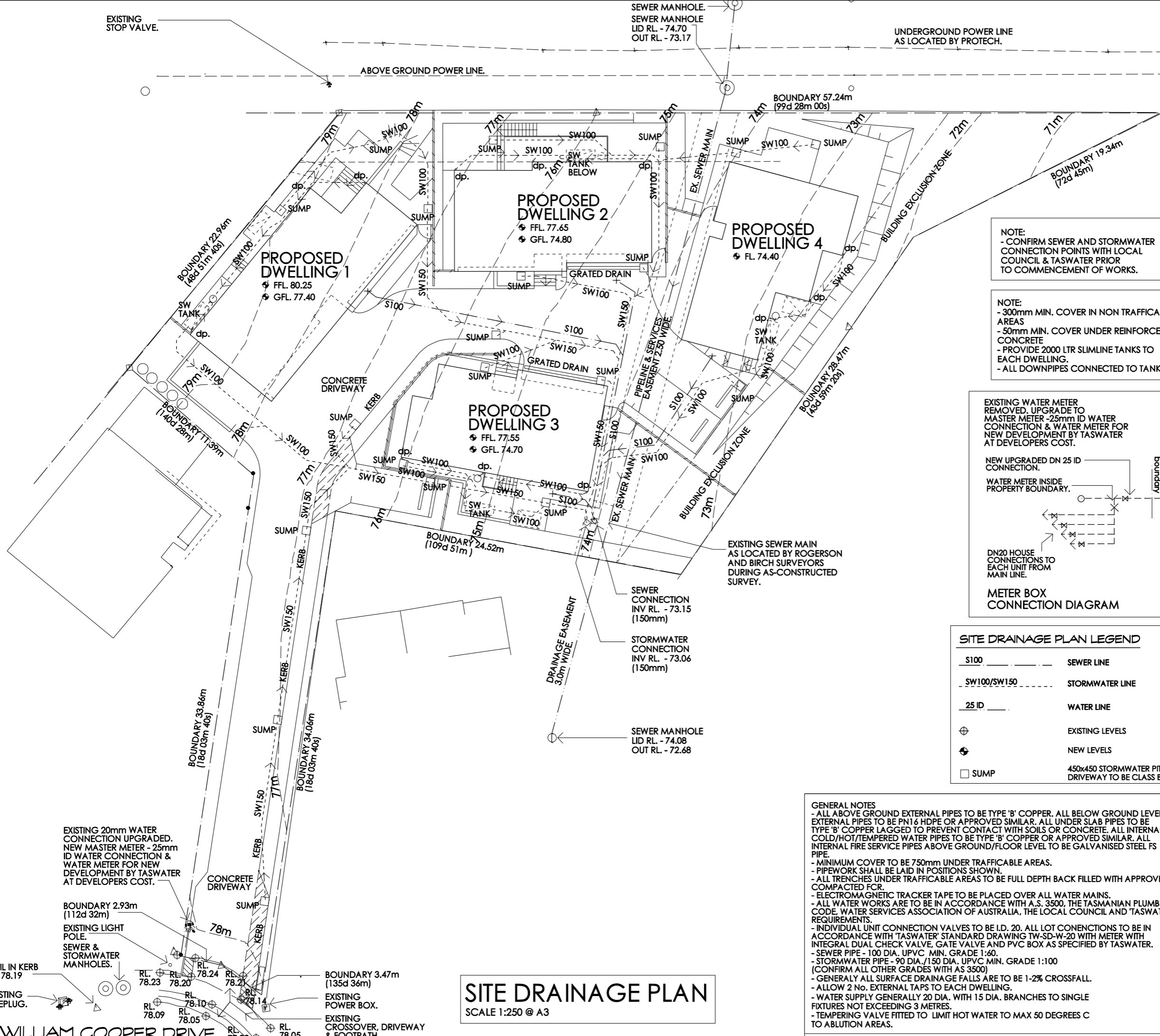
Drawing No.: sk02 Rev. -

EXISTING STOP VALVE.

SEWER MANHOLE.
SEWER MANHOLE
LID RL. - 74.70
OUT RL. - 73.17

UNDERGROUND POWER LINE
AS LOCATED BY PROTECH.

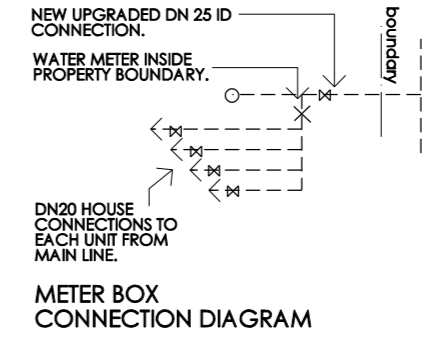
ABOVE GROUND POWER LINE.



NOTE:
- CONFIRM SEWER AND STORMWATER CONNECTION POINTS WITH LOCAL COUNCIL & TASWATER PRIOR TO COMMENCEMENT OF WORKS.

NOTE:
- 300mm MIN. COVER IN NON TRAFFICABLE AREAS
- 50mm MIN. COVER UNDER REINFORCED CONCRETE
- PROVIDE 2000 LTR SLIMLINE TANKS TO EACH DWELLING.
- ALL DOWNPIPES CONNECTED TO TANKS.

EXISTING WATER METER REMOVED. UPGRADE TO MASTER METER - 25mm ID WATER CONNECTION & WATER METER FOR NEW DEVELOPMENT BY TASWATER AT DEVELOPERS COST.



SITE DRAINAGE PLAN LEGEND

S100	SEWER LINE
SW100/SW150	STORMWATER LINE
25 ID	WATER LINE
⊕	EXISTING LEVELS
⊕	NEW LEVELS
□	450x450 STORMWATER PITS. DRIVEWAY TO BE CLASS B.

GENERAL NOTES
 - ALL ABOVE GROUND EXTERNAL PIPES TO BE TYPE 'B' COPPER. ALL BELOW GROUND LEVEL EXTERNAL PIPES TO BE PN16 HDPE OR APPROVED SIMILAR. ALL UNDER SLAB PIPES TO BE TYPE 'B' COPPER LAGGED TO PREVENT CONTACT WITH SOILS OR CONCRETE. ALL INTERNAL COLD/HOT/TEMPERED WATER PIPES TO BE TYPE 'B' COPPER OR APPROVED SIMILAR. ALL INTERNAL FIRE SERVICE PIPES ABOVE GROUND/FLOOR LEVEL TO BE GALVANISED STEEL FS PIPE.
 - MINIMUM COVER TO BE 750mm UNDER TRAFFICABLE AREAS.
 - PIPEWORK SHALL BE LAID IN POSITIONS SHOWN.
 - ALL TRENCHES UNDER TRAFFICABLE AREAS TO BE FULL DEPTH BACK FILLED WITH APPROVED COMPACTED FCR.
 - ELECTROMAGNETIC TRACKER TAPE TO BE PLACED OVER ALL WATER MAINS.
 - ALL WATER WORKS ARE TO BE IN ACCORDANCE WITH A.S. 3500, THE TASMANIAN PLUMBING CODE, WATER SERVICES ASSOCIATION OF AUSTRALIA, THE LOCAL COUNCIL AND TASWATER REQUIREMENTS.
 - INDIVIDUAL UNIT CONNECTION VALVES TO BE I.D. 20. ALL LOT CONNECTIONS TO BE IN ACCORDANCE WITH TASWATER STANDARD DRAWING TW-SD-W-20 WITH METER WITH INTEGRAL DUAL CHECK VALVE, GATE VALVE AND PVC BOX AS SPECIFIED BY TASWATER.
 - SEWER PIPE - 100 DIA. UPVC. MIN. GRADE 1:60.
 - STORMWATER PIPE - 90 DIA./150 DIA. UPVC. MIN. GRADE 1:100 (CONFIRM ALL OTHER GRADES WITH AS 3500)
 - GENERALLY ALL SURFACE DRAINAGE FALLS ARE TO BE 1-2% CROSSFALL.
 - ALLOW 2 No. EXTERNAL TAPS TO EACH DWELLING.
 - WATER SUPPLY GENERALLY 20 DIA. WITH 15 DIA. BRANCHES TO SINGLE FIXTURES NOT EXCEEDING 3 METRES.
 - TEMPERING VALVE FITTED TO LIMIT HOT WATER TO MAX 50 DEGREES C TO ABLUTION AREAS.

SITE DRAINAGE PLAN
SCALE 1:250 @ A3

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Project and Address:
Proposed 4 No. Dwellings at 37 William Cooper Drive, New Town.

Client:
MK Constructions

Drawing Title:
Site Drainage Plan

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

Plot Date: 19/11/2025 Drawn: KJR

Approval: preliminary Scale: 1:250 @ A3

Project No.:
25.18

Drawing No.: sk03 Rev. -

NORTH

WILLIAM COOPER DRIVE

EXISTING 20mm WATER CONNECTION UPGRADED. NEW MASTER METER - 25mm ID WATER CONNECTION & WATER METER FOR NEW DEVELOPMENT BY TASWATER AT DEVELOPERS COST.

BOUNDARY 2.93m (112d 32m)
EXISTING LIGHT POLE.
SEWER & STORMWATER MANHOLES.

NAIL IN KERB RL. 78.19
EXISTING FIREPLUG.

CONCRETE DRIVEWAY

BOUNDARY 3.47m (135d 36m)
EXISTING POWER BOX.
EXISTING CROSSOVER, DRIVEWAY & FOOTPATH.

EXISTING SEWER MAIN AS LOCATED BY ROGERSON AND BIRCH SURVEYORS DURING AS-CONSTRUCTED SURVEY.

SEWER CONNECTION INV RL. - 73.15 (150mm)

STORMWATER CONNECTION INV RL. - 73.06 (150mm)

SEWER MANHOLE LID RL. - 74.08 OUT RL. - 72.68

DRAINAGE EASEMENT 3.0m WIDE

EXISTING SEWER MAIN AS LOCATED BY ROGERSON AND BIRCH SURVEYORS DURING AS-CONSTRUCTED SURVEY.

SEWER CONNECTION INV RL. - 73.15 (150mm)

STORMWATER CONNECTION INV RL. - 73.06 (150mm)

SEWER MANHOLE LID RL. - 74.08 OUT RL. - 72.68

DRAINAGE EASEMENT 3.0m WIDE

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DRAINAGE EASEMENT 3.0m WIDE

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DRAINAGE EASEMENT 3.0m WIDE

EXISTING SEWER MAIN AS LOCATED BY ROGERSON AND BIRCH SURVEYORS DURING AS-CONSTRUCTED SURVEY.

SEWER CONNECTION INV RL. - 73.15 (150mm)

STORMWATER CONNECTION INV RL. - 73.06 (150mm)

SEWER MANHOLE LID RL. - 74.08 OUT RL. - 72.68

DRAINAGE EASEMENT 3.0m WIDE

EXISTING SEWER MAIN AS LOCATED BY ROGERSON AND BIRCH SURVEYORS DURING AS-CONSTRUCTED SURVEY.

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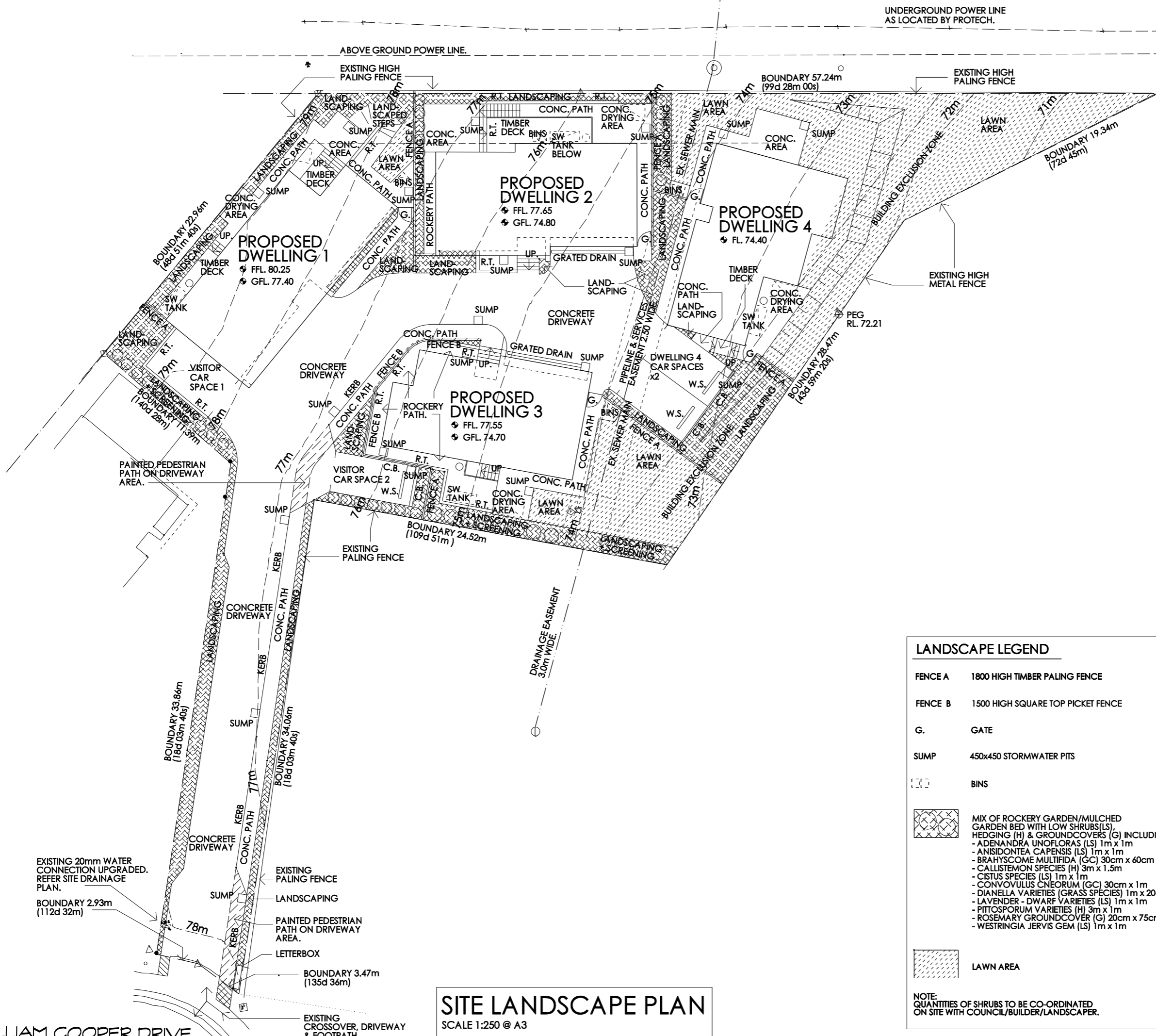
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DRAINAGE EASEMENT 3.0m WIDE



SITE LANDSCAPE PLAN
SCALE 1:250 @ A3

LANDSCAPE LEGEND

FENCE A	1800 HIGH TIMBER PALING FENCE
FENCE B	1500 HIGH SQUARE TOP PICKET FENCE
G.	GATE
SUMP	450x450 STORMWATER PITS
[Symbol]	BINS
[Symbol]	MIX OF ROCKERY GARDEN/MULCHED GARDEN BED WITH LOW SHRUBS(LS), HEDGING (H) & GROUNDCOVERS (G) INCLUDING: - ADENANDRA UNIFLORAS (LS) 1m x 1m - ANISIDONTEA CAPENSIS (LS) 1m x 1m - BRAHYSCOME MULTIFIDA (GC) 30cm x 60cm - CALLISTEMON SPECIES (H) 3m x 1.5m - CISTUS SPECIES (LS) 1m x 1m - CONVIVULUS CNEORUM (GC) 30cm x 1m - DIANELLA VARIETIES (GRASS SPECIES) 1m x 20cm - LAVENDER - DWARF VARIETIES (LS) 1m x 1m - PITTOSPORUM VARIETIES (H) 3m x 1m - ROSEMARY GROUNDCOVER (G) 20cm x 75cm - WESTRINGIA JERVIS GEM (LS) 1m x 1m
[Symbol]	LAWN AREA

NOTE: QUANTITIES OF SHRUBS TO BE CO-ORDINATED ON SITE WITH COUNCIL/BUILDER/LANDSCAPER.

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Project and Address:
Proposed 4 No. Dwellings at 37 William Cooper Drive, New Town.

Client:
MK Constructions

Drawing Title:
Site Landscape Plan

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

Plot Date: 19/11/2025 Drawn: KJR
 Approval: preliminary Scale: 1:250 @ A3

Project No.:
25.18
 Drawing No.: sk04 Rev. -

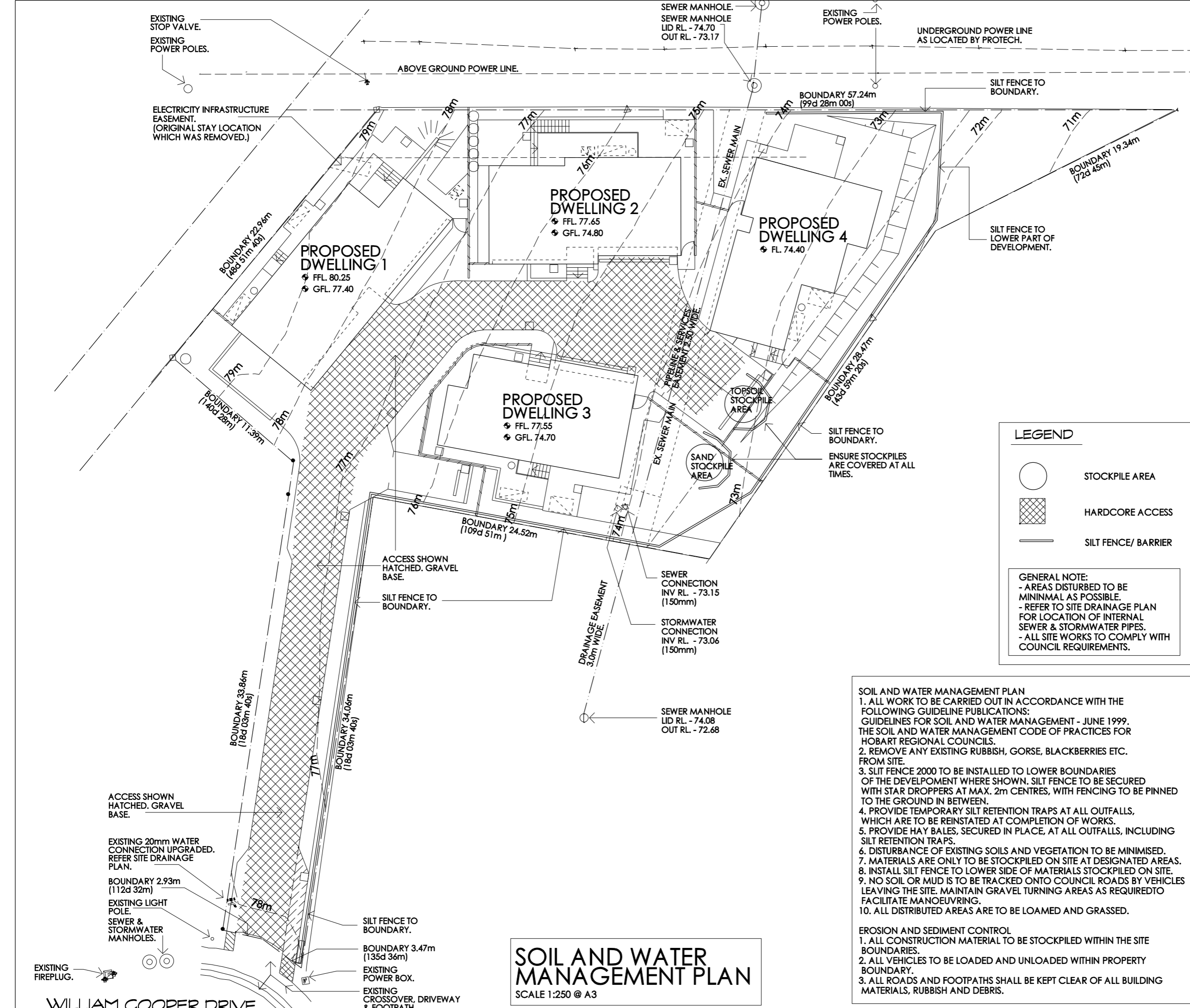


WILLIAM COOPER DRIVE

EXISTING 20mm WATER CONNECTION UPGRADED. REFER SITE DRAINAGE PLAN.

EXISTING PALING FENCE
 LANDSCAPING
 PAINTED PEDESTRIAN PATH ON DRIVEWAY AREA.
 LETTERBOX

EXISTING CROSSOVER, DRIVEWAY & FOOTPATH.



LEGEND

- STOCKPILE AREA
- HARDCORE ACCESS
- SILT FENCE/ BARRIER

GENERAL NOTE:
 - AREAS DISTURBED TO BE MINIMAL AS POSSIBLE.
 - REFER TO SITE DRAINAGE PLAN FOR LOCATION OF INTERNAL SEWER & STORMWATER PIPES.
 - ALL SITE WORKS TO COMPLY WITH COUNCIL REQUIREMENTS.

SOIL AND WATER MANAGEMENT PLAN

1. ALL WORK TO BE CARRIED OUT IN ACCORDANCE WITH THE FOLLOWING GUIDELINE PUBLICATIONS:
 GUIDELINES FOR SOIL AND WATER MANAGEMENT - JUNE 1999.
 THE SOIL AND WATER MANAGEMENT CODE OF PRACTICES FOR HOBART REGIONAL COUNCILS.

2. REMOVE ANY EXISTING RUBBISH, GORSE, BLACKBERRIES ETC. FROM SITE.

3. SILT FENCE 2000 TO BE INSTALLED TO LOWER BOUNDARIES OF THE DEVELOPMENT WHERE SHOWN. SILT FENCE TO BE SECURED WITH STAR DROPPERS AT MAX. 2m CENTRES, WITH FENCING TO BE PINNED TO THE GROUND IN BETWEEN.

4. PROVIDE TEMPORARY SILT RETENTION TRAPS AT ALL OUTFALLS, WHICH ARE TO BE REINSTATED AT COMPLETION OF WORKS.

5. PROVIDE HAY BALES, SECURED IN PLACE, AT ALL OUTFALLS, INCLUDING SILT RETENTION TRAPS.

6. DISTURBANCE OF EXISTING SOILS AND VEGETATION TO BE MINIMISED.

7. MATERIALS ARE ONLY TO BE STOCKPILED ON SITE AT DESIGNATED AREAS.

8. INSTALL SILT FENCE TO LOWER SIDE OF MATERIALS STOCKPILED ON SITE.

9. NO SOIL OR MUD IS TO BE TRACKED ONTO COUNCIL ROADS BY VEHICLES LEAVING THE SITE. MAINTAIN GRAVEL TURNING AREAS AS REQUIRED TO FACILITATE MANOEUVRING.

10. ALL DISTRIBUTED AREAS ARE TO BE LOAMED AND GRASSED.

EROSION AND SEDIMENT CONTROL

1. ALL CONSTRUCTION MATERIAL TO BE STOCKPILED WITHIN THE SITE BOUNDARIES.

2. ALL VEHICLES TO BE LOADED AND UNLOADED WITHIN PROPERTY BOUNDARY.

3. ALL ROADS AND FOOTPATHS SHALL BE KEPT CLEAR OF ALL BUILDING MATERIALS, RUBBISH AND DEBRIS.

SOIL AND WATER MANAGEMENT PLAN
 SCALE 1:250 @ A3

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Project and Address:
Proposed 4 No. Dwellings at 37 William Cooper Drive, New Town.

Client:
MK Constructions

Drawing Title:
Soil and Water Management Plan

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

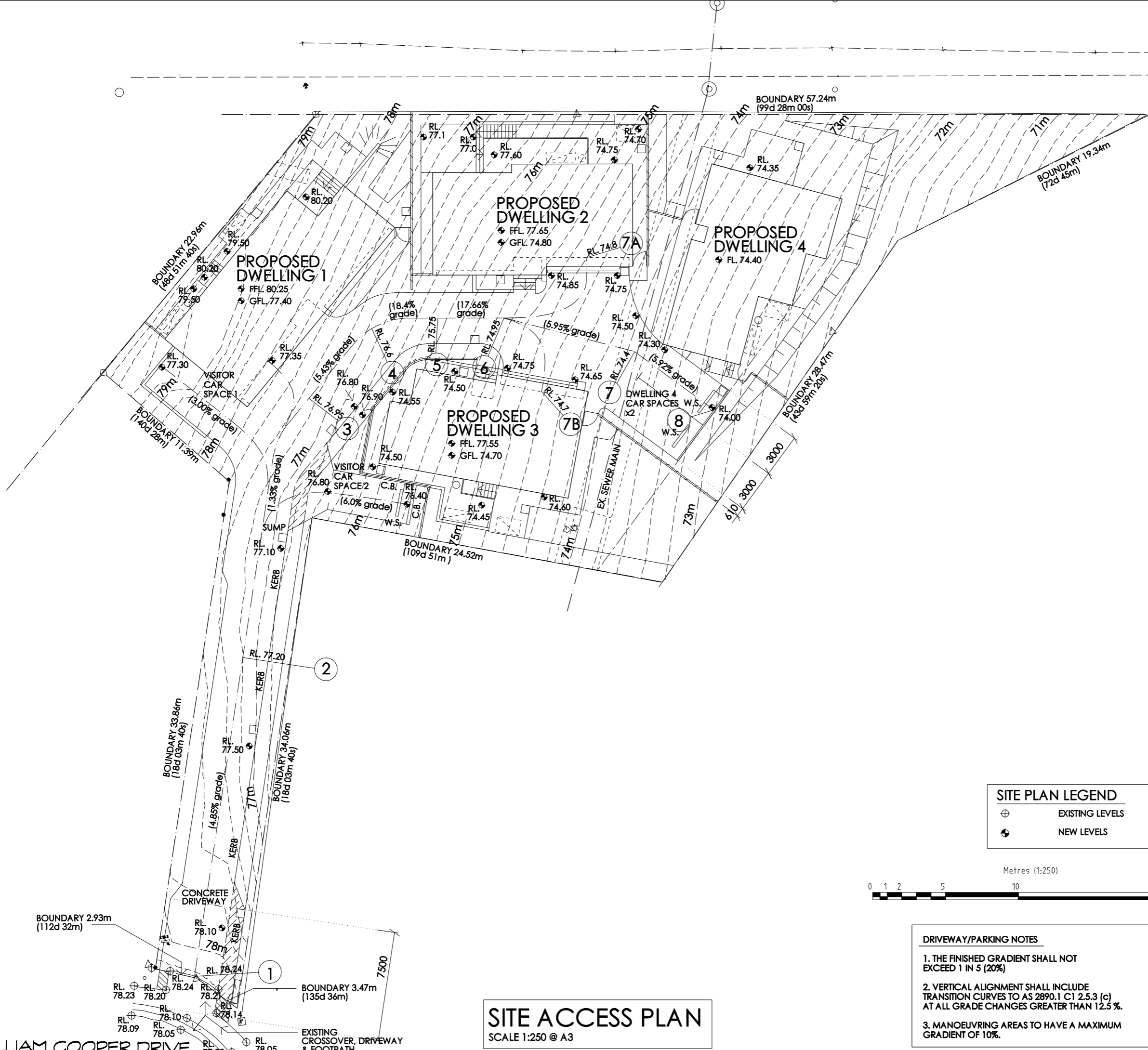
Plot Date: 19/11/2025 Drawn: KJR

Approval: preliminary Scale: 1:250 @ A3

Project No.: **25.18**

Drawing No.: **sk05** Rev. -

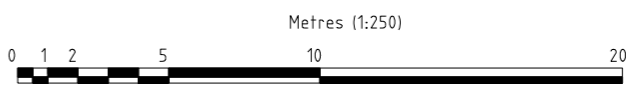
NORTH



SITE ACCESS PLAN
SCALE 1:250 @ A3

SITE PLAN LEGEND

⊕	EXISTING LEVELS
⊕	NEW LEVELS



- DRIVEWAY/PARKING NOTES**
1. THE FINISHED GRADIENT SHALL NOT EXCEED 1 IN 5 (20%)
 2. VERTICAL ALIGNMENT SHALL INCLUDE TRANSITION CURVES TO AS 2890.1 C1 2.5.3 (c) AT ALL GRADE CHANGES GREATER THAN 12.5 %.
 3. MANOEUVRING AREAS TO HAVE A MAXIMUM GRADIENT OF 10%.

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Client:
MK Constructions

Drawing Title:
Site Access Plan

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Plot Date: 19/11/2025 Drawn: KJR

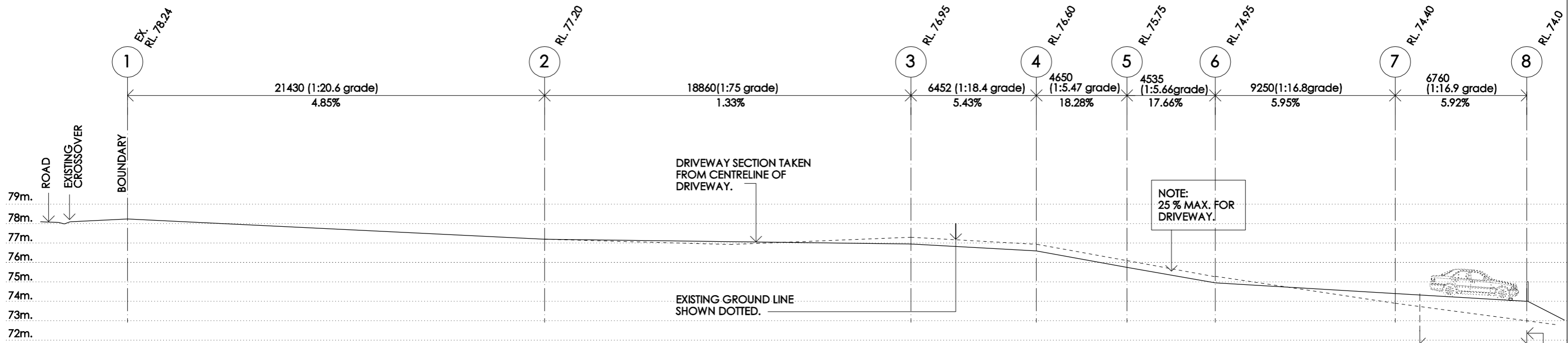
Approval: preliminary Scale: 1:250 @ A3

Project No.: **25.18**

Drawing No.: **sk06** Rev. -

NORTH

WILLIAM COOPER DRIVE



DRIVEWAY SECTION

SCALE 1:200
NOTE: - DRIVEWAY TO COMPLY WITH AS. 2890.1

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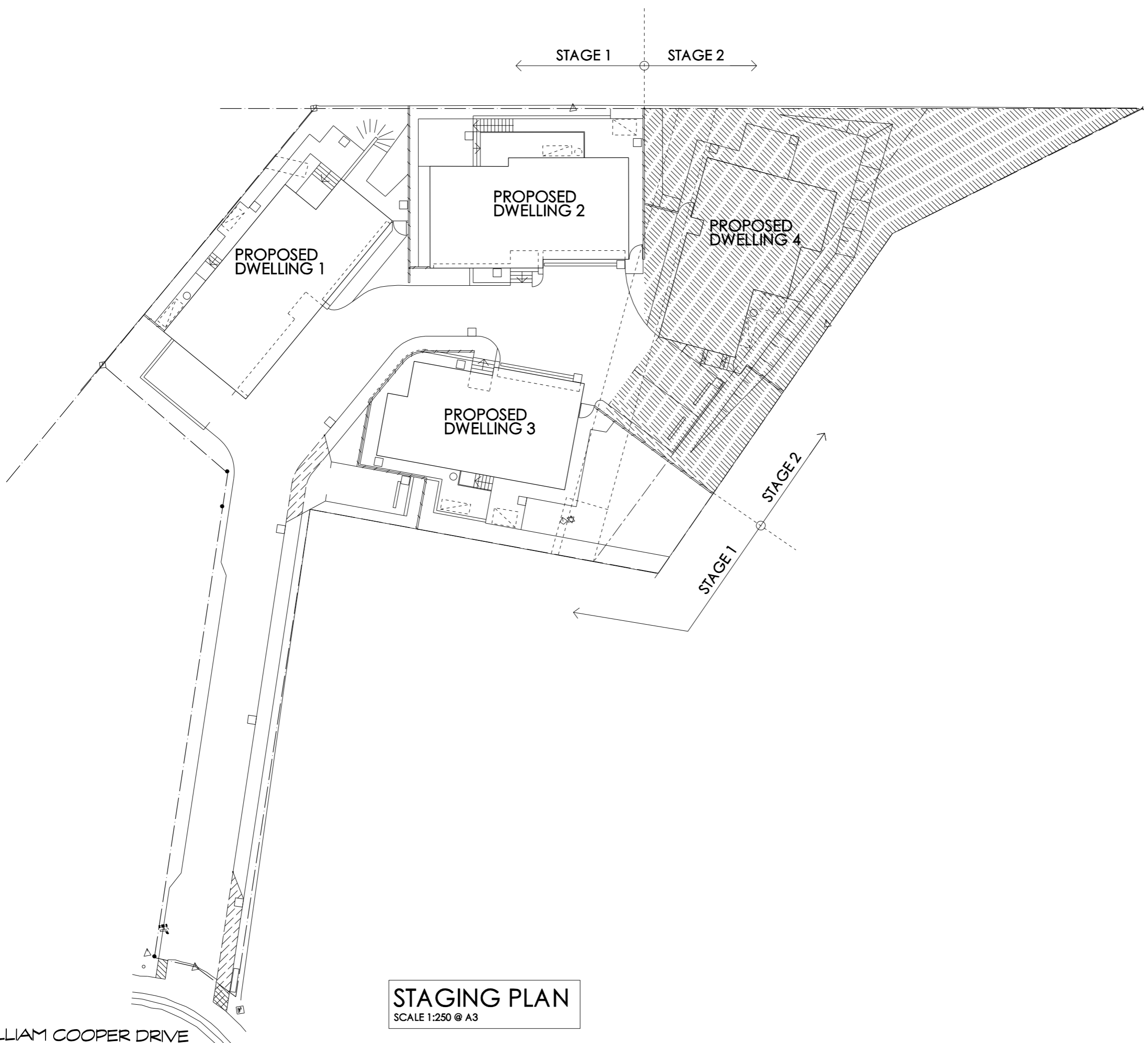
Drawing Title:
Driveway Section

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Project No.:
25.18
Drawing No.: sk07 Rev. -



STAGING PLAN
SCALE 1:250 @ A3

WILLIAM COOPER DRIVE

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Client:
MK Constructions

Drawing Title:
Site Access Plan

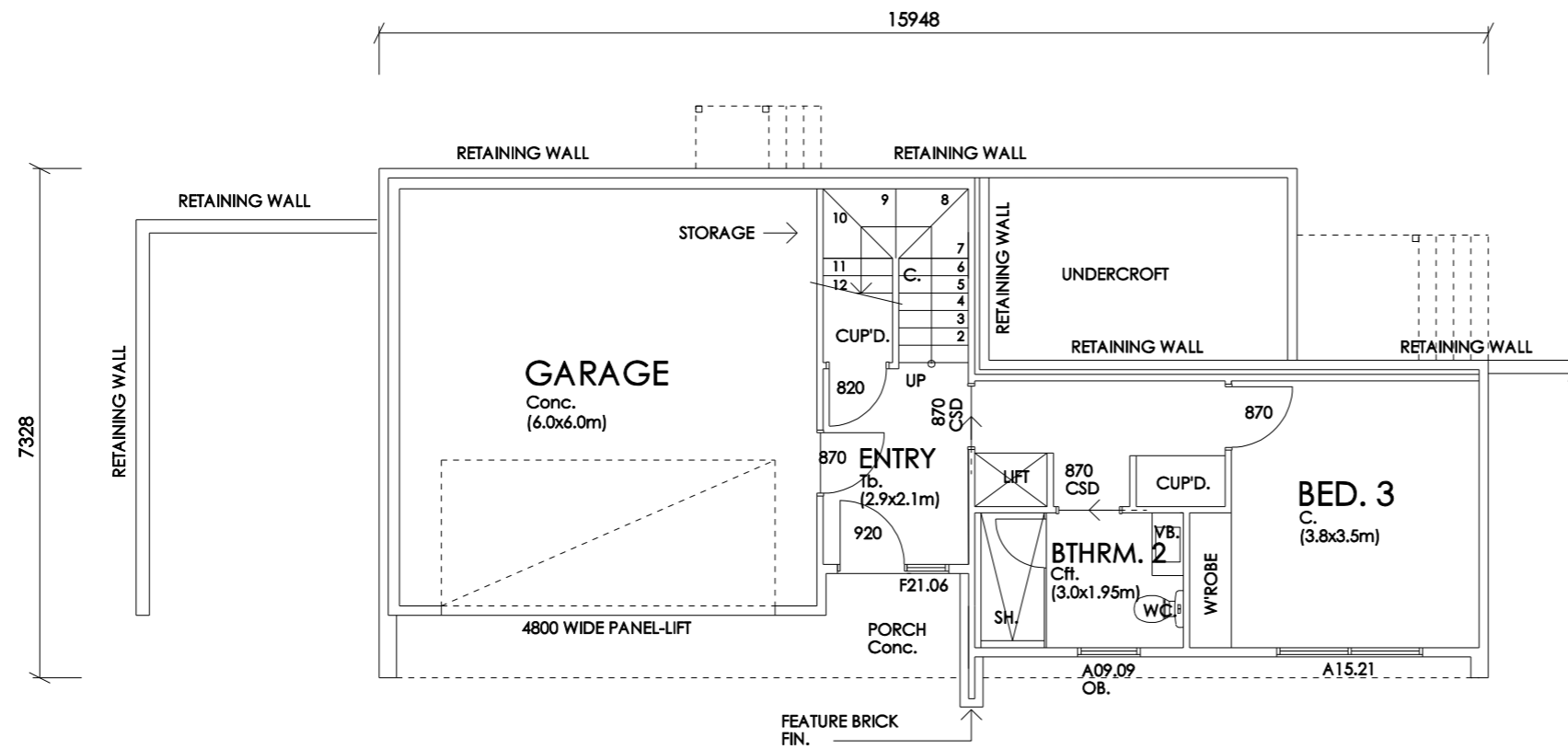
Please note: - Verify all dimensions on site. Figured
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Plot Date: 19/11/2025	Drawn: KJR
Approval: preliminary	Scale: 1:250 @ A3

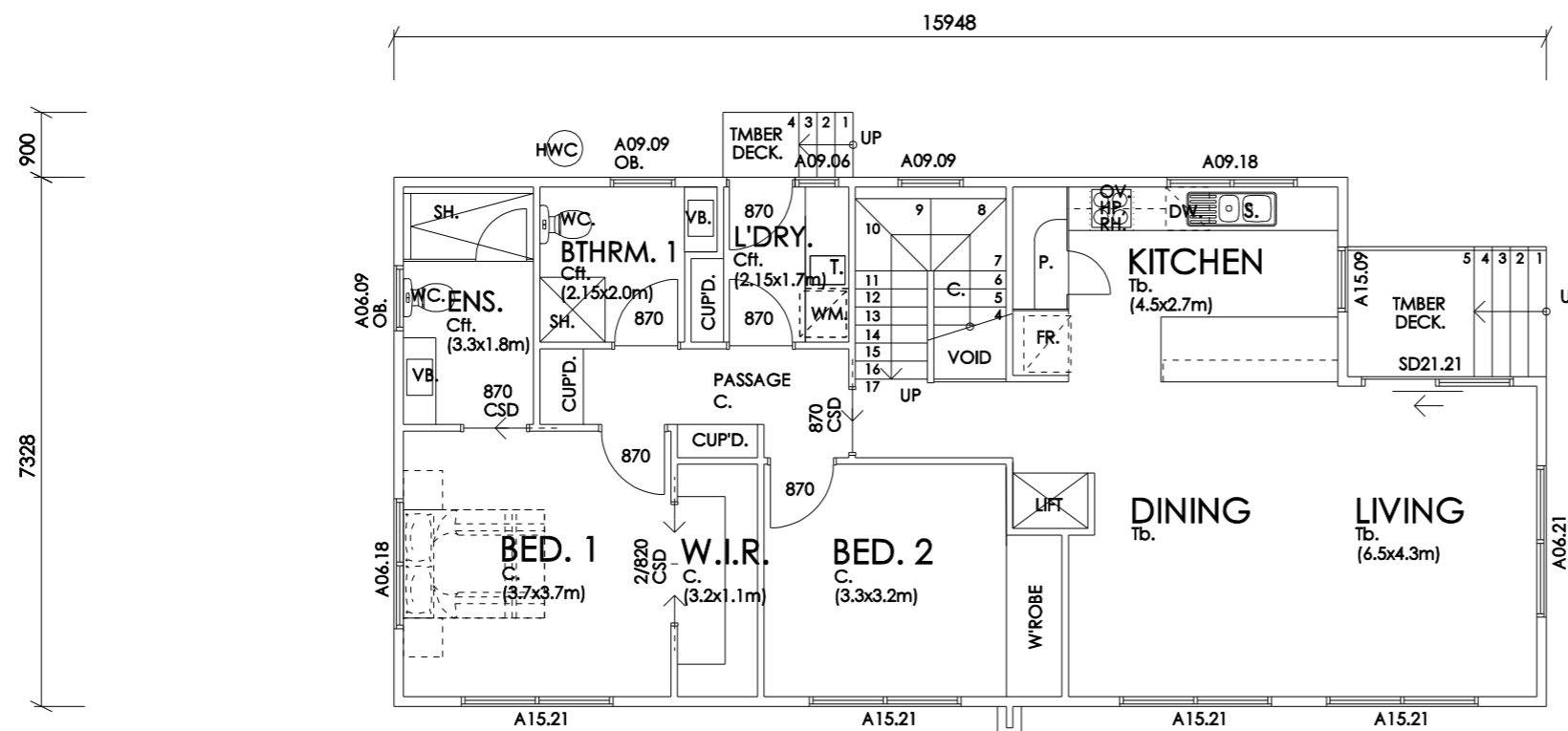


Project No.:	25.18
Drawing No.:	sk08
Rev.:	-

DWELLING 1



GROUND FLOOR PLAN - DWELLING 1
SCALE 1:100



FIRST FLOOR PLAN - DWELLING 1
SCALE 1:100

GROUND FLOOR AREA: - 85 sq.m.
FIRST FLOOR AREA: - 104 sq.m.
TOTAL FLOOR AREA: - 189 sq.m., 20.3 sq.
DECK AREA: - 4 sq.m.

FLOOR PLAN LEGEND

	RETAIN WALL & STUD FRAME
	134mm EXTERNAL STUD WALLS WITH 35mm BATTEN & 9mm F.C. CLADDING.
	90mm STUD WALLS.
C.	CARPET
Tb.	TIMBER OVERLAY FLOOR.
Cft.	CERAMIC FLOOR TILES
Conc.	CONCRETE FLOOR

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Client:
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Drawing Title:
Floor Plans - Dwelling 1

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Plot Date: 19/11/2025 Drawn: KJR

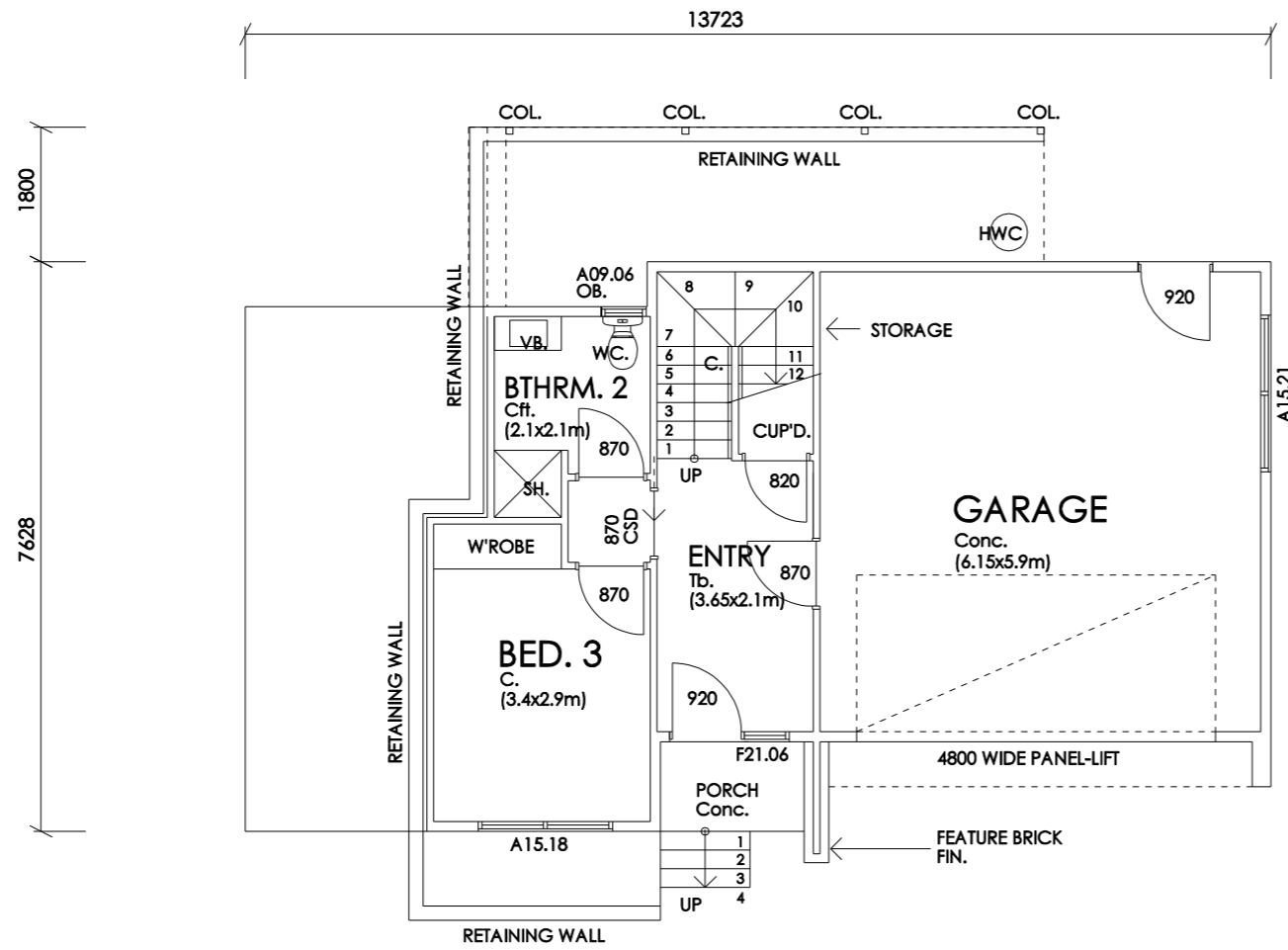
Approval: preliminary Scale: 1:100 @ A3

Project No.:
25.18

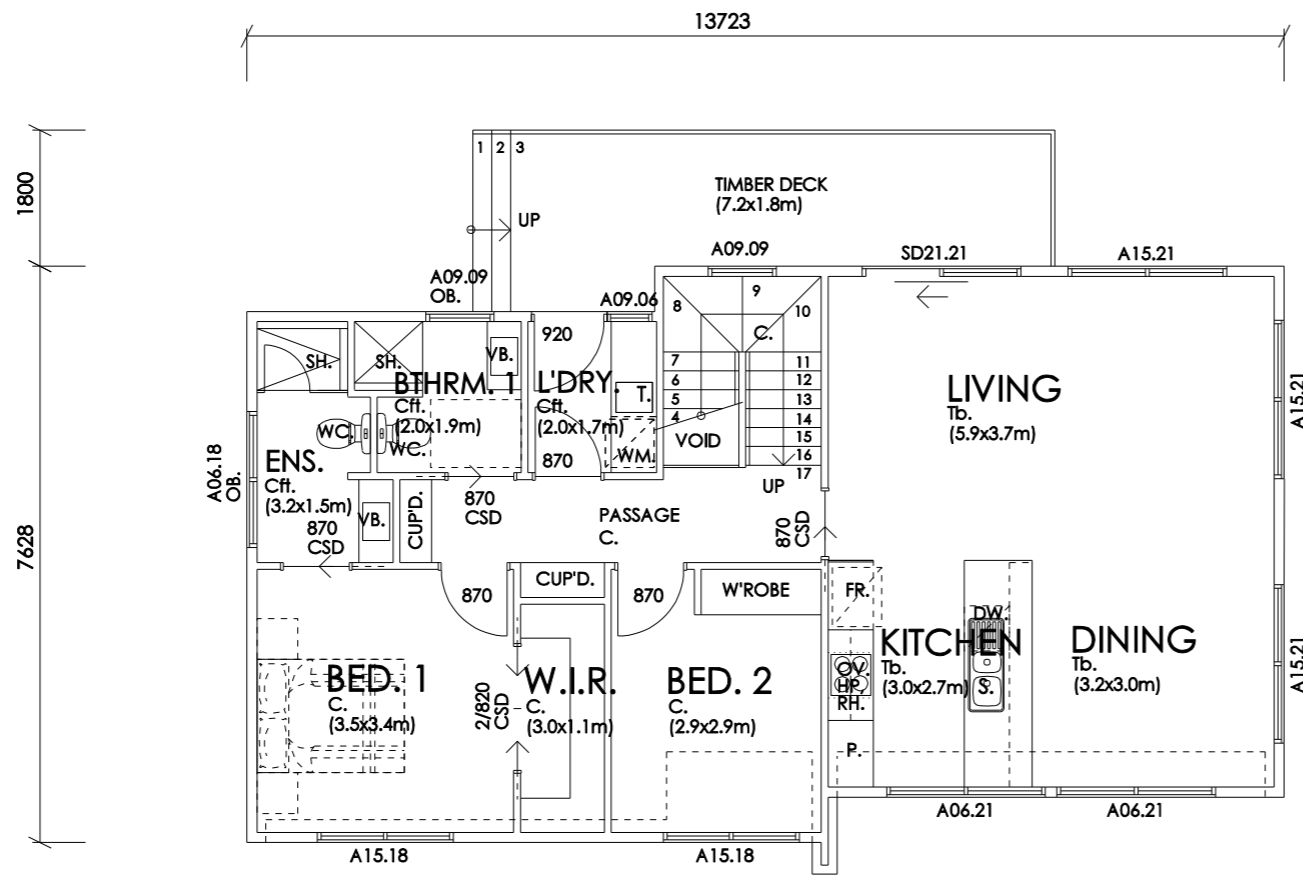
Drawing No.: **sk09** Rev. -

NORTH

DWELLING 2



GROUND FLOOR PLAN - DWELLING 2
SCALE 1:100



FIRST FLOOR PLAN - DWELLING 2
SCALE 1:100

GROUND FLOOR AREA: - 73 sq.m.
FIRST FLOOR AREA: - 92 sq.m.
TOTAL FLOOR AREA: - 165 sq.m., 17.7 sq.
DECK AREA: - 14 sq.m.

FLOOR PLAN LEGEND

	RETAIN WALL & STUD FRAME
	134mm EXTERNAL STUD WALLS WITH 35mm BATTEN & 9mm F.C. CLADDING.
	90mm STUD WALLS.
C.	CARPET
Tb.	TIMBER OVERLAY FLOOR.
Cft.	CERAMIC FLOOR TILES
Conc.	CONCRETE FLOOR

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Client:
MK Constructions

Drawing Title:
Floor Plans - Dwelling 2

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

Plot Date: 19/11/2025 Drawn: KJR

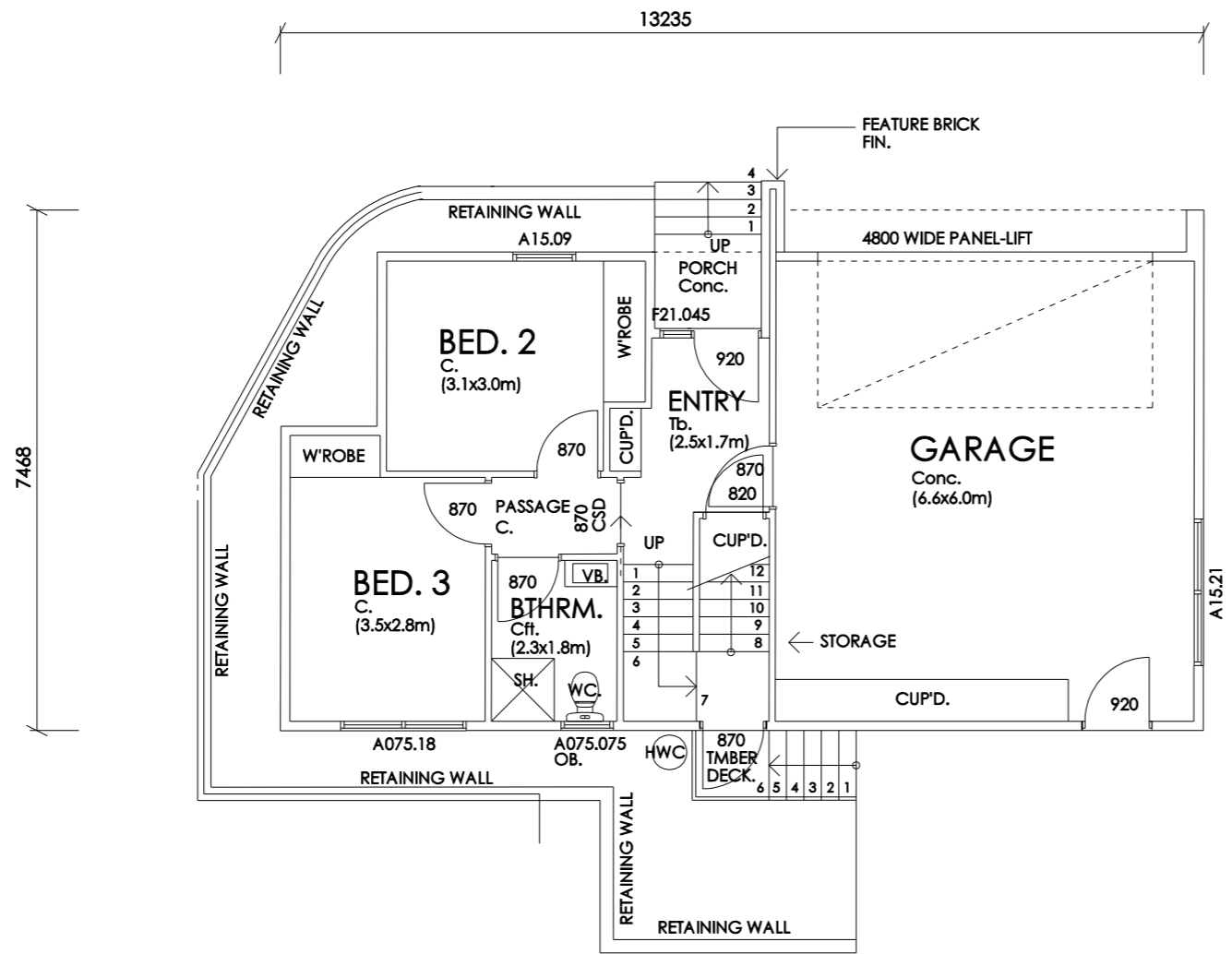
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Project No.: **25.18**

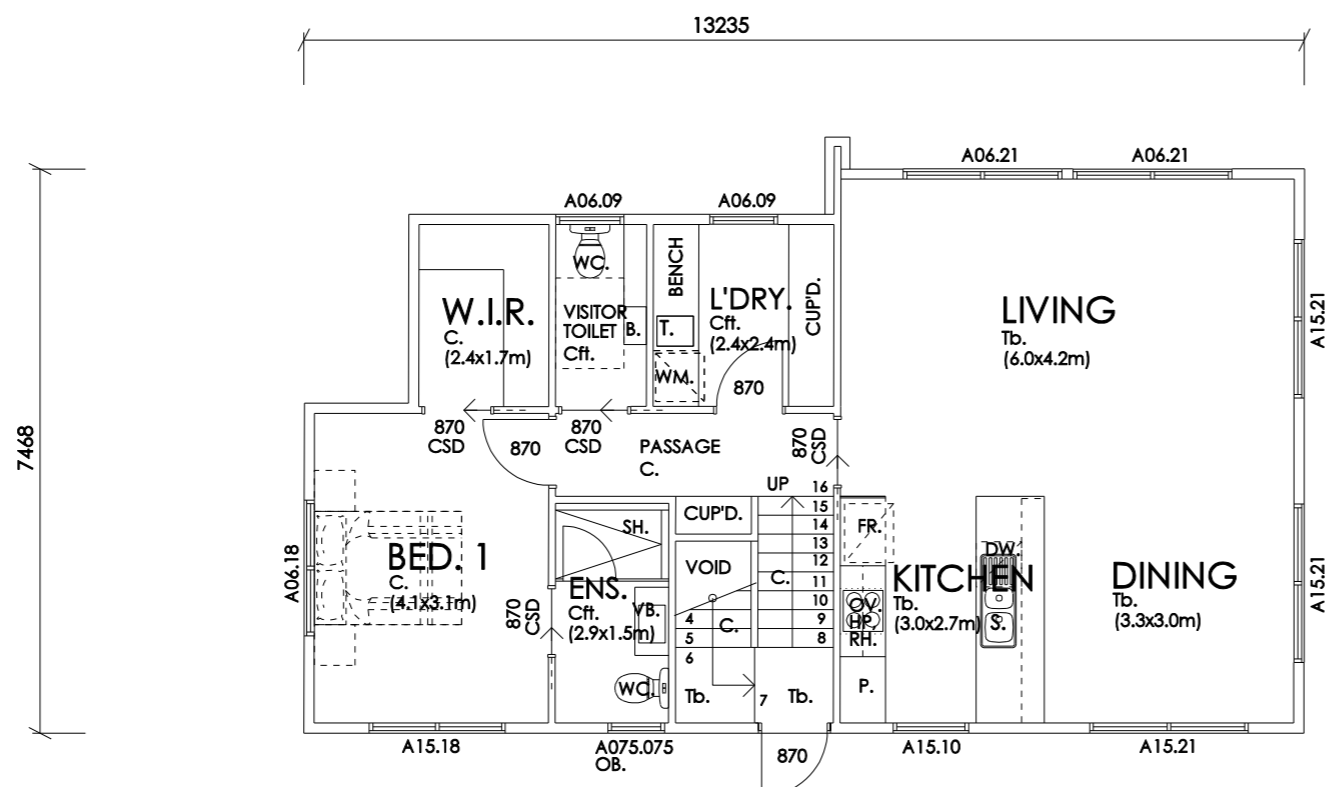
Drawing No.: **sk10** Rev. -

NORTH

DWELLING 3



GROUND FLOOR PLAN - DWELLING 3
SCALE 1:100



FIRST FLOOR PLAN - DWELLING 3
SCALE 1:100

GROUND FLOOR AREA: - 86 sq.m.
FIRST FLOOR AREA: - 86 sq.m.
TOTAL FLOOR AREA: - 172 sq.m., 18.5 sq.

FLOOR PLAN LEGEND

	RETAIN WALL & STUD FRAME
	134mm EXTERNAL STUD WALLS WITH 35mm BATTEN & 9mm F.C. CLADDING.
	90mm STUD WALLS.
C.	CARPET
Tb.	TIMBER OVERLAY FLOOR.
Cft.	CERAMIC FLOOR TILES
Conc.	CONCRETE FLOOR

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Project and Address:
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Client:
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Drawing Title:
Floor Plans - Dwelling 3

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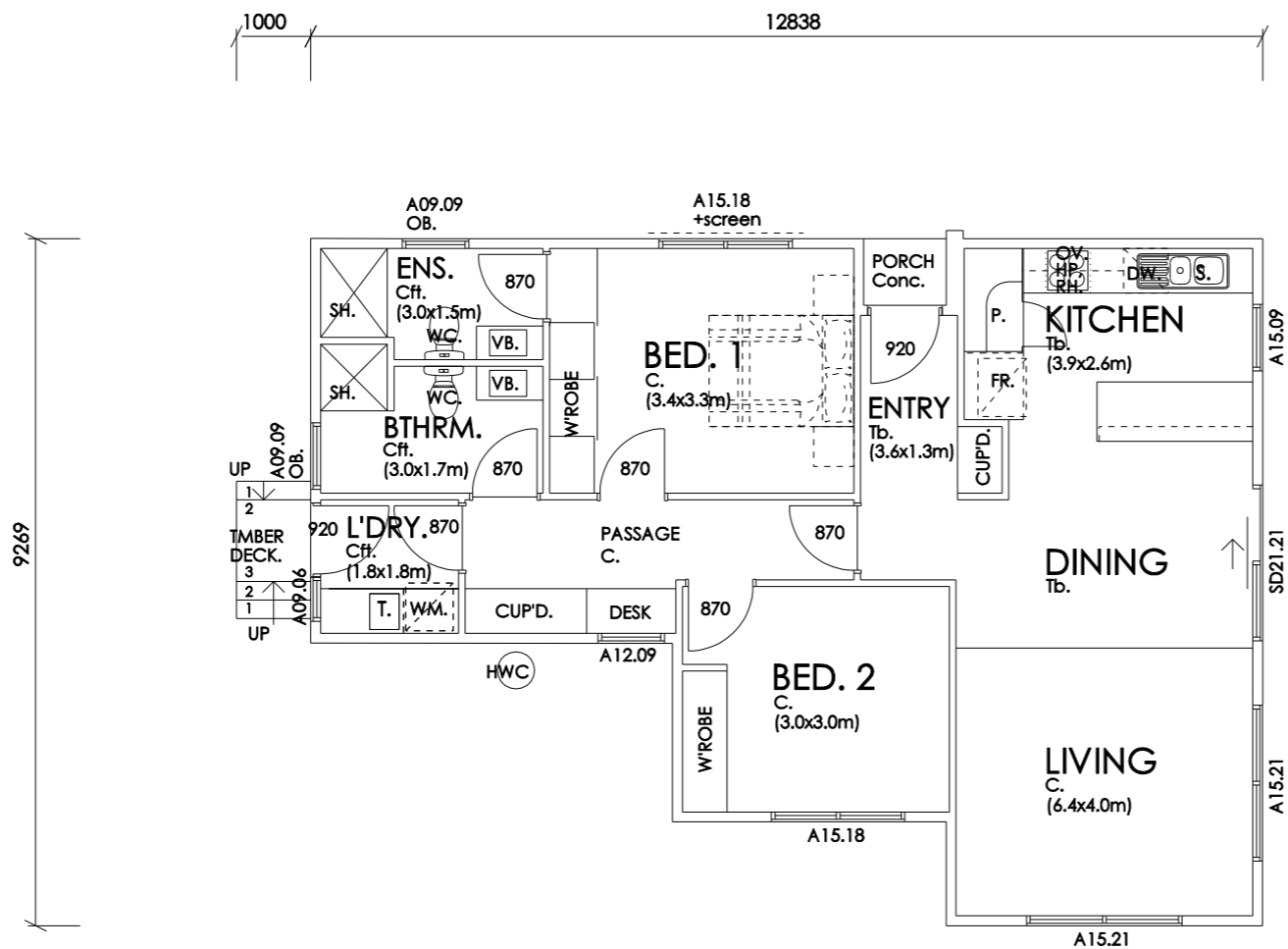
Plot Date: 19/11/2025 Drawn: KJR

Approval: preliminary Scale: 1:100 @ A3

Project No.: **25.18**

Drawing No.: **sk11** Rev. -

NORTH



FLOOR PLAN - DWELLING 4
SCALE 1:100

TOTAL FLOOR AREA: - 94 sq.m., 10.1 sq.

FLOOR PLAN LEGEND

- ==== 134mm EXTERNAL STUD WALLS WITH 35mm BATTEN & 9mm F.C. CLADDING.
- ==== 90mm STUD WALLS.
- C. CARPET
- Tb. TIMBER OVERLAY FLOOR.
- Cft. CERAMIC FLOOR TILES
- Conc. CONCRETE FLOOR

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residential commercial

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Project and Address:
Proposed 4 No. Dwellings at 37 William Cooper Drive, New Town.

Client:
MK Constructions

Drawing Title:
Floor Plan - Dwelling 4

Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

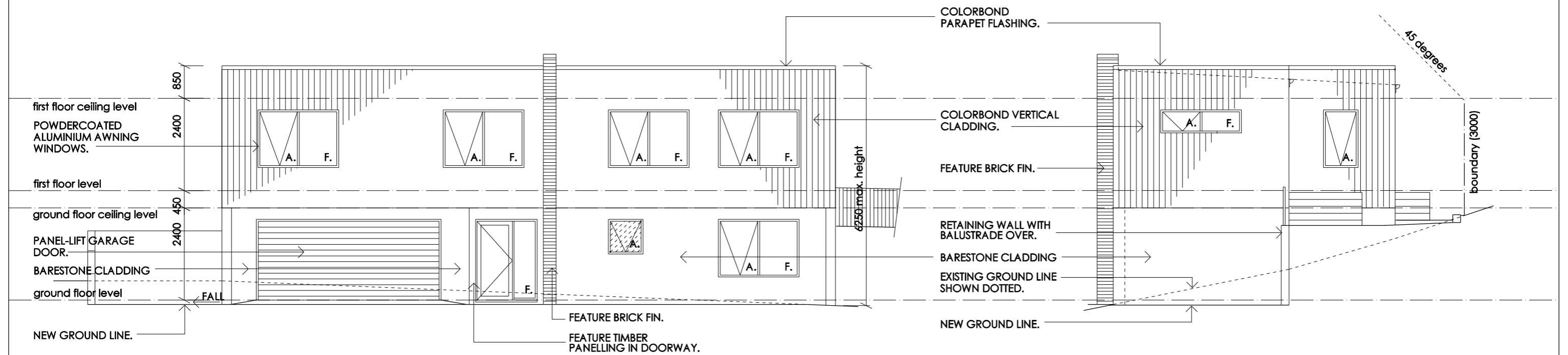
Plot Date: 19/11/2025 Drawn: KJR

Approval: preliminary Scale: 1:100 @ A3

Project No.:
25.18

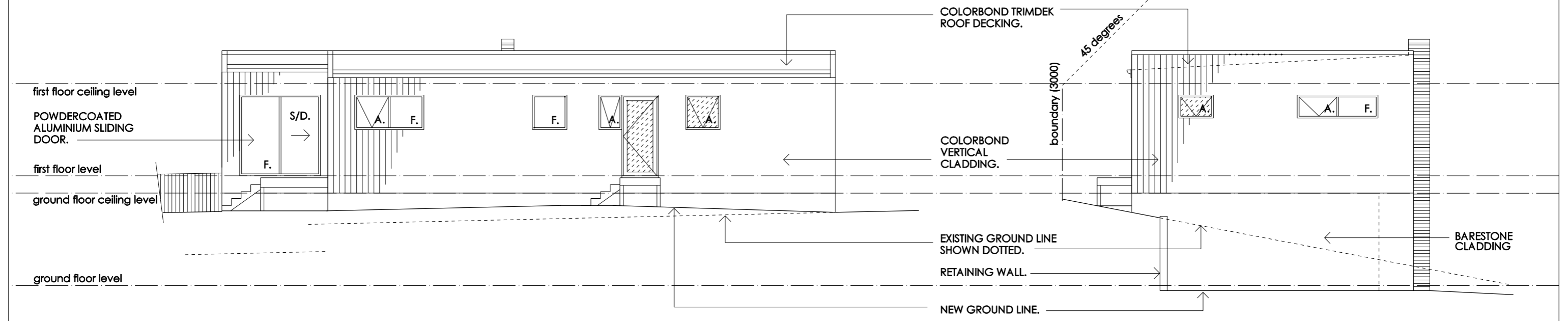
Drawing No.: sk12 Rev. -

NORTH



SOUTH-EAST ELEVATION - DWELLING 1
SCALE 1:100

NORTH-EAST ELEVATION - DWELLING 1
SCALE 1:100



NORTH-WEST ELEVATION - DWELLING 1
SCALE 1:100

SOUTH-WEST ELEVATION - DWELLING 1
SCALE 1:100

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Client:
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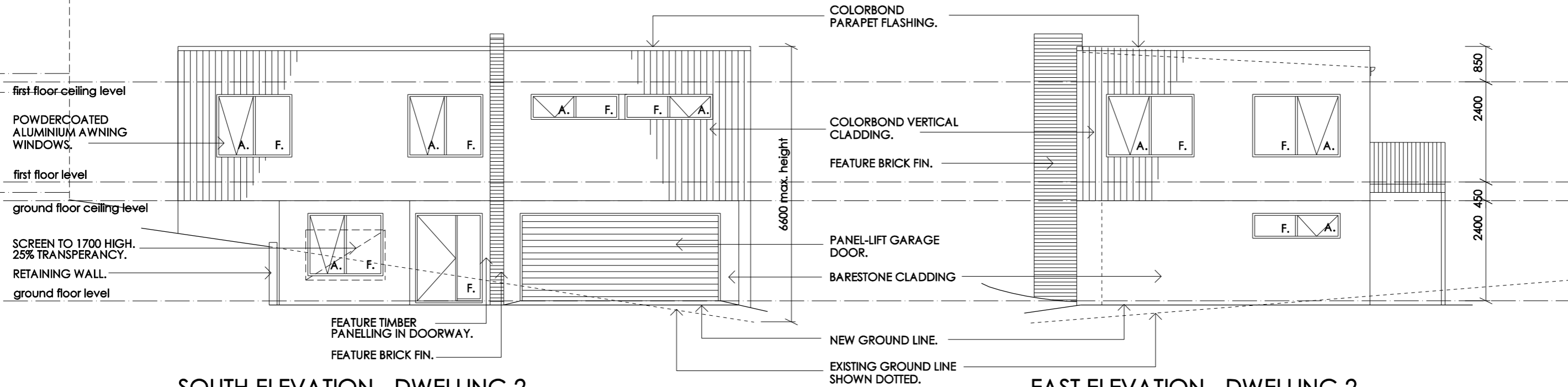
Drawing Title:
Elevations - Dwelling 1

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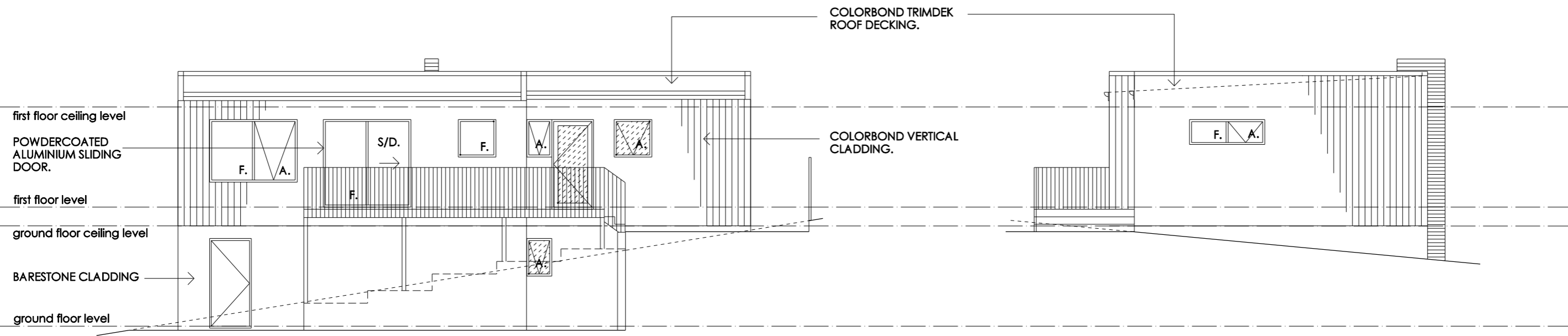
Plot Date: 19/11/2025	Drawn: KJR
Approval: preliminary	Scale: 1:100 @ A3
Project No.:	25.18
Drawing No.:	sk13
Rev.:	-

DWELLING 1



SOUTH ELEVATION - DWELLING 2
SCALE 1:100

EAST ELEVATION - DWELLING 2
SCALE 1:100



NORTH ELEVATION - DWELLING 2
SCALE 1:100

WEST ELEVATION - DWELLING 2
SCALE 1:100

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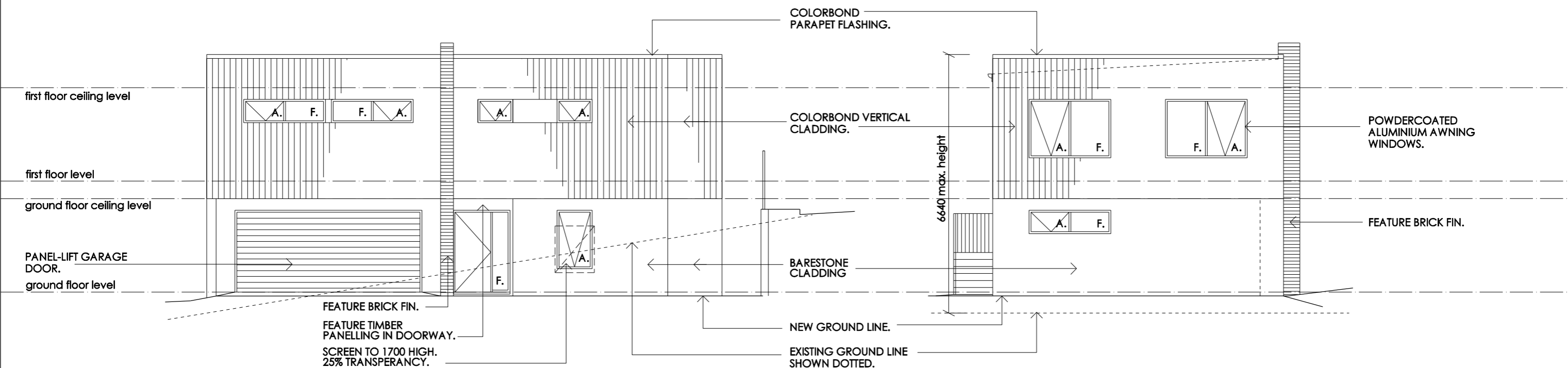
Client:
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Drawing Title:
Elevations - Dwelling 2

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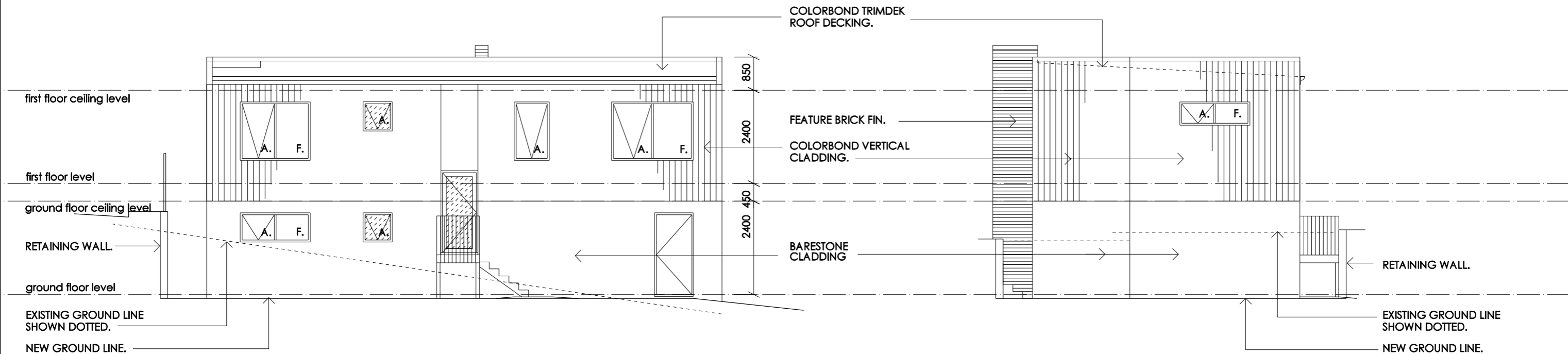
Please note: - Verify all dimensions on site. Figured dimensions take precedence over scale readings.

Plot Date: 19/11/2025	Drawn: KJR
Approval: preliminary	Scale: 1:100 @ A3
Project No.:	25.18
Drawing No.:	sk14
Rev.:	-



NORTH ELEVATION - DWELLING 3
SCALE 1:100

EAST ELEVATION - DWELLING 3
SCALE 1:100



SOUTH ELEVATION - DWELLING 3
SCALE 1:100

WEST ELEVATION - DWELLING 3
SCALE 1:100

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Drawing Title:
Elevations - Dwelling 3

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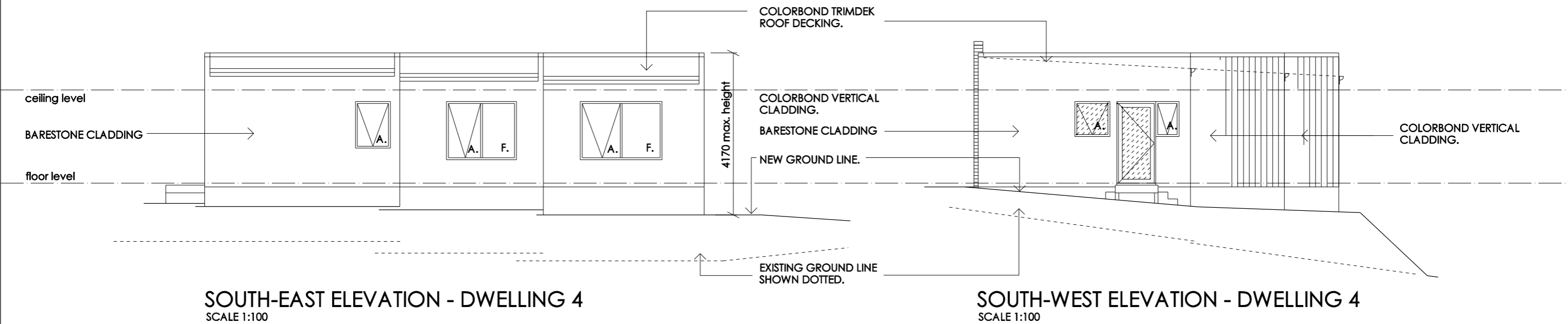
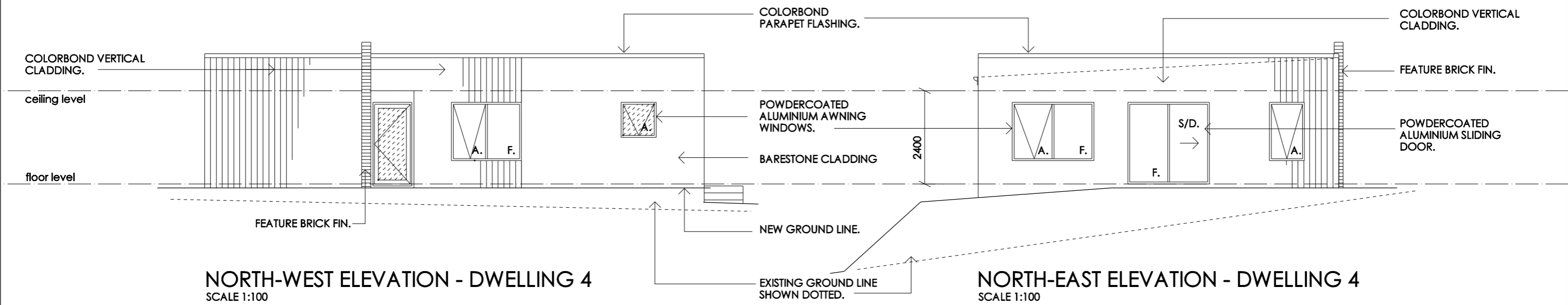
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Approval: preliminary Scale: 1:100 @ A3

Project No.:
25.18

Drawing No.: **sk15** Rev. -



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Drawing Title:
Elevations - Dwelling 4

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Project No.:
25.18

Drawing No.: Rev.
sk16 -