



Devonport City Council

PUBLIC NOTICE

APPLICATION FOR PLANNING PERMIT

Section 57(3) Land Use Planning Approvals Act 1993

An application for a planning permit has been made which may affect you.

Application Details

Application Number:	PA2025.0057
Proposed Use or Development:	Business and Professional Services (new building for Medical Centre) and car park
Address of the Land:	6-10 Steele Street, Devonport
Date of Notice:	04/10/2025

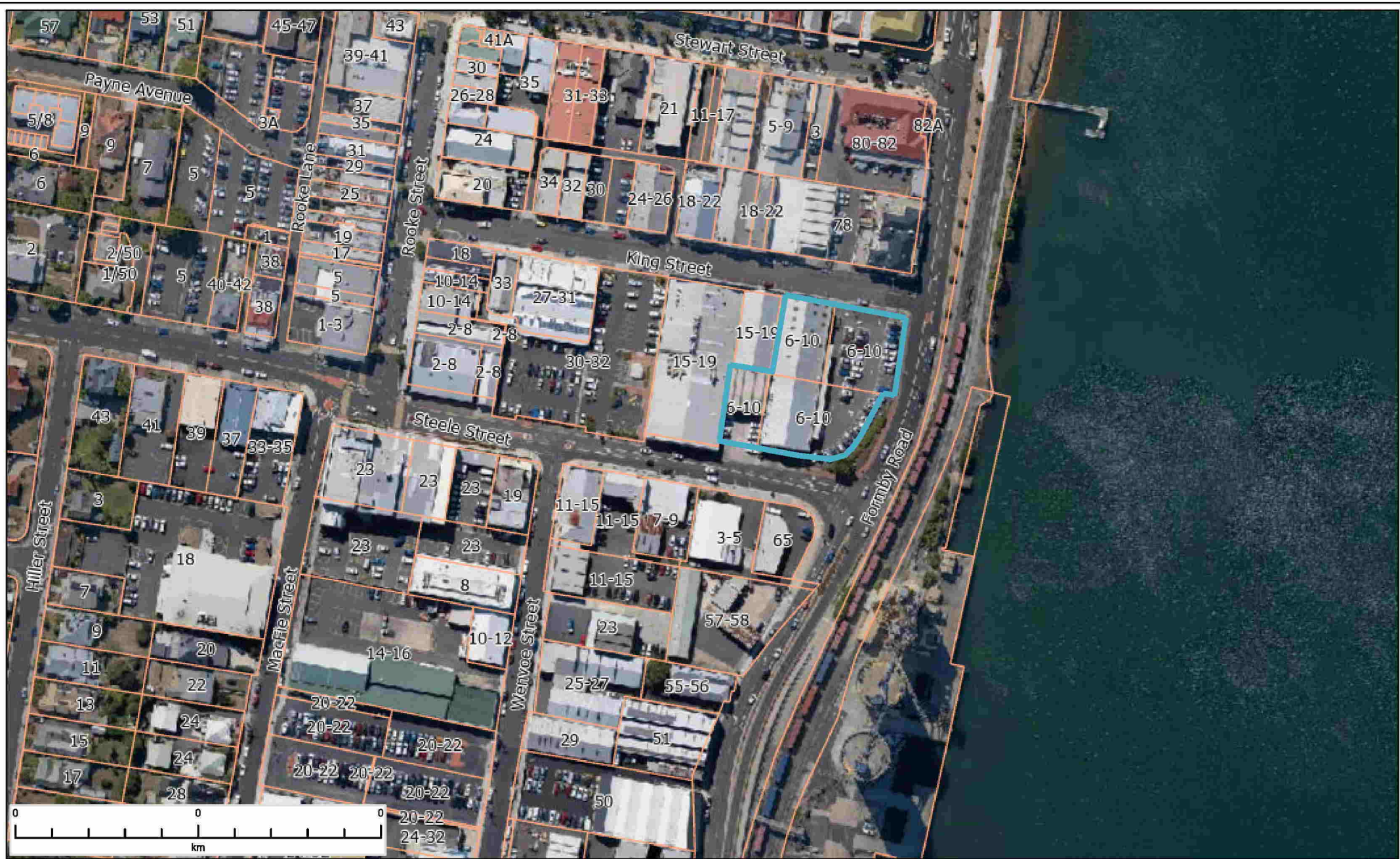
You are invited to view the application and any documents and plans accompanying it on the ground floor of the parnaple centre at 137 Rooke Street, Devonport or on Council's website www.devonport.tas.gov.au

Any person may make a representation relating to the application in accordance with section 57(5) of the *Land Use Planning Approvals Act 1993*, during a period of 14 days commencing on the date of this notice.

Your representation must:

- be received by close of business on **17/10/2025**;
- be in writing; and
- addressed to the Chief Executive Officer, Devonport City Council:
 - P.O. Box 604, Devonport, Tasmania, 7310; or
 - townplanning@devonport.tas.gov.au

If you make a representation then Council must consider your submission before making its decision on the application.



PA2025.0057 - 6-10 Steele Street Devonport



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**Devonport
City Council**

13/06/25

Devonport City Council – Planning Assessments

ATTENTION: To the assessment officer

REFERENCE: Development Application for 6-10 Steele Street, Devonport

This letter has been provided as a summary of the planning application made for the construction of a new 3 level office building at 6-10 Steele Street. It has been updated to address additional information request from council in email correspondence on the 01/05/25.

Council is in the process of reconfiguring several allotments to establish 2 allotments, the larger will be for this proposed building whilst a smaller lot will become a car park and established with a “no-built” covenant. This application is for the construction of a building only on the larger allotment. The creation of the allotments and any subdivision requirements of authorities is part of work being done by council and not relevant to this Development Application, which is for the demolition of existing buildings and construction of the new building only.

We believe the application should be considered in the following way regarding the planning scheme requirements.

The project is in a *Central Business Zone*. Within the use table we believe the building should be considered a permitted use being for *Business* use.

We understand the site is impacted by the following code overlays:

- *Parking precinct plan code*
- *Airports code overlay* (The proposed building is well below the AHD limit nominated in the Airport Obstacle Limitation Overlay and thus the requirements of this overlay are complied with)

We have provided a summary within this document to illustrate compliance with the Central Business Zone objectives and the Parking Precinct Plan Code objectives.

We trust that this letter and the accompanying drawings and engineering details provide council sufficient information to commence a development assessment for the project.

If further information is required, you can contact us directly on the details provided.

Yours Sincerely



Dylan Graham (mobile 0419 982 047)

General Manager - Developments



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Central Business Zone

The following provides a summary of the building's alignment to the relevant Use Standards detailed in the scheme within a Central Business Zone

16.3.1 All uses		
Objective:	That uses do not cause an unreasonable loss of amenity to residential zones.	
Acceptable Solutions	Performance Criteria	Response
<p>A1 Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation, Residential, Utilities or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must be within the hours of:</p> <p>(a) 7.00am to 9.00pm Monday to Saturday; and (b) 8.00am to 9.00pm Sunday and public holidays.</p>	<p>P1 Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation, Residential, Utilities or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones having regard to:</p> <p>(a) the timing, duration or extent of vehicle movements; and (b) noise, lighting or other emissions.</p>	<p>The proposed hours of operation for the building are within the acceptable solution as nominated in item A1 – the site is not within 50m of a General Residential Zone or Inner Residential Zone</p>
<p>A2 External lighting for a use, excluding Natural and Cultural Values Management, Passive Recreation, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must:</p> <p>(a) not operate within the hours of 11.00pm to 6.00am, excluding any security lighting; and (b) if for security lighting, be baffled so that direct light does not extend into the adjoining property in those zones.</p>	<p>P2 External lighting for a use, excluding Natural and Cultural Values Management, Passive Recreation, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to:</p> <p>(a) the level of illumination and duration of lighting; and (b) the distance to habitable rooms of an adjacent dwelling.</p>	<p>N/A - The development is not within 50m of a general residential zone</p>
<p>A3 Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must be within the hours of:</p> <p>(a) 7.00am to 9.00pm Monday to Saturday; and (b) 8.00am to 9.00pm Sunday and public holidays</p>	<p>P3 Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to:</p> <p>(a) the time and duration of commercial vehicle movements; (b) the number and frequency of commercial vehicle movements; (c) the size of commercial vehicles involved; (d) manoeuvring required by the commercial vehicles, including the amount of reversing and associated warning noise; (e) any noise mitigation measures between the vehicle movement areas and the residential zone; and (f) potential conflicts with other traffic.</p>	<p>The proposed hours of operation for the building are within the acceptable solution as nominated in item A1 – the site is not within 50m of a General Residential Zone or Inner Residential Zone</p>

The following provides a summary of the building's alignment to the relevant Development Standards for Building Work detailed in the scheme within a Central Business Zone

16.4.1 Building height		
Objective:	That building height: (a) is compatible with the streetscape; and (b) does not cause an unreasonable loss of amenity to adjoining residential zones.	
Acceptable Solutions	Performance Criteria	Response
A1 Building height must be not more than 20m.	P1 Building height must be compatible with the streetscape and character of development existing on established properties in the area, having regard to: (a) the topography of the site; (b) the height, bulk and form of existing buildings on the site and adjacent properties; (c) the bulk and form of proposed buildings; (d) the apparent height when viewed from the adjoining road and public places; and (e) any overshadowing of public places.	The building is below 20m in height from natural ground
A2 Building height: (a) within 10m of the General Residential Zone must not be more than 8.5m; or (b) within 10m of an Inner Residential Zone must not be more than 9.5m.	P2 Building height within 10m of the General Residential Zone or Inner Residential Zone must be consistent with building height on adjoining properties and not cause an unreasonable loss of residential amenity having regard to: (a) overshadowing and reduction in sunlight to habitable rooms and private open space of dwellings; (b) overlooking and reduction of privacy; or (c) visual impacts caused by the apparent scale, bulk or proportions of the building when viewed from adjoining properties.	N/A - The development is not within 10m of General Residential zone The development is not within 10m of Inner Residential zone
16.4.2 Setbacks		
Objective:	That building setback: (a) is compatible with the streetscape; (b) does not cause an unreasonable loss of amenity to adjoining residential zones; and (c) minimises opportunities for crime and anti-social behaviour through setback of buildings.	
Acceptable Solutions	Performance Criteria	Response
A1 Buildings must be: (a) built to the frontage at ground level; or (b) have a setback of not more or less than the maximum and minimum setbacks of the buildings on adjoining properties.	P1 Buildings must have a setback from a frontage that is compatible with the streetscape and minimises opportunities for crime or anti-social behaviour, having regard to: (a) providing small variations in building alignment to break up long building façades; (b) providing variations in building alignment appropriate to provide a forecourt space for public use, such as outdoor dining or landscaping; (c) the avoidance of concealment spaces; (d) the ability to achieve passive surveillance; and (e) the availability of lighting.	The building is proposed to be built to the frontage alignment typically across Steele Street and Formby Road - The development complies with the acceptable solution A1

<p>A2 Buildings must have a setback from an adjoining property within a General Residential Zone or Inner Residential Zone of not less than: (a) 6m; or (b) half the wall height of the building, whichever is the greater.</p>	<p>P2 Buildings must be sited to not cause an unreasonable loss of amenity to adjoining properties within a General Residential Zone or Inner Residential Zone, having regard to: (a) overshadowing and reduction in sunlight to habitable rooms and private open space of dwellings; (b) overlooking and reduction of privacy to the adjoining property; or (c) visual impacts caused by the apparent scale, bulk or proportions of the building when viewed from the adjoining property.</p>	<p>N/A - The development is not adjoining a General Residential zone or Inner Residential zone</p>
<p>A3 Air extraction, pumping, refrigeration systems or compressors must be separated a distance of not less than 10m from a General Residential Zone or Inner Residential Zone.¹</p>	<p>P3 Air conditioning, air extraction, pumping, heating or refrigeration systems or compressors within 10m of a General Residential Zone or Inner Residential Zone, must be designed, located, baffled or insulated to not cause an unreasonable loss of amenity to the adjoining residential zones, having regard to: (a) the characteristics and frequency of emissions generated; (b) the nature of the proposed use; (c) the topography of the site and location of the sensitive use; and (d) any proposed mitigation measures.</p>	<p>N/A - The development is more than 10m from a General Residential zone or Inner Residential zone</p>
<p>16.4.3 Design</p>		
<p>Objective:</p>	<p>That building façades promote and maintain high levels of pedestrian interaction, amenity, and safety and are compatible with the streetscape.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	<p>Response</p>
<p>A1 New buildings must be designed to satisfy all of the following: (a) mechanical plant and other service infrastructure, such as heat pumps, air conditioning units, switchboards, hot water units and the like, must be screened from the street and other public places; (b) roof-top mechanical plant and service infrastructure, including lift structures, must be contained within the roof; (c) not include security shutters or grilles over windows or doors on a façade facing the frontage or other public places; and (d) provide external lighting to illuminate external vehicle parking areas and pathways.</p>	<p>P1 New buildings must be designed to be compatible with the streetscape having regard to: (a) minimising the visual impact of mechanical plant and other service infrastructure, such as heat pumps, air conditioning units, switchboards, hot water units and the like, when viewed from the street or other public places; (b) minimising the visual impact of security grilles and shutters and roof-top service infrastructure, including lift structures; and (c) providing suitable lighting to vehicle parking areas and pathways for the safety and security of users.</p>	<p>a) The project proposes to screen any plant / infrastructure from the street and other public spaces. b) Roof top plant locations are indicated in architectural drawings. They are located away from the edge of the roof and obscured by the parapet design of the building. Any visible extent of the mechanical equipment is proposed to be screened, and this is illustrated in the architectural drawings. c) N/A – no security shutters or grilles are included. d) External lighting is to be provided as follows: – the pedestrian entry from Steele Street will be provided compliant to the relevant Australian standards. – Lighting throughout the parking spaces outside the building will be provided consistent with Australian standards and council requirements. – The lighting to parking areas below the building will be provide to the Australian standards and national construction code The development proposes to comply with A1 acceptable solution</p>

<p>A2 New buildings or alterations to an existing façade must be designed to satisfy all of the following:</p> <p>(a) provide a pedestrian entrance to the building that is visible from the road or publicly accessible areas of the site;</p> <p>(b) if for a ground floor level façade facing a frontage:</p> <p>(i) have not less than 40% of the total surface area consisting of windows or doorways; or</p> <p>(ii) not reduce the surface area of windows or doorways of an existing building, if the surface area is already less than 40%;</p> <p>(c) if for a ground floor level façade facing a frontage must:</p> <p>(i) not include a single length of blank wall greater than 30% of the length of façade on that frontage; or</p> <p>(ii) not increase the length of an existing blank wall, if already greater than 30% of the length of the façade on that frontage; and</p> <p>(d) provide awnings over a public footpath if existing on the site or on adjoining properties.</p>	<p>P2 New buildings or alterations to an existing façade must be designed to be compatible with the streetscape having regard to:</p> <p>(a) how the main pedestrian access to the building addresses the street or other public places;</p> <p>(b) windows on the façade facing the frontage for visual interest and passive surveillance of public spaces;</p> <p>(c) providing architectural detail or public art on large expanses of blank walls on the façade facing the frontage and other public spaces so as to contribute positively to the streetscape and public spaces;</p> <p>(d) installing security shutters or grilles over windows or doors on a façade facing the frontage or other public spaces only if it is essential for the security of the premises and any other alternatives are not practical; and</p> <p>(e) providing awnings over a public footpath.</p>	<p>a) A principal pedestrian entry is provided from the frontage on Steele Street. This has been illustrated in the architectural documents.</p> <p>b) The south elevation facing Steele Street comprises a ground level façade with window and doorways greater than 40% of the solid face areas and is consistent with A2(i)</p> <p>c) The façade facing Steele Street does not contain a blank wall greater than 30% of its overall length.</p> <p>d) Awnings are not a predominate feature within the streetscape and it has been proposed not to include them</p>
<p>¹An exemption applies to air conditioners and heat pumps in this zone – see Table 4.6.</p>		
<p>16.4.4 Fencing</p>		
<p>Objective:</p>	<p>That fencing:</p> <p>(a) is compatible with the streetscape; and</p> <p>(b) does not cause an unreasonable loss of residential amenity to adjoining residential zones.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	<p>Response</p>
<p>A1 No Acceptable Solution.²</p>	<p>P1 A fence (including a free-standing wall) within 4.5m of a frontage must be compatible with the streetscape, having regard to:</p> <p>(a) its height, design, location and extent;</p> <p>(b) its degree of transparency; and</p> <p>(c) the proposed materials and construction.</p>	<p>Typically, there are no fences within the project. There is a screening wall at the lower level facing Steele Street and Formby Road. The part-height wall provides screening to the ground level car park area. It comprises several materials and varying heights. The wall forms part of the building envelope and is compatible with the surrounding streetscape. The height and material sections are illustrated in the development drawings</p>
<p>A2 Common boundary fences with a property in a General Residential Zone or Inner Residential Zone, if not within 4.5m of a frontage, must:</p> <p>(a) have a height above existing ground level of not more than 2.1m; and</p> <p>(b) not contain barbedwire.²</p>	<p>P2 Common boundary fences with a property in a General Residential Zone or Inner Residential Zone, if not within 4.5m of a frontage, must not cause an unreasonable loss of residential amenity, having regard to:</p> <p>(a) their height, design, location and extent; and</p> <p>(b) the proposed materials and construction.</p>	<p>The site is not adjacent to a General Residential or Inner Residential Zone</p>
<p>² An exemption applies for fences in this zone – see Table 4.6.</p>		
<p>16.4.5 Outdoor storage areas</p>		
<p>Objective:</p>	<p>That outdoor storage areas do not detract from the appearance of the site or locality.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	
<p>A1 Outdoor storage areas, excluding for the display of goods for sale, must not be visible from any road or public open space adjoining the site.</p>	<p>P1 Outdoor storage areas, excluding for the display of goods for sale, must be located, treated or screened to not cause an unreasonable loss of visual amenity.</p>	<p>Storage areas are located away from primary frontages and not directly visible from any public road or open space</p>

Parking and Sustainable Transport Code

The following provides a summary of the building's alignment to the relevant objectives detailed in the parking and sustainable transport code

C2.5.1 Car parking numbers		
Objective:	That an appropriate level of car parking spaces are provided to meet the needs of the use.	
Acceptable Solutions	Performance Criteria	Response
<p>A1</p> <p>The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:</p> <ul style="list-style-type: none"> (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; (b) the site is contained within a parking precinct plan and subject to Clause C2.7; (c) the site is subject to Clause C2.5.5; or (d) it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or (ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: $N = A + (C - B)$ N = Number of on-site car parking spaces required A = Number of existing on site car parking spaces B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1 C= Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1. 	<p>P1.1</p> <p>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the availability of off-street public car parking spaces within reasonable walking distance of the site; (b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> (i) variations in car parking demand over time; or (ii) efficiencies gained by consolidation of car parking spaces; (c) the availability and frequency of public transport within reasonable walking distance of the site; (d) the availability and frequency of other transport alternatives; (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping; (f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; (g) the effect on streetscape; and (h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development. <p>P1.2</p> <p>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the nature and intensity of the use and car parking required; (b) the size of the dwelling and the number of bedrooms; and (c) the pattern of parking in the surrounding area. 	<p>N/A the site is understood to be subject to C2.7</p>
C2.5.2 Bicycle parking numbers		
Objective:	That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.	
Acceptable Solutions	Performance Criteria	Response

<p>A1 Bicycle parking spaces must:</p> <ul style="list-style-type: none"> (a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in Table C2.1. 	<p>P1 Bicycle parking spaces must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the likely number of users of the site and their opportunities and likely need to travel by bicycle; and (b) the availability and accessibility of existing and any planned parking facilities for bicycles in the surrounding area. 	<p>Bicycle parking is proposed to be provided to the levels nominated in table C2.1 Level 1 and 2 of the building provide office space with a total floor area approximately 1800m² per floor. This requires a minimum of 8 bicycle parking spaces.</p> <p>The building design proposes a secure bike storage area as show on the architectural plans that will exceed this number</p> <p>It is proposed to provide 2 voluntary public bike parking spaces at the front entry of the building</p>
<p>C2.5.3 Motorcycle parking numbers</p>		
<p>Objective:</p>	<p>That the appropriate level of motorcycle parking is provided to meet the needs of the use.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	
<p>A1 The number of on-site motorcycle parking spaces for all uses must:</p> <ul style="list-style-type: none"> (a) be no less than the number specified in Table C2.4; and (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained. 	<p>P1 Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the proposed use and development; (b) the topography of the site; (c) the location of existing buildings on the site; (d) any constraints imposed by existing development; and (e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area. 	<p>Motorcycle parking has been provided in accordance with table C2.4.</p> <p>2 spaces only are required:</p> <ul style="list-style-type: none"> - 1 – in the external parking areas - 1 – in the parking below the building
<p>C2.6.1 Construction of parking areas</p>		
<p>Objective:</p>	<p>That parking areas are constructed to an appropriate standard.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	<p>Response</p>
<p>A1 All parking, access ways, maneuvering and circulation spaces must:</p> <ul style="list-style-type: none"> (a) be constructed with a durable all weather pavement; (b) be drained to the public stormwater system, or contain stormwater on the site; and (c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement. 	<p>P1 All parking, access ways, maneuvering and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the use; (b) the topography of the land; (c) the drainage system available; (d) the likelihood of transporting sediment or debris from the site onto a road or public place; (e) the likelihood of generating dust; and (f) the nature of the proposed surfacing. 	<p>All parking, access ways, maneuvering and circulation spaces are proposed to be constructed in accordance with A1</p>
<p>C2.6.2 Design and layout of parking areas</p>		
<p>Objective:</p>	<p>That parking areas are designed and laid out to provide convenient, safe and efficient parking.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	
<p>A1.1 Parking, access ways, maneuvering and circulation spaces must either:</p> <ul style="list-style-type: none"> (a) comply with the following: <ul style="list-style-type: none"> (i) have a gradient in accordance with <i>Australian Standard AS 2890 - Parking facilities, Parts 1-6</i>; (ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces; (iii) have an access width not less than 	<p>P1 All parking, access ways, maneuvering and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to:</p> <ul style="list-style-type: none"> (a) the characteristics of the site; (b) the proposed slope, dimensions and layout; (c) useability in all weather conditions; (d) vehicle and pedestrian traffic safety; (e) the nature and use of the development; (f) the expected number and type of 	<p>Parking, access ways, maneuvering and circulation spaces are intended to comply with A1.1</p>

<p>the requirements in Table C2.2;</p> <p>(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;</p> <p>(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;</p> <p>(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and</p> <p>(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or</p> <p>(b) comply with <i>Australian Standard AS 2890- Parking facilities, Parts 1-6.</i></p> <p>A1.2 Parking spaces provided for use by persons with a disability must satisfy the following:</p> <p>(a) be located as close as practicable to the main entry point to the building;</p> <p>(b) be incorporated into the overall car park design; and</p> <p>(c) be designed and constructed in accordance with <i>Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities.</i>¹</p>	<p>vehicles;</p> <p>(g) the likely use of the parking areas by persons with a disability;</p> <p>(h) the nature of traffic in the surrounding area;</p> <p>(i) the proposed means of parking delineation; and</p> <p>(j) the provisions of <i>Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking</i> and <i>AS 2890.2 - 2002 Parking facilities, Part 2: Off-street commercial vehicle facilities</i></p>	
<p>¹ Requirements for the number of accessible car parking spaces are specified in part D3 of the National Construction Code 2016.</p>		
<p>C2.6.3 Number of accesses for vehicles</p>		
<p>Objective:</p>	<p>That:</p> <p>(a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;</p> <p>(b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and</p> <p>(c) the number of accesses minimise impacts on the streetscape.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	
<p>A1 The number of accesses provided for each frontage must:</p> <p>(a) be no more than 1; or</p> <p>(b) no more than the existing number of accesses, whichever is the greater.</p>	<p>P1 The number of accesses for each frontage must be minimised, having regard to:</p> <p>(a) any loss of on-street parking; and</p> <p>(b) pedestrian safety and amenity;</p> <p>(c) traffic safety;</p> <p>(d) residential amenity on adjoining land; and</p> <p>(e) the impact on the streetscape.</p>	<p>1 vehicle access point is provided from each frontage</p>
<p>A2 Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.</p>	<p>P2 Within the Central Business Zone or in a pedestrian priority street, any new accesses must:</p> <p>(a) not have an adverse impact on:</p> <p>(i) pedestrian safety and amenity; or</p> <p>(ii) traffic safety; and</p> <p>(b) be compatible with the streetscape.</p>	<p>The design proposes 1 vehicle entry from both frontages – Steele Street and King Street Both streets have existing access points that are being adjusted, and no additional access is proposed.</p>
<p>C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone</p>		
<p>Objective:</p>	<p>That parking and vehicle circulation roads and pedestrian paths within the General Business Zone and Central Business Zone, which are used outside daylight hours, are provided with lighting to a standard which:</p> <p>(a) enables easy and efficient use;</p> <p>(b) promotes the safety of users;</p> <p>(c) minimises opportunities for crime or anti-social behaviour; and</p> <p>(d) prevents unreasonable light overspill impacts.</p>	
<p>Acceptable Solutions</p>	<p>Performance Criteria</p>	

<p>A1 In car parks within the General Business Zone and Central Business Zone, parking and vehicle circulation roads and pedestrian paths serving 5 or more car parking spaces, which are used outside daylight hours, must be provided with lighting in accordance with Clause 3.1 “Basis of Design” and Clause 3.6 “Car Parks” in <i>Australian Standard/New Zealand Standard AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting – Performance and design requirements.</i></p>	<p>P1 In car parks within the General Business Zone and Central Business Zone, parking and vehicle circulation roadways and pedestrian paths, which are used outside daylight hours must be provided with lighting, having regard to:</p> <ul style="list-style-type: none"> (a) enabling easy and efficient use of the area; (b) minimizing potential for conflicts involving pedestrians, cyclists and vehicles; (c) minimizing opportunities for crime or anti-social behaviour though the creation of concealment spaces; (d) any unreasonable impact on the amenity of adjoining properties through light overspill; and (e) the hours of operation of the use. 	<p>The parking areas will be provided with lighting to the requirements of A1</p> <p>The parking areas below the building will be provided with Lighting to the requirements of A1 and will be controlled to only be fully illuminated during the building’s hours of operation.</p> <p>A concept lighting plan has been provide in the DA submission</p>
<p>C2.6.5 Pedestrian access</p>		
<p>Objective:</p>	<p>That pedestrian access within parking areas is provided in a safe and convenient manner.</p>	
<p>Acceptable Solutions</p>		<p>Performance Criteria</p>
<p>A1.1 Uses that require 10 or more car parking spaces must:</p> <ul style="list-style-type: none"> (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by: <ul style="list-style-type: none"> (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and (b) be signed and line marked at points where pedestrians cross access ways or parking aisles. <p>A1.2 In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.</p>	<p>P1 Safe and convenient pedestrian access must be provided within parking areas, having regard to:</p> <ul style="list-style-type: none"> (a) the characteristics of the site; (b) the nature of the use; (c) the number of parking spaces; (d) the frequency of vehicle movements; (e) the needs of persons with a disability; (f) the location and number of footpath crossings; (g) vehicle and pedestrian traffic safety; (h) the location of any access ways or parking aisles; and (i) any protective devices proposed for pedestrian safety. 	<p>Parking within the site comprises the following parts:</p> <ul style="list-style-type: none"> – parking below the building footprint - below the building parking is accessed by authorized people who are familiar with the operations of the building and the parking configuration. The parking movements within the area will be low volume. In consideration of this pedestrian circulation is proposed to be shared with the general vehicle aisles. This is proposed consistent with the performance requirements P1 given the number of parking spaces, frequency of movement, and the authorized only users with high familiarity of the parking arrangements – parking below the building facing accessed from Steele Street – these parking spaces have been developed in consultation with council. The layout of the parking area provides a designated pathway to the frontage of Steele Street. This is proposed to be consistent with acceptable solution A1.1 – The external parking fronting King Street provides clear sightlines and constitutes a single aisle. It is proposed that this area has a shared pedestrian and vehicle zone to the King Street foot path. This is proposed consistent with the performance requirements P1 given the character of the site, the number of parking spaces, the clear sight lines for safety, the low-speed environment (which is proposed to include traffic speed devices, signage, line marking etc. – the detailed configuration is proposed to be developed jointly with council in detailed design) – The disabled parking location is directly accessible to dedicated pedestrian zone paving with direct access to the front entry of the building and Steele Street foot path. This is proposed to be consistent with acceptable solution A1.2
<p>C2.6.6 Loading bays</p>		
<p>Objective:</p>	<p>That the area and dimensions of loading bays are adequate to provide safe and efficient delivery and collection of goods.</p>	
<p>Acceptable Solutions</p>		<p>Performance Criteria</p>

<p>A1 The area and dimensions of loading bays and access way areas must be designed in accordance with <i>Australian Standard AS 2890.2-2002, Parking facilities, Part 2: Off-street commercial vehicle facilities</i>, for the type of vehicles likely to use the site.</p>	<p>P1 Loading bays must have an area and dimensions suitable for the use, having regard to: (a) the types of vehicles likely to use the site; (b) the nature of the use; (c) the frequency of loading and unloading; (d) the area and dimensions of the site; (e) the topography of the site; (f) the location of existing buildings on the site; and (g) any constraints imposed by existing development.</p>	<p>N/A there is no loading bay proposed</p>
<p>A2 The type of commercial vehicles likely to use the site must be able to enter, park and exit the site in a forward direction in accordance with <i>Australian Standard AS 2890.2 – 2002, Parking Facilities, Part 2: Parking facilities - Off-street commercial vehicle facilities</i>.</p>	<p>P2 Access for commercial vehicles to and from the site must be safe, having regard to: (a) the types of vehicles associated with the use; (b) the nature of the use; (c) the frequency of loading and unloading; (d) the area and dimensions of the site; (e) the location of the site and nature of traffic in the area of the site; (f) the effectiveness or efficiency of the surrounding road network; and (g) site constraints such as existing buildings, slope, drainage, vegetation, parking and landscaping.</p>	<p>N/A there is no loading bay proposed</p>
<p>C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone</p>		
<p>Objective:</p>	<p>That parking for bicycles are safe, secure and convenient, within the General Business Zone and Central Business Zone.</p>	
<p>Acceptable Solutions</p>		<p>Performance Criteria</p>
<p>A1 Bicycle parking for uses that require 5 or more bicycle spaces in Table C2.1 must: (a) be accessible from a road, cycle path, bicycle lane, shared path or access way; (b) be located within 50m from an entrance; (c) be visible from the main entrance or otherwise signed; and (d) be available and adequately lit during the times they will be used, in accordance with Table 2.3 of <i>Australian/New Zealand Standard AS/NZS 1158.3.1: 2005 Lighting for roads and public spaces - Pedestrian area (Category P) lighting - Performance and design requirements</i>.</p>	<p>P1 Bicycle parking must be provided in a safe, secure and convenient location, having regard to: (a) the accessibility to the site; (b) the characteristics of the site; (c) the nature of the proposed use; (d) the number of employees; (e) the users of the site and the likelihood of travel by bicycle; (f) the location and visibility of proposed parking for bicycles; (g) whether there are other parking areas on the site; and (h) the opportunity for sharing bicycle parking on nearby sites.</p>	<p>The building proposes providing a minimum of 8 bicycle parking spaces (consistent with provisions of table C2.1). These are provided within the parking area below the building and provided for the building occupants</p> <p>It is proposed to provide 2 voluntary public bike parking spaces at the front entry of the building</p>
<p>A2 Bicycle parking spaces must: (a) have dimensions not less than: (i) 1.7m in length; (ii) 1.2m in height; and (iii) 0.7m in width at the handlebars; (b) have unobstructed access with a width of not less than 2m and a gradient not steeper than 5% from a road, cycle path, bicycle lane, shared path or access way; and (c) include a rail or hoop to lock a bicycle that satisfies <i>Australian Standard AS 2890.3-2015 Parking facilities - Part 3: Bicycle parking</i>.</p>	<p>P2 Bicycle parking spaces and access must be convenient, safe, secure and efficient to use, having regard to: (a) the characteristics of the site; (b) the space available; (c) the safety of cyclists; and (d) the provisions of <i>Australian Standard AS 2890.3- 2015 Parking facilities - Part 3: Bicycle parking</i>.</p>	<p>The 2 public bike parking facilities will be provided in accordance with A2</p> <p>The secure bicycle parking area will be provided within a dedicated secure area and be provided consistent with considerations of both the Australian Standard and AusRoads guidelines</p>
<p>C2.6.8 Siting of parking and turning areas</p>		
<p>Objective:</p>	<p>That the siting of vehicle parking and access facilities in an Inner</p>	

	Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.	
Acceptable Solutions	Performance Criteria	
A1 Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas must be located behind the building line of buildings, excluding if a parking area is already provided in front of the building line.	P1 Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas, may be located in front of the building line where this is the only practical solution and does not cause an unreasonable loss of amenity to adjoining properties, having regard to: (a) topographical or other site constraints; (b) availability of space behind the building line; (c) availability of space for vehicle access to the side or rear of the property; (d) the gradient between the front and the rear of existing or proposed buildings; (e) the length of access or shared access required to service the car parking; (f) the location of the access driveway at least 2.5m from a window of a habitable room of a dwelling; (g) the visual impact of the vehicle parking and access on the site; (h) the streetscape character and amenity; (i) the nature of the zone in which the site is located and its preferred uses; and (j) opportunities for passive surveillance of the road.	The parking on the site has been provided to minimize the impact of parking on the streetscape. All parking fronting Steele Street and Formby Road has been designed to be below the line of the building and significantly screened, to eliminate the loss of amenity to adjoining properties. The parking arrangements to King Street have been configured to respond to the adjoining future carpark proposed on the smaller allotment of the existing title. There are 6 parking spaces accessed off the land forming the right of way provided for the smaller allotment. These spaces are parallel to King Street and have been proposed as appropriate given the intended use of the adjoining site and wider streetscape character and amenity
A2 Within the Central Business Zone, on-site parking at ground level adjacent to a frontage must: (a) have no new vehicle accesses, unless an existing access is removed; (b) retain an active street frontage; and (c) not result in parked cars being visible from public places in the adjacent roads.	P2 Within the Central Business Zone, on-site parking at ground level adjacent to a frontage must be designed to screen the views of cars from public places in the adjacent roads, without blank walls facing onto a road, having regard to: (a) the streetscape; (b) any unreasonable loss of amenity of the occupants of adjoining properties; and maintaining opportunities for active uses on a street frontage in a pedestrian priority street.	In agreement with the council some parking within the site will be assigned public metered parking. These spaces are provided with direct access and visibility from the street frontage. This design has been developed in consultation with the council and aligns with council objectives. The private parking areas are provided below the building footprint. These spaces predominantly face Formby Road and incorporate part height walls and landscape zones to screen the parking areas The visual effect of the parking and mitigation of its impact to street frontage is further addressed in item C2.7.1
C2.7.1 Parking precinct plan		
Objective:	To minimise the amount of on-site car parking spaces within an area defined by a parking precinct plan, and that parking does not detract from the streetscape of the area.	
Acceptable Solutions	Performance Criteria	Response

<p>A1 Within a parking precinct plan, on-site car parking must:</p> <ul style="list-style-type: none"> (a) not be provided; or (b) not be increased above existing parking numbers. 	<p>P1 Within a parking precinct plan, on-site car parking must be necessary for the operation of the use and not detract from the streetscape, having regard to:</p> <ul style="list-style-type: none"> (a) the availability of off-street public parking spaces within reasonable walking distance; (b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> (i) variations in parking demand over time; or (ii) efficiencies gained by consolidation of parking spaces; (c) the availability and frequency of public transport within reasonable walking distance of the site; (d) the availability and frequency of other transport alternatives; (e) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; (f) the streetscape; (g) the topography of the site; (h) the location of existing buildings on the site; (i) any constraints imposed by existing development; and (j) any assessment by a suitably qualified person of the actual parking demand, determined having regard to the scale and nature of the use and development, and not exceed the number specified in Table C2.1. 	<p>Response:</p> <p>The subject site is located within a Parking Precinct Plan area as defined with the Tasmanian Planning Scheme and the provision of 66 on-site car parking spaces is proposed with the application. Therefore, assessment relies on compliance with the requirements of Performance Criteria P1.</p> <p>The application features a three storey-building which will accommodate a Mental Health Centre and associated Department of Health office space. For the purposes of clause 6.2 of the Tasmanian Planning Scheme, this use can be appropriately categorised as <i>Business and Professional Services</i>.</p> <p>It is suggested that the provision of on-site parking is not unreasonable in the circumstances and that an appropriate satisfaction can be found with the requirements of P1 above, when regard is given to the following considerations:</p> <p>Minimising of on-site car parking</p> <ul style="list-style-type: none"> (i) The site currently features an existing car park which generally operates under a 'Care Park Australia' arrangement – essentially a user pays parking system. A site inspection identified an approximate total of 78 parking spaces existing as part of this arrangement. Of these 78 car spaces, 63 are on proposed Lot 1 and 15 on proposed Lot 2. With the plans for Lot 1 showing 66 car spaces, there are only 3 additional car spaces proposed on Lot 1 above what currently exists on that portion of the site. (ii) The 78 spaces currently located on 6-10 Steele Street are managed by Care Park, who rent out spaces on both a long-term permit system and a short-term pay by the hour/day system. Notably this existing use of the site as a car park operates as a standalone activity – it does not represent parking associated with another use occurring on the site. (iii) The 66 car spaces to be built as part of this development will be utilised by staff and patients of the site. This will result in the persons currently parking on lot 1 having to find alternative parking spaces, likely pushing a greater demand onto the Council owned King Street Car Park. The proposed Mental Health Service and offices to be located on this site are new to Devonport and consequently will introduce new additional car parking demand to the CBD. (iv) Whilst it is recognised that the existing carpark further up King/Steele Street, surrounding on-street car parking, they have insufficient capacity to cater for both the transferral of the current users of the carpark on Lot 1 and the new demand created by this development. (v) Similarly, whilst the site is well serviced for public transport and bike paths, they aren't currently well used and are unlikely to significantly reduce car parking demand on the site. (vi) The on-site parking proposed is limited to what is appropriate for the intended use and development. In further recognition of the sensitivities around patient care at the Mental Health Centre – it is not unreasonable to provide on-site parking. Additionally, we note that patients accessing health facilities often have health additional mobility issues requiring parking to be closely located to the facility.
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		<p>(vii) The onsite parking proposed does not exceed the number specified in Table C2.1 of the Tasmanian Planning Scheme which equates to a requirement of 115 car parking spaces¹. The 66 on-site parking spaces proposed with this application is significantly less than this requirement, representing approximately 57% of that figure. Furthermore, and based on advice from the proponent, there would be an expected general building occupancy of approximately 160 people. The number of on-site parking spaces proposed with this application represents only 41% of that expected occupancy. Following this logic, there is a minimising circumstance for the on-site parking proposed.</p> <p>(viii) The parking associated with this application will be subservient to the proposed Mental Health Centre and associated Department of Health office space. In this context there is a shift away from the use of the site as a dedicated/specific car park towards parking that is directly associated with and subservient to another use on the site.</p> <p>(ix) The provision of on-site parking has been minimised and limited to what is appropriate for the intended operation of the site and following the rationale presented in paragraphs (i)-(viii) above, a reasonable justification is made that there is a 'minimising' context for the provision of that parking.</p> <p><i>No unreasonable detractor from the streetscape</i></p> <p>(x) The subject site features road frontages onto Formby Road, Steele Street, and King Street. The site contains an existing car park which is currently visually prominent from all three frontages.</p> <p>(xi) As shown with the submitted plans (including the render design detail shown with Drawing DA-410-010) the building elevations onto the Formby Road and Steele Street frontages represent the predominant streetscape presentation.</p> <p>(xii) The proposed on-site parking areas have been located and suitably screened so as not to visually dominate the main frontages of the site nor to dominate or otherwise detract from any of the main building elevations. Notably, the majority of the proposed parking spaces will be contained within the proposed building footprint and with the upper building levels extending over these areas and also with appropriate screening treatments applied. These considerations together with the landscaping works proposed with the application will minimise any unreasonable visual impact of parking within the site upon the streetscape.</p> <p>(xiii) Additionally, the site is located within an established built-up area with a readily apparent mixture of building form, and with a number of existing on-site external parking areas observed on adjacent sites within the</p>
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¹ Following the *Business and Professional Services* use classification under Table C2.1 of the Tasmanian Planning and utilizing the figure of 1 space per 30m² of floor area. As detailed with the submitted application plans, Drawing Nos. DA-210-010 and DA-210-020 show a combined office space of 3444m² for building levels 1 and 2.

DESIGN MEMORANDUM

PROJECT:	6-10 Steele Street Devonport	DM NO:	02
PROJECT NO:	25088	DATE:	17/04/2025
FROM:	Daniel Carew		
ATTENTION:	COMPANY:	EMAIL:	
Luis Sorrentino	NH Architecture	Luis.Sorrentino@nharch.net	
Dominique Pozvek	NH Architecture	Dominique.Pozvek@nharch.net	
Sam Perversi-Brooks	NH Architecture	sam.perversi-brooks@nharch.net	

Services Summary

The following summarises the site services for the proposed development at 6-10 Steele Street, Devonport, Tasmania.

Effective Tenements (Water)

Based on the TasWater ET schedule where all levels are considered office space (GFA x 0.006),

GF – 267.39m²

L1 – 1986.78m²

L2 – 1986.78m²

Total – 4240.95m²

ET – 4240.95 x 0.006 = **25.45**

Water Demand

For 25.45 ET's the probable simultaneous demand is **3.06L/s**.

Calculated from AS/NZS 3500.1:2021.

Fire Water Demand

The fire water flow requirements are as follows:

Fire hydrants: 20 L/s.

Required Simultaneous Demand (Water)

The total required simultaneous demand is 23.5 L/s @ 550 kPa.

Effective Tenements (Sewer)

Referring to TasWater Supplement to WSA-02 2014-3.1

Average dry weather flow (ADWF)

Assumed loading rating of 450L/ET/day as this building is considered new

$$450 \times 25.45 = 11,452.5\text{L/day} = \mathbf{0.133\text{L/s}}$$

Diversified Maximum Demand

AS/NZS 3000 diversified maximum demand attached for the site, resulting in estimated load of 470kVA.

Based on early engagement meeting with TasNetworks conducted on 01/04/2025, this will require an onsite substation.

Substation details also attached as provided by TasNetworks. TasNetworks are currently conducting a Connection Capacity Report to assess the HV feeders in the area and confirm most appropriate origin of connection. Kiosk substation is planned on site based on this preliminary advice, and noting TasNetworks' preference for a kiosk over and indoor substation.

— End of Design Memorandum —

MAXIMUM DEMAND CALCULATIONS - NON-DOMESTIC ELECTRICAL INSTALLATIONS
WRAP ENGINEERING



Project: 6-10 Steele Street Devonport TAS - Devonport Mental Health Hub

Job Number: 25088

Calculated by: CA/WD

Date: 20-Mar-25

S:\Projects\25000 Projects\25088 - 6-10 Steele Street Devonport TAS - Devonport Mental Health Hub\06 Engineering\Elec\MD\25088 yymmdd 6-10 Steele Street Devonport TAS - Devonport Mental Health Hub - Campbells AS3000 Maximum Demand Rev_3.xlsm]Template (Tbl C3)

GRAND TOTAL:	
SPARE	0%
kVA	470 kVA
A/PH	679 A/PH

Calculations Based on AS/NZS 3000:2018 Appendix C, Table C3

Edit Reference Table

Select reference area / Edit directly

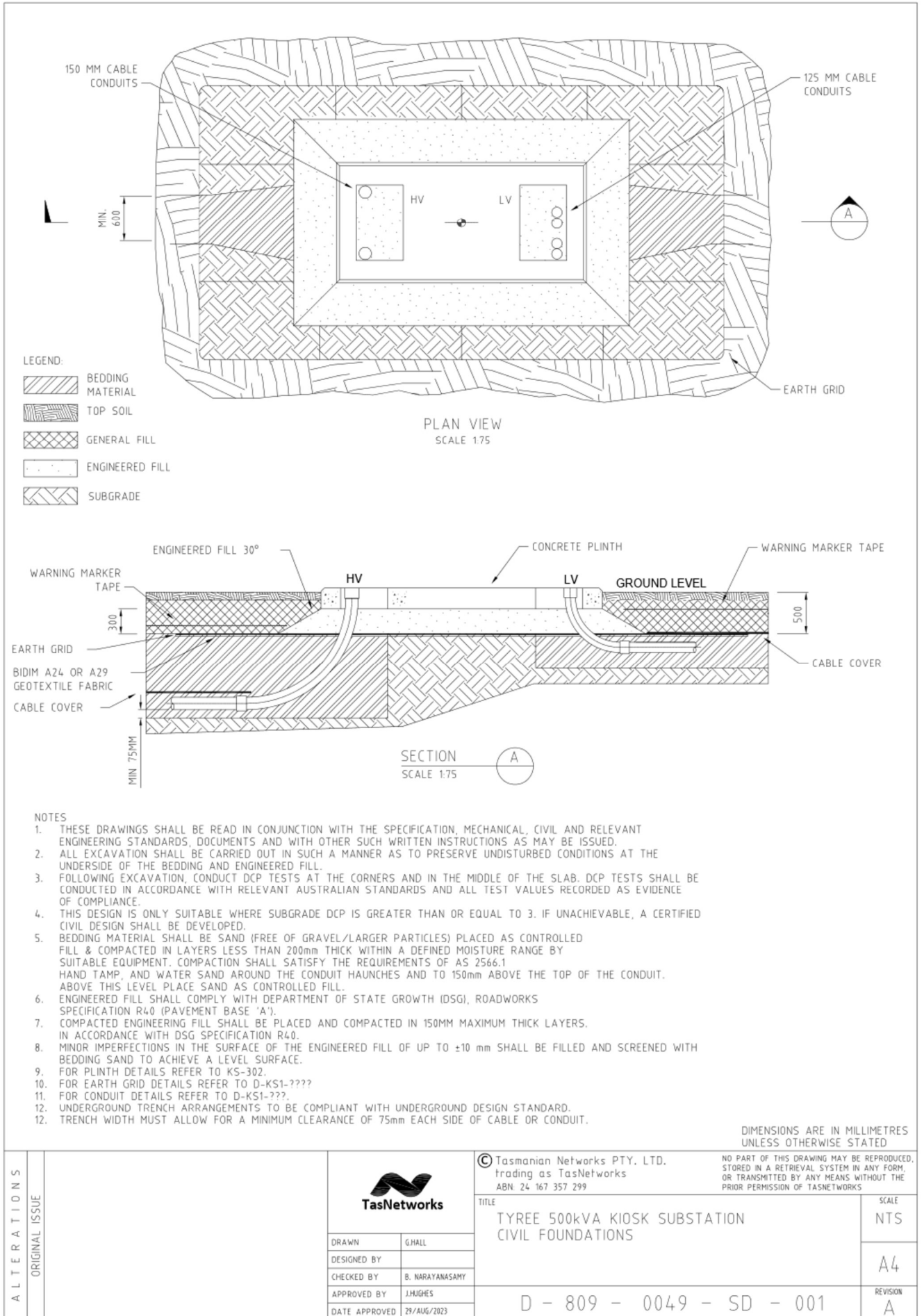
Level	Type of Area	Area	Reference	Lighting (VA/m ²)	Power (VA/m ²)	Mech (VA/m ²)	TOTAL (VA/m ²)	Load	Notes
Ground	Carpark	2775 m ²	Carpark	5	0	0	5	14 kVA	
	BOH	18 m ²	Plant/BOH	5	10	5	20	0 kVA	
	EOT	67 m ²	EOT	10	15	35	60	4 kVA	
	MHH Lobby	65.49 m ²	Lobby	15	15	50	80	5 kVA	
	Office lobby	59 m ²	Lobby	15	15	50	80	5 kVA	
	Services	17 m ²	Plant/BOH	5	10	5	20	0 kVA	
	Transfer Zone	46 m ²	Plant/BOH	5	10	5	20	1 kVA	
Level 01	Mental Health Hub	1767 m ²	Office	15	35	50	100	177 kVA	
	Amenities	58.94 m ²	Toilets	5	10	15	30	2 kVA	
	Stairwell	25.96 m ²	Core	5	5	10	20	1 kVA	
	Stairwell 02	37 m ²	Core	5	5	10	20	1 kVA	
	Stairwell 03	14 m ²	Core	5	5	10	20	0 kVA	
Level 02	Office	1774 m ²	Office	15	35	50	100	177 kVA	
	Amenities	58.94 m ²	Toilets	5	10	15	30	2 kVA	
	Stairwell	25.96 m ²	Core	5	5	10	20	1 kVA	
	Stairwell 02	37 m ²	Core	5	5	10	20	1 kVA	
Total								390 kVA	
									563 A/PH

Other Loads (Base Building)

Service	Plant Equipment	PHASE	Voltage	A/PH	kVA	Diversity	Load
Hydraulics	Hot Water Heat Pump	3	400	40.0	27.71	1.00	28 kVA
						0.50	
Electrical - Lifts	Lift01	3	400	32.0	22.17	1.25	28 kVA
						0.75	17 kVA
Mechanical						0.80	
						0.80	
EV	6No. As per NCC	3	400	15.0	10.39	0.80	8 kVA
						1.00	
Total						0.80	80 kVA
							116 A/PH

Total Load (Table C3) - A/3-ph	678.8 A (Three Phase)
Total Load (Table C3) - kVA	470.3 kVA

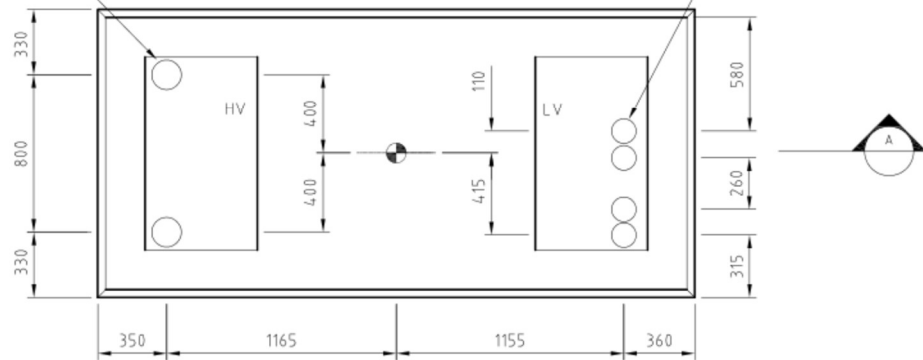
7.1.2 Standard 500 kVA kiosk



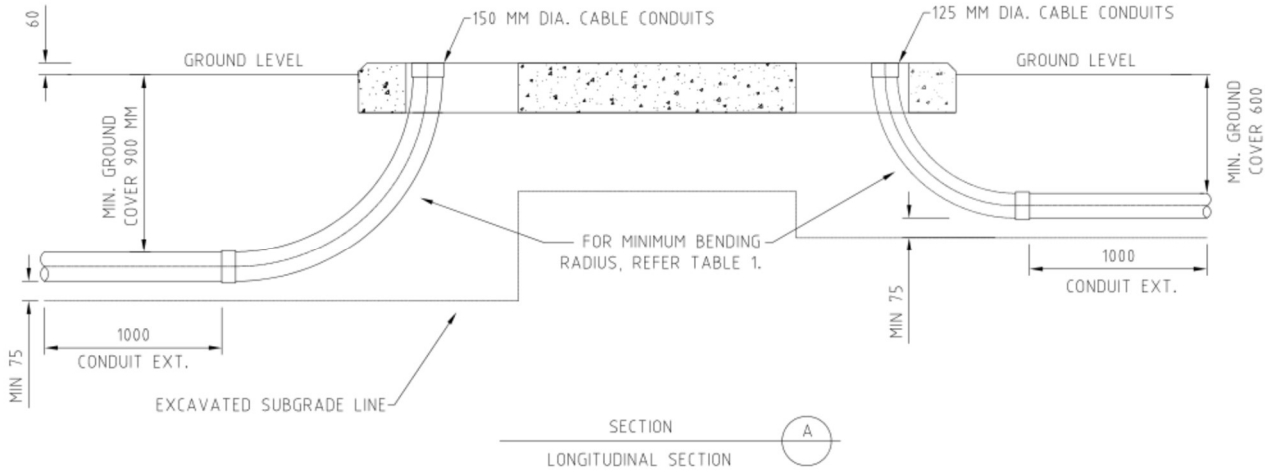
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150MM DIA. CABLE CONDUITS
STD. LOCATION OF HV CONDUITS
ALTERNATIVE ARRANGEMENTS
TO BE SPECIFIED BY DESIGNER.

125MM DIA. CABLE CONDUITS
STD. LOCATION OF LV CONDUITS
ALTERNATIVE ARRANGEMENTS
TO BE SPECIFIED BY DESIGNER.



PLAN VIEW
CONDUIT DETAILS



SECTION
LONGITUDINAL SECTION

NOTES:

1. CONDUITS SPACING SUITABLE FOR TYREE 500KVA KIOSKS ONLY.
2. THIS ARRANGEMENT SHALL FORM PART OF THE SITE SPECIFIC CIVIL DESIGN.
3. PRIOR TO ANY CONSTRUCTION WORK COMMENCING, THE ABILITY TO APPLY THIS DESIGN WITHOUT COMPROMISE SHALL BE CONFIRMED.
4. FOR CIVIL FOUNDATION DETAILS, REFER TO:
TYREE: D-0809-0049-SD-001
SCHNEIDER: KS-317
5. FOR EARTHING SYSTEM DETAILS, REFER TO DRAWINGS K-309.
6. FOR KIOSK PLINTH DETAILS, REFER TO DRAWING K-302.
7. THE CABLE MINIMUM BENDING RADII PROVIDE IN TABLE 1 ARE ONLY FOR CABLES BEING PUSHED THROUGH THE CONDUITS DURING INSTALLATION. NO TENSION IS ALLOWED FOR IN THE INSTALLATION. IF CABLES ARE BEING PULLED IN, THEN THE MANUFACTURER'S MINIMUM RADIUS LISTED FOR 'DURING INSTALLATION' SHALL BE APPLIED'.
8. FOR DETAILED CABLE INFORMATION, REFER TO DISTRIBUTION DESIGN STANDARD - UNDERGROUND SYSTEMS - R0000392089.

TABLE 1: CABLE BENDING RADII

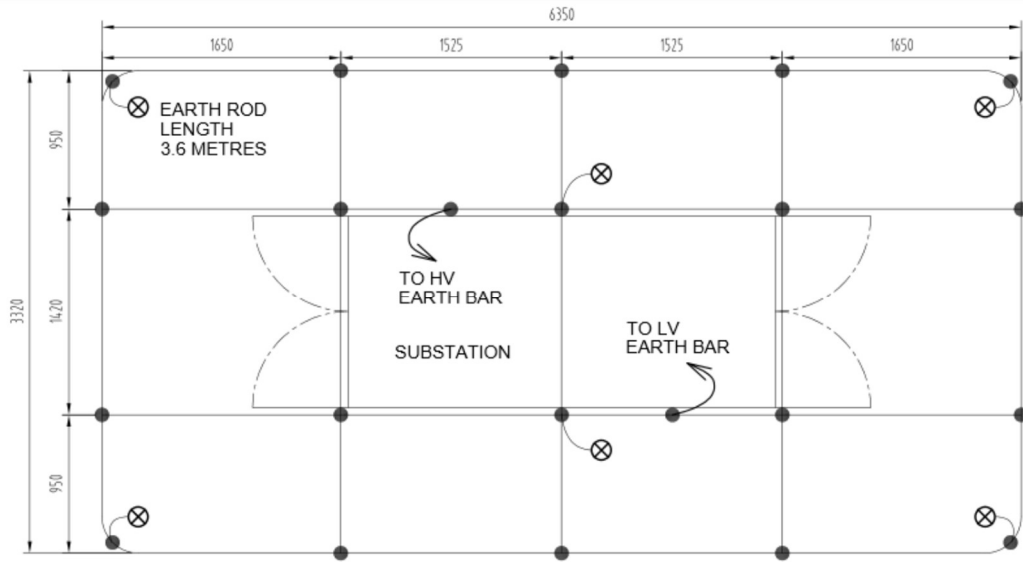
HV CABLE		LV CABLE	
CABLE SIZE	MIN. RADIUS (mm)	CABLE SIZE	MIN. RADIUS (mm)
11kV 185mm ²	985	185mm ²	355
11kV 240mm ²	1060	240mm ²	465
22kV 185mm ²	985	300mm ²	500
22kV 240mm ²	1060	400mm ²	570

DIMENSIONS ARE IN MILLIMETRES
UNLESS OTHERWISE STATED

ALTERATIONS	ORIGINAL ISSUE			© Tasmanian Networks PTY. LTD. trading as TasNetworks ABN 24 167 357 299		NO PART OF THIS DRAWING MAY BE REPRODUCED, STORED IN A RETRIEVAL SYSTEM IN ANY FORM, OR TRANSMITTED BY ANY MEANS WITHOUT THE PRIOR PERMISSION OF TASNETWORKS	
	LV CONDUIT SPACING REVISED.			TITLE TYREE 500KVA KIOSK SUBSTATION CONDUIT DETAILS		SCALE NTS	
	B			DRAWN G.HALL		A4	
	DESIGNED BY G.HALL			CHECKED BY A.MKAY		REVISION B	
APPROVED BY J.HUGHES	DATE APPROVED 29/AUG/2023	D - 809 - 0050 - SD - 001					

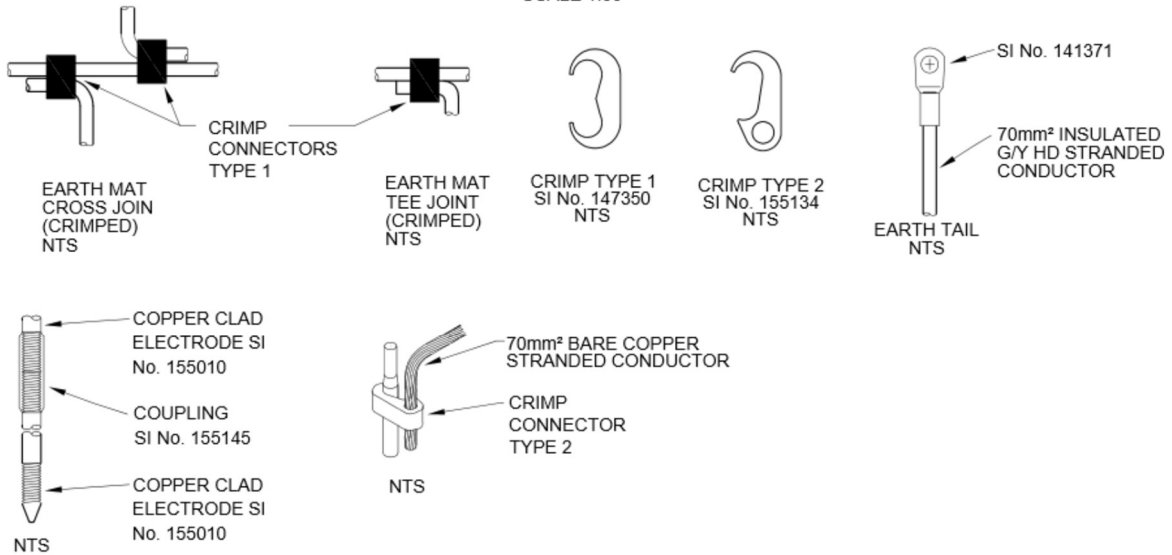
Distribution Design and Construction Standard - Kiosk Substations

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EARTH GRID PLAN

SCALE 1:50



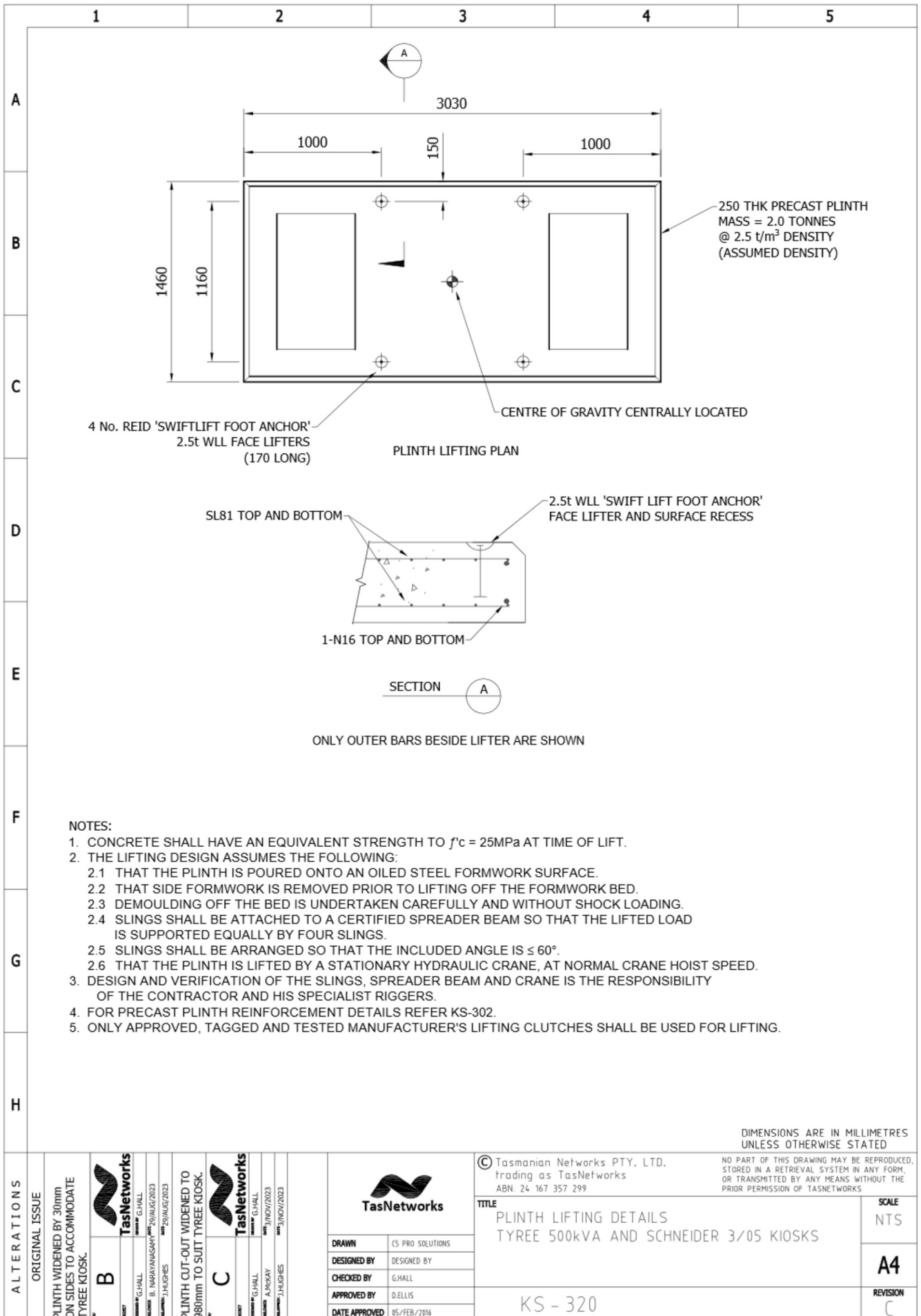
NOTES:

- DESIGNED TO SUIT TYREE AND SCHNEIDER 500kVA KIOSK SUBSTATIONS.
- STANDARD DESIGN ONLY TO BE USED WHERE SITE CONDITIONS ALLOW. REFER KIOSK DESIGN STANDARD, SECTION 2 FOR SUITABILITY.
- DEPTH OF EARTH MAT MUST NOT BE LESS THAN 500mm BELOW FINISHED SUBSTATION GROUND LEVEL.
- WHERE COPPER CLAD EARTH RODS ARE USED TO THEY MUST NOT BE BENT OVER TO REMOVE "EXCESS" BELOW THE SURFACE. EXCESS MUST BE CUT OFF.
- EARTHING TO BE RUN AS FAR AS POSSIBLE IN ONE CONTINUOUS LENGTH WITH NO SHARP BENDS.
- ALL CONNECTIONS ON EARTH GRID TO BE CRIMPED WITH APPROVED CRIMPS.
- EARTH GRID 70mm² Cu CONDUCTOR.
- EARTH GRID CONNECTIONS 120mm² Cu CONDUCTOR.
- FOR PLINTH DETAIL REFER TO KS-302.

500kVA KIOSK EARTHING SYSTEM BOM : 730026

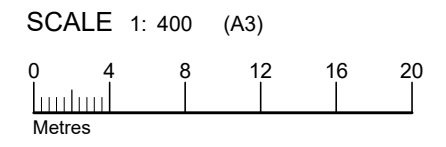
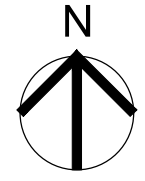
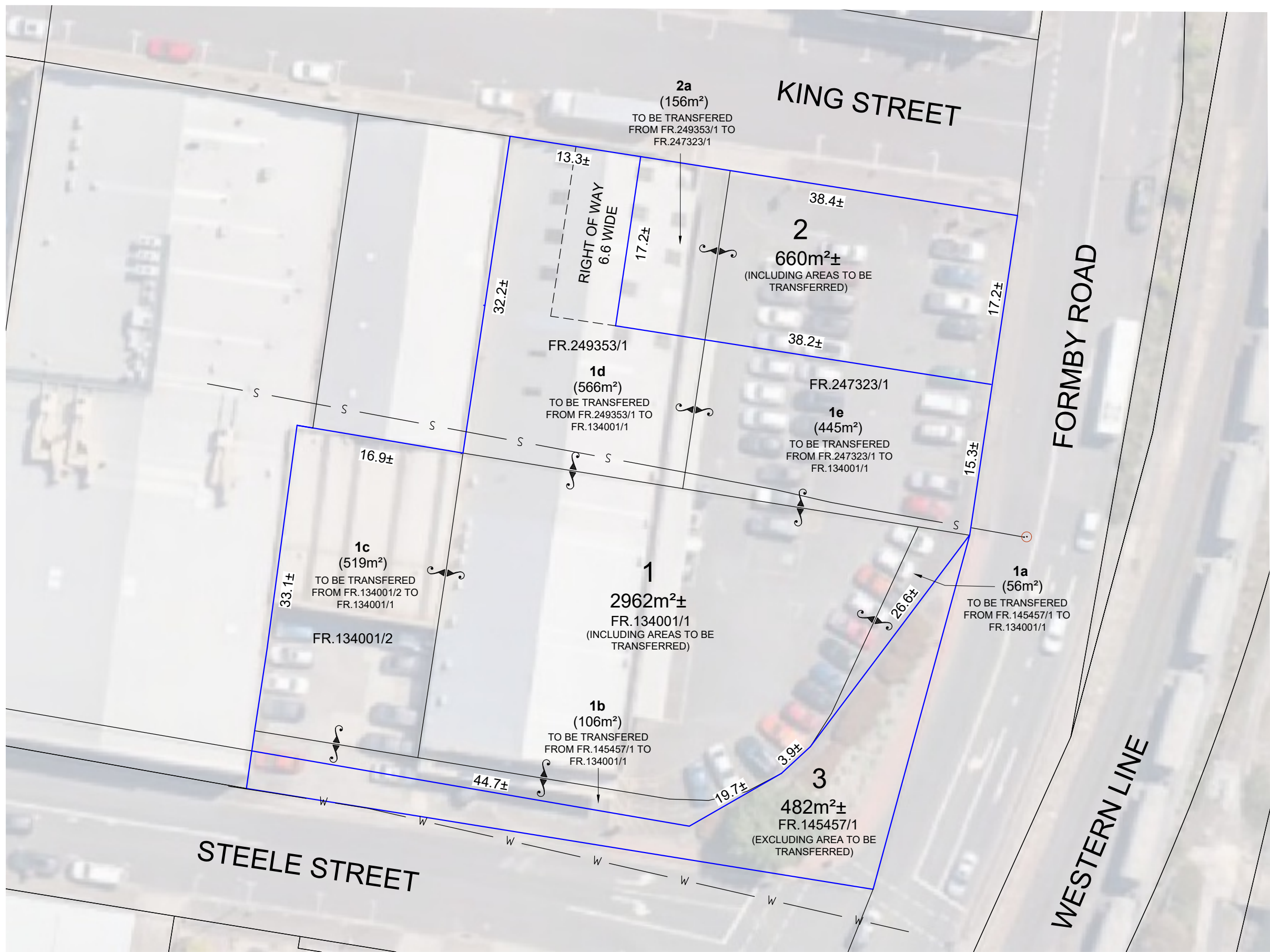
MATERIAL	QTY	S.I. NO.
70MM2 BARE COPPER CONDUCTOR	45 M	102311
EARTH ROD 1800 MM	12	155010
EARTH ROD COUPLING	6	155145
CRIMP - CONDUCTOR TO COND.	18	147350
CRIMP - CONDUCTOR TO ROD	6	155134
70MM2 INSULATED CONDUCTOR	5 M	94433
CRIMP LUG FOR 70MM2 CONDUCTOR	2	141371

ALTERATIONS		DRAWN BY G.HALL DATE 29/AUG/2023	TasNetworks PTY. LTD. ABN: 24 167 357 299	© COPYRIGHT - TASNWORKS PTY. LTD. NO PART OF THIS DRAWING MAY BE REPRODUCED, STORED IN A RETRIEVAL SYSTEM IN ANY FORM, OR TRANSMITTED BY ANY MEANS WITHOUT PRIOR PERMISSION OF TASNWORKS.	TITLE	SCALES			
		DESIGNED BY G.HALL DATE 29/AUG/2023			DIMENSIONS ARE IN MILLIMETRES, UNLESS OTHERWISE STATED.	KIOSK EARTH GRID AND FITTINGS TYREE 500kVA AND SCHNEIDER 3/05 KIOSKS	AS INDICATED		
		DRAFTING CHECK H.WESTBURY DATE 29/AUG/2023						DRAWN CS PRO-SOLUTIONS	SIZE
		ENG. APPROV. J.HUGHES DATE 29/AUG/2023						CHECKED G.HALL	A4
REV PROJECT D	APPROVED DATE 05/02/2016	APPROVED DATE 05/02/2016	KS-309	REVISION D					



DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED

ALTERATIONS	ORIGINAL ISSUE	<p>PROJECT: TasNetworks DESIGNED BY: G.HALL CHECKED BY: B. KARAYANAKIS DATE: 29/AUG/2023</p>	<p>PROJECT: TasNetworks DESIGNED BY: G.HALL CHECKED BY: A. HICKAY DATE: 31/NOV/2023</p>		<p>© Tasmanian Networks PTY. LTD. trading as TasNetworks ABN: 24 167 357 299</p> <p>NO PART OF THIS DRAWING MAY BE REPRODUCED, STORED IN A RETRIEVAL SYSTEM IN ANY FORM, OR TRANSMITTED BY ANY MEANS WITHOUT THE PRIOR PERMISSION OF TASNETWORKS</p>	SCALE
	PLINTH WIDENED BY 30mm ON SIDES TO ACCOMMODATE TYREE KIOSK.					PLINTH CUT-OUT WIDENED TO 980mm TO SUIT TYREE KIOSK.
					PLINTH LIFTING DETAILS TYREE 500kVA AND SCHNEIDER 3/05 KIOSKS	A4
					KS - 320	REVISION C



LEGEND:

- EXISTING SEWER
- EXISTING WATER
- DESIGN BOUNDARY
- EXISTING BOUNDARY

PRELIMINARY

20/03/2025

Owners Devonport City Council	Title References FR.134001/2 & 134001/1, 247323/1 & 249353/1 & 145457/1	PID 6345667	Council Devonport City Council	Planning scheme Tasmanian Planning Scheme	Zone 16.Central Business	Zone Codes 2.PST, 16.SAC	Schedule of Easements (Existing easements to be retained unless noted otherwise.) Right of Way 6.6m Wide In Favor of Lot 1
<p>This plan has been prepared only for the purpose of obtaining preliminary subdivision approval from the Council and the information shown hereon should be used for no other purpose. All measurements and areas are subject to final survey.</p>				PLANNER: PP DRAWN: CB JOB MANAGER: MICHAEL FRENCH PRINTED DATE: 20/03/2025	REVIEWED: - REVIEWED: MF	CLIENT: DEVONPORT CITY COUNCIL PROJECT DESCRIPTION: DEVELOPMENT APPLICATION ADDRESS: 6-10 STEEL STREET, DEVONPORT DRAWING TITLE: PLAN OF SUBDIVISION 3 LOTS	<div style="text-align: center;"> <p>PDA</p> <p>SURVEYORS, ENGINEERS & PLANNERS</p> </div> <p>6 Queen Street Burnie, Tasmania, 7320 PHONE: +61 03 6431 4400 FAX: +61 03 6431 6663 EMAIL: pda.bne@pda.com.au www.pda.com.au</p> <p>Also at: Launceston, Devonport, Hobart & Kingston</p>
REV AMENDMENTS DRAWN DATE APPR. THIS SHEET MAY BE PRINTED USING COLOUR AND MAY BE INCOMPLETE IF COPIED		SCALE 1:400 (A3) JOB NUMBER DISCIPLINE SHEET REVISION 54268 DA 01 -					

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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25



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DEVELOPMENT APPLICATION DRAWING REGISTER

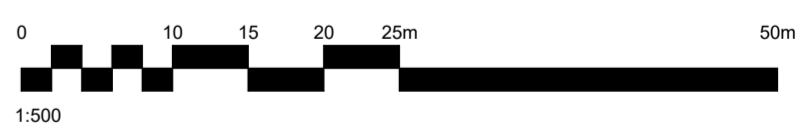
DA-000-000	COVER SHEET	B	12.06.25
DA-100-000	OVERALL EXISTING SITE PLAN	A	17.04.25
DA-101-000	DEMOLITION PLAN	A	17.04.25
DA-110-000	PROPOSED SITE PLAN	B	12.06.25
DA-210-000	GENERAL ARRANGEMENT - GROUND	A	17.04.25
DA-210-010	GENERAL ARRANGEMENT - LEVEL 01	A	17.04.25
DA-210-020	GENERAL ARRANGEMENT - LEVEL 02	A	17.04.25
DA-210-040	GENERAL ARRANGEMENT - ROOF	A	17.04.25
DA-300-010	ELEVATIONS	B	12.06.25
DA-300-020	ELEVATIONS	B	12.06.25
DA-310-010	SECTIONS	B	12.06.25
DA-310-020	SECTIONS	B	12.06.25
DA-410-010	VIEWS	A	17.04.25
DA-510-010	MATERIALS & FINISHES	A	17.04.25

PROJECT AREA SUMMARY

GROUND LEVEL		LEVEL 02		TOTALS	
Function	Area	Function	Area	Function	Area
AMENITIES	41 m ²	AMENITIES	56 m ²	AMENITIES	153 m ²
EXTERNAL SPACE	224 m ²	EXTERNAL SPACE	73 m ²	EXTERNAL	75 m ²
GENERAL BUILD	275 m ²	GENERAL BUILD	1832 m ²	EXTERNAL SPACE	297 m ²
PRIVATE CARPARK	1139 m ²	STAIRS	64 m ²	GENERAL BUILD	3936 m ²
PUBLIC CARPARK	1143 m ²			PRIVATE CARPARK	1139 m ²
				PUBLIC CARPARK	1143 m ²
				ROOF	1889 m ²
				SECURE BIKE PARKING	44 m ²
				STAIRS	201 m ²
				Total	8878 m ²
LEVEL 01		ROOF LEVEL			
Function	Area	Function	Area		
AMENITIES	56 m ²	ROOF	1889 m ²		
EXTERNAL	75 m ²	STAIRS	19 m ²		
GENERAL BUILD	1829 m ²				
STAIRS	64 m ²				

DRAWN CHECKED SCALE @A1

LS SPB



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
240064

STAGE
DESIGN DEVELOPMENT

DRAWING TITLE
COVER SHEET

DRAWING No. **DA-000-000** REVISION **B**

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REV.	DETAILS	DATE
A	DA SET	17.04.25



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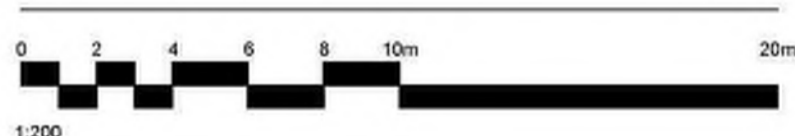


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 DEVONPORT TAS



NH Architecture

DRAWN: LS CHECKED: SPB SCALE @A1: 1:200 NORTH



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 03/03/25

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
OVERALL EXISTING SITE PLAN

DRAWING No. **DA-100-000** REVISION **A**

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REV.	DETAILS	DATE
A	DA SET	17.04.25

- GENERAL NOTES:**
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 2. ALL WORKS OUTSIDE OF TITLE BOUNDARY, INCLUDING BUT NOT LIMITED TO: ROAD RE-ALIGNMENTS, CURBS AND LANDSCAPING, ARE INDICATIVE ONLY AND TO BE DETERMINED AND DESIGNED BY DEVONPORT CITY COUNCIL
 3. RED DASHED AND CROSSHATCH TO BE DEMOLISHED. EXISTING SERVICES TO BE DEMOLISHED OR CUT AND SEALED OFF AS REQUIRED. DIVERT SERVICES AFFECTED BY DEMOLITION IN ACCORDANCE WITH ENGINEERS DOCUMENTATION.

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LINETYPE LEGEND
 RED DASHED LINES DENOTE DEMOLITION WORKS

DRAWN: LS, CHECKED: SPB, SCALE: @A1, NORTH: As indicated



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064, DATE 03/03/25

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
DEMOLITION PLAN

DRAWING No. **DA-101-000**, REVISION **A**



KING ST

STEELE ST

FORMBY RD

15-19 KING ST

15-19 KING ST

6-10 STEELE ST
 FR: 149353/1
 OWNER: DEVONPORT CITY COUNCIL

6-10 STEELE ST
 FR: 247323/1
 OWNER: DEVONPORT CITY COUNCIL

6-10 STEELE ST
 FR: 134001/2
 OWNER: DEVONPORT CITY COUNCIL

6-10 STEELE ST
 FR: 134001/1
 OWNER: DEVONPORT CITY COUNCIL

5 STEELE ST

65 FORMBY RD

ROAD
 FR: 145457/1
 OWNER: DEVONPORT CITY COUNCIL

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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25
C	DA SET	03.07.25



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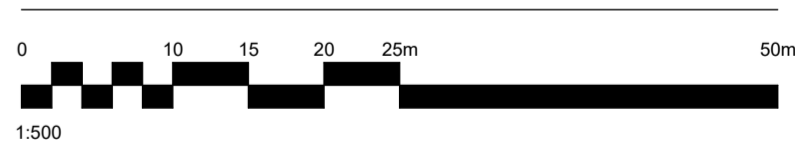
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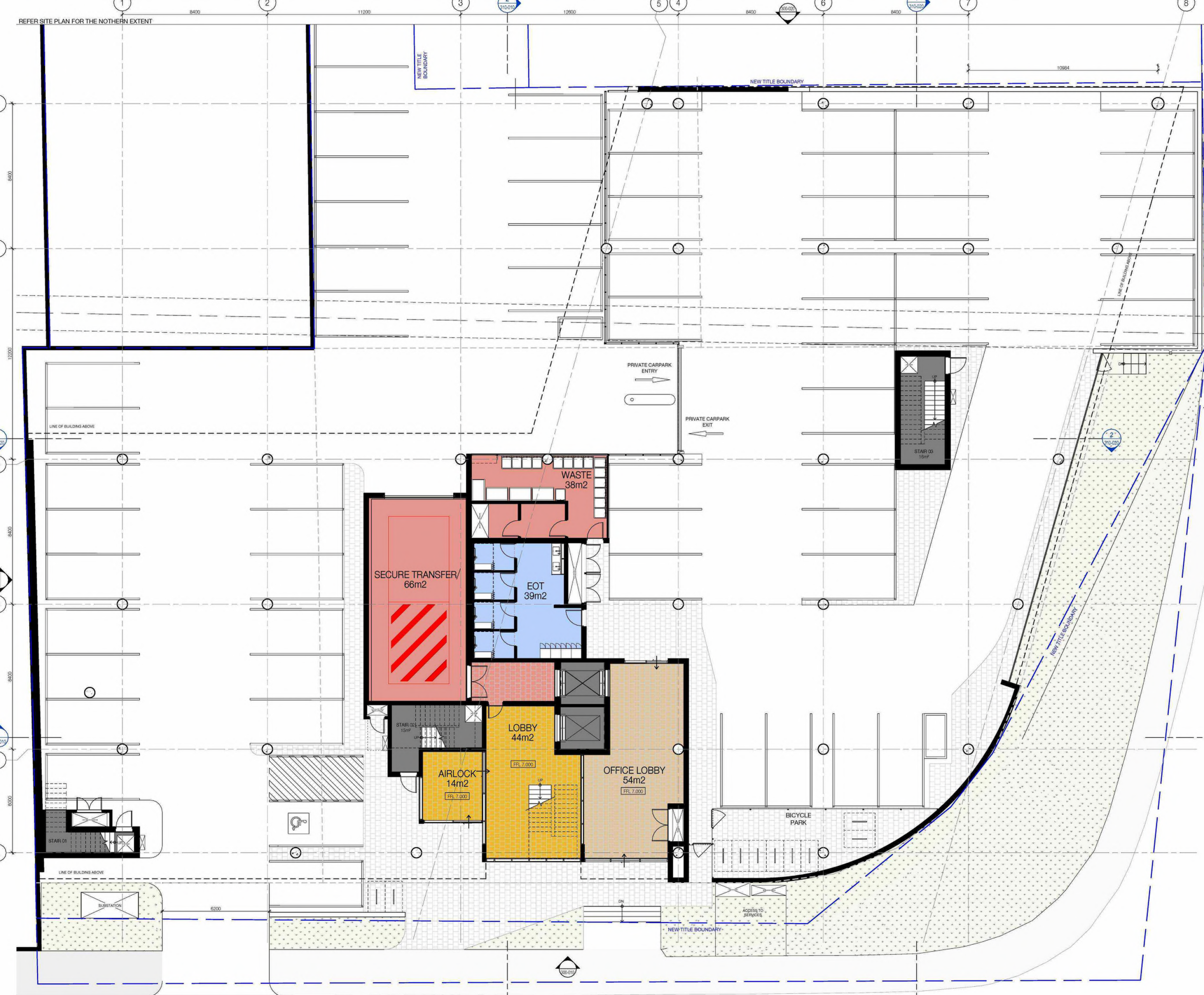
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
 240064

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
PROPOSED SITE PLAN

DRAWING No. **DA-110-000** REVISION **C**



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REV.	DETAILS	DATE
A	DA SET	17.04.25

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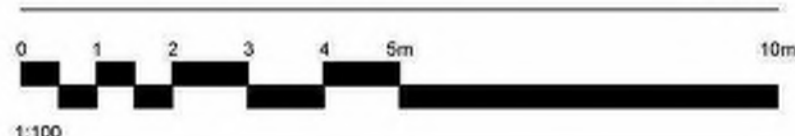


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NH Architecture

DRAWN: LS CHECKED: SPB SCALE: @A1 NORTH



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 10/23/24

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
GENERAL ARRANGEMENT - GROUND

DRAWING No. **DA-210-000** REVISION **A**

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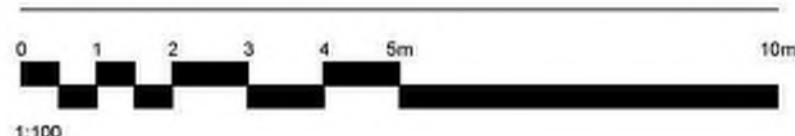


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DRAWN: LS CHECKED: SPB SCALE: @A1 NORTH



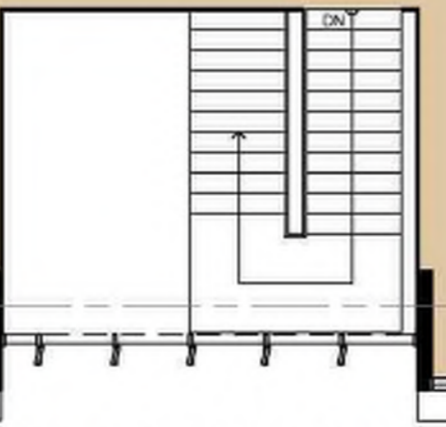
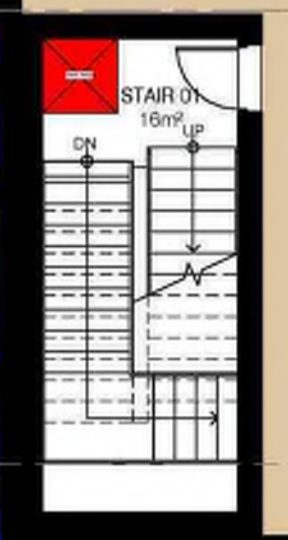
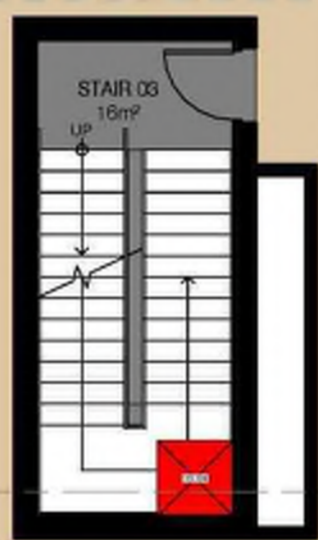
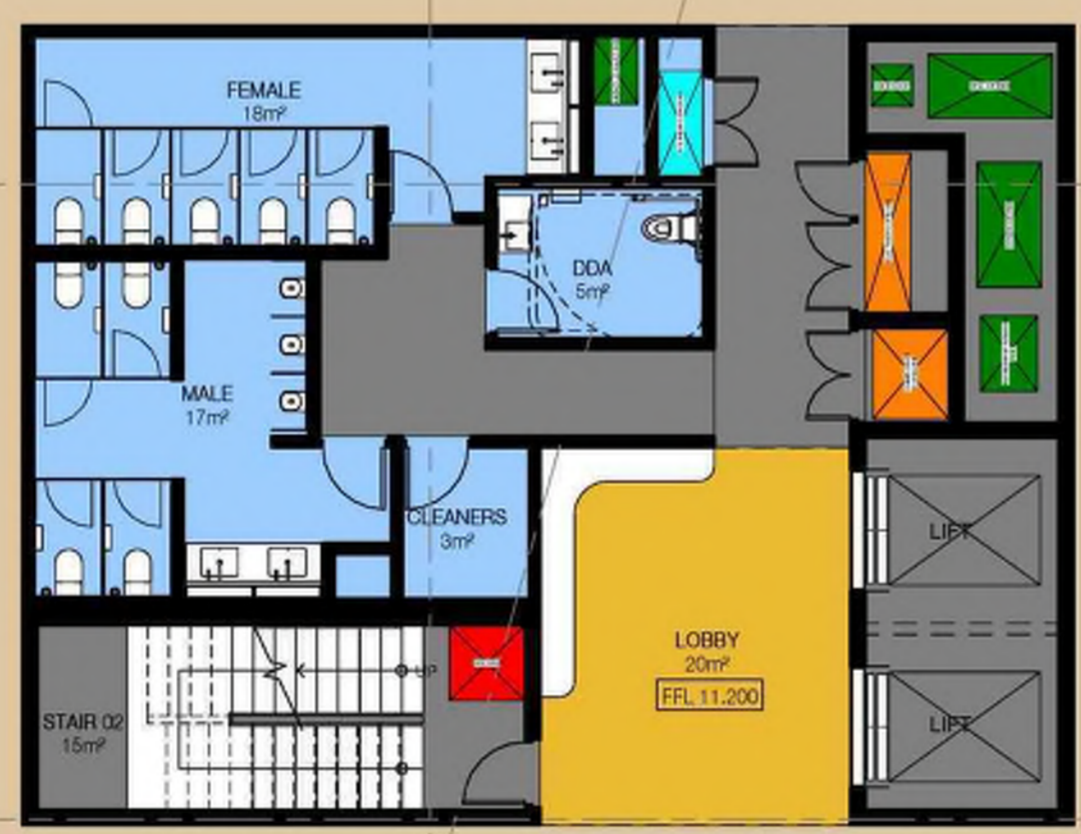
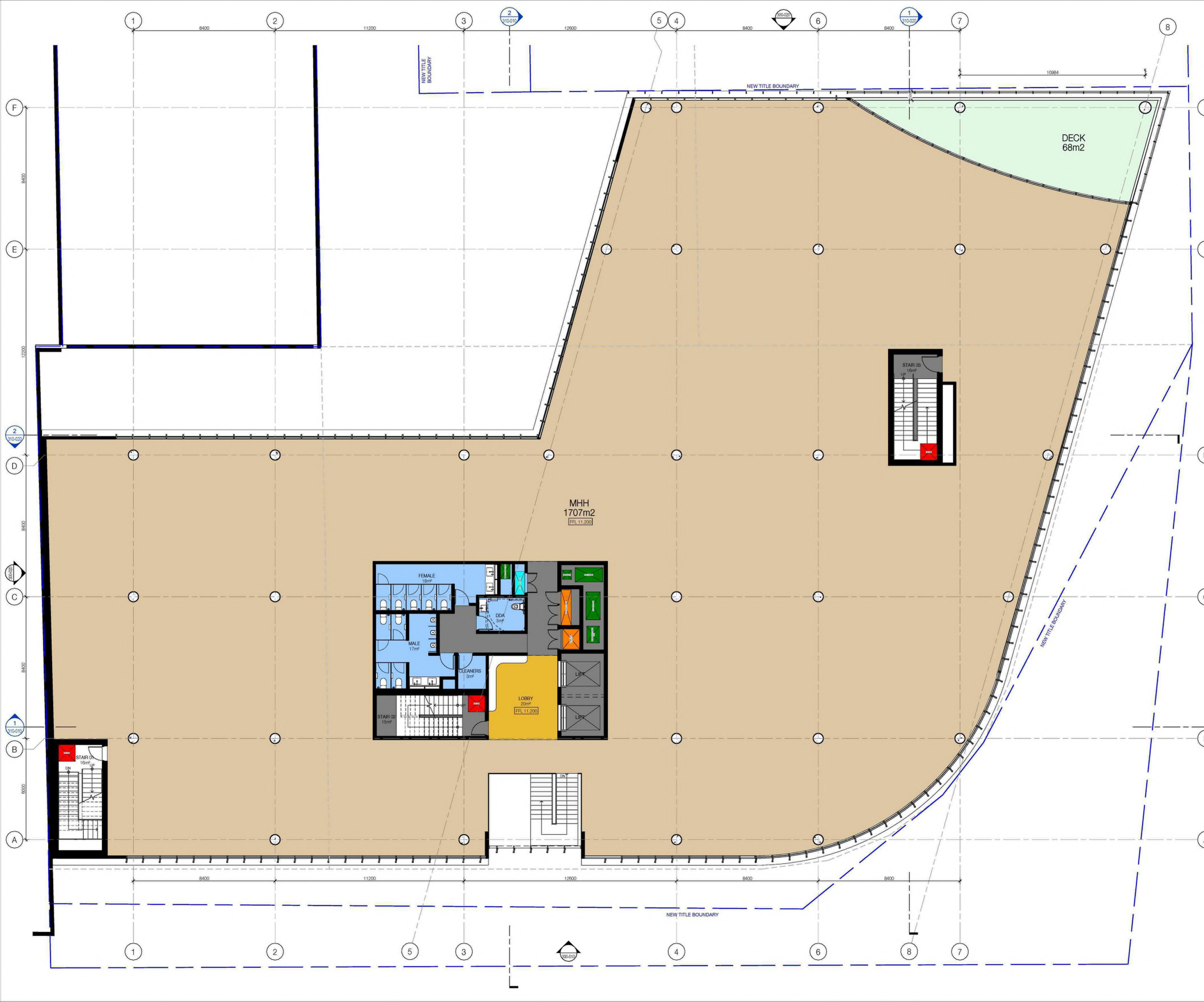
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 10/23/24

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
GENERAL ARRANGEMENT - LEVEL 01

DRAWING No. **DA-210-010** REVISION **A**



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A	DA SET	17.04.25

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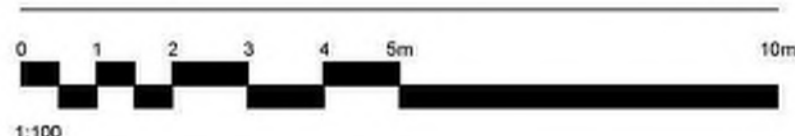


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NH Architecture

DRAWN: LS CHECKED: SPB SCALE: @A1 NORTH



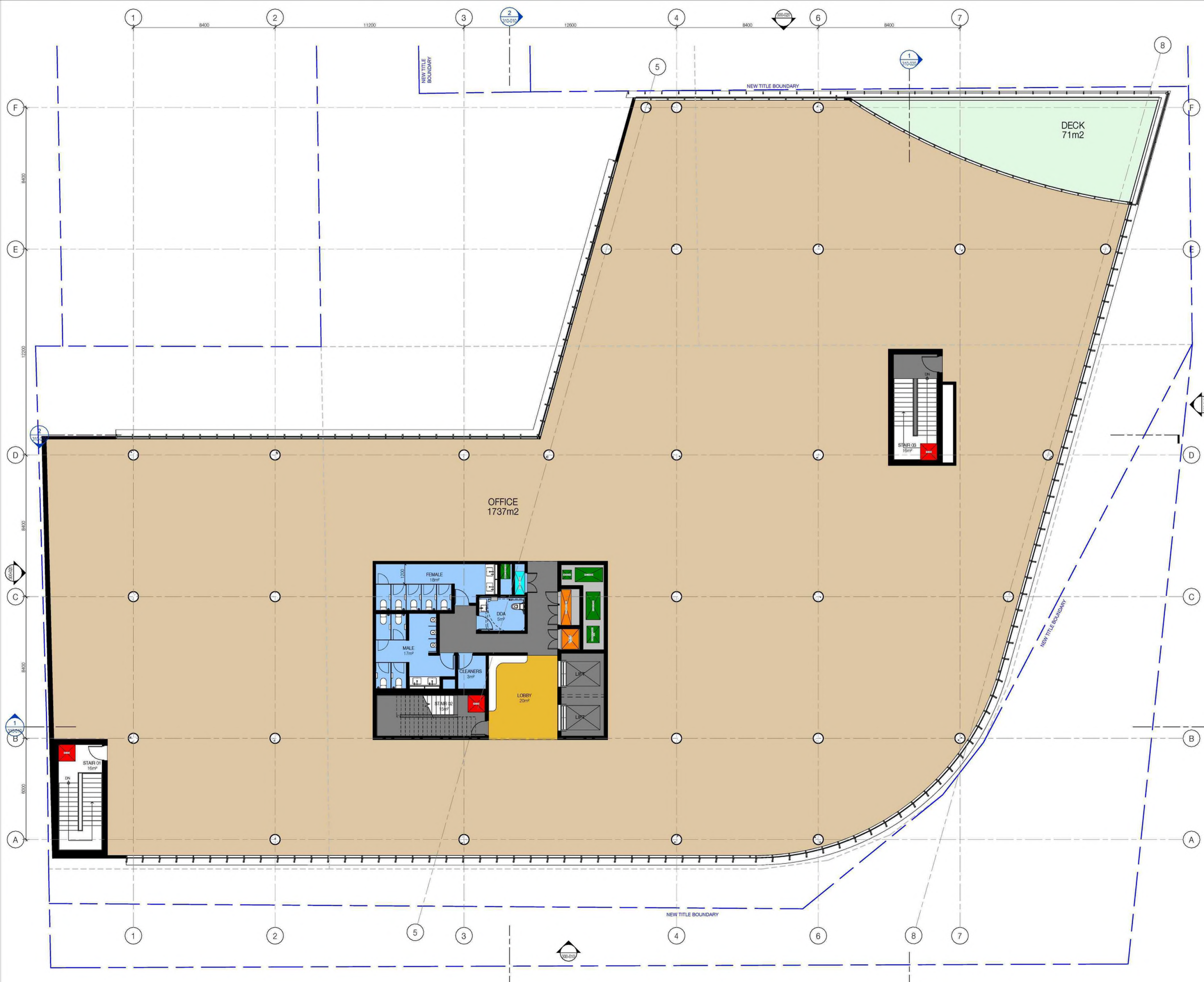
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 10/23/24

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
GENERAL ARRANGEMENT - LEVEL 02

DRAWING No. **DA-210-020** REVISION **A**



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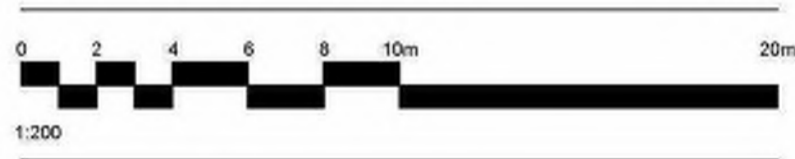


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NH Architecture

DRAWN: LS
 CHECKED: SPB
 SCALE: @A1
 NORTH



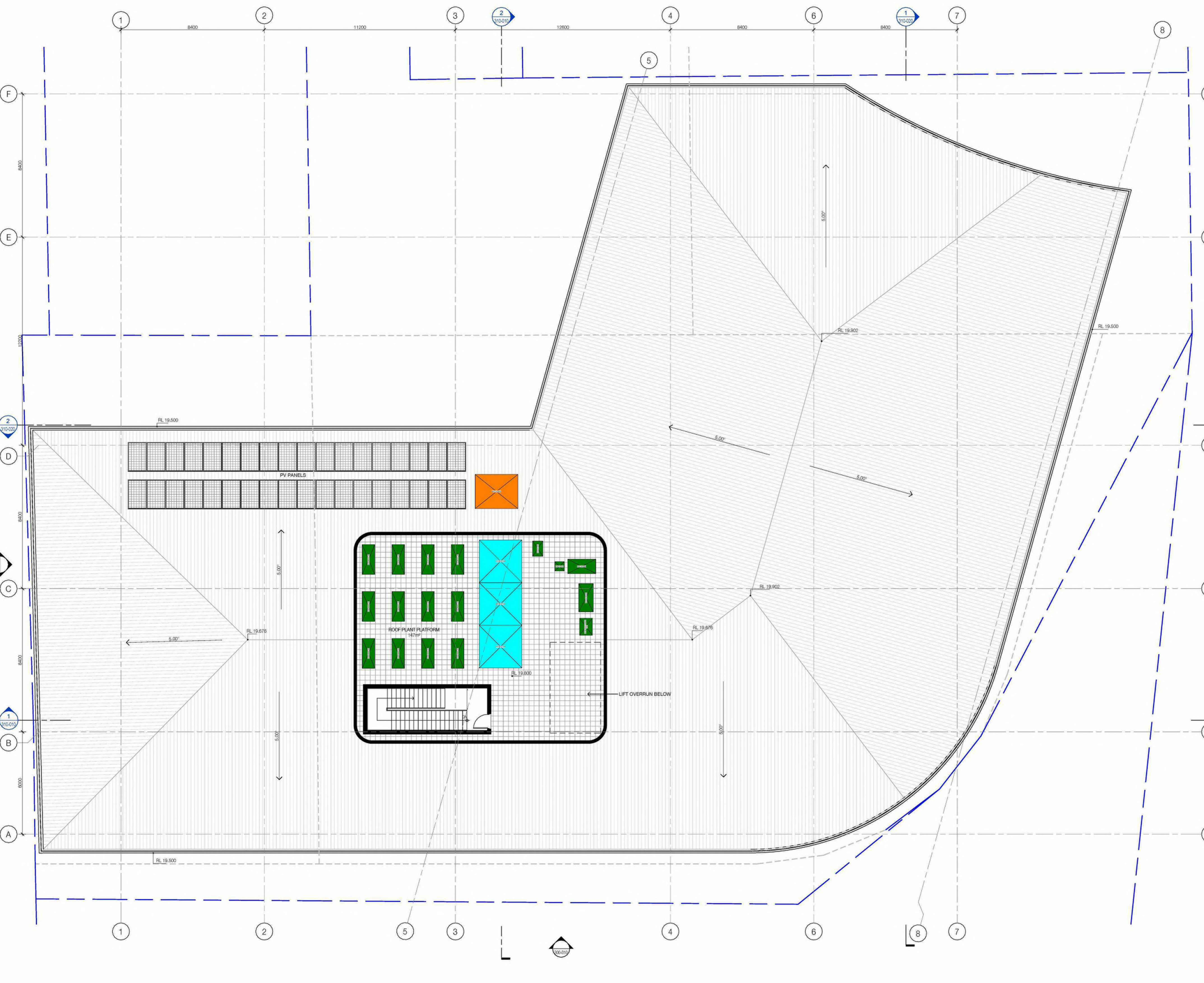
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064
 DATE 10/23/24

STAGE
 DESIGN DEVELOPMENT

DRAWING TITLE
GENERAL ARRANGEMENT - ROOF

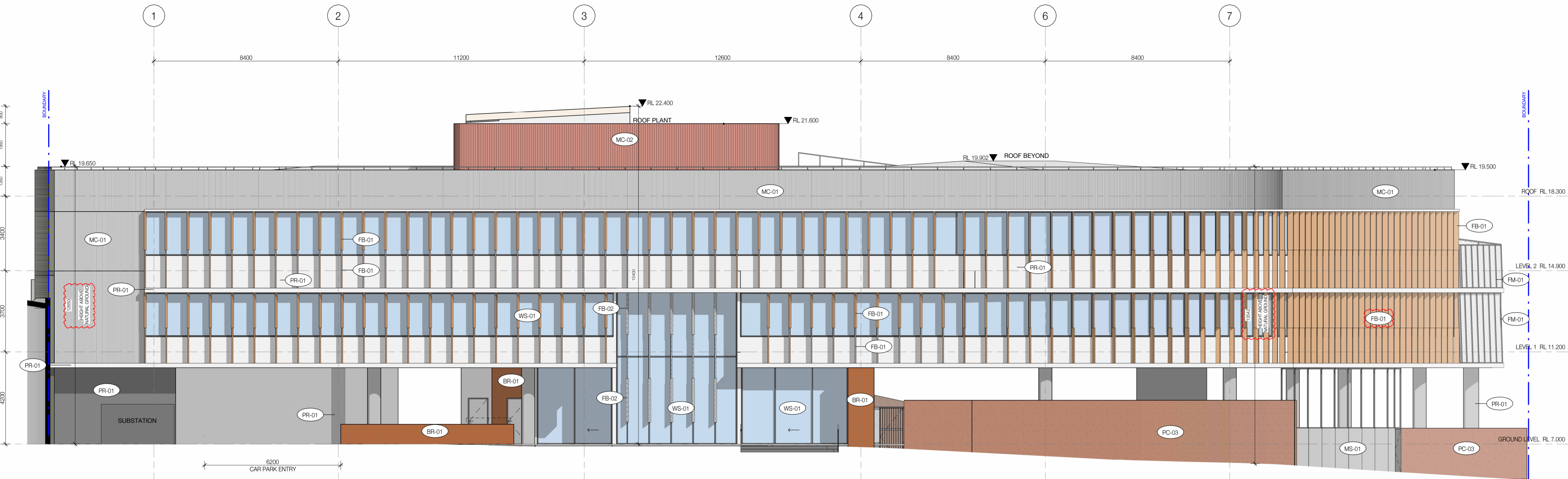
DRAWING No. **DA-210-040**
 REVISION **A**



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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25



1 SOUTH ELEVATION
DA-210-000 1 : 100

STRUCTURAL & CIVIL
GANDY & ROBERTS
159 Davey St, Hobart TAS 7000



SERVICES
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Level 2/600 Church St, Cremorne
VIC 3121

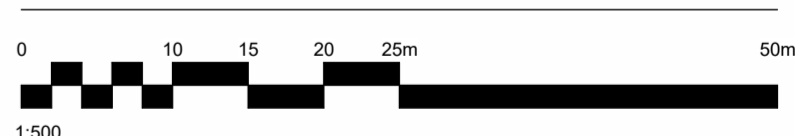


CLIENT
**FAIRBROTHER
DEVONPORT TAS**



NH Architecture

DRAWN LS CHECKED SPB SCALE @A1 NORTH 1 : 100



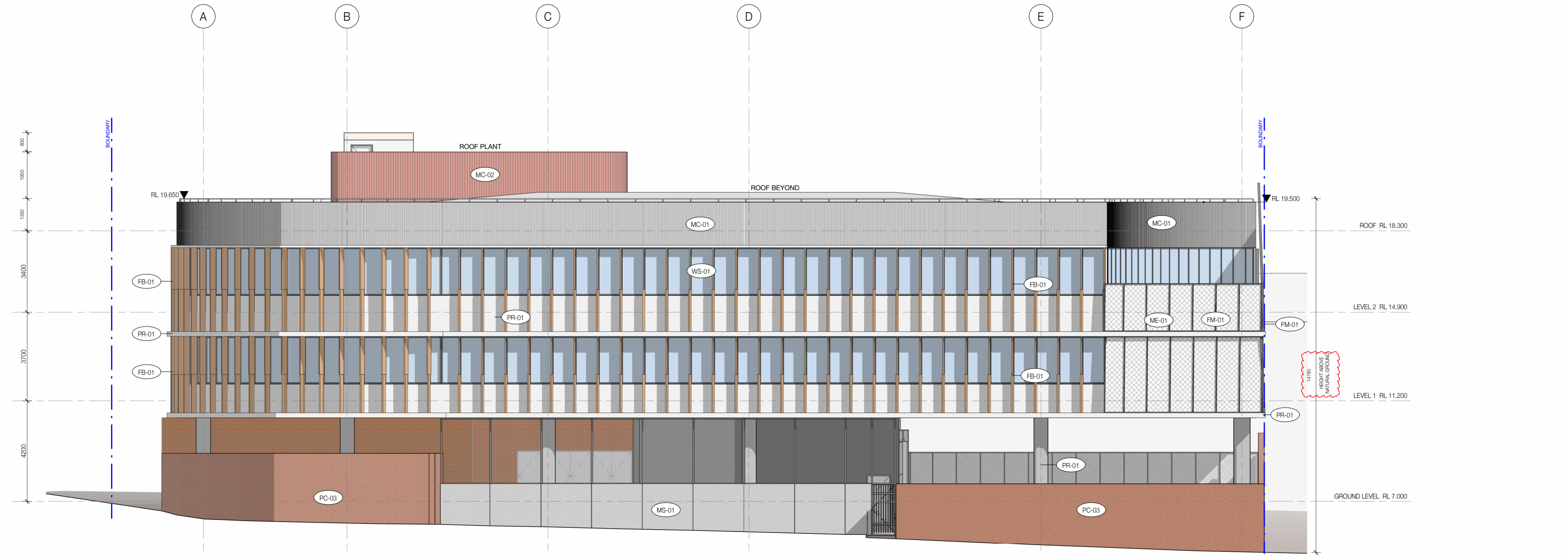
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
240064

STAGE
DESIGN DEVELOPMENT

DRAWING TITLE
ELEVATIONS

DRAWING No. **DA-300-010** REVISION **B**

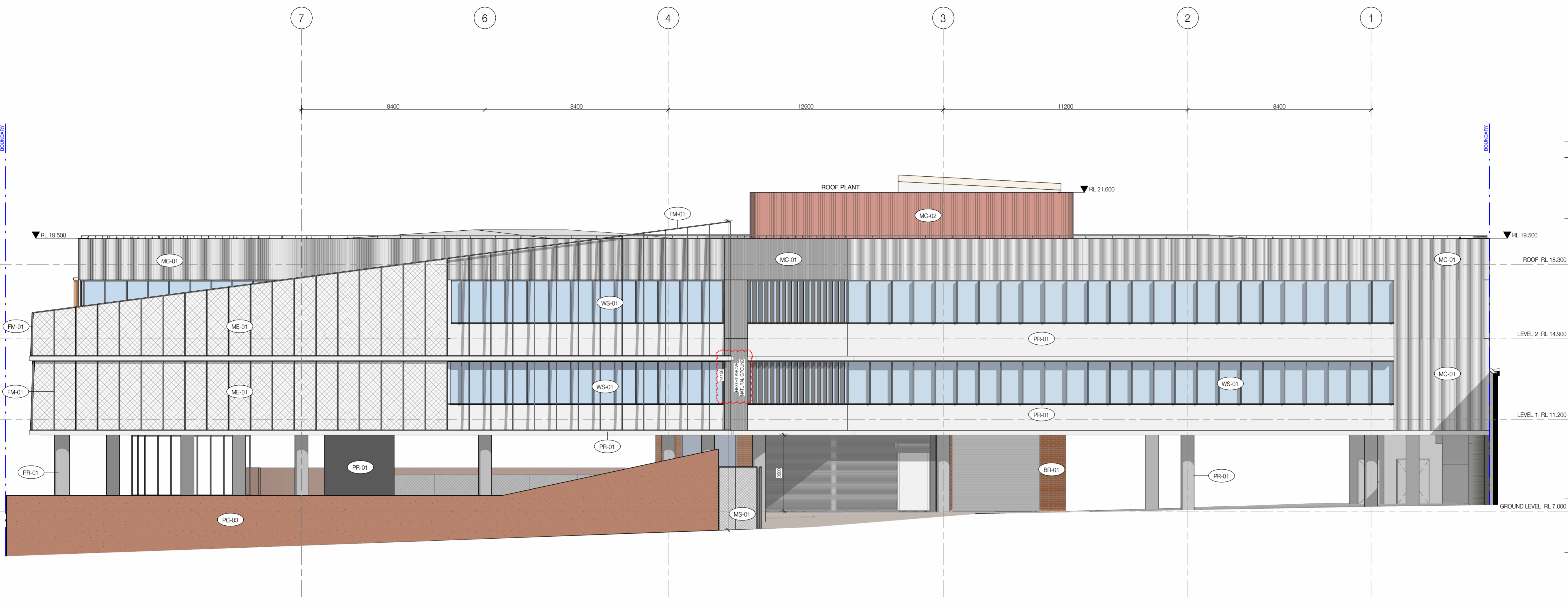


2 EAST ELEVATION
DA-210-000 1 : 100

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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25



1 NORTH ELEVATION
DA-210-000 1:100

STRUCTURAL & CIVIL
GANDY & ROBERTS
159 Davey St, Hobart TAS 7000



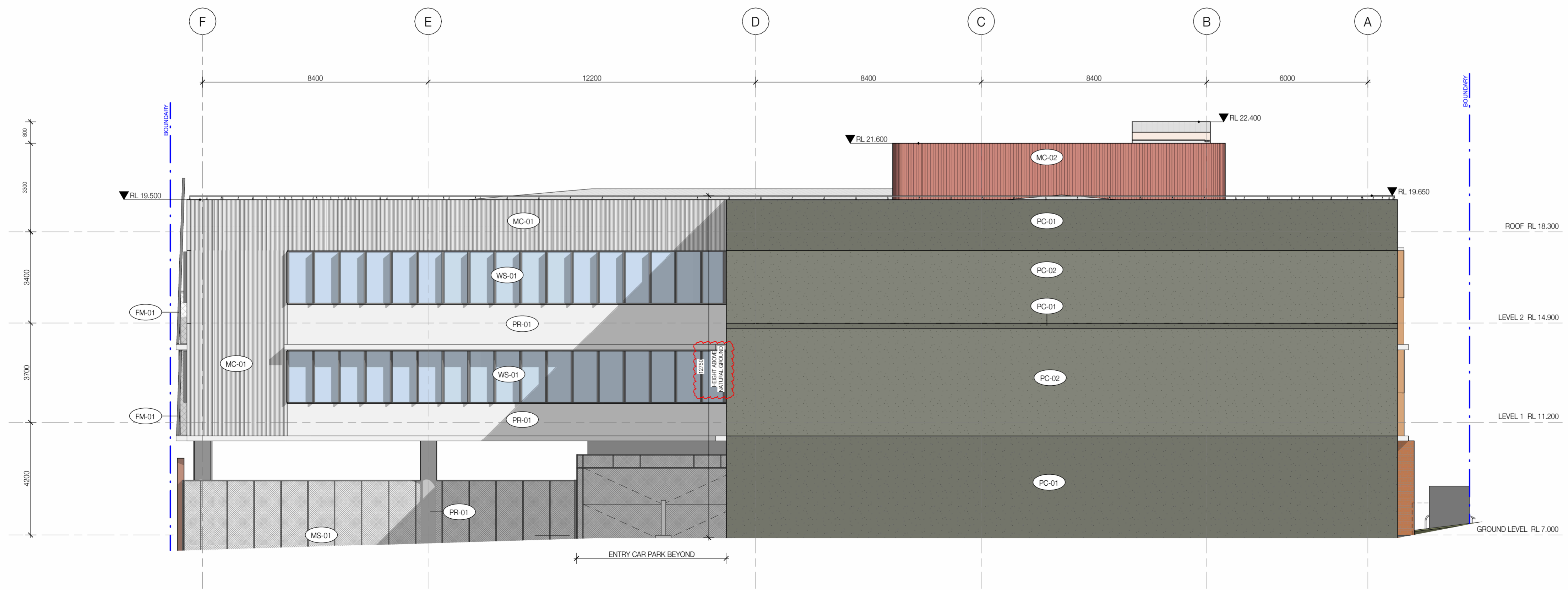
SERVICES
WRAP
Level 2/600 Church St, Cremorne
VIC 3121



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FAIRBROTHER
DEVONPORT TAS

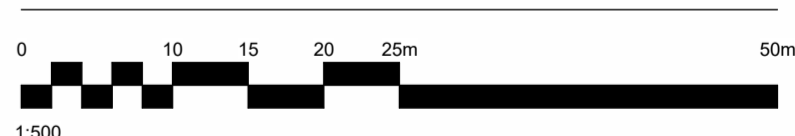


NH Architecture



2 WEST ELEVATION
DA-210-000 1:100

DRAWN CHECKED SCALE @A1 NORTH
LS SPB 1:100



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
240064

STAGE
DESIGN DEVELOPMENT

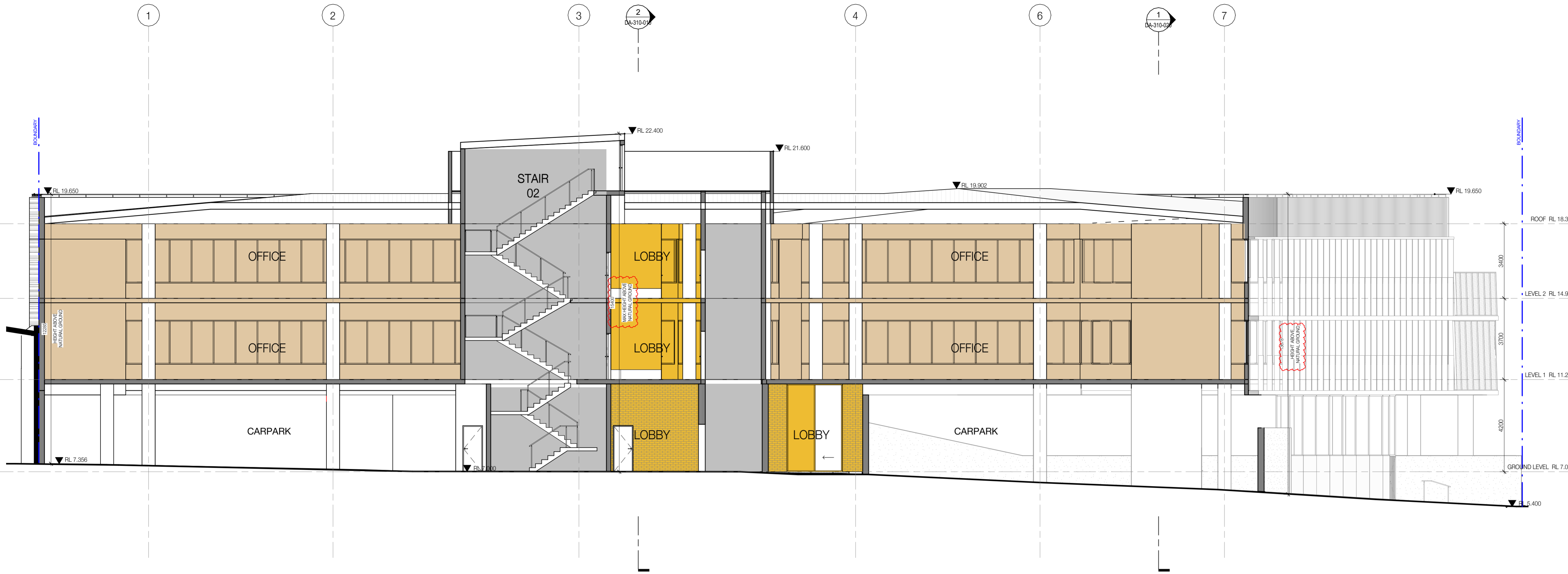
DRAWING TITLE
ELEVATIONS

DRAWING No. **DA-300-020** REVISION **B**

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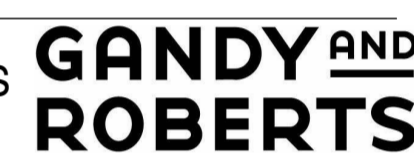
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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25



1 SECTION E/W 01
DA-210-000 1:100

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GANDY & ROBERTS
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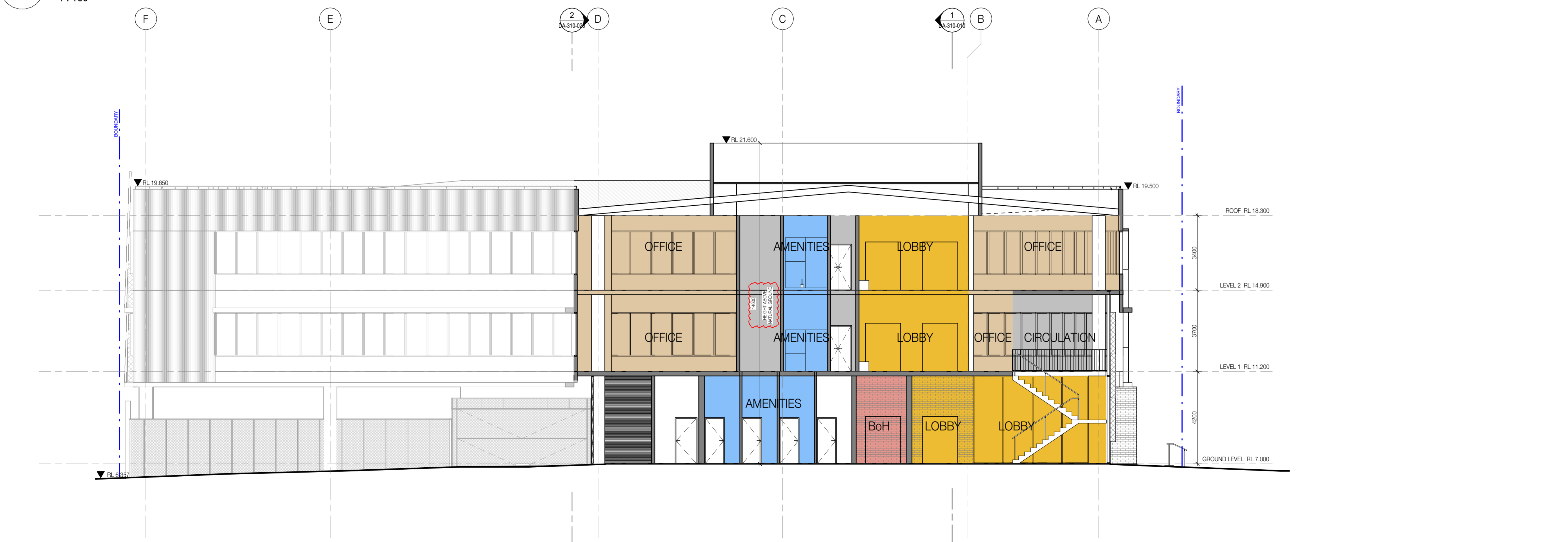
SERVICES
WRAP
Level 2/600 Church St, Cremorne
VIC 3121



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FAIRBROTHER
DEVONPORT TAS

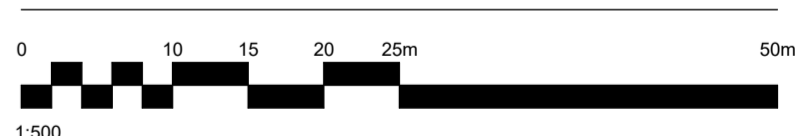


NH Architecture



2 SECTION N/S 01
DA-210-000 1:100

DRAWN LS CHECKED SPB SCALE @A1 NORTH



PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
240064

STAGE
DESIGN DEVELOPMENT

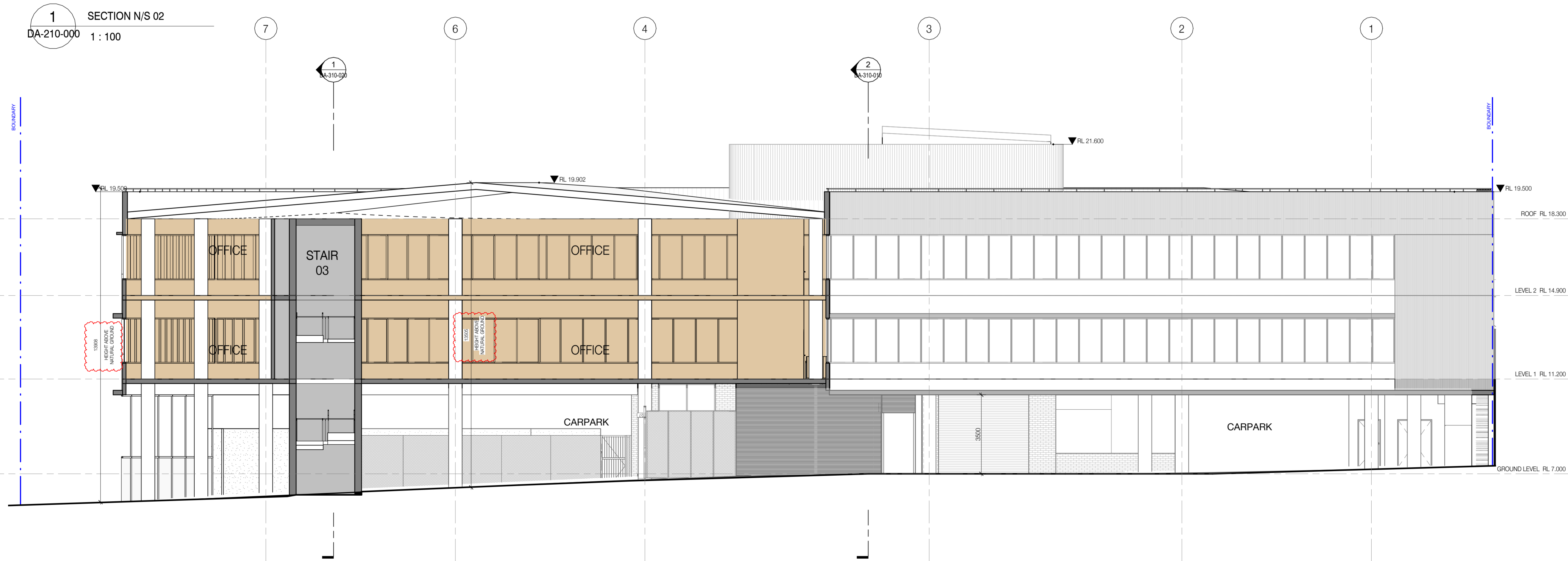
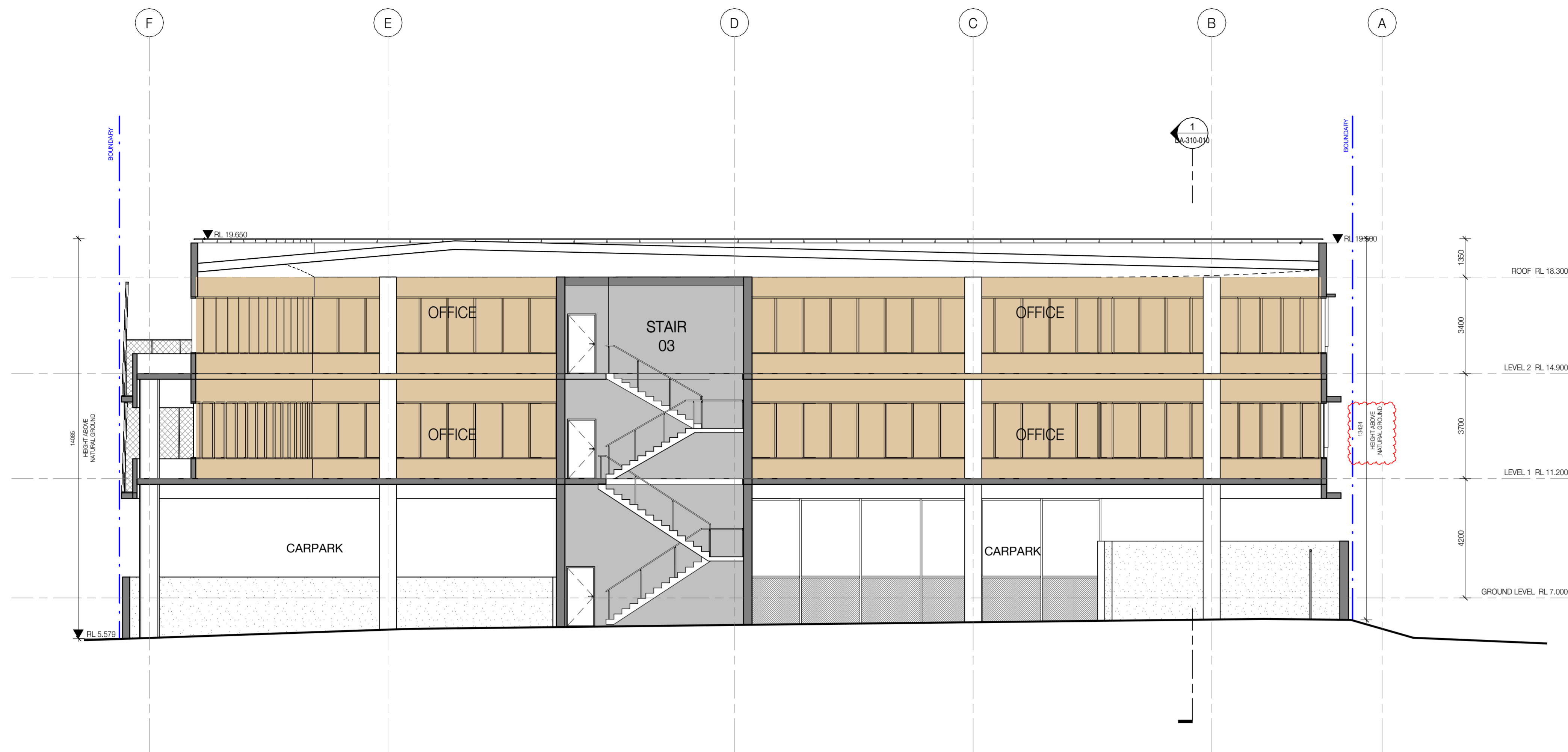
DRAWING TITLE
SECTIONS

DRAWING No. DA-310-010 REVISION B

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REV.	DETAILS	DATE
A	DA SET	17.04.25
B	DA SET	12.06.25



STRUCTURAL & CIVIL
GANDY & ROBERTS
159 Davey St, Hobart TAS 7000



SERVICES
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VIC 3121

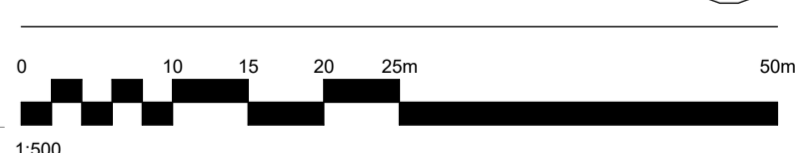


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FAIRBROTHER
DEVONPORT TAS



NH Architecture

DRAWN: LS, CHECKED: SPB, SCALE @A1, NORTH



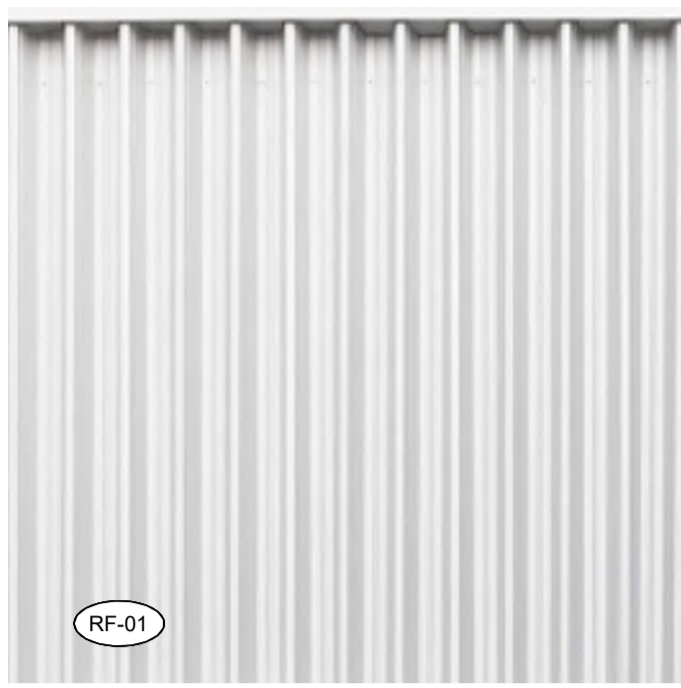
PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No.
240064

STAGE
DESIGN DEVELOPMENT

DRAWING TITLE
SECTIONS

DRAWING No. DA-310-020 REVISION B



METAL DECK ROOFING

LIGHT GREY



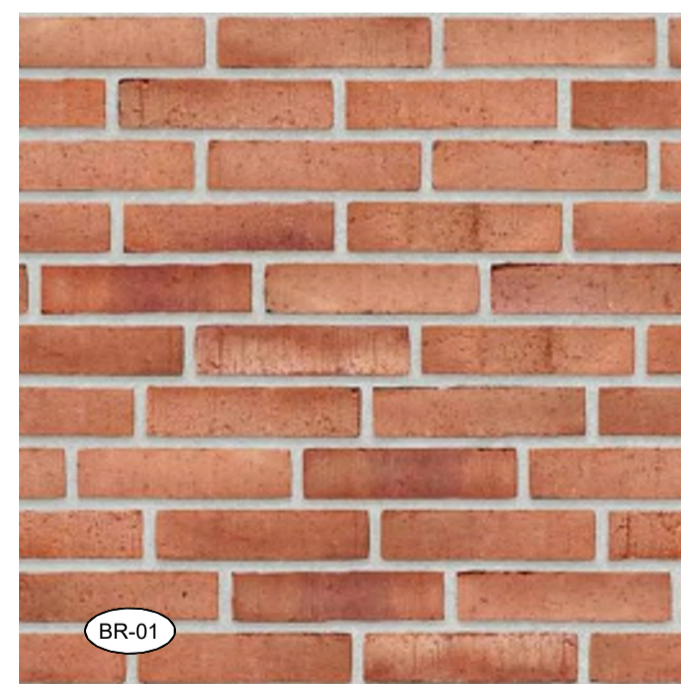
FACADE BATTEN SYSTEM

COLOURED ALUMINIUM



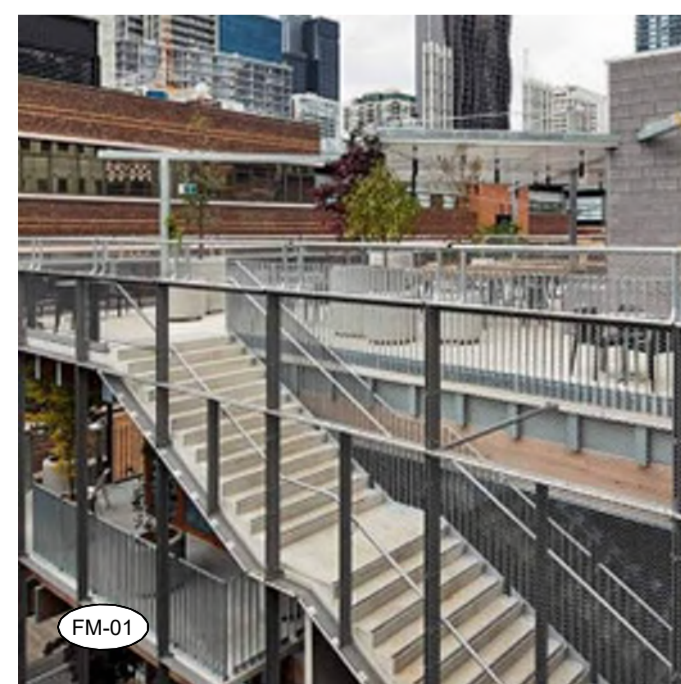
FACADE BATTEN SYSTEM

PERFORATED METAL



BRICK

RED BRICK



FABRICATED METAL

LIGHT GREY



PRECAST CONCRETE

PIGMENTED COLOUR



BH-02



PR-01

MESH SCREEN

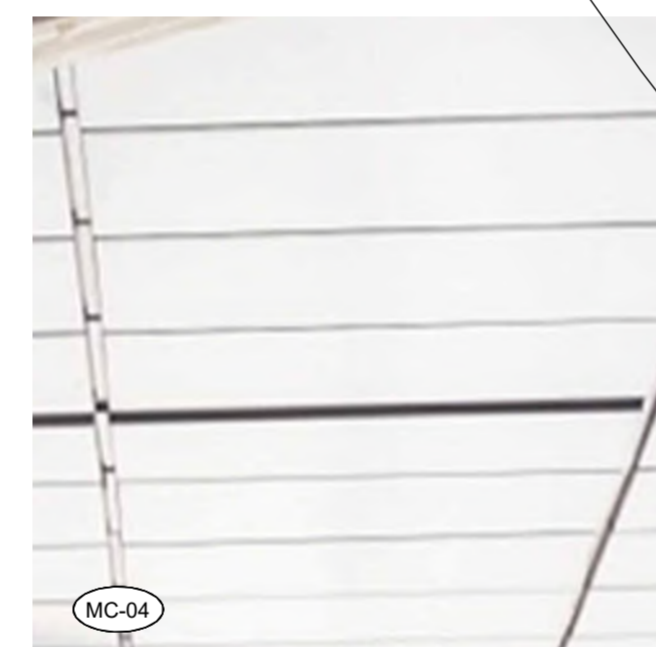
WHITE



MS-01

METAL SOFFIT

LIGHT GREY



MC-04

METAL FRAME / BALUSTRADE



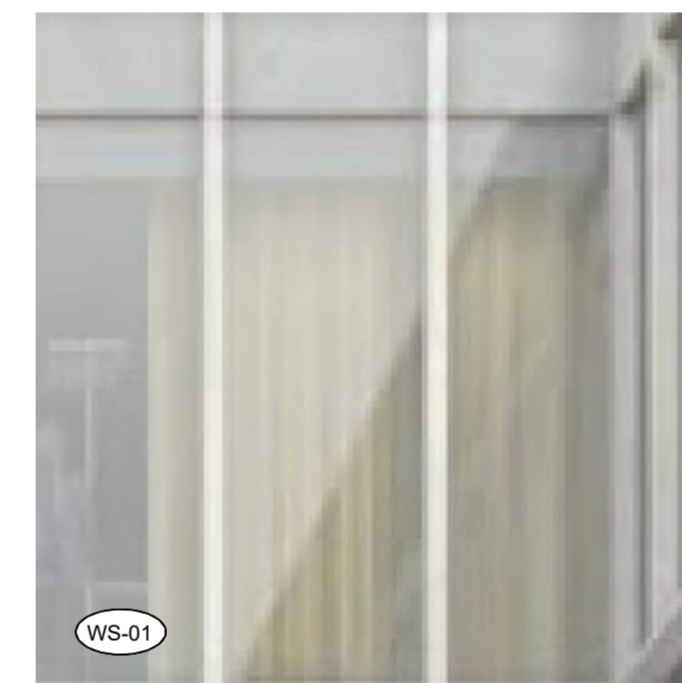
METAL CLADDING

LIGHT GREY

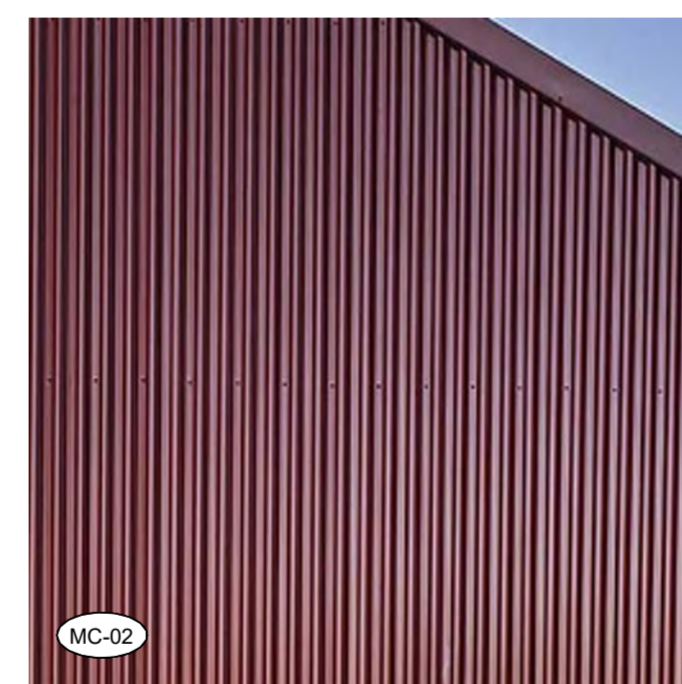


SPANDREL SYSTEM

LIGHT GREY

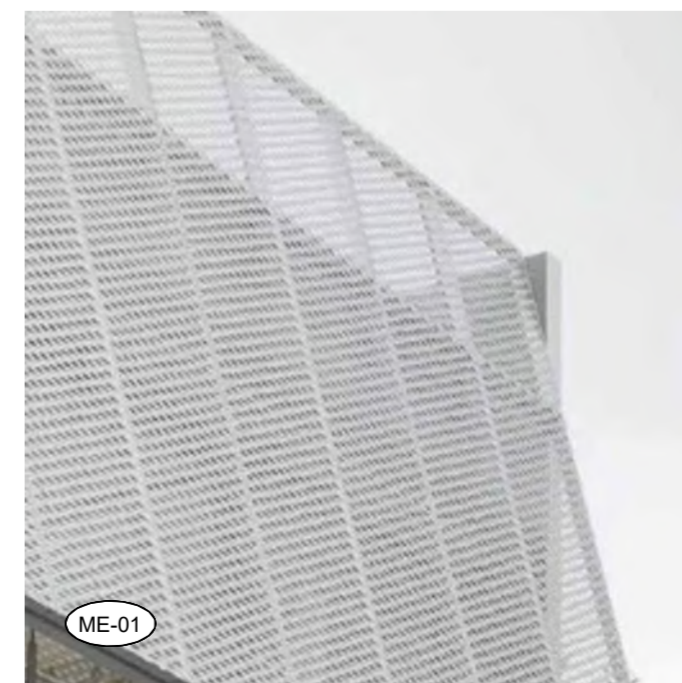


PERFORMANCE WINDOW SYSTEM



METAL CLADDING

RED



MESH

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REV.	DETAILS	DATE
A	DA SET	17.04.25

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DEVONPORT TAS
Fairbrother

NH Architecture

DRAWN LS CHECKED SPB SCALE @A1 NORTH

PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 10/23/24

STAGE
DESIGN DEVELOPMENT

DRAWING TITLE
VIEWS

DRAWING No. **DA-410-010** REVISION **A**

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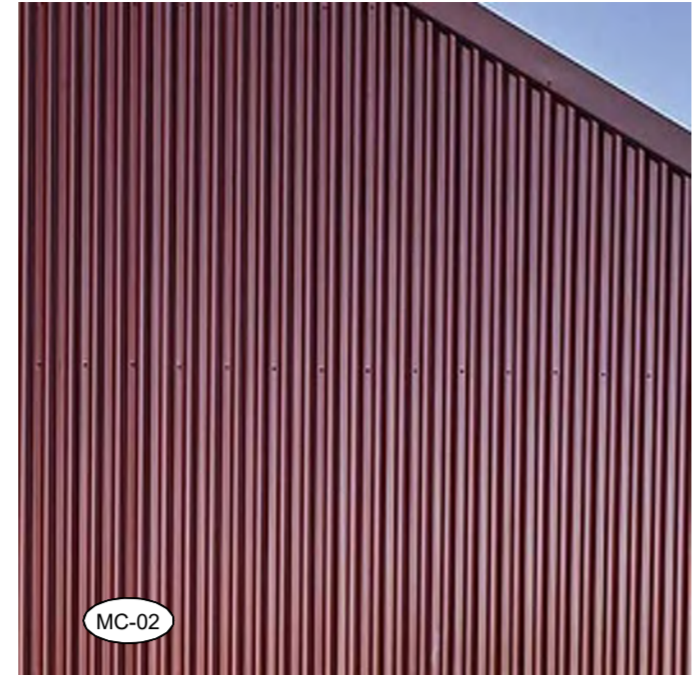
REV.	DETAILS	DATE
A	DA SET	17.04.25



METAL DECK ROOFING
LIGHT GREY



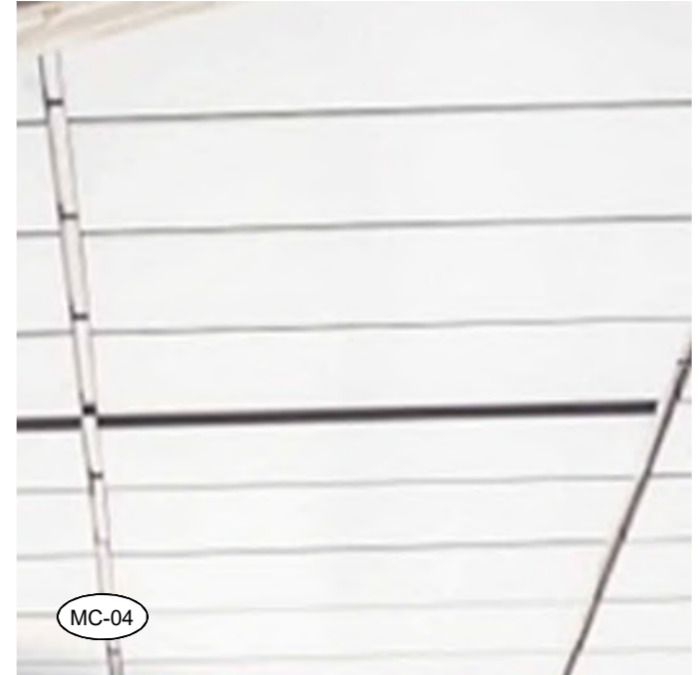
METAL CLADDING
LIGHT GREY



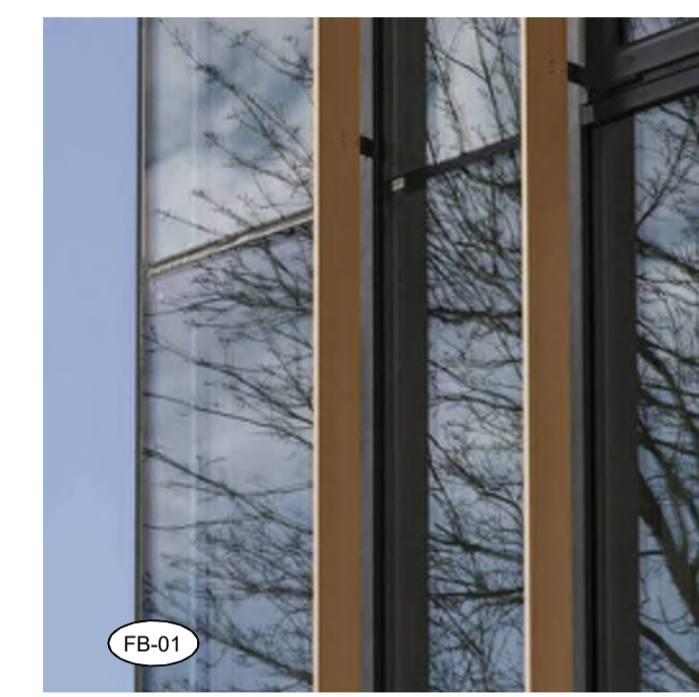
METAL CLADDING
RED



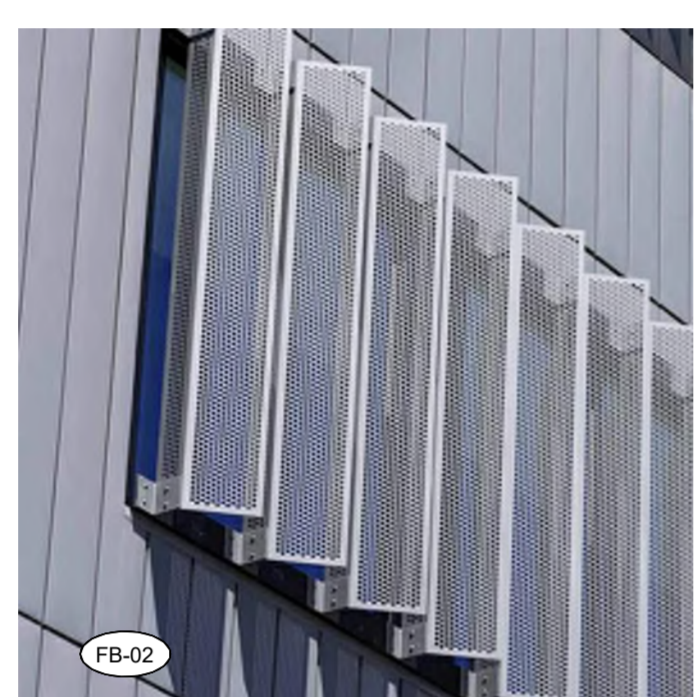
SPANDREL SYSTEM
LIGHT GREY



METAL SOFFIT
LIGHT GREY



FACADE BATTEN SYSTEM
COLOURED ALUMINIUM



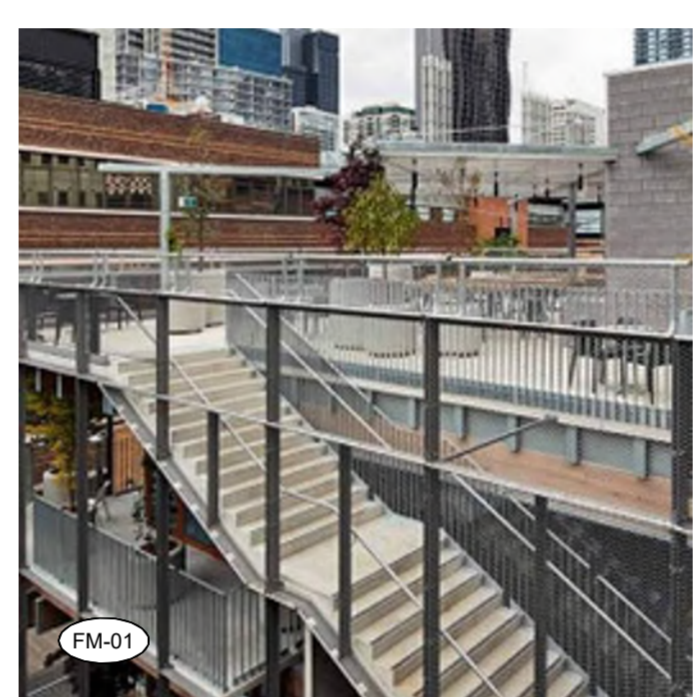
FACADE BATTEN SYSTEM
PERFORATED METAL



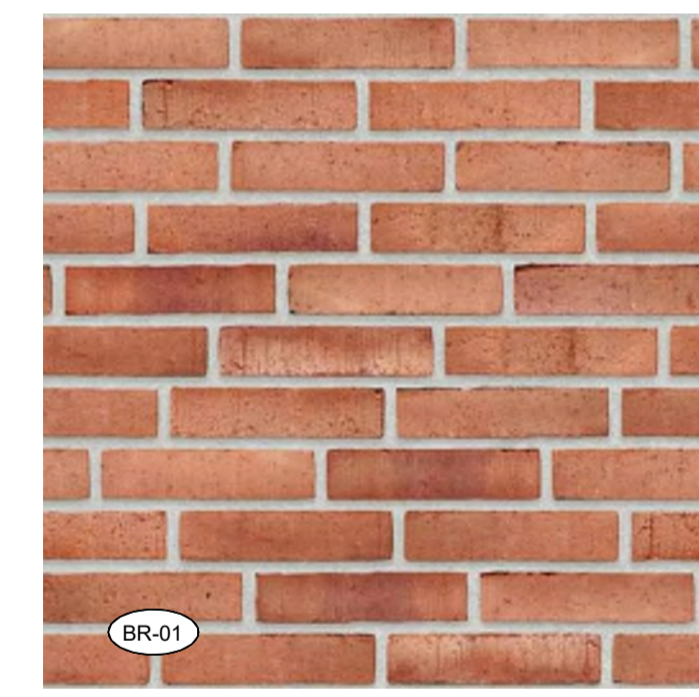
PERFORMANCE WINDOW SYSTEM



MESH SCREEN
WHITE



FABRICATED METAL FRAME



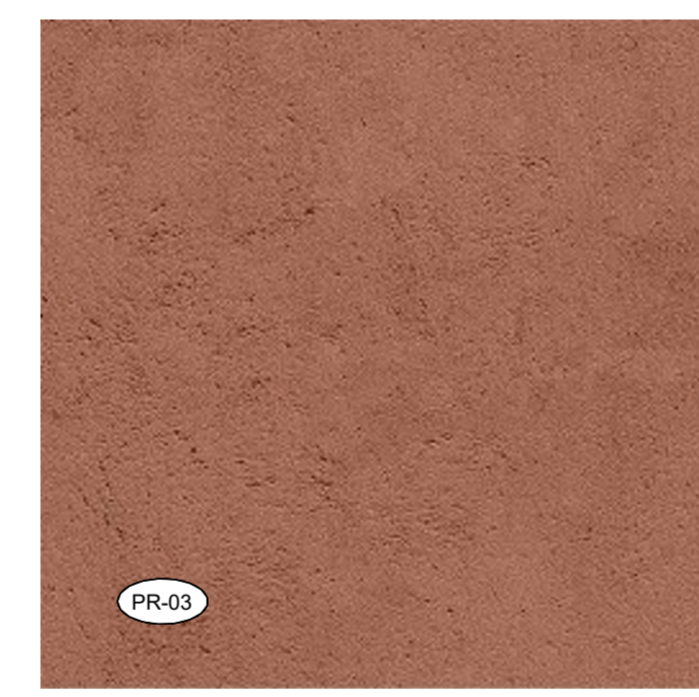
BRICK
RED BRICK



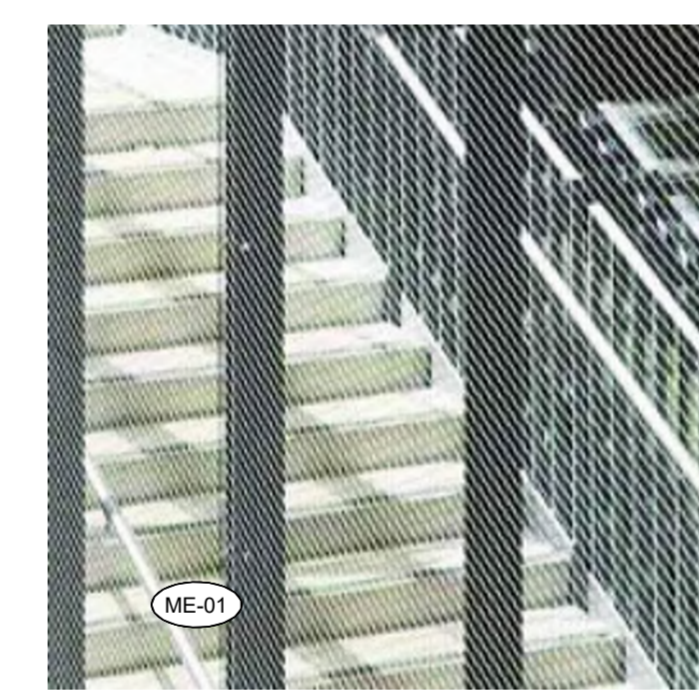
PRECAST CONCRETE



GROOVED PRECAST CONCRETE



PIGMENTED PRECAST CONCRETE



X-TEND MESH

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FAIRBROTHER
DEVONPORT TAS

Fairbrother

NH Architecture

DRAWN: LS CHECKED: SPB SCALE @A1 NORTH

PROJECT NAME
DEVONPORT MENTAL HEALTH HUB

PROJECT No. 240064 DATE 10/23/24

STAGE
DESIGN DEVELOPMENT

DRAWING TITLE
MATERIALS & FINISHES

DRAWING No. **DA-510-010** REVISION **A**

19 September 2025

Town Planning
Devonport City Council – Planning Assessments

ATTENTION: Jennifer,

Dear Jennifer and Robert

REFERENCE: 6-10 Steele Street; PA2025.0057 and TWDA 2025/00439-DCC

Thank you for considering the information provided by Fairbrother, and ultimately accommodating the scenario whereby the sewer is retaining in its current location. We are comfortable to make the suggested amendments so as to improve TasWater's access to the assets.

In response to the specific items raised within this third TasWater Request for Information (RFI), I present the following responses, numbered to align with the RFI items.

1. The aforementioned access to the private carpark via a roller door is operated after-hours by a proximity card or fob. This mechanism can be augmented with the addition of a key operated switch. Fairbrother proposes keying this switch to the TasWater master-key system, providing unfettered access to TasWater infrastructure.
- 1 - 3 Please find attached the following drawings, amended to reflect the requirements of items 1, 2 and 3. Further to this, we have taken the advice provided regarding a possible inspection opening access option and relocated the retaining walls and opening. This is also reflected in the drawings below.

- * AR-110-000(D) Proposed Site Plan
- * AR-210-000(D) General Arrangement Plan – Ground Level

A compiled set of Civil engineer drawings consisting of:

- * C001(E) Drawing Index and Notes
- * C200(E) Siteworks Plan 01
- * C201(E) Siteworks Plan 02
- * C202(E) Siteworks Plan 03 - Vehicular Access
- * C400(E) Stormwater Plan
- * C401(E) Stormwater Longitudinal Section
- * C500(E) Sewer Plan
- * C501(E) Sewer Longitudinal Section 1
- * C600(E) Water Plan
- * C700(E) Details 01



**Construction, Joinery &
Facility Management**

Devonport
12 Stony Rise Road
Devonport Tasmania 7310
P 03 6420 7000

Launceston
55 Gleadow Street
Launceston Tasmania 7248
P 03 6333 2500

Hobart
59 Sandy Bay Road
Battery Point Tasmania 7004
P 03 6220 9000

Bendigo
PO Box 5076
Sandhurst East Victoria 3550
P 03 5445 9700

Warrnambool
PO Box 571
Warrnambool Victoria 3280
P 03 5565 2800

Geelong
Level 1, 80 Pakington Street
Geelong West Victoria 3218
P 03 5223 4900

Melbourne
Level 1, 155 Roden Street
West Melbourne,
Victoria, 3003
P 03 8327 0500

info@fairbrother.com.au
www.fairbrother.com.au

ABN 51 009 510 561



We do seek clarification on one item arising from resolving the above items: With the relocation of the retaining walls and access opening, thus allowing unfettered access via public space, does TasWater still require all-hours access to the private carpark via a keyed switch as proposed in 1. above?

Thank you for your assistance in bringing this matter to a resolution.

A handwritten signature in black ink, appearing to be 'Dylan Graham', with a long horizontal flourish extending to the right.

Yours Sincerely

Dylan Graham (mobile 0419 982 047)

General Manager - Developments

FAIRBROTHER DEVONPORT MENTAL HEALTH HUB 6-10 STEELE STREET DEVONPORT, TASMANIA 7310

DWG No.	DRAWING TITLE	REV	ISSUE DATE
C001	DRAWING INDEX AND NOTES	E	19.09.2025
C200	SITWORKS PLAN 01	E	19.09.2025
C201	SITWORKS PLAN 02	E	19.09.2025
C202	SITWORKS PLAN 03 - VEHICULAR ACCESS	E	19.09.2025
C400	STORMWATER PLAN	E	19.09.2025
C401	STORMWATER LONGITUDINAL SECTION	E	19.09.2025
C500	SEWER PLAN	E	19.09.2025
C501	SEWER LONGITUDINAL SECTION 1	E	19.09.2025
C600	WATER PLAN	E	19.09.2025
C700	DETAILS 01	E	19.09.2025

CIVIL/HYDRAULIC NOTES

GENERAL

- G1. THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ALL OTHER CONTRACT DRAWINGS AND SPECIFICATIONS. ANY DISCREPANCIES SHALL BE REFERRED TO GANDY AND ROBERTS FOR CLARIFICATION.
- G2. SETTING OUT DIMENSIONS AND LEVELS SHOWN ON THE DRAWINGS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCEMENT.
- G3. DIMENSIONS SHALL NOT BE OBTAINED BY SCALING THESE DRAWINGS.
- G4. DURING CONSTRUCTION THE CONTRACTOR SHALL MAINTAIN EXCAVATIONS AND STRUCTURES IN A STABLE CONDITION AND ENSURE THAT NO PART IS OVERSTRESSED UNDER CONSTRUCTION ACTIVITIES.
- G5. THE CONTRACTOR IS RESPONSIBLE FOR THE CREATION AND MAINTENANCE OF TEMPORARY SITE ACCESSES. STRENGTHENING OF DESIGN PAVEMENTS TO CARRY CONSTRUCTION VEHICLES (IN EXCESS OF THE DESIGN ALLOWANCE) SHALL BE AT THE CONTRACTOR'S EXPENSE.
- G6. LOCATION AND VERIFICATION OF EXISTING SERVICES IS THE CONTRACTOR'S RESPONSIBILITY. REFER ANY SERVICES DISCOVERED ON SITE WHICH ARE NOT SHOWN ON THE DRAWINGS, OR ARE IN A DIFFERENT LOCATION TO THAT SHOWN, TO GANDY AND ROBERTS. THE CONTRACTOR SHALL UNDERTAKE ALL NECESSARY INVESTIGATIONS, INCLUDING LIAISON WITH SERVICE AUTHORITIES, TO DETERMINE IF THE DISCOVERED SERVICES ARE LIVE. THE CONTRACTOR SHALL NOTIFY GANDY AND ROBERTS IN WRITING THAT REDUNDANT SERVICES HAVE BEEN LOCATED AND REQUEST APPROVAL TO SEAL AND ABANDON THOSE SERVICES.
- G7. PROTECT ALL EXISTING SERVICES AND OTHER INFRASTRUCTURE FROM DAMAGE DURING CONSTRUCTION. SHOULD DAMAGE OCCUR, ADVISE GANDY AND ROBERTS IMMEDIATELY ALONG WITH DETAILS OF PROPOSED REMEDIAL ACTION. THE COST OF REMEDIAL WORK (INCLUDING REDESIGN IF REQUIRED) SHALL BE BORNE BY THE CONTRACTOR.
- G8. THE CONTRACTOR IS RESPONSIBLE FOR UNDERTAKING WHATEVER DILAPIDATION SURVEYS OF EXISTING BUILDINGS/INFRASTRUCTURE THEY CONSIDER NECESSARY PRIOR TO CONSTRUCTION COMMENCING, AND CONSULTATION WITH ADJOINING LAND OWNERS TO MINIMISE DISRUPTION TO SERVICES/ACCESS ETC. DURING CONSTRUCTION.
- G9. ALL SURPLUS CONSTRUCTION MATERIALS (INCLUDING EXCESS CUT AND FILL MATERIAL) SHALL BE REMOVED FROM THE SITE (UNLESS INSTRUCTED OTHERWISE) AT COMPLETION OF THE WORKS.
- G10. SURVEY INFORMATION HAS BEEN SUPPLIED BY PDA SURVEYORS FOR THE PURPOSES OF PREPARING THE DESIGN DRAWINGS. ALL OTHER SURVEY REQUIRED TO SETOUT AND CONSTRUCT THE WORKS SHALL BE PROVIDED BY THE CONTRACTOR USING A REGISTERED SURVEYOR.
- G11. ALL WORKS ARE TO BE UNDERTAKEN BY THE CONTRACTOR AND THEIR SUBCONTRACTORS UNLESS NOTED OTHERWISE ON THE DRAWINGS.
- G12. PROPOSED CHANGES TO THE DESIGN OF ANY PART OF THE WORKS SHALL BE SUBMITTED TO GANDY AND ROBERTS FOR REVIEW. THE CONTRACTOR SHALL BEAR ALL COSTS ASSOCIATED WITH THE DESIGN CHANGE.
- G13. THE CONTRACTOR IS TO ALLOW FOR ALL TESTING OF RAW MATERIALS AND CONSTRUCTED WORKS THAT IS REQUIRED TO DEMONSTRATE COMPLIANCE WITH THE NOMINATED AUSTRALIAN STANDARDS, SPECIFICATIONS, AND STANDARD DRAWINGS. RESULTS OF TESTS SHALL BE PROVIDED TO GANDY AND ROBERTS ON REQUEST.
- G14. IF PROTECTION WORKS ARE REQUIRED THE CONTRACTOR SHALL ADVISE THE PROPERTY OWNER THAT THEY ARE REQUIRED TO NOTIFY ADJOINING PROPERTY OWNERS OF THEIR INTENTION TO UNDERTAKE PROTECTION WORKS IN ACCORDANCE WITH PART 6 OF THE BUILDING ACT 2016. THE CONTRACTOR SHALL REFER THE PROPERTY OWNER TO FORM 6 - NOTICE FOR PROPOSED PROTECTION WORK AVAILABLE ON THE CBOS WEBSITE. THE CONTRACTOR SHALL NOT COMMENCE WORKS UNTIL CONFIRMATION HAS BEEN RECEIVED FROM THE PROPERTY OWNER THAT THIS PROCESS HAS BEEN COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE BUILDING ACT 2016.
- G15. THE IDENTIFICATION OF A PROPRIETARY ITEM DOES NOT NECESSARILY IMPLY EXCLUSIVE PREFERENCE FOR THE ITEM IDENTIFIED BUT SHALL BE DEEMED TO INDICATE THE REQUIRED PROPERTIES OF THE ITEM, SUCH AS QUALITY, PERFORMANCE AND THE LIKE. SIMILAR ITEMS HAVING THE REQUIRED PROPERTIES MAY BE OFFERED BY THE CONTRACTOR FOR APPROVAL, WHEN OFFERING AN ALTERNATIVE FOR APPROVAL THE CONTRACTOR MUST PROVIDE ALL AVAILABLE TECHNICAL INFORMATION REQUESTED BY THE SUPERINTENDENT.
- G16. PROPRIETARY ITEMS SHALL BE USED, FITTED, INSTALLED AND FINISHED IN ACCORDANCE WITH THE WRITTEN INSTRUCTIONS AND RECOMMENDATIONS OF THE SUPPLIER OR MANUFACTURER.
- G17. ON COMPLETION, THE CONTRACTOR SHALL SUPPLY AS CONSTRUCTED DRAWINGS (PREPARED BY A REGISTERED SURVEYOR IN ACCORDANCE WITH AS 1100.4-01) AND FILL SERVICE MANUAL IN BOTH HARD COPY (3 SETS) AND ELECTRONIC (PDF AND DWG) FORMATS. AS CONSTRUCTED DRAWINGS SHALL BE PREPARED IN ACCORDANCE WITH TASWATER'S, THE LOCAL AUTHORITY'S AND/OR THE CLIENT'S REQUIREMENTS.

TEMPORARY WORKS

THE CONTRACTOR IS REQUIRED TO CARRY OUT ALL TEMPORARY WORKS NECESSARY TO ENABLE COMPLETION OF THE WORKS (INCLUDING THE ENGAGEMENT OF SUITABLY QUALIFIED DESIGNERS AND IS RESPONSIBLE FOR ALL ASSOCIATED COSTS). THIS INCLUDES (BUT IS NOT LIMITED TO) THE FOLLOWING: PRECAST PANEL PROPPING, FORMWORK, SCAFFOLDING, SHORING, BACK PROPPING OF SUSPENDED SLABS.

APPROVALS

- A1. PRIOR TO CONSTRUCTION COMMENCING, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT A VALID BUILDING AND ENGINEERING PERMIT IS IN PLACE FOR THE WORK AND THAT THE RELEVANT AUTHORITIES ARE NOTIFIED AND ALLOWED TO INSPECT AT THE NOMINATED HOLD POINTS.
- A2. WHERE PUBLIC WORKS ARE BEING UNDERTAKEN THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE LOCAL AUTHORITY AND/OR TASWATER PERMIT TO CONSTRUCT, AS APPLICABLE. PRIOR TO CONSTRUCTION COMMENCING THIS INCLUDES SUBMITTING REQUESTS FOR LOCAL AUTHORITY ROAD OPENING, STORMWATER CONNECTIONS AND TASWATER SERVICE CONNECTIONS.
- A3. UNLESS NOMINATED OTHERWISE, THE FOLLOWING INSPECTION REGIME SHALL BE ADOPTED:
- ROAD FORMATIONS
 - INSPECTION OF SUBGRADE, SUBBASE AND BASE LIFTS, KERBING AND SEAL BY THE LOCAL AUTHORITY AND GANDY AND ROBERTS.
 - STORMWATER
 - INSPECTION OF STORMWATER LINES BY THE LOCAL AUTHORITY.
 - SEWER AND WATER
 - SEWER AND WATER INFRASTRUCTURE TO BE OWNED BY TASWATER TO BE INSPECTED AND SELF-CERTIFIED BY CIVIL CONTRACTOR OR THEIR SUBCONTRACTOR. SEWER PIPELINE PRIOR TO BACKFILLING BY GANDY AND ROBERTS.
 - AS-BUILT SERVICES SURVEYS
 - WATER, SEWER, STORMWATER SURVEYS UNDERTAKEN BY CONTRACTOR'S REGISTERED LAND SURVEYOR (DEPTH OF WATER RETICULATION RECORDED PRIOR TO BACKFILLING).
 - INSTALLATION OF OTHER IN-GROUND SERVICES
 - POWER, COMMUNICATIONS, GAS ETC UNDERTAKEN BY THE RELEVANT MANAGING AUTHORITY.
 - DEFECTS LIABILITY INSPECTION BY GANDY AND ROBERTS.
- A4. A MINIMUM OF 24 HOURS NOTICE IS REQUIRED FOR GANDY AND ROBERTS TO ATTEND THE SITE. DO NOT RELY UPON FACSIMILE OR EMAIL TO COMMUNICATE REQUESTS - MAKE CONTACT WITH OUR OFFICE TO CONFIRM ATTENDANCE.
- A5. INSPECTION OF ROAD FORMATIONS MAY INVOLVE PROOF ROLLING WITH A TEST VEHICLE. CONFIRM WITH GANDY AND ROBERTS AND ENSURE A SUITABLE VEHICLE IS AVAILABLE AT THE TIME OF INSPECTION. REFER NOTE R11 FOR ADDITIONAL REQUIREMENTS.
- A6. PHOTOGRAPHIC DOCUMENTATION IS NOT AN ADEQUATE BASIS TO PROCEED BEYOND A HOLD POINT UNLESS APPROVED BY GANDY AND ROBERTS.

WORK HEALTH AND SAFETY

- H51. THE MAIN CONTRACTOR AND ALL SUB-CONTRACTORS SHALL COMPLY WITH THE STATE WORK HEALTH AND SAFETY ACT, REGULATIONS, AND ALL RELEVANT CODES OF PRACTICE.
- H52. THE GANDY AND ROBERTS DESIGN SAFETY REPORT 24 0387 REVISION A FORMS AN INTEGRAL PART OF THIS DOCUMENTATION. THIS REPORT IDENTIFIES SAFETY RISKS AND PROPOSES CONTROL MEASURES TO BE FOLLOWED BY THE CONTRACTOR AND THE BUILDING OPERATOR. CONTROLS AND HAZARDS REQUIRING MORE EXPLANATION THAN IN THE SAFETY REPORT ARE HIGHLIGHTED IN OUR DRAWINGS WITH AN EXCLAMATION MARK IN THE TRIANGLE SYMBOL AS SHOWN.
- H53. SHOULD THE MAIN CONTRACTOR OR SUB-CONTRACTORS IDENTIFY OMISSIONS OR ERRORS IN THE REPORT RELATED TO THE SCOPE OF GANDY AND ROBERTS' WORK ON THE PROJECT, OR HAVE SAFER WAYS OF WORKING, THEY SHOULD CONTACT GANDY AND ROBERTS PRIOR TO CONSTRUCTION.
- H54. SHOULD THE CONTRACTOR PROPOSE AN ALTERNATIVE DESIGN, THIS SHALL BE PRESENTED WITH APPROPRIATE SAFETY RISK PLANNING TO GANDY AND ROBERTS FOR REVIEW.



EARTHWORKS

- E1. ALL EARTHWORKS SHALL BE IN ACCORDANCE WITH AS3798 "GUIDELINES ON EARTHWORKS FOR COMMERCIAL AND RESIDENTIAL DEVELOPMENTS" WITH TESTING METHODS IN ACCORDANCE WITH AS1289 "METHODS OF TESTING SOILS FOR ENGINEERING PURPOSES".
- E2. THE EXISTING SURFACE INCLUDING VEGETATION AND DEBRIS UNDER THE BUILDING AND PAVED AREAS SHALL BE TOTALLY REMOVED OR REMOVED TO A DEPTH OF NOT LESS THAN 200 mm WHICHEVER IS THE LESSER TREATMENT. THE LAYER TO BE REMOVED INCLUDES ANY MATERIAL THAT MAY BE UNSUITABLE TO SUPPORT THE PROPOSED WORKS. TOP SOIL TO BE STOCKPILED AS DIRECTED, AND VEGETATION AND DEBRIS REMOVED FROM SITE UNLESS NOTED OTHERWISE. TREE STUMPS SHALL BE FULLY GRUBBED, MINIMUM DEPTH OF 300 mm, AND HOLES FILLED WITH MATERIAL SIMILAR TO THE SURROUNDING MATERIAL AND COMPACTED TO THE SAME DEGREE AS THE SURROUNDING MATERIAL.
- E3. FOR EXCAVATION PURPOSES, ROCK IS DEFINED AS HARD OR STRONGLY CEMENTED BEDS OR MASSES WHICH CANNOT BE RIPPED AT THE FOLLOWING PRODUCTION RATES (INSITU VOLUMES) FOR THE PARTICULAR CLASS OF EQUIPMENT, AS DEFINED IN AS2868

- GENERAL EXCAVATION MACHINE EQUIPPED WITH A HEAVY DUTY, SINGLE TINE PARALLELOGRAM RIPPER (APPROVED BY THE MACHINE MANUFACTURER FOR USE ON THE PARTICULAR MACHINE IN ROCK).
- EXCAVATOR OPERATING MASS ≥27T ±3BT EXCAVATION RATE SOLID 10 m³ PER HOUR
EXCAVATOR OPERATING MASS ≥38T ±4.1T EXCAVATION RATE SOLID 15 m³ PER HOUR
- TRENCH EXCAVATION MACHINE FITTED WITH A HEAVY DUTY BUCKET AND TEETH WITH HIGH PENETRATION BOOTS (APPROVED BY THE MACHINE MANUFACTURER FOR USE ON THE PARTICULAR MACHINE IN ROCK).
- EXCAVATOR OPERATING MASS ≥12.5T ±1.5T, 450mm WIDE BUCKET, MAXIMUM PRODUCTION RATE SOLID OF 15 m³ PER HOUR
EXCAVATOR OPERATING MASS ≥19T ±2.3T, 600mm WIDE BUCKET, MAXIMUM PRODUCTION RATE SOLID OF 3 m³ PER HOUR
- E4. ANY INTERFACE BETWEEN CUT AND FILL SHALL BE NO STEEPER THAN 1V:4H CUT HORIZONTAL BENCHES FOR ANY FILL PLACED ON GROUND STEEPER THAN 1V:4H.
- E5. ALL EXCAVATIONS SHALL BE INSPECTED BY GANDY AND ROBERTS AND/OR THE LOCAL AUTHORITY BEFORE PROCEEDING ANY FURTHER. INSPECTION AND TESTING SHALL OCCUR AFTER EACH LIFT DURING FILLING. TESTING IN ACCORDANCE WITH TABLE 8.1 OF AS 3798) SHALL BE ARRANGED BY THE CONTRACTOR SUCH THAT THE RESULTS ARE AVAILABLE AT THE TIME OF INSPECTION. THE CONTRACTOR SHALL ENGAGE A SUITABLY QUALIFIED GEOTECHNICAL ENGINEER TO UNDERTAKE LEVEL 1 INSPECTION AND TESTING OF COMPACTED FILL WITHIN COMMERCIAL AND RESIDENTIAL PROPERTIES IN ACCORDANCE WITH AS 3798 AND PROVIDE A STATEMENT OF COMPLIANCE. EARTHWORKS WITHIN PUBLIC ROAD RESERVATIONS ARE NOT REQUIRED TO BE SUPERVISED BY THE GEOTECHNICAL ENGINEER.
- E6. THE SUBGRADE SHALL BE COMPACTED TO ACHIEVE 98% STANDARD DENSITY RATIO TO A DEPTH OF 300 mm PRIOR TO FILLING. SUBGRADE IS TO BE PROOF ROLL TESTED. THE TEST SHALL CONSIST OF WITNESSING SOIL DEFLECTION FROM THE TYRE OF A SINGLE REAR AXLE TRUCK DRIVEN AT WALKING SPEED WITH A MINIMUM 8 TONNE REAR AXLE LOAD AND A TYRE PRESSURE OF 550 kPa. EACH LAYER SHALL BE PROOF ROLL TESTED WITH NO VISIBLE MOVEMENT OBSERVED. OTHER VEHICLES THAT MAY BE ALLOWED BY GANDY AND ROBERTS ARE A 12 TONNE STATIC ROLLER WITH 6 TONNE/m LOAD, OR 20 TONNE PLANT WITH 450 kPa TYRES AND GREATER THAN 0.035 m² CONTACT AREA PER TYRE.
- E7. FILL SHALL BE PLACED IN HORIZONTAL LAYERS OF 200 TO 300 mm DEEP. LOOSE MEASUREMENT, UNLESS TESTING CAN DEMONSTRATE TO GANDY AND ROBERTS THAT COMPACTION IS ADEQUATE WITHIN LARGER LIFTS. COMPACT EACH LAYER OF FILL WITHIN 1% OF ITS OPTIMUM MOISTURE CONTENT. MAXIMUM PARTICLE SIZE IS TWO THIRDS DEPTH OF EACH LIFT. EACH LAYER IS TO BE PROOF ROLL TESTED. NUCLEAR DENSITY TESTING TO BE UNDERTAKEN AT A FREQUENCY BASED ON AS3798 (TYPICALLY THE GREATER OF FOUR TESTS PER INSPECTION OR ONE TEST PER 1000 m³). FOR MATERIAL 60 mm AND COARSER, IN-LEU OF DENSITY TESTING A TEST BY DEFLECTION SHALL BE DONE USING SPOT LEVEL DIFFERENCE AT REPRESENTATIVE LOCATIONS BEFORE AND AFTER ROLLING THREE TIMES WITH A 12 TONNE STATIC ROLLER, WITH ACCEPTABLE DIFFERENCES BEING LESS THAN 2 mm.
- E8. ALL PROOF ROLLING SHALL BE WITNESSED BY GANDY AND ROBERTS. A MINIMUM OF TWO PASSES FOR EACH SECTION BEING TESTED IS REQUIRED, WITH A PASS DEFINED AS TRAVEL ACROSS ONE FULL LENGTH OF THE AREA BEING TESTED. COMPLIANCE WITH PROOF ROLLING REQUIREMENTS SHALL BE WHEN AN AREA WITHSTANDS PROOF ROLLING WITHOUT VISIBLE DEFORMATION OR SPRINGING.
- E9. COHESIONLESS (GRANULAR) FILL SHALL BE USED UNLESS OTHERWISE APPROVED BY GANDY AND ROBERTS. COHESIONLESS (GRANULAR) FILL SHALL HAVE LESS THAN 15% PASSING THE 75 MICRON SIEVE, WITH GRADING CURVES SUBMITTED FOR APPROVAL. COHESIONLESS FILL SHALL BE COMPACTED TO THE REQUIREMENTS OF TABLE 5.1 OF AS 3798. COHESIVE FILL SHALL HAVE A MINIMUM 4 DAY SOAKED CBR OF 5% AND A MAXIMUM CBR SWELL OF 1%. MINIMUM STANDARD DENSITY RATIOS FOR COHESIVE MATERIAL SHALL BE AS PER TABLE 5.1 OF AS 3798. REACTIVE CLAY SHALL HAVE A MAXIMUM STANDARD DENSITY RATIO OF 100%. LANDSCAPING ZONES SHOULD BE COMPACTED TO STANDARD DENSITY RATIO OF 85% UNLESS NOTED OTHERWISE.

ROADWORKS

- R1. ALL ROADWORKS, FOOTPATHS & DRIVEWAYS SHALL BE IN ACCORDANCE WITH THE LOCAL GOVERNMENT ASSOCIATION TASMANIA "TASMANIAN MUNICIPAL STANDARDS" INCLUDING TMS SPECIFICATIONS, DRAWINGS AND SUBDIVISION GUIDELINES. IPWEA STANDARD DRAWINGS ARE REFERENCED IN THIS DRAWING SET BY THE ABBREVIATION "TSD".
- R2. IT IS ASSUMED ROADS ACCESSING THE DEVELOPMENT SITE ARE ADEQUATE TO TAKE THE DESIGN TRAFFIC LOAD DURING THE DESIGN LIFE OF 40 YEARS.
- R3. PAVEMENT DEPTH SHALL BE AS SHOWN ON THE TYPICAL CROSS SECTION BUT SHALL BE SUBJECT TO CBR TESTING OF THE SUBGRADE OR PROOF ROLLING BY THE CONTRACTOR, WITH FINAL DEPTH TO BE CONFIRMED BY GANDY AND ROBERTS.
- R4. KERB AND CHANNEL SHALL BE FORMED ON A MINIMUM OF 100 mm SUB-BASE WHICH SHALL EXTEND A MINIMUM 150 mm BEYOND THE BACK OF THE KERB.
- R5. SUBSOIL DRAINS SHALL BE PROVIDED AT ALL LOCATIONS WHERE THE PAVEMENT IS BELOW GROUND LEVEL AND AS SHOWN ON THE DRAWINGS. PIPE AND FITTINGS SHALL BE PROPRIETARY ITEMS COMPLYING WITH AS2439.1.
- R7. ALL RADII ARE TO THE BACK OF KERB UNLESS NOTED OTHERWISE.

ROADWORKS (CONTINUED)

- R8. THE ROAD PROFILE AND CROSS FALL SHALL BE FINISHED TO THE SATISFACTION OF GANDY AND ROBERTS AND SHALL BE TO THE LINE AND LEVEL INDICATED ON THE DRAWINGS, FREE OF ANY LOCAL HIGH OR LOW AREAS WHICH MAY HOLD WATER.
- R9. ALL GRAVEL SHALL COMPLY WITH THE FOLLOWING TASMANIAN DEPARTMENT OF STATE GROWTH (DSG) SPECIFICATIONS:
BASE COURSE: CLASS 2 FINE CRUSHED ROCK (FCR) (PREVIOUSLY R40 CLASS A - 20 mm)
SUB-BASE COURSE: CLASS 3 FCR (PREVIOUSLY SUB-BASE 1 - 40 mm)
- R10. SUB-BASE AND BASE SHALL HAVE A MINIMUM DRY DENSITY RATIO OF 95% AND 98% MODIFIED COMPACTION RESPECTIVELY, WITH NUCLEAR DENSITY TEST RESULTS AVAILABLE AT THE PROOF ROLL INSPECTION. TESTS TO BE TAKEN AT A FREQUENCY BASED ON AS3798 (TYPICALLY THE GREATER OF FOUR TESTS PER INSPECTION OR ONE TEST PER 1000 m²).
- R11. EACH PAVEMENT COURSE SHALL BE PROOF ROLL TESTED. ALL PROOF ROLL TESTING SHALL BE WITNESSED BY GANDY AND ROBERTS. THE TEST SHALL CONSIST OF WITNESSING DEFLECTION FROM THE TYRE OF A SINGLE REAR AXLE TRUCK DRIVEN AT WALKING SPEED WITH A MINIMUM 8 TONNE REAR AXLE LOAD AND A TYRE PRESSURE OF 550 kPa. COMPLIANCE WITH THE TEST ROLLING REQUIREMENTS SHALL BE WHEN AN AREA WITHSTANDS TEST ROLLING WITHOUT VISIBLE DEFORMATION OR SPRINGING.
- R12. PAVEMENT COURSE TOLERANCES SHALL COMPLY WITH THE DSG SPECIFICATION, SECTION 304, CLAUSE 304.06 WITH SCALE C SURFACE LEVEL REQUIREMENTS APPLYING.
- R13. TRENCHES AND EXCAVATIONS WITHIN TRAFFICABLE PAVEMENTS SHALL BE BACKFILLED TO SUBGRADE LEVEL WITH 20 mm FCR TO A MINIMUM DENSITY OF 95%. STANDARD COMPACTION. MATCH EXISTING PAVEMENT ABOVE SUBGRADE. NUCLEAR DENSITY TESTING TO BE UNDERTAKEN AS DIRECTED WITHIN COMMERCIAL SITES AND PUBLIC ROAD RESERVES.
- R14. ASPHALT SEALS SHALL BE A TYPE B MIX WITH CLASS 170 BITUMEN IN ACCORDANCE WITH AAPA ADVISORY NOTE 5, UNLESS NOTED OTHERWISE ON THE DRAWINGS. THE SEAL SHALL BE PLACED ON A CLEAN SWEPT SURFACE WITH A BITUMEN EMULSION COAT APPLIED.
- R15. SPRAYED BITUMINOUS SURFACINGS SHALL BE IN ACCORDANCE WITH DSG SPECIFICATION SECTION 408. REMOVAL OF LOOSE AGGREGATE SHALL BE UNDERTAKEN IN ACCORDANCE WITH CLAUSE 408.14 OF THE SPECIFICATION.
- R16. ALL LANDSCAPED AREAS AFFECTED BY THE WORKS SHALL BE REINSTATED TO MATCH EXISTING. REFER LANDSCAPE ARCHITECT FOR SPECIFIC REQUIREMENTS.

STORMWATER

- SW1. ALL WORKS SHALL BE IN ACCORDANCE WITH LOCAL GOVERNMENT ASSOCIATION TASMANIA - IPWEA STANDARD DRAWINGS, REFERENCED IN THIS DRAWING SET BY THE ABBREVIATION "TSD".
- SW2. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LOCAL AUTHORITY'S SPECIFICATIONS, BY - LAWS AND AS/NZS 3500.3.
- SW3. PIPE AND CHANNEL INFRASTRUCTURE HAS BEEN DESIGNED TO CONVEY 5% ANNUAL EXCEEDANCE PROBABILITY (AEP) STORMS, WITH OVERLAND FLOW PATHS PROVIDED FOR 1% AEP STORMS. IT IS ASSUMED THAT WATER FLOWING ONTO THE DEVELOPMENT SITE IS CONTAINED WITHIN LOCAL AUTHORITY INFRASTRUCTURE FOR 5% AEP STORMS AND THE ROAD RESERVE FOR 100 YEAR ARI STORMS.
- SW4. STORMWATER TRENCHES, PIPE BEDDING AND BACK FILLING SHALL COMPLY WITH AS/NZS 3725 INSTALLATION REQUIREMENTS FOR TYPE HS2 SUPPORT.
- SW5. BELOW GROUND PIPEWORK AND FITTINGS SHALL BE PVC-U DWV S110 FOR DN100 AND S115 FOR DN150 OR GREATER. JOINTS SHALL BE OF SOLVENT CEMENT TYPE OR FLEXIBLE JOINTS MADE WITH APPROVED RUBBER RINGS UNLESS OTHERWISE STATED ON THE DRAWINGS.
- SW6. ALL TRENCHES UNDER TRAFFICKED AREAS, INCLUDING FUTURE DRIVEWAY EXTENSIONS, SHALL BE BACK FILLED WITH APPROVED COMPACTED CLASS 2 FCR OR CEMENT STABILISED SAND WITH NOT LESS THAN 4% CEMENT BY WEIGHT OF CEMENT.
- SW7. MINIMUM GRADE OF PAVED AREAS AND PIPEWORK SHALL BE 1 IN 100. PAVED AREAS SHALL BE SHAPED TO DRAIN TO GRATED PITS AND TRENCHES WITHOUT PONDING (ACCEPTABLE LIMIT IS 3 mm UNDER A 2 m STRAIGHT EDGE).
- SW8. SURFACE WATER DRAINS, CATCH PITS/GRATED PITS AND JUNCTION BOXES SHALL BE CONSTRUCTED AS DETAILED OR AS SPECIFIED BY THE MANUFACTURER. GRATED PITS TO HAVE SUMPS AS NOTED ON THE DRAWINGS. PITS AND LIDS TO BE CLASS A IN NON-TRAFFICKED AREAS AND CLASS B IN RESIDENTIAL DRIVEWAYS. PRE-CAST CONCRETE PITS AND LIDS WITH CLASS C OR CLASS D RATING SHALL BE USED ELSEWHERE OR AS NOTED ON THE DRAWINGS. ALL COVERS AND GRATES SHALL COMPLY WITH AS3796. CONVEY TRENCH WATER INTO PITS/MANHOLES THROUGH WEEP HOLES ON UPSTREAM SIDE USING 2 m OF DN100 SUBSOIL DRAIN WITH FILTER SOCK.
- SW9. SUBSOIL DRAINS SHALL BE INSTALLED TO THE REQUIREMENTS OF AS/NZS 3500 AND PART 3.12 OF THE BCA. PERFORATED PVC DRAINAGE PIPE AND FITTINGS SHALL COMPLY WITH AS2439.1. ALL CONNECTIONS AND FITTINGS SHALL BE MADE USING PURPOSE SPECIFIC PROPRIETARY ITEMS.
- SW10. ALL HYDRAULIC CONNECTIONS SHALL BE CLEAR OF DRIVEWAYS AND TRAFFICKED AREAS.
- SW11. WHERE BOTH STORMWATER AND SEWER LINES ARE ALONG REAR AND SIDE BOUNDARIES THEY SHALL BE LOCATED TO FIT INSIDE A 3.0 m EASEMENT UNLESS NOTED OTHERWISE. A SINGLE STORMWATER LINE SHALL FIT WITHIN A 2.5 m EASEMENT.
- SW12. ALL MANHOLES SHALL BE LOCATED CLEAR OF FUTURE FENCE LINES.
- SW13. PROPERTY CONNECTIONS SHALL BE CLEAR OF DRIVEWAYS, CLEAR OF FUTURE FENCE LINES AND BE LOCATED 600mm INSIDE THE BOUNDARY.
- SW14. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996.2019.

PUBLIC SEWER

- S1. ALL WORKS SHALL BE IN ACCORDANCE WITH THE SEWERAGE CODE OF AUSTRALIA (SCA) W.S.A. 02-2014-3:1 MRWA EDITION - VERSION 2 AND TASWATER'S CURRENT SUPPLEMENT TO THE CODE.
- S2. PROPERTY CONNECTIONS SHALL BE DN100 PVC-U WITH A MINIMUM GRADE OF 1 IN 60 (REFER ABOVE CODE FOR DETAILS) AND SHALL BE LOCATED CLEAR OF TRAFFICKED AREAS, DRIVEWAYS AND FENCES.
- S3. WHERE BOTH STORMWATER AND SEWER LINES ARE LOCATED ALONG A REAR OR SIDE BOUNDARY THEY SHALL BE LOCATED IN AN EASEMENT THAT WHOLLY CONTAINS BOTH SERVICES AND THEIR RESPECTIVE APPURTENANCES. EASEMENTS AND OFFSETS SHALL BE IN ACCORDANCE WITH TABLE 111-C DRAWING MRWA-S-111 IN THE ABOVE CODE AND TASWATER'S SUPPLEMENT CLAUSE 5.2.8. CLEARANCES TO OTHER SERVICES SHALL BE IN ACCORDANCE WITH TABLE 5.4 IN THE ABOVE CODE.
- S4. ALL MAINTENANCE STRUCTURES SHALL BE LOCATED CLEAR OF FUTURE FENCE LINES WITH END OF LINES TO BE 12 m PAST THE BOUNDARY FOR ANY FUTURE EXTENSION.
- S5. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996.2019.

PUBLIC WATER

- W1. ALL WORKS SHALL BE IN ACCORDANCE WITH THE WATER SUPPLY CODE OF AUSTRALIA (WSCA) W.S.A. 03-2011-3:1 MRWA EDITION - VERSION 2 AND TASWATER'S CURRENT SUPPLEMENT TO THE CODE.
- W2. UNLESS NOTED OTHERWISE, SINGLE HOSE CONNECTIONS SHALL BE DN25 HDPE PN16 TO TASWATER'S STANDARD DRAWING TWS-W-0002 SERIES WITH VALVE, SENSUS WATER METER, AND BOX TO EACH LOT. CONNECTIONS SHALL BE LOCATED 500 mm INSIDE BOUNDARY AND 500 mm FROM EDGE OF DRIVEWAY ON MIDDLE SIDE OF LOT, UNLESS NOTED OTHERWISE.
- W3. ALL WATER MAINS SHALL BE TESTED AND WITNESSED BY TASWATER TO STATIC PRESSURE PLUS 50% PRIOR TO BACKFILLING.
- W4. ALL HYDRAULIC CONNECTIONS AND TAPPINGS TO BE CLEAR OF DRIVEWAYS AND TRAFFICKED AREAS.
- W5. FOR MINIMUM COVER OVER PIPES REFER WSCA DRAWING MRWA-W-202.
- W6. FOR MINIMUM CLEARANCES TO OTHER SERVICES REFER TO TASWATER'S SUPPLEMENT CLAUSE 5.12.5.2 RECOMMENDATIONS.

PUBLIC WATER (CONTINUED)

- W7. ALL TRENCHES UNDER TRAFFICKED AREAS SHALL BE BACK FILLED WITH APPROVED COMPACTED CLASS 2 FCR INCLUDING FUTURE DRIVEWAY EXTENSIONS.
- W8. FLUSHING OF MAINS TO BE CARRIED OUT IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.
- W9. ELECTROMAGNETIC TRACKER TAPE SHALL BE PLACED IN ALL WATER MAIN TRENCHES ABOVE THE PIPE.
- W10. WATER MAINS SHALL BE BEDDED ON 80 mm APPROVED 7 mm CLEAN METAL.
- W11. CONCRETE ANCHOR BLOCKS SHALL BE PROVIDED AT ALL SUDDEN CHANGES OF DIRECTION, BOTH VERTICALLY AND HORIZONTALLY AT TEES AND END OF LINES. REFER TO WSCA DRAWINGS MRWA-W-205A, MRWA-W-205B AND MRWA-W-205C.
- W12. ROAD CROSSINGS:
DN100 PVC-U CONDUITS, CLASS S16, FOR ALL HDPE
DICTL WITH PE WRAPPING SLEEVE TO AS 3680.
- W13. FOR VALVE AND HYDRANT SURFACE BOX MARKINGS REFER TO TASWATER'S SUPPLEMENT CLAUSE 8.8 & CLAUSE 8.10.3. HYDRANT ROAD MARKINGS SHALL COMPLY WITH THE IPWEA TASMANIA DIVISION DOCUMENT TITLED FIRE HYDRANT GUIDELINES - REFER SECTION 8. ALL VALVES AND HYDRANTS TO BE RESILIENT SEATED POWDER COATED CLASS 16 AND ALL COMPONENTS TO BE DN100.
- W14. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996.2019.

CONCRETE

- C1. ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AS3600.
- C2. CONCRETE GRADES (UNO ON DRAWINGS):
ELEMENT GRADE ELEMENT GRADE
GENERAL N25 BLINDING N15
FOOTINGS N20 PAVEMENT N25
- C3. CONCRETE SHALL NOT BE POURED WHEN THE SITE TEMPERATURES ARE BELOW 5°C.
- C4. CONCRETE SHALL BE CURED BY CONTINUOUS WETTING (WATER SPRAY, PONDING OR IRRIGATED HESSIAN) OR APPLICATION OF AN IMPERMEABLE MEMBRANE (SECURED PLASTIC OR CURING COMPOUND) FOR AN APPROPRIATE PERIOD OF TIME (NOT LESS THAN 3 DAYS) IN HOT DRY AND WINDY WEATHER. SPRAY THE SURFACE WITH ALIPHATIC ALCOHOL WHILE CONCRETE IS PLASTIC. WATER CURE FOR AT LEAST 24 HOURS THEN COVER WITH IMPERMEABLE MEMBRANE (OR CONTINUE TO WATER CURE) FOR A FURTHER 2 DAYS.
- C5. CONSTRUCTION JOINTS SHALL BE PROPERLY FORMED AND USED ONLY WHERE SHOWN OR SPECIFICALLY APPROVED BY GANDY AND ROBERTS. SAWN JOINTS SHALL BE CUT ONE THIRD OF THE WAY THROUGH A SLAB. THROUGH THE TOP MESH FOR 100 mm SLABS AND IN THICKER SLABS THE MESH SHALL BE PLACED TO AVOID BEING CUT. UNLESS NOTED ELSEWHERE, SAWN JOINTS SHALL BE AT 4.5 m CENTRES AND AT POINTS OF CHANGES IN GEOMETRY AND CONSTRUCTION JOINTS AT 18 m WITH JOINTED AREAS TO HAVE A PLAN ASPECT RATIO NO SLENDERER THAN 1:2.
- C7. REINFORCEMENT SHALL BE DEFORMED, 500 MPa YIELD STRENGTH, NORMAL (N) DUCTILITY IN ACCORDANCE WITH AS/NZS 4671 FOR BARS AND LOW (L) DUCTILITY FOR MESH.
- C8. FORMWORK SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH AS 3610 AND IS THE RESPONSIBILITY OF THE CONTRACTOR.
- C9. ALL STEEL ITEMS TO BE CAST INTO THE CONCRETE SURFACE SHALL BE HOT DIP GALVANISED.

LEGEND

● 9.80	EXISTING SURFACE LEVEL
● 9.80	PROPOSED BULK EARTHWORKS LEVEL
● 9.60	PROPOSED FINISHED SURFACE LEVEL
(Δ - 2.5)	CUT (L)-/FILL DEPTH
— EX W — EX W — EX W —	EXISTING WATER SUPPLY EXTERNAL TO BUILDING
— W — W — W —	PROPOSED WATER SUPPLY EXTERNAL TO BUILDING
— EX FS — EX FS — EX FS —	EXISTING FIRE SUPPLY
— FS — FS — FS —	PROPOSED FIRE SUPPLY
— EX S — EX S — EX S —	EXISTING SEWER DRAIN
— S — S — S —	PROPOSED SEWER DRAIN
— EX SW — EX SW — EX SW —	EXISTING STORMWATER DRAIN
— SW — SW — SW —	PROPOSED STORMWATER DRAIN
— S — S — S —	PROPOSED STORMWATER (LARGER)
— SS — SS — SS —	PROPOSED DN100 SUBSOIL DRAIN WITH GEOTEXTILE SOCK
— C J — C J — C J —	PROPOSED CONCRETE CONSTRUCTION JOINT
— K J — K J — K J —	PROPOSED CONCRETE KEY JOINT
— S J — S J — S J —	PROPOSED CONCRETE SAWN JOINT
— TOP — TOP — TOP —	EXISTING BATTER
— TOP — TOP — TOP —	PROPOSED BATTER
— — — — —	PROPERTY BOUNDARY
— E J —	EXPANSION JOINT
— S E J —	SWIVEL EXPANSION JOINT

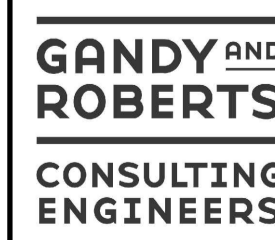


LOCALITY PLAN
SCALE 1:2000

REV	DESCRIPTION	APP'D	DATE
E	PLANNING APPROVAL	DH	19.09.2025
D	100% DESIGN DEVELOPMENT	DH	25.06.2025
C	50% DESIGN DEVELOPMENT	DH	14.05.2025
B	PLANNING APPROVAL	DH	17.04.2025

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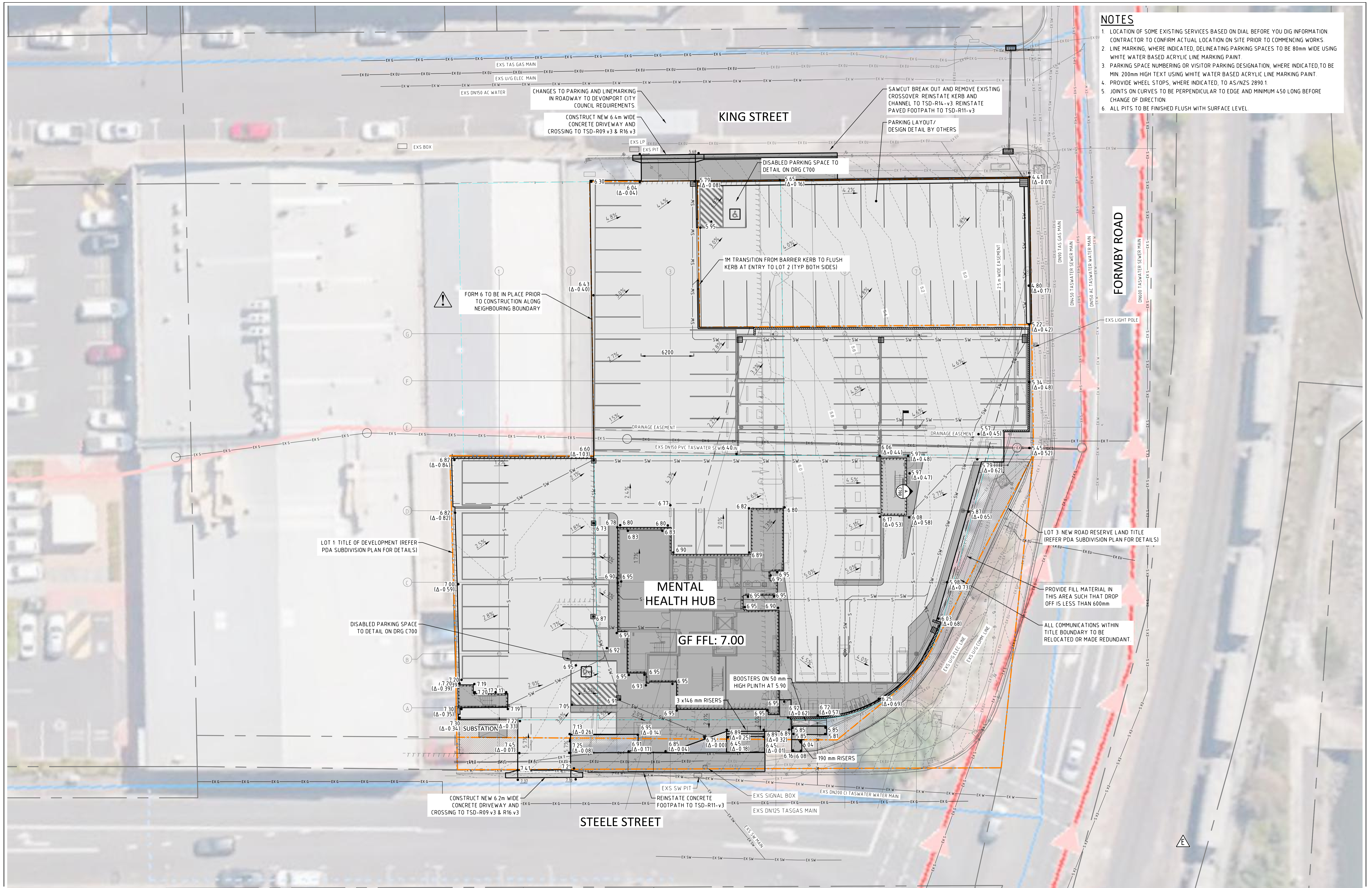
REV	DESCRIPTION	APP'D	DATE



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DEVONPORT MENTAL HEALTH HUB
6-10 STEELE STREET
TASMANIA 7310 DEVONPORT
DRAWING TITLE
DRAWING INDEX AND NOTES

DESIGNED DS	DRAWN DS	CHECKED DH
24.0387	C001	E



- NOTES**
1. LOCATION OF SOME EXISTING SERVICES BASED ON DIAL BEFORE YOU DIG INFORMATION CONTRACTOR TO CONFIRM ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS
 2. LINE MARKING, WHERE INDICATED, DELINEATING PARKING SPACES TO BE 80mm WIDE USING WHITE WATER BASED ACRYLIC LINE MARKING PAINT
 3. PARKING SPACE NUMBERING OR VISITOR PARKING DESIGNATION, WHERE INDICATED TO BE MIN 200mm HIGH TEXT USING WHITE WATER BASED ACRYLIC LINE MARKING PAINT
 4. PROVIDE WHEEL STOPS, WHERE INDICATED, TO AS/NZS 2890.1
 5. JOINTS ON CURVES TO BE PERPENDICULAR TO EDGE AND MINIMUM 450 LONG BEFORE CHANGE OF DIRECTION
 6. ALL PITS TO BE FINISHED FLUSH WITH SURFACE LEVEL

REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE
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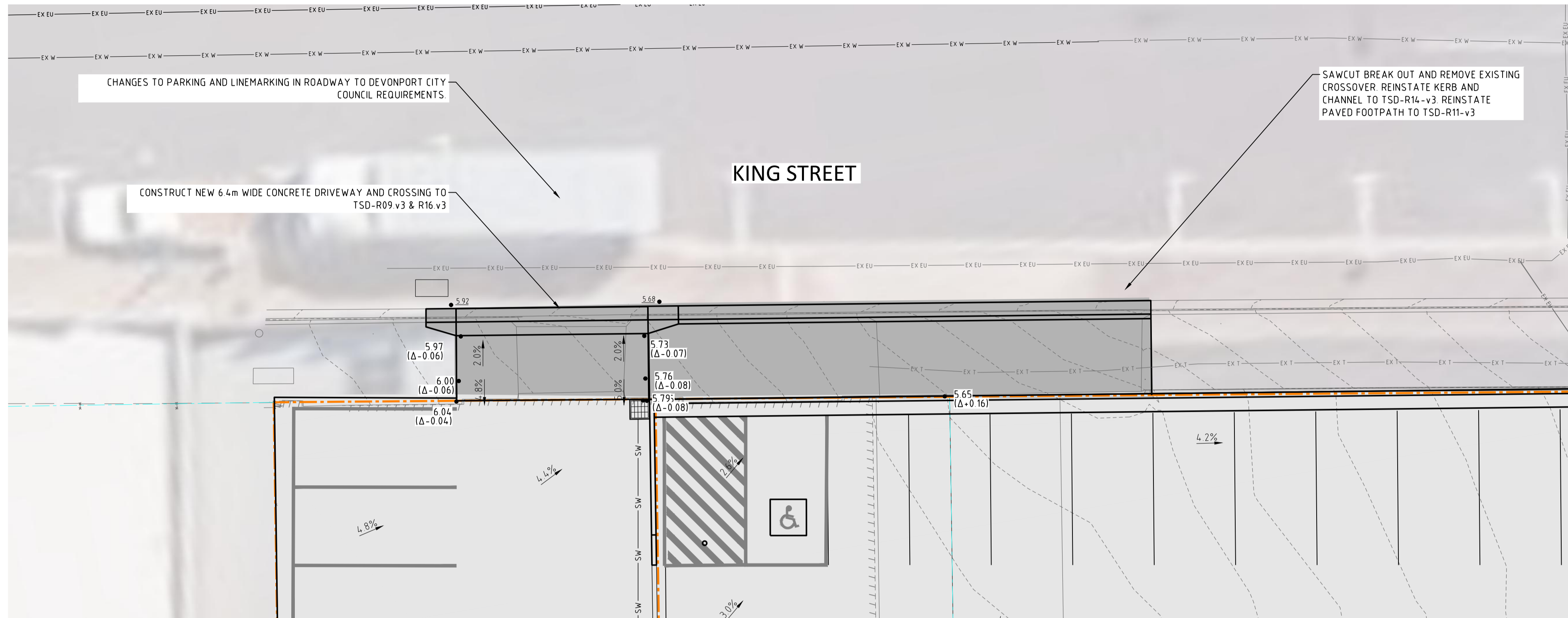
DEVONPORT MENTAL HEALTH HUB
 6-10 STEELE STREET
 TASMANIA 7310 DEVONPORT
 DRAWING TITLE
SITWORKS PLAN 01

DESIGNED DS	DRAWN DS	CHECKED DH
24.0387	C200	E

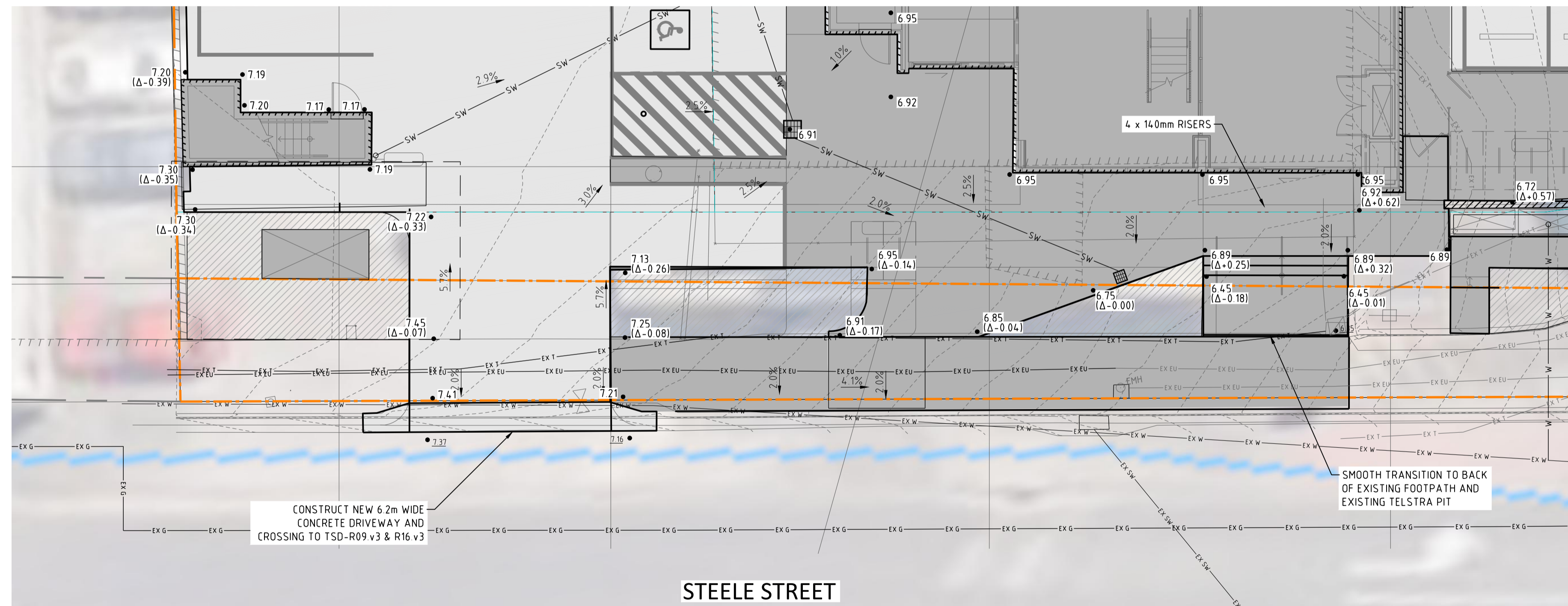
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KING ST ENTRY WORKS DETAILS
SCALE 1:100

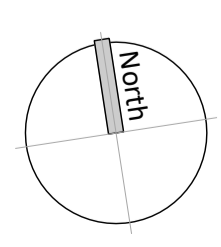


STEELE ST ENTRY WORKS DETAILS
SCALE 1:100

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B	PLANNING APPROVAL		17.04.2025				

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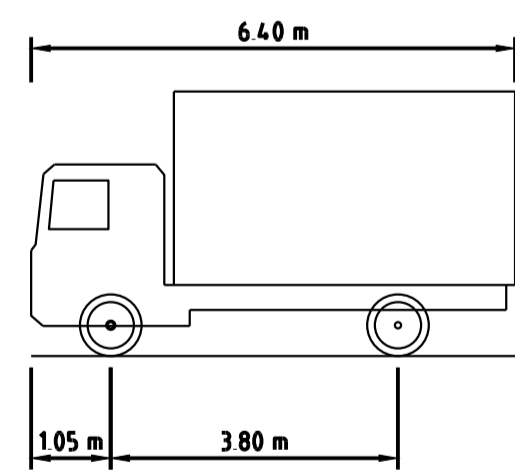
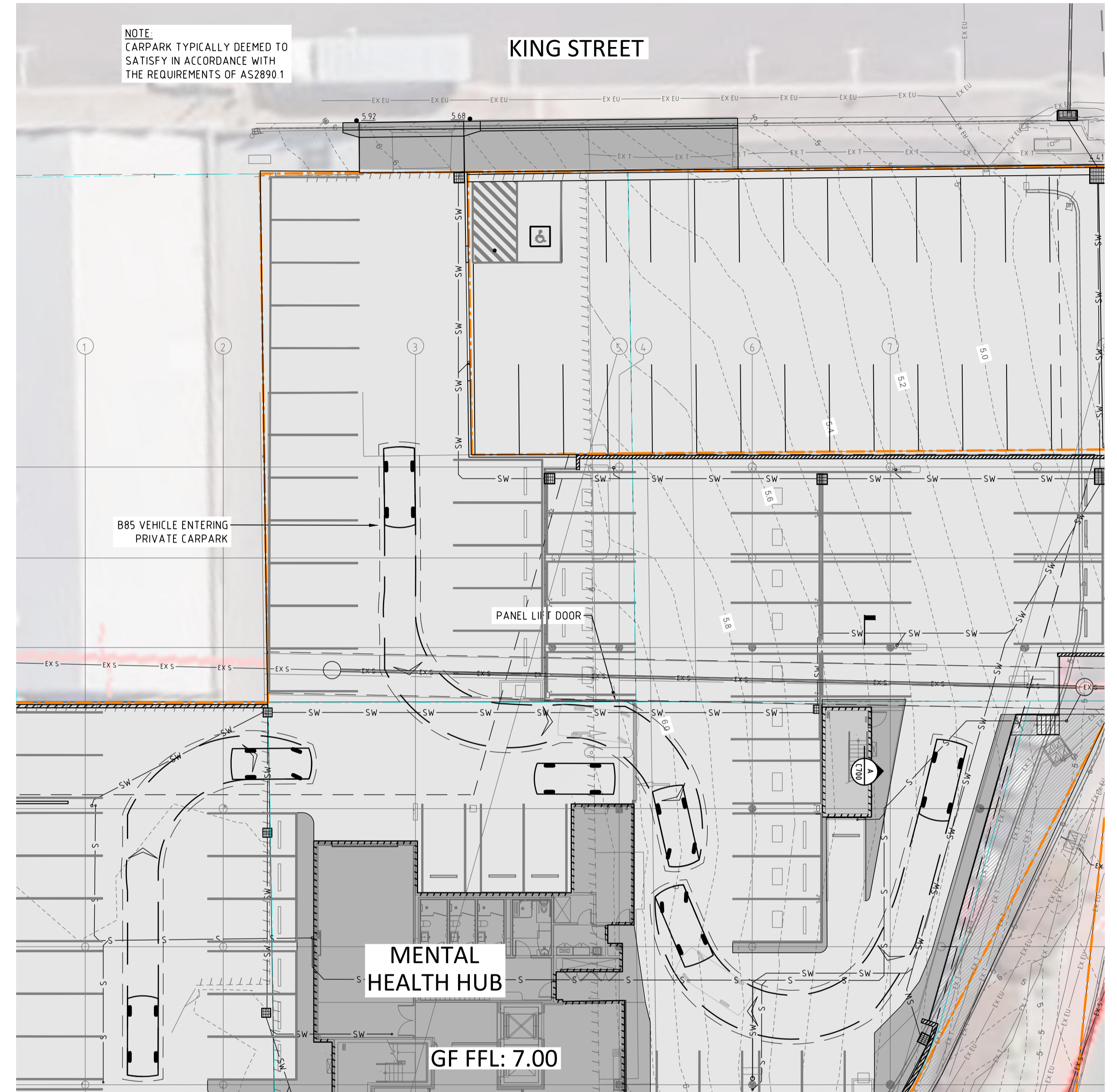
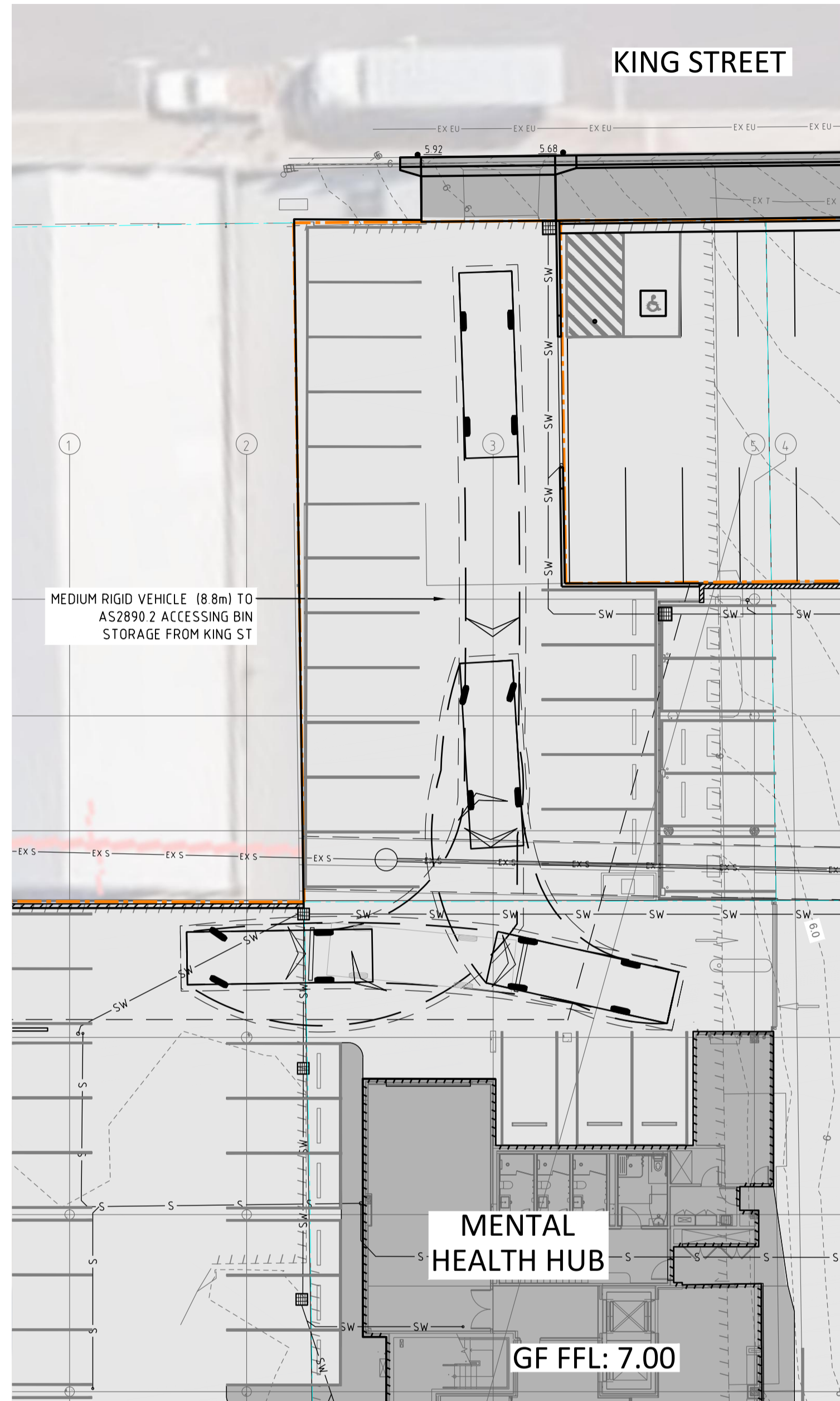
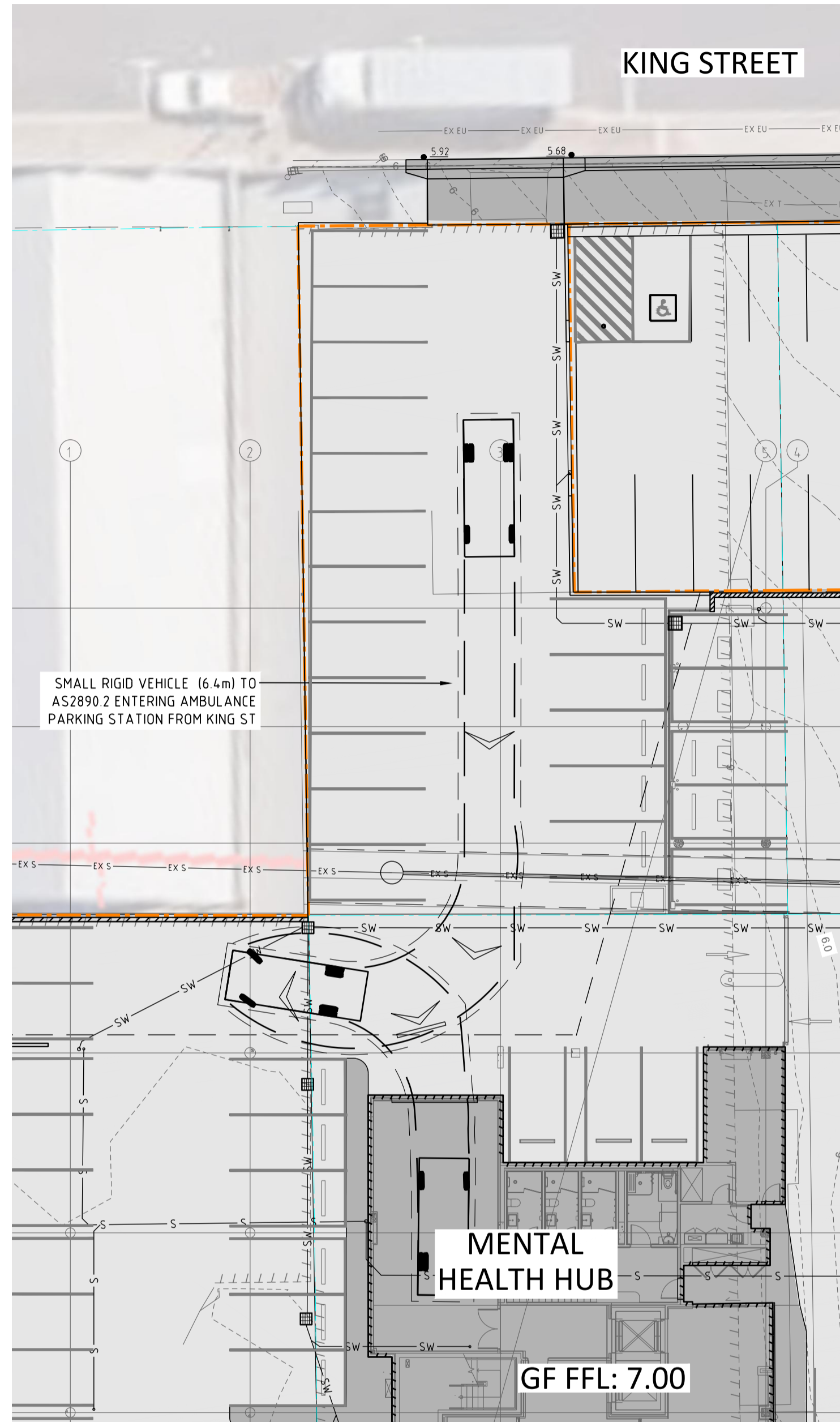


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DEVONPORT MENTAL HEALTH HUB
 6-10 STEELE STREET
 TASMANIA 7310 DEVONPORT
 DRAWING TITLE
SITWORKS PLAN 02

DESIGNED DS		DRAWN DS		CHECKED DH	
PROJECT 24.0387		DRAWING C201		REVISION E	

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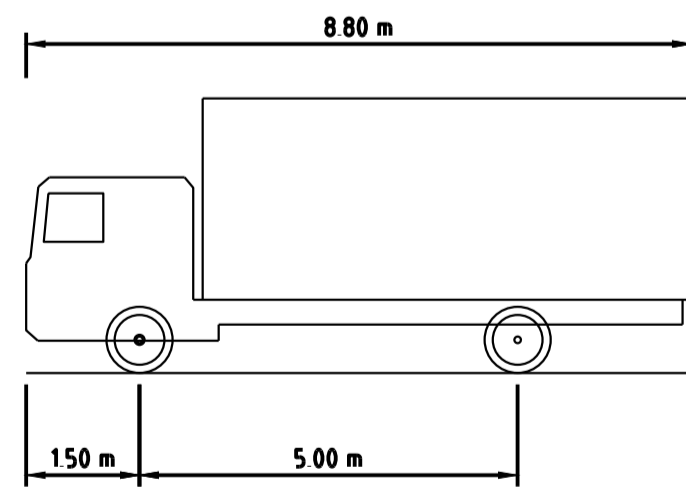
STANDARDS AUSTRALIA AS/NZS 2890 1:2004
SRV - SMALL RIGID VEHICLE

OVERALL LENGTH	6.40 m
OVERALL WIDTH	2.33 m
OVERALL BODY HEIGHT	3.50 m
MIN BODY GROUND CLEARANCE	0.40 m
TRACK WIDTH	2.33 m
LOCK-TO-LOCK TIME	4.00 s
KERB TO KERB TURNING RADIUS	7.10 m

SWEPT PATHS GENERATED USING AUTODESK VEHICLE TRACKING 2024 SOFTWARE

SMALL RIGID VEHICLE PROFILE

NTS



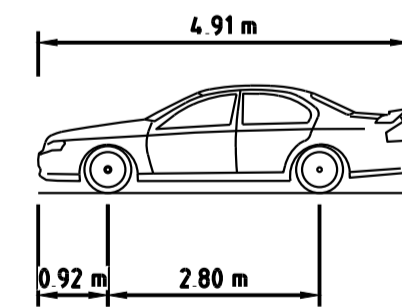
STANDARDS AUSTRALIA AS/NZS 2890 1:2004
MRV - MEDIUM RIGID VEHICLE

OVERALL LENGTH	8.80 m
OVERALL WIDTH	2.50 m
OVERALL BODY HEIGHT	3.66 m
MIN BODY GROUND CLEARANCE	0.42 m
TRACK WIDTH	2.50 m
LOCK-TO-LOCK TIME	4.00 s
KERB TO KERB TURNING RADIUS	10.00 m

SWEPT PATHS GENERATED USING AUTODESK VEHICLE TRACKING 2024 SOFTWARE

MEDIUM RIGID VEHICLE PROFILE

NTS



STANDARDS AUSTRALIA AS/NZS 2890 1:2004
885 VEHICLE (REALISTIC MIN RADIUS)

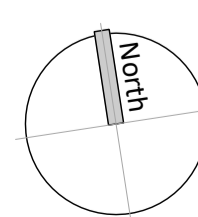
OVERALL LENGTH	4.91 m
OVERALL WIDTH	1.87 m
OVERALL BODY HEIGHT	1.42 m
MIN BODY GROUND CLEARANCE	0.15 m
TRACK WIDTH	1.77 m
LOCK-TO-LOCK TIME	4.00 s
KERB TO KERB TURNING RADIUS	5.75 m

SWEPT PATHS GENERATED USING AUTODESK VEHICLE TRACKING 2024 SOFTWARE

885 VEHICLE PROFILE

NTS

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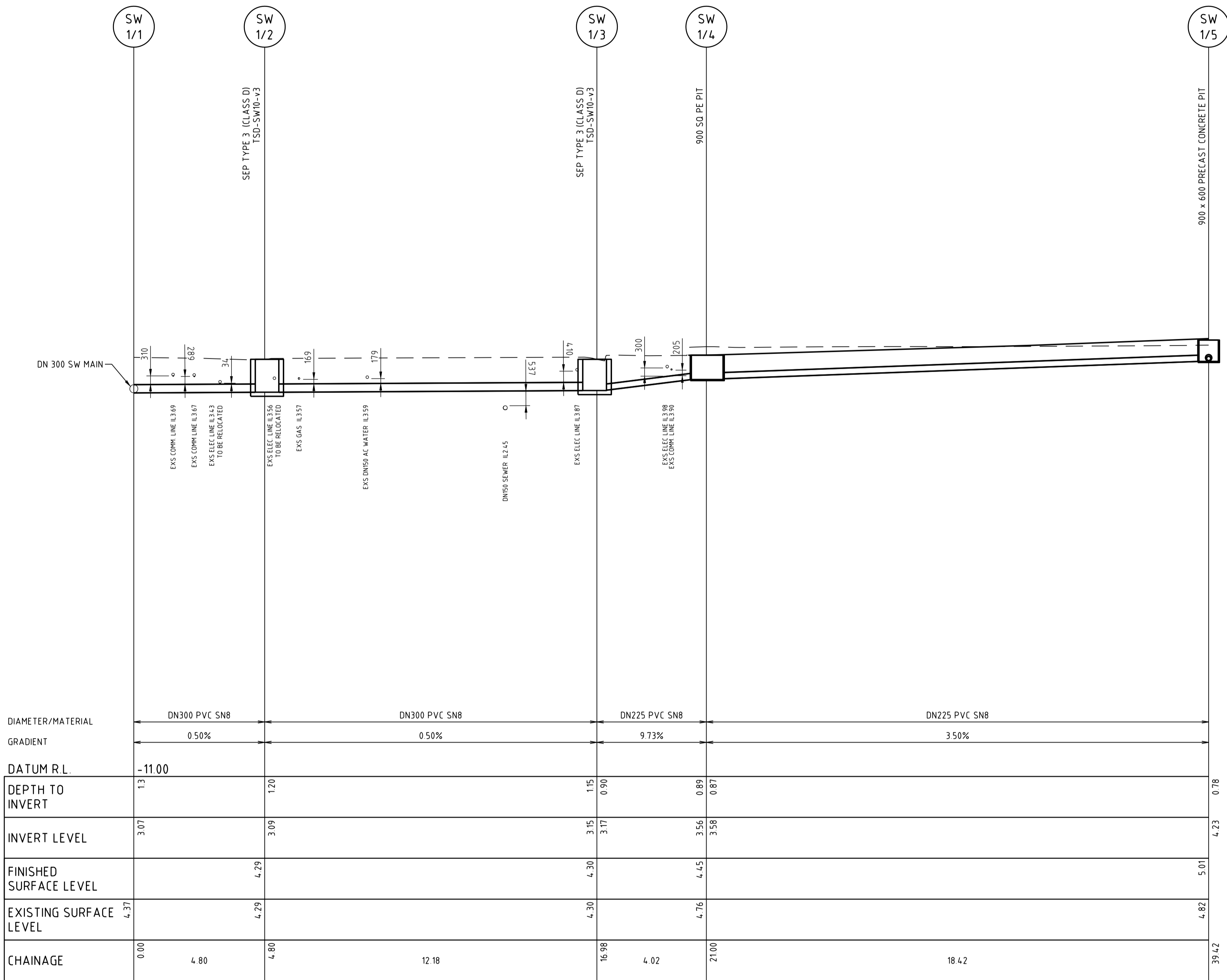


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6-10 STEELE STREET
TASMANIA 7310 DEVONPORT
DRAWING TITLE
SITWORKS PLAN 03 - VEHICULAR ACCESS

0		50mm	SCALE
			1:200@A1
DESIGNED DS	DRAWN DS	CHECKED DH	
PROJECT 24.0387	DRAWING C202	REVISION E	



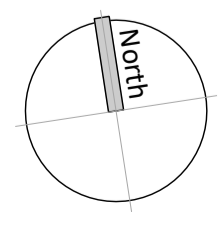
LINE No. SW 01

LONGITUDINAL SECTION
SCALE HOR 1:100 VER 1:100

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E	PLANNING APPROVAL		19.09.2025				
D	100% DESIGN DEVELOPMENT		25.06.2025				
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B	PLANNING APPROVAL		17.04.2025				

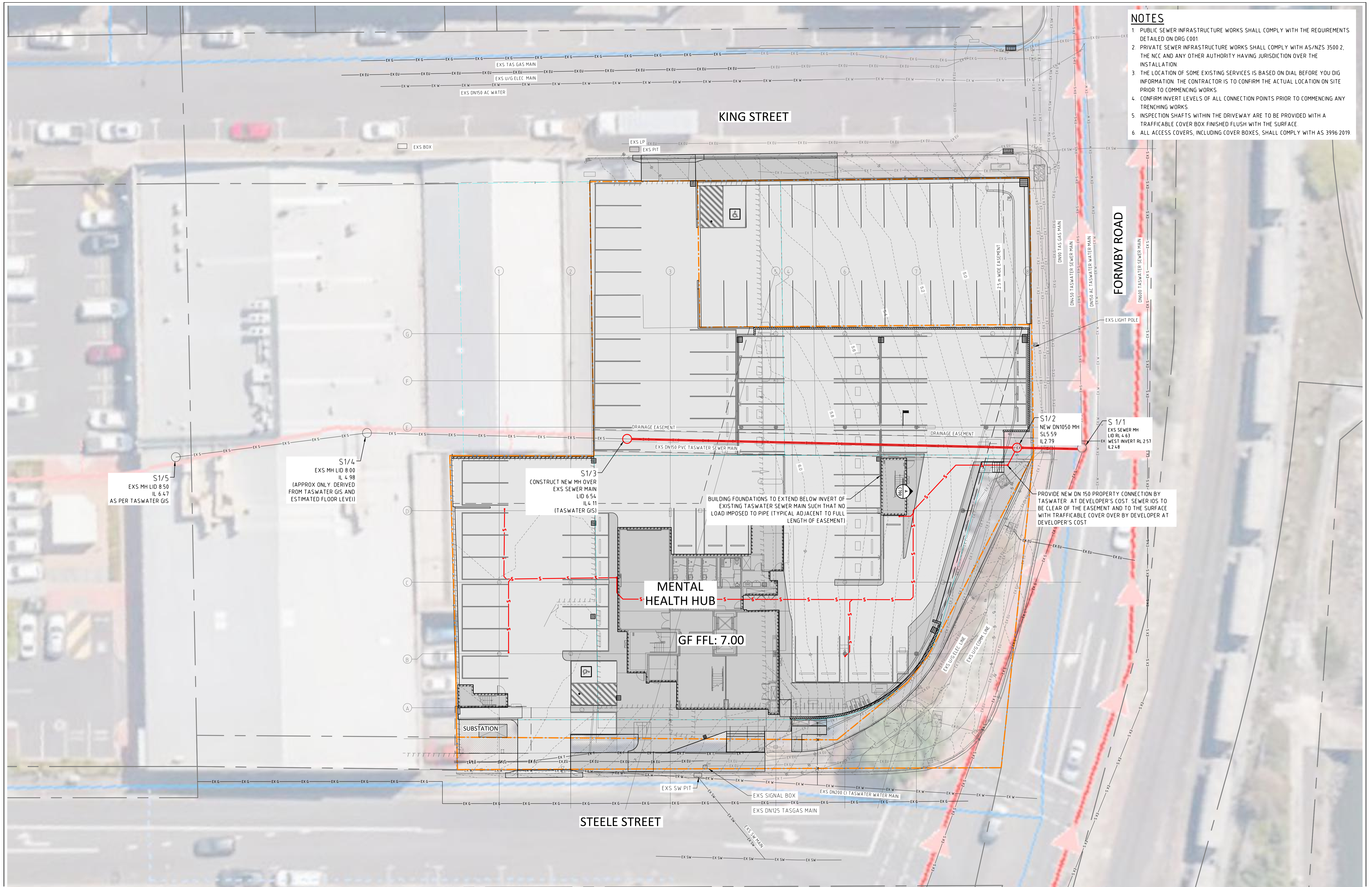
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DEVONPORT MENTAL HEALTH HUB
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 DRAWING TITLE
STORMWATER LONGITUDINAL SECTION

DESIGNED DS		DRAWN DS		CHECKED DH	
PROJECT 24.0387		DRAWING C401		REVISION E	



- NOTES**
- 1 PUBLIC SEWER INFRASTRUCTURE WORKS SHALL COMPLY WITH THE REQUIREMENTS DETAILED ON DRG C001
 - 2 PRIVATE SEWER INFRASTRUCTURE WORKS SHALL COMPLY WITH AS/NZS 3500.2, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION
 - 3 THE LOCATION OF SOME EXISTING SERVICES IS BASED ON DIAL BEFORE YOU DIG INFORMATION. THE CONTRACTOR IS TO CONFIRM THE ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS
 - 4 CONFIRM INVERT LEVELS OF ALL CONNECTION POINTS PRIOR TO COMMENCING ANY TRENCHING WORKS
 - 5 INSPECTION SHAFTS WITHIN THE DRIVEWAY ARE TO BE PROVIDED WITH A TRAFFICABLE COVER BOX FINISHED FLUSH WITH THE SURFACE
 - 6 ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996:2019

S1/5
EXS MH LID 8.50
IL 6.47
AS PER TASWATER GIS

S1/4
EXS MH LID 8.00
IL 4.98
(APPROX ONLY. DERIVED FROM TASWATER GIS AND ESTIMATED FLOOR LEVEL)

S1/3
CONSTRUCT NEW MH OVER EXS SEWER MAIN LID 6.54 IL 4.11 (TASWATER GIS)

S1/2
NEW DN1050 MH SLS 5.59 IL 2.79

S1/1
EXS SEWER MH LID RL 4.63 EX WEST INVERT RL 2.57 IL 2.48

MENTAL HEALTH HUB
GF FFL: 7.00

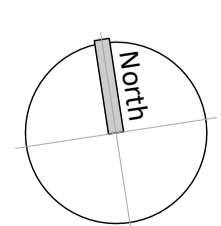
BUILDING FOUNDATIONS TO EXTEND BELOW INVERT OF EXISTING TASWATER SEWER MAIN SUCH THAT NO LOAD IMPOSED TO PIPE (TYPICAL ADJACENT TO FULL LENGTH OF EASEMENT)

PROVIDE NEW DN 150 PROPERTY CONNECTION BY TASWATER AT DEVELOPER'S COST. SEWER IOS TO BE CLEAR OF THE EASEMENT AND TO THE SURFACE WITH TRAFFICABLE COVER OVER BY DEVELOPER AT DEVELOPER'S COST

19/10/2025 12:14:04 PM S:\Projects\24\24-03-000-Devonport Mental Health Hub\5 - GP drawings\01 - Working drawings model\5.0381_C517A02_2.dwg

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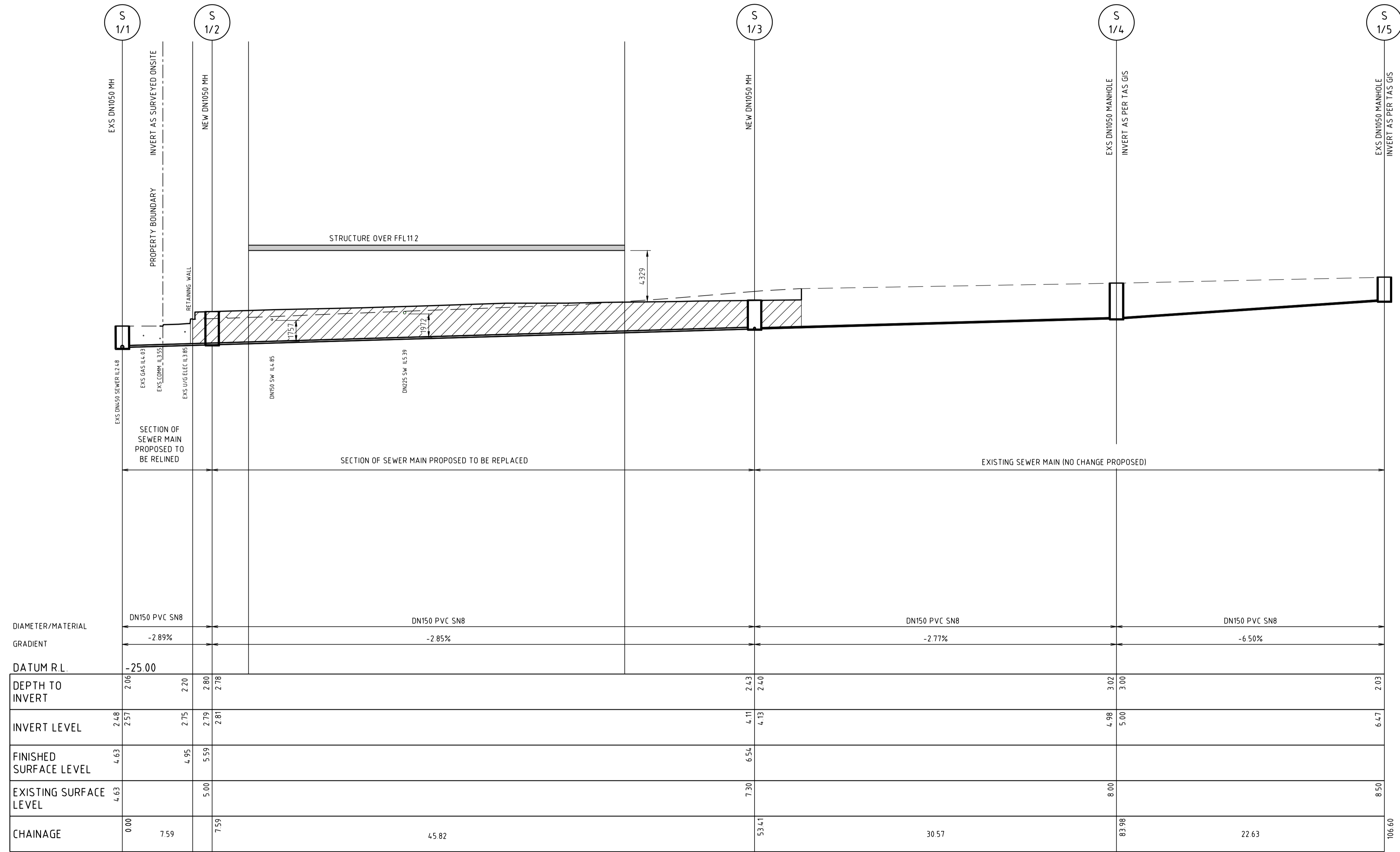
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DEVONPORT MENTAL HEALTH HUB
 6-10 STEELE STREET
 TASMANIA 7310 DEVONPORT
 DRAWING TITLE
SEWER PLAN

DESIGNED DS		DRAWN DS		CHECKED DH	
PROJECT	24.0387	DRAWING	C500	REVISION	E

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LINE No. EXS S 01

LONGITUDINAL SECTION
SCALE HOR 1:200 VER 1:200

DIAMETER/MATERIAL	DN150 PVC SN8		DN150 PVC SN8		DN150 PVC SN8		DN150 PVC SN8		
GRADIENT	-2.89%		-2.85%		-2.77%		-6.50%		
DATUM R.L.	-25.00								
DEPTH TO INVERT	2.06	2.20	2.80	2.78	2.43	3.02	3.00	2.03	
INVERT LEVEL	2.48	2.57	2.75	2.79	4.11	4.98	5.00	6.47	
FINISHED SURFACE LEVEL	4.63	4.95	5.59	5.59	6.54	6.54	6.54	6.54	
EXISTING SURFACE LEVEL	4.63		5.00		7.30	8.00		8.50	
CHAINAGE	0.00	7.59	7.59	45.82	53.41	30.57	83.98	22.63	106.60

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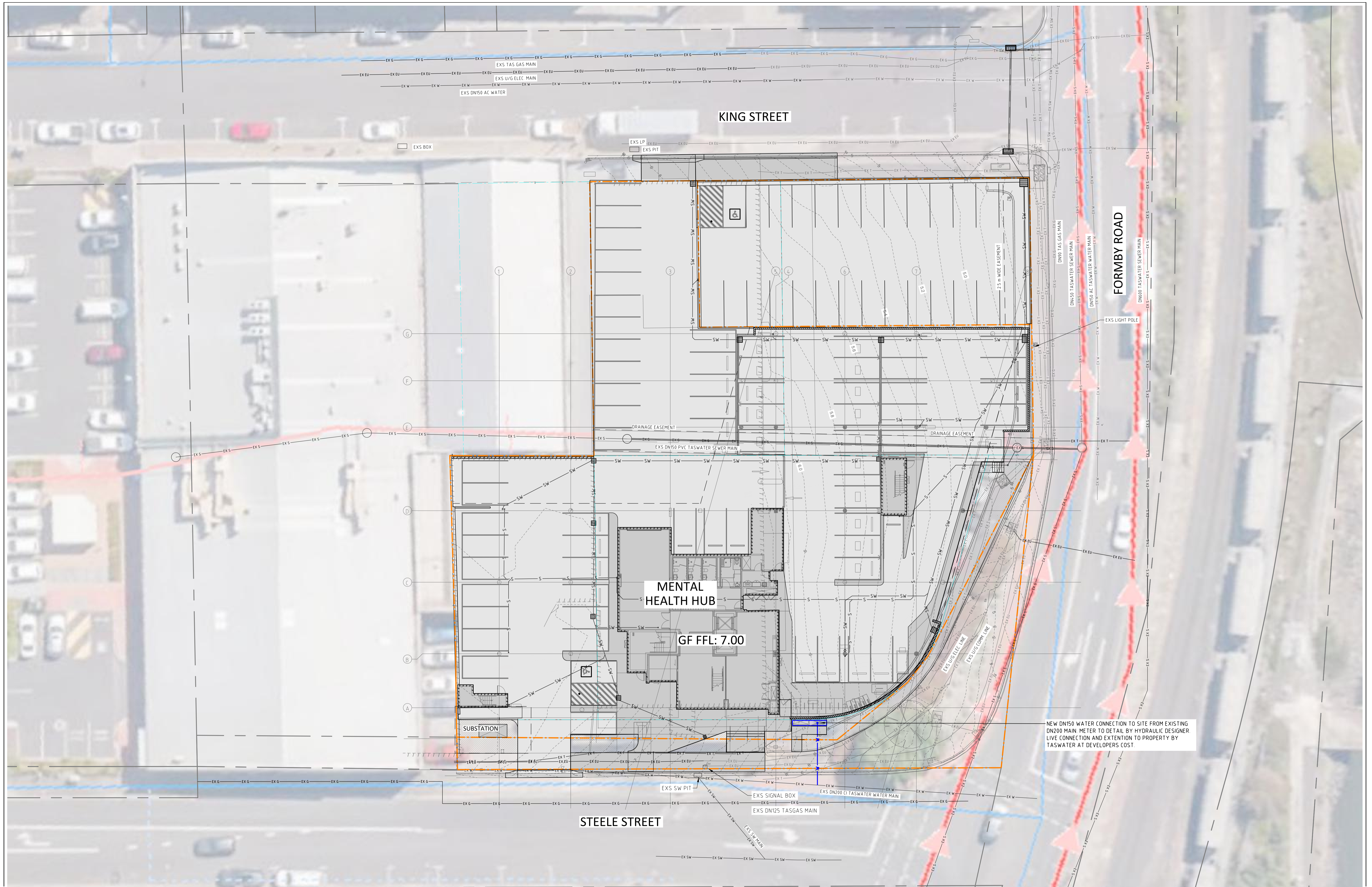
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DEVONPORT MENTAL HEALTH HUB
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TASMANIA 7310 DEVONPORT
DRAWING TITLE
SEWER LONGITUDINAL SECTION 1

DESIGNED DS	DRAWN DS	CHECKED DH
PROJECT 24.0387	DRAWING C501	REVISION E

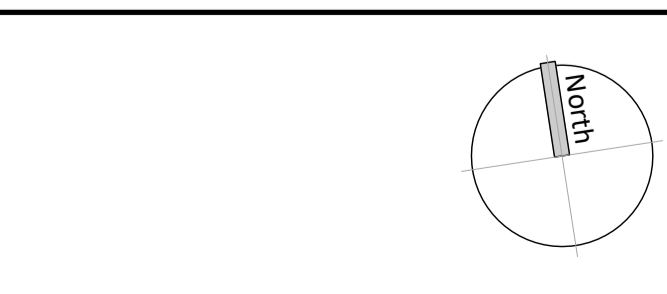
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19/10/2025 12:43:55 PM S:\projects\32024\32024_0387\Development\Mental Health Hub\5 - GR drawings\DWG - Working drawings - Model\32024_0387_051A_EE.dwg
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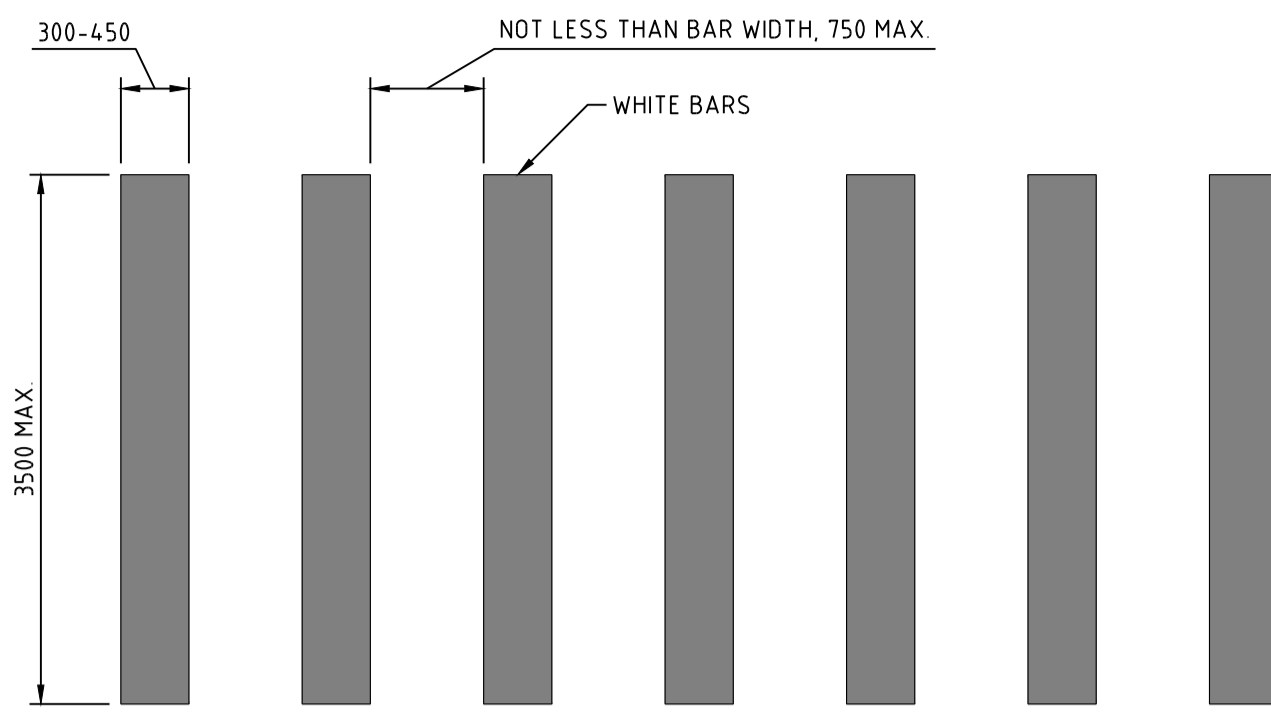


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DEVONPORT MENTAL HEALTH HUB
 6-10 STEELE STREET
 TASMANIA 7310 DEVONPORT
 DRAWING TITLE
WATER PLAN

DESIGNED DS		DRAWN DS		CHECKED DH	
PROJECT	24.0387	DRAWING	C600	REVISION	E

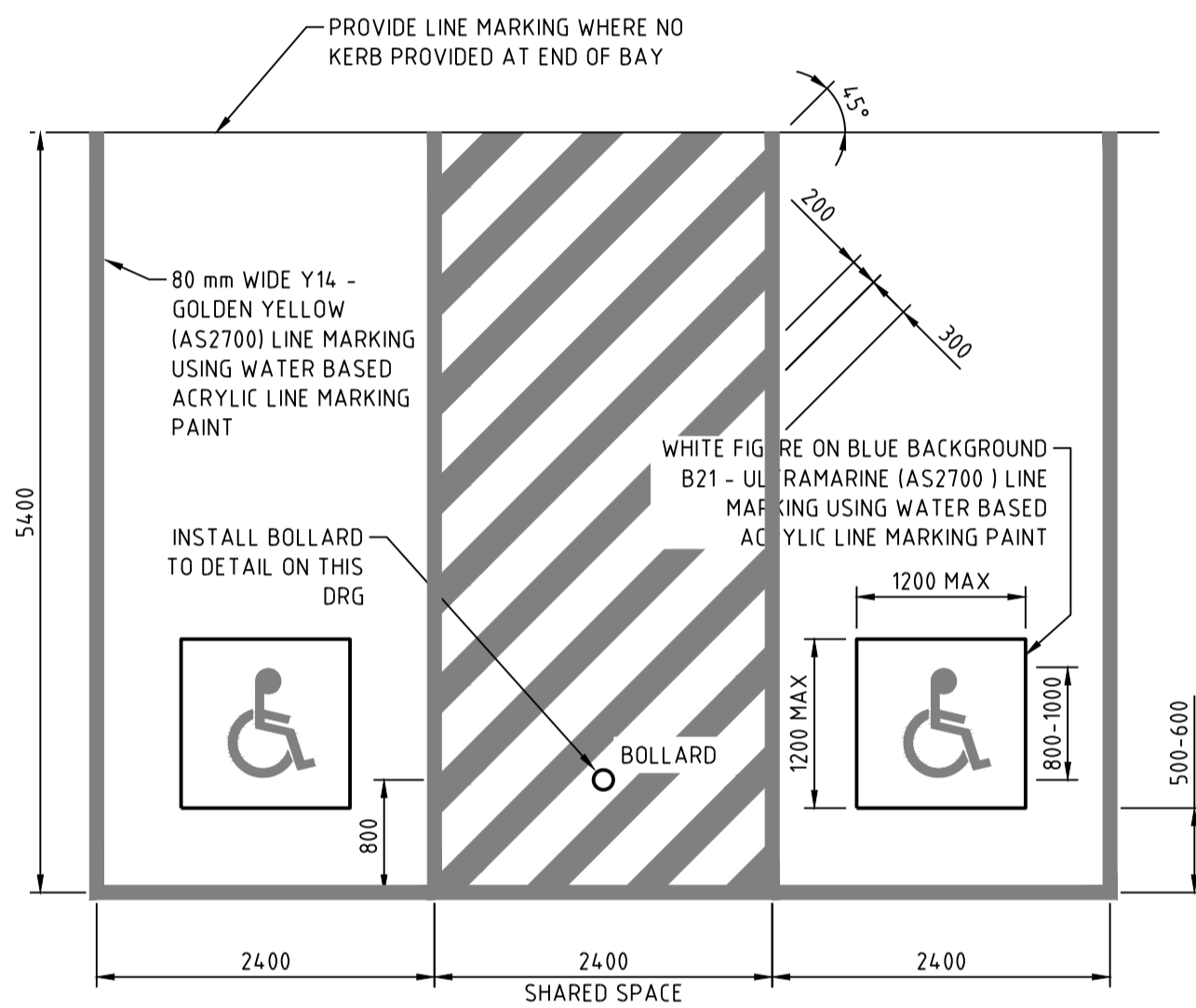
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- NOTE:
- THE SLIP RESISTANCE OF PEDESTRIAN CROSSING LINE MARKING SHALL BE A MINIMUM OF CLASS P4 (WET PENDULUM TEST) AS SPECIFIED IN AS/NZS 4586
 - SIGNAGE SHALL BE INSTALLED IN ACCORDANCE WITH AS 1742.10-2009

PEDESTRIAN CROSSING MARKINGS (AS2890.1)

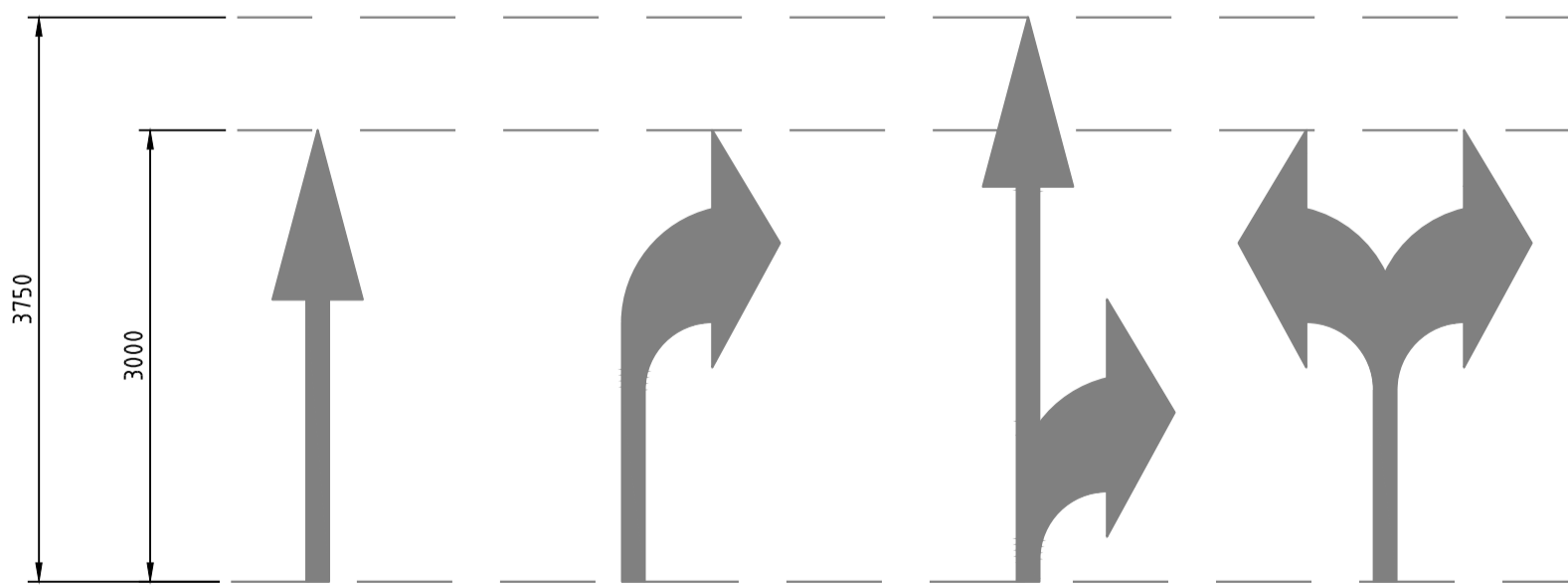
SCALE 150



- NOTE:
1. THE SLIP RESISTANCE OF DISABLED PARKING SPACE LINE MARKING SHALL BE A MINIMUM OF CLASS P4 (WET PENDULUM TEST) AS SPECIFIED IN AS/NZS 4586
 2. ASPHALT SURFACES SHALL BE FULLY CURED BEFORE APPLYING PAINT
 3. ALL WATER BASED ACRYLIC LINE MARKING PAINT SHALL BE APPLIED TO ACHIEVE 300 MICRON DFT.
 4. REAPPLY PAINT 4 WEEKS AFTER PRACTICAL COMPLETION IS REACHED IT SHALL BE ASSUMED THAT THE CAR PARK IS OPERATIONAL AND ALL ALLOWANCES NECESSARY TO UNDERTAKE THIS WORK SHALL BE INCLUDED IN THE CONTRACT SUM

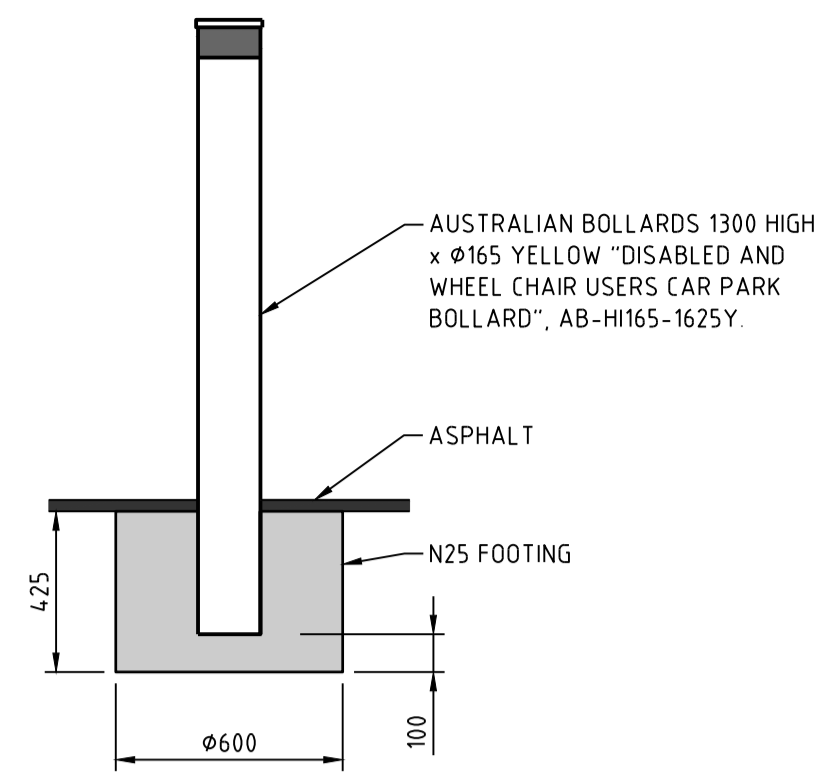
DISABLED PARKING SPACE LINE MARKING

SCALE 150



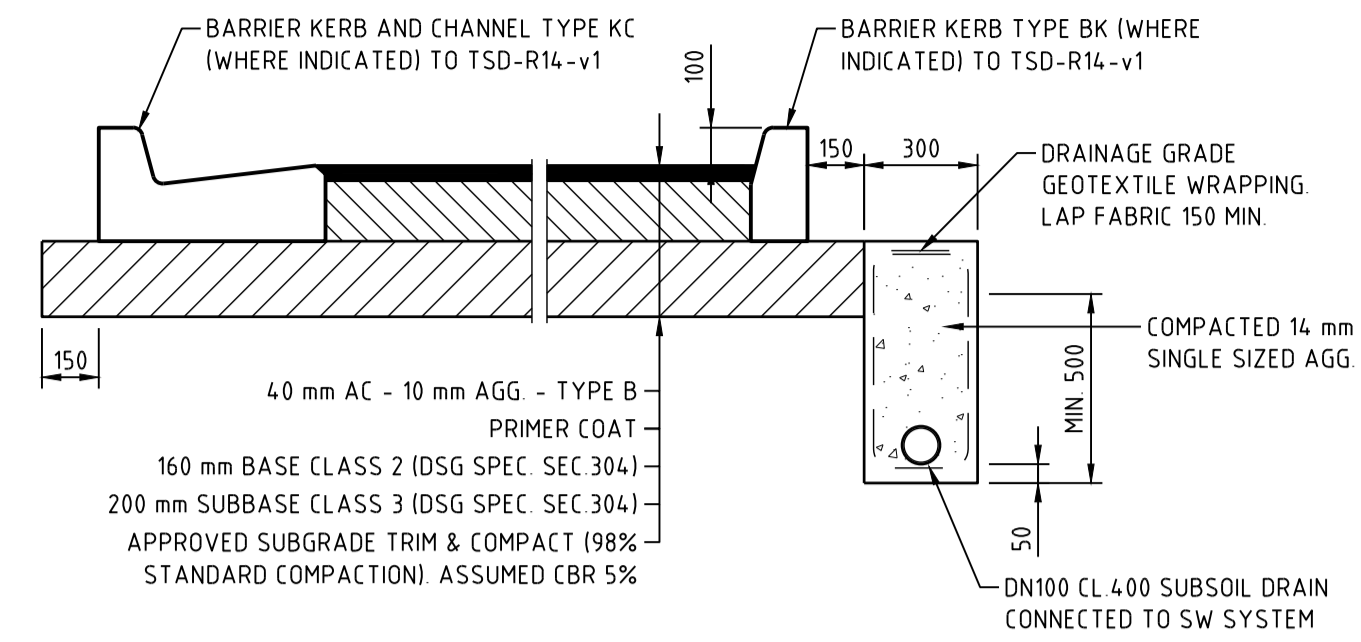
CAR PARK PAVEMENT ARROWS

SCALE 150



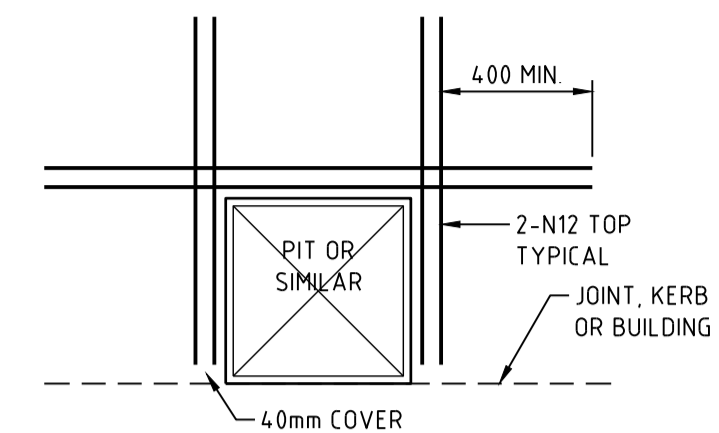
TYPICAL SHARED SPACE IN GROUND BOLLARD

SCALE 120



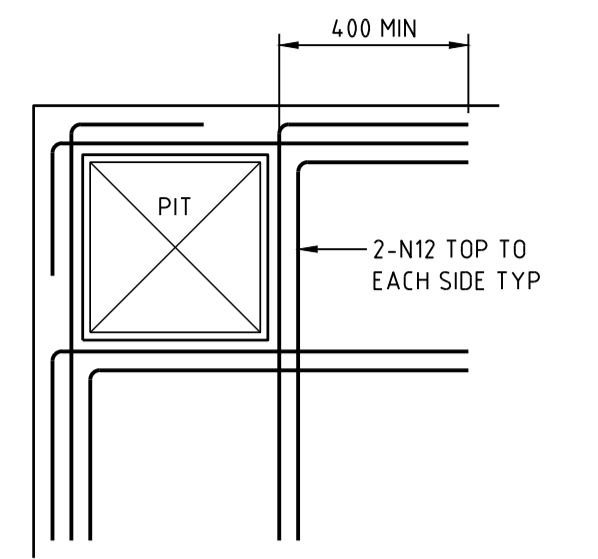
TYPICAL PAVEMENT DETAIL

SCALE 120



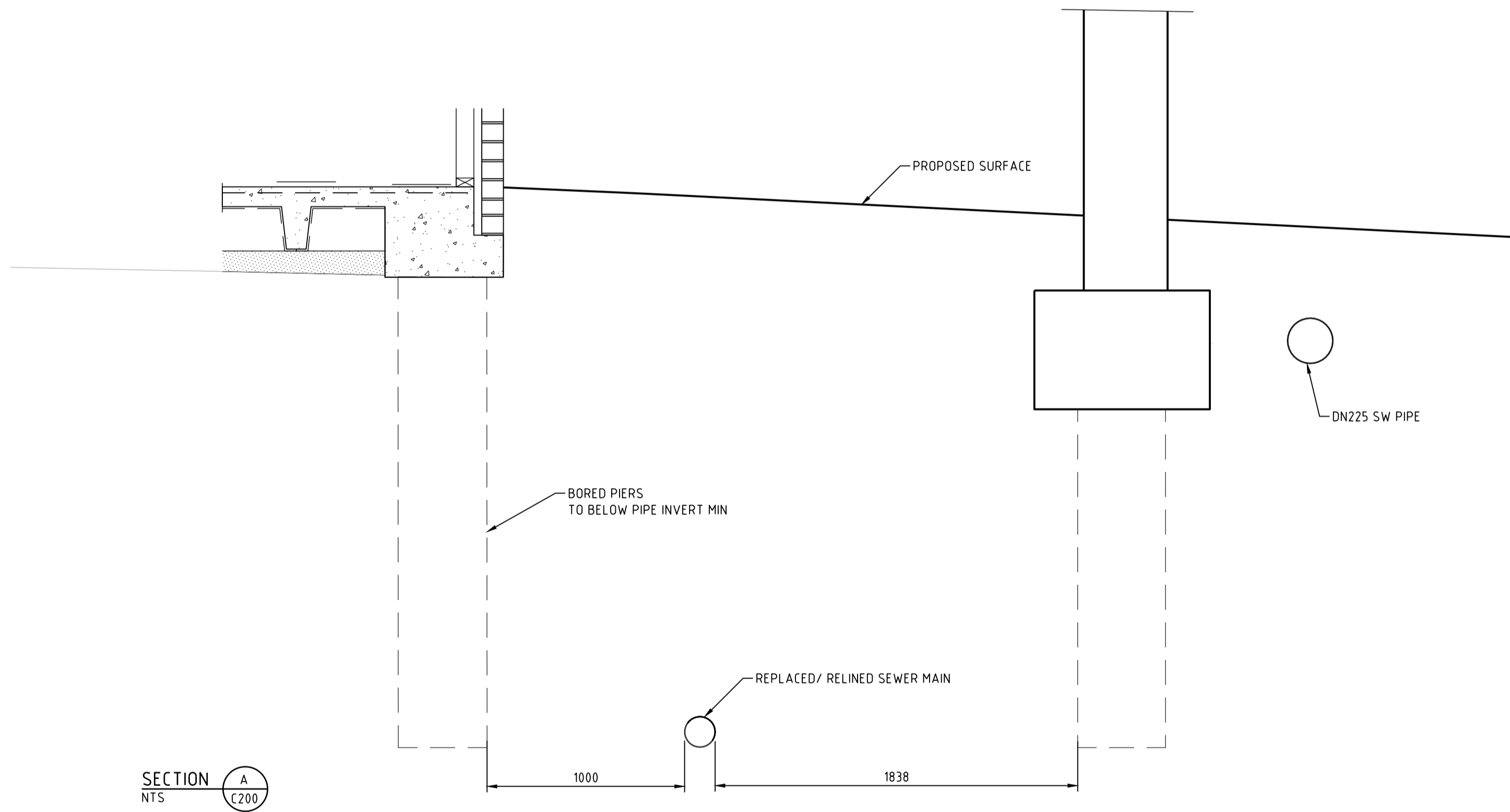
TYPICAL REINFORCEMENT TO PENETRATIONS

NTS



TYPICAL REINFORCEMENT TO PITS AT SLAB EDGE

NTS



SECTION A-C200
NTS

17/10/2025 12:14:36 PM S:\projects\1524\1524-03-DB-Development\1524-03-DB-Development\1524-03-DB-Development.dwg - GP - 24/05/2025 - Working drawings - Model\1524-03-DB-Development.dwg

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DEVONPORT MENTAL HEALTH HUB
 6-10 STEELE STREET
 TASMANIA 7310 DEVONPORT
 DRAWING TITLE
 DETAILS 01

DESIGNED DS		DRAWN DS		CHECKED DH	
PROJECT	24.0387	DRAWING	C700	REVISION	E

SCALE AS SHOWN @A1

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