



DEVELOPMENT APPLICATION

PDPLANPMTD-2025/051507

PROPOSAL: Boundary adjustment, partial demolition, retaining wall, works, & approval of an existing access to 46 Scotts Road.

LOCATION: 44 & 46 Scotts Road, Risdon Vale

RELEVANT PLANNING SCHEME: Tasmanian Planning Scheme - Clarence

ADVERTISING EXPIRY DATE: 06 October 2025

The relevant plans and documents can be inspected at the Council offices, 38 Bligh Street, Rosny Park, during normal office hours until 06 October 2025. In addition to legislative requirements, plans and documents can also be viewed at www.ccc.tas.gov.au during these times.

Any person may make representations about the application to the Chief Executive Officer, by writing to PO Box 96, Rosny Park, 7018 or by electronic mail to clarence@ccc.tas.gov.au. Representations must be received by Council on or before 06 October 2025.

To enable Council to contact you if necessary, would you please also include a day time contact number in any correspondence you may forward.

Any personal information submitted is covered by Council's privacy policy, available at www.ccc.tas.gov.au or at the Council offices.

Clarence City Council



APPLICATION FOR DEVELOPMENT / USE OR SUBDIVISION

The personal information on this form is required by Council for the development of land under the Land Use Planning and Approvals Act 1993. We will only use your personal information for this and other related purposes. If this information is not provided, we may not be able to deal with this matter. You may access and/or amend your personal information at any time. How we use this information is explained in our **Privacy Policy**, which is available at www.ccc.tas.gov.au or at Council offices.

Proposal:

Boundary adjustment (44 and 46 Scotts Road); Retrospective approval of an existing access to 46 Scotts Road; and Surface treatment and stormwater infrastructure for 44 and 46 Scotts Road.

Location:

Address 44 and 46 Scotts Road

Suburb/Town Risdon Vale TAS Postcode 7016

Current Owners/s:

Applicant:

Personal Information Removed

Tax Invoice for application fees to be in the name of (if different from applicant)

Estimated cost of development

\$ 87,000

Is the property on the Tasmanian Heritage Register?

Yes

No

(if yes, we recommend you discuss your proposal with Heritage Tasmania prior to lodgement as exemptions may apply which may save you time on your proposal)

If you had pre-application discussions with a Council Officer, please give their name

Current Use of Site: Storage (Contractor's Yard)

Does the proposal involve land administered or owned by the Crown or Council?

Yes

No

Declaration:

- *I have read the Certificate of Title and Schedule of Easements for the land and am satisfied that this application is not prevented by any restrictions, easements or covenants.*
- *I authorise the provision of a copy of any documents relating to this application to any person for the purposes of assessment or public consultation. I agree to arrange for the permission of the copyright owner of any part of this application to be obtained. I have arranged permission for Council's representatives to enter the land to assess this application*
- *I declare that, in accordance with Section 52 of the Land Use Planning and Approvals Act 1993, that I have notified the owner of the intention to make this application. Where the subject property is owned or controlled by Council or the Crown, their signed consent is attached. Where the application is submitted under Section 43A, the owner's consent is attached.*
- *I declare that the information in this declaration is true and correct.*

Acknowledgement:

- *I acknowledge that the documentation submitted in support of my application will become a public record held by Council and may be reproduced by Council in both electronic and hard copy format in order to facilitate the assessment process; for display purposes during public consultation; and to fulfil its statutory obligations. I further acknowledge that following determination of my application, Council will store documentation relating to my application in electronic format only.*

Applicant's
Signature:

Signature	Personal Information Removed	Date <u>28/3/2025</u>
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PLEASE REFER TO THE DEVELOPMENT/USE AND SUBDIVISION CHECKLIST ON THE FOLLOWING PAGES TO DETERMINE WHAT DOCUMENTATION MUST BE SUBMITTED WITH YOUR APPLICATION.



Documentation required:

1. **MANDATORY DOCUMENTATION**

This information is required for the application to be valid. An application lodged without these items is unable to proceed.

- Details of the location of the proposed use or development.
- A copy of the current Certificate of Title, Sealed Plan, Plan or Diagram and Schedule of Easements and other restrictions for each parcel of land on which the use or development is proposed.
- Full description of the proposed use or development.
- Description of the proposed operation.
May include where appropriate: staff/student/customer numbers; operating hours; truck movements; and loading/unloading requirements; waste generation and disposal; equipment used; pollution, including noise, fumes, smoke or vibration and mitigation/management measures.
- Declaration the owner has been notified if the applicant is not the owner.
- Crown or Council consent (if publically-owned land).
- Any reports, plans or other information required by the relevant zone or code.
- Fees prescribed by the Council.

Application fees (please phone 03 6217 9550 to determine what fees apply). An invoice will be emailed upon lodgement.

2. **ADDITIONAL DOCUMENTATION**

In addition to the mandatory information required above, Council may, to enable it to consider an application, request further information it considers necessary to ensure that the proposed use or development will comply with any relevant standards and purpose statements in the zone, codes or specific area plan, applicable to the use or development.

- Site analysis plan and site plan**, including where relevant:

- Existing and proposed use(s) on site.
- Boundaries and dimensions of the site.
- Topography, including contours showing AHD levels and major site features.
- Natural drainage lines, watercourses and wetlands on or adjacent to the site.
- Soil type.
- Vegetation types and distribution, and trees and vegetation to be removed.
- Location and capacity of any existing services or easements on/to the site.
- Existing pedestrian and vehicle access to the site.
- Location of existing and proposed buildings on the site.
- Location of existing adjoining properties, adjacent buildings and their uses.
- Any natural hazards that may affect use or development on the site.
- Proposed roads, driveways, car parking areas and footpaths within the site.
- Any proposed open space, communal space, or facilities on the site.
- Main utility service connection points and easements.
- Proposed subdivision lot boundaries.

Clarence City Council

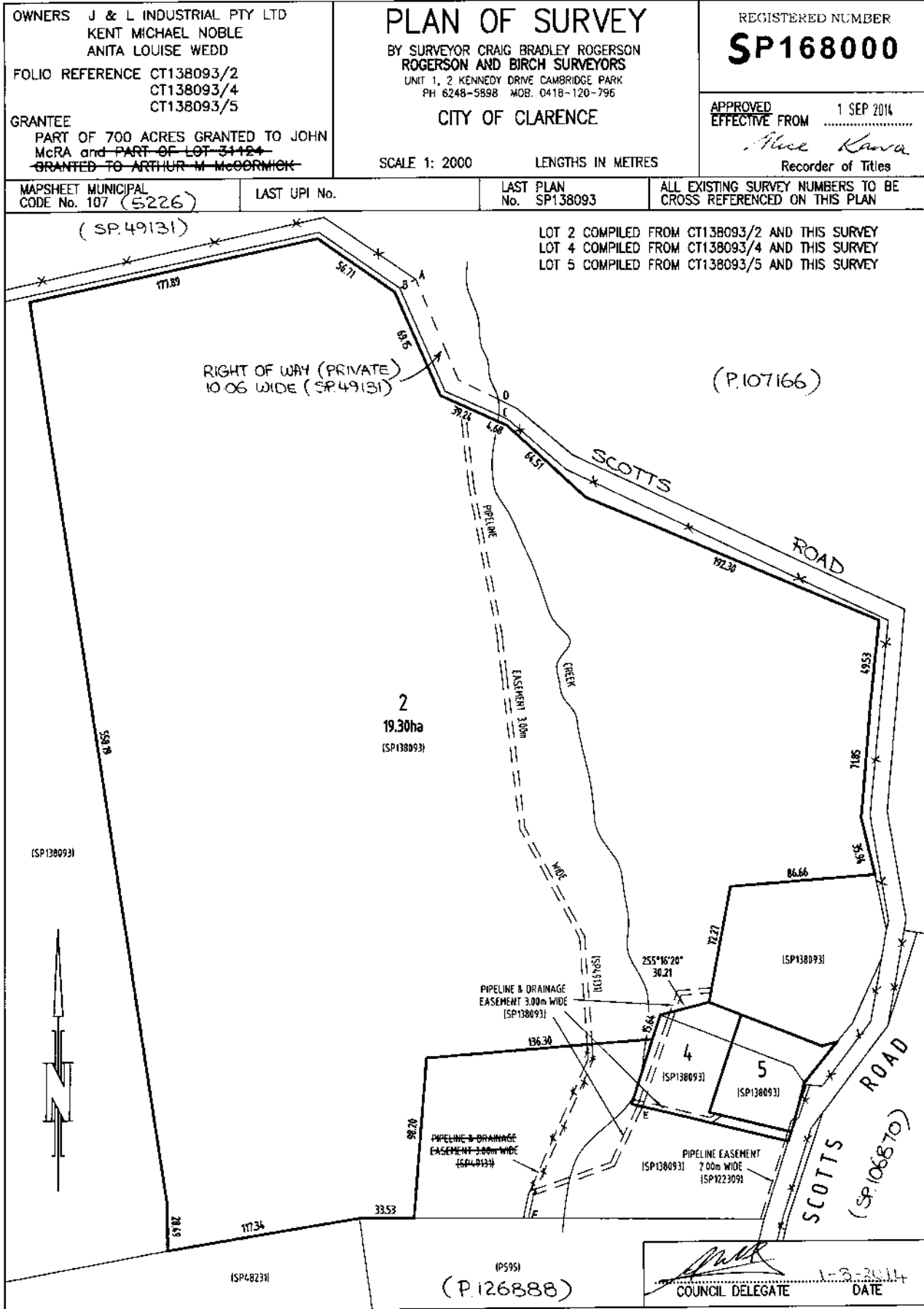
DEVELOPMENT/USE OR SUBDIVISION CHECKLIST



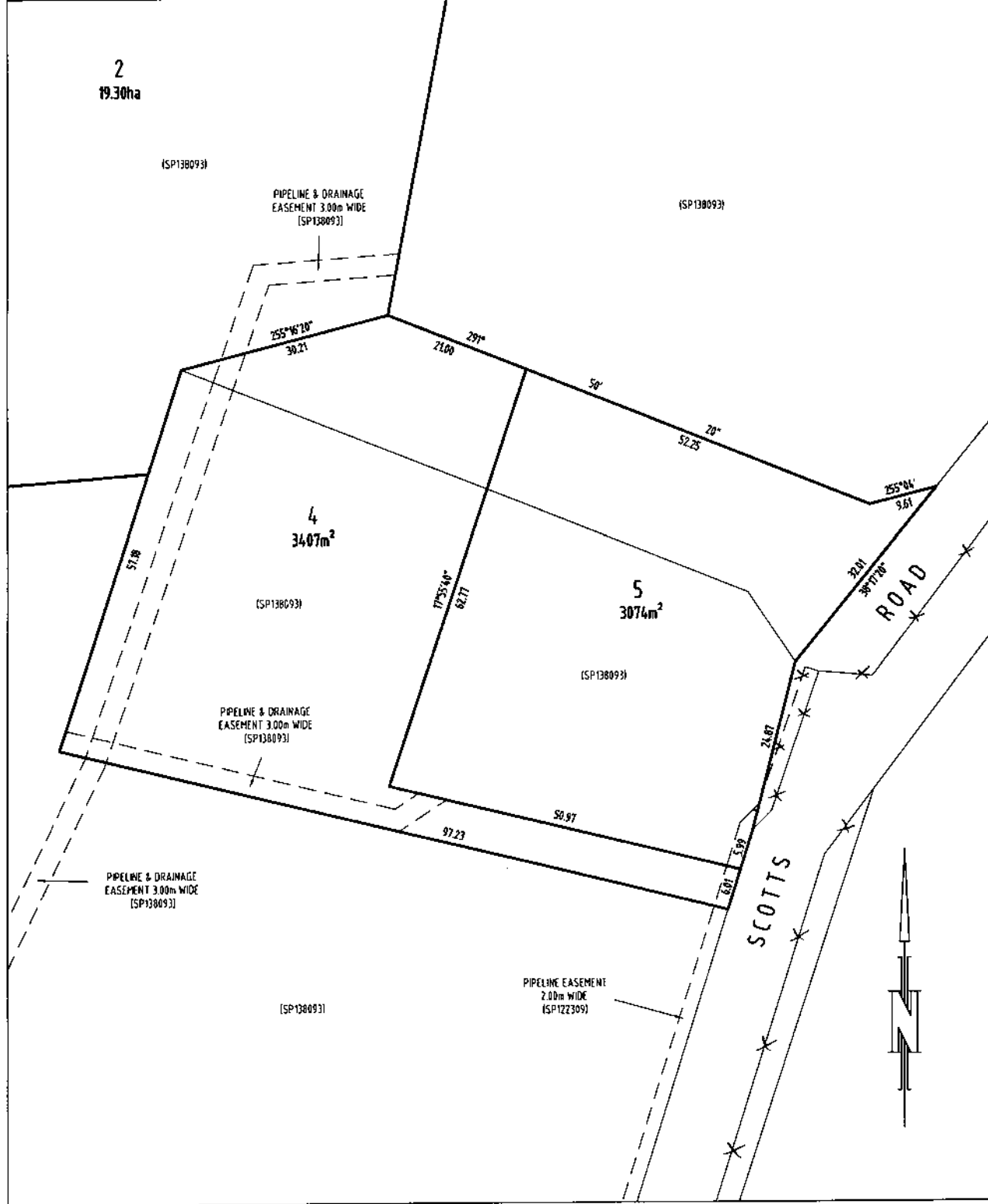
Clarence... a brighter place

- NA Where it is proposed to erect buildings, **detailed plans** with dimensions at a scale of 1:100 or 1:200 showing:
- *Internal layout of each building on the site.*
 - *Private open space for each dwelling.*
 - *External storage spaces.*
 - *Car parking space location and layout.*
 - *Major elevations of every building to be erected.*
 - *Shadow diagrams of the proposed buildings and adjacent structures demonstrating the extent of shading of adjacent private open spaces and external windows of buildings on adjacent sites.*
 - *Relationship of the elevations to natural ground level, showing any proposed cut or fill.*
 - *Materials and colours to be used on rooves and external walls.*
- NA Where it is proposed to erect buildings, a plan of the proposed **landscaping** showing:
- *Planting concepts.*
 - *Paving materials and drainage treatments and lighting for vehicle areas and footpaths.*
 - *Plantings proposed for screening from adjacent sites or public places.*
- Any additional reports, plans or other information required by the relevant zone or code.

This list is not comprehensive for all possible situations. If you require further information about what may be required as part of your application documentation, please contact Council's Planning Officers on (03) 6217 9550 who will be pleased to assist.



<p>PLAN OF SURVEY ANNEXURE SHEET SHEET 1 OF 1 SHEETS</p>	<p>OWNER J & L INDUSTRIAL PTY LTD, KENT MICHAEL NOBLE, ANITA LOUISE WEDD FOLIO REFERENCE CT138093/2, CT138093/4, CT138093/5 SCALE 1: 500 LENGTHS IN METRES</p>	<p>Registered Number SP 168000</p>
<p>SIGNED FOR IDENTIFICATION PURPOSES <i>[Signature]</i> Council Delegate 13-8-2014 Date</p>	<p>THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN THE SURVEYOR'S CERTIFICATE EXTENDS TO THE DETAILS ON THIS SHEET <i>[Signature]</i> Registered Land Surveyor 13/8/14 Date</p>	<p>APPROVED EFFECTIVE FROM 1-SEP-2014 <i>[Signature]</i> Recorder of Titles</p>



SEARCH OF TORRENS TITLE

VOLUME 168000	FOLIO 5
EDITION 5	DATE OF ISSUE 23-Jul-2020

SEARCH DATE : 21-Mar-2025

SEARCH TIME : 02.14 PM

DESCRIPTION OF LAND

City of CLARENCE

Lot 5 on Sealed Plan 168000

Derivation : Part of 700 Acres Granted to John McRa

Prior CTs 138093/5 and 138093/2

SCHEDULE 1

D160749 TRANSFER to KNAW PROPERTY INVESTMENTS PTY LTD

Registered 01-May-2015 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

SP168000 EASEMENTS in Schedule of Easements

SP168000 COVENANTS in Schedule of Easements

SP110495, SP122309, and SP138093 COVENANTS in Schedule of
Easements

SP138093 FENCING COVENANT in Schedule of Easements

30390 BOUNDARY FENCES CONDITION in Transfer

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 168000

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.
- (3) a Right of Drainage over the Drainage Easement in favour of the Clarence City Council and Tasmanian Water and Sewerage Corporation Pty Limited ("TasWater").

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

EASEMENTS Lot 5 on the Plan is together with a Pipeline and Drainage Easement over the Pipeline and Drainage Easement 3.00 wide shown passing through Lot 4 on the Plan.

1. Lot 2 is SUBJECT TO a Pipeline and Drainage Easement over the Pipeline and Drainage Easement 3.00m wide as appurtenant to Lot 1 on SP 49131 shown on the Plan.
2. Lot 2 is TOGETHER WITH a Right of Carriageway (appurtenant to the land formerly comprised in Lot 1 on D49246) over the Right of Way 10.06m wide marked ABCD on Diagram D49246 and as shown on the Plan.
3. Lots 2 and 4 are SUBJECT TO a Pipeline and Drainage Easement (appurtenant to Lot 6 on ~~the Plan~~ ^{Sealed Plan 138093}) over the Pipeline and Drainage Easement 3.00m wide shown on the Plan.
4. Lot 4 is SUBJECT TO a Pipeline and Drainage Easement (appurtenant to Lot 5 on the Plan) over the "Pipeline and Drainage Easement 3.00m wide" shown on the Plan.
5. ~~Those parts of Lots 4 and 5 formerly comprising Lot 1 on Sealed Plan 122309~~ are SUBJECT TO a Pipeline Easement (appurtenant to those parts of Lots 1, 2, 4, 5 and 6 ~~formerly comprising Lot 1 on Sealed Plan 123379~~) over the "Pipeline Easement 2.00m wide" shown on the Plan. ^{on Sealed Plan 138093}
6. Those parts of Lots 2, 4 and 5 formerly comprising ^{part of} Lot 1 on Sealed Plan 123379 are TOGETHER WITH a Pipeline Easement over ~~those parts of Lots 4 and 5 on the Plan~~ ^{the land} marked "Pipeline Easement 2.00 metres wide" shown on the Plan.
7. That part of ^{which formerly comprised Lot 4 on Sealed Plan 138093} Lot 4 on the Plan is ~~TOGETHER WITH a Pipeline and Drainage Easement over that part of Lot 3 on the Plan marked "Pipeline and Drainage Easement 3.00m wide"~~ ^{marked EF} shown on the Plan.
8. Lot 2 is SUBJECT TO a Pipeline Easement A (as hereinafter defined) over the Pipeline Easement 3.00 wide and Pipeline and Drainage Easement 3.00 wide passing through that Lot in favour of TasWater.

James Wedd (USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: J & L INDUSTRIAL PTY LTD (ACN 107 303 819), KENT MICHAEL NOBLE and ANITA LOUISE WEDD FOLIO REF: VOLUME 138093 FOLIOS 2, 4 AND 5 SOLICITOR & REFERENCE: MURDOCH CLARKE (RJB:1302608:ENS)	PLAN SEALED BY: CLARENCE CITY COUNCIL DATE: 1-8-2014 SD-2013/48 REF NO. <i>[Signature]</i> Council Delegate
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NOTE: The Council Delegate must sign the Certificate for the purposes of identification.

R.Q.XTE *x.w*

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 168000</p>
<p>SUBDIVIDER: J & L INDUSTRIAL PTY LTD (ACN 107 303 819), KENT MICHAEL NOBLE and ANITA LOUISE WEDD</p> <p>FOLIO REFERENCE: VOLUME 138093 FOLIOS 2, 4 and 5</p>	

9. Lot 2 is SUBJECT TO a Right of Drainage over the Pipeline Easement 3.00 wide and Pipeline and Drainage Easement 3.00 wide passing through that Lot in favour of Clarence City Council.
10. Lot 4 is SUBJECT TO a Pipeline Easement A (as hereinafter defined) over the Pipeline and Drainage Easement 3.00 wide shown passing through that Lot on the Plan hereon in favour of TasWater.
11. Lot 4 is SUBJECT TO a Right of Drainage over the Pipeline and Drainage Easement 3.00 wide and Pipeline Easement 2.00 wide passing through that Lot as shown on the Plan hereon in favour of the Clarence City Council.
12. Lot 5 is SUBJECT TO a Pipeline Easement A (as hereinafter defined) over the Pipeline Easement 2.00 wide passing through that Lot on the Plan hereon in favour of TasWater.
13. Lot 5 is SUBJECT TO a Right of Drainage over the Pipeline Easement 2.00 wide passing through that Lot as shown on the Plan hereon in favour of the Clarence City Council.

COVENANTS Those parts of Lots 4 and 5 on the Plan which formerly comprised part of Lot 1 on Sealed Plan 122309 are burdened by the restrictive covenant fully set forth in Sealed Plans 110495, 122309 and 138093, namely:

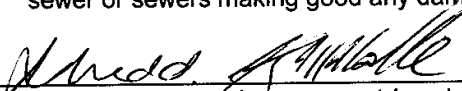
~~Lots 4 and 5 on the Plan are subject to the benefit and burden of the Covenant created in Sealed Plan 138093.~~

- Not to site any habitable dwelling on those parts of Lots 4 and 5 which formerly comprised part of Lot 1 on Sealed Plan 122309 unless it is capable of connection to a Council sewer.

INTERPRETATION

'PIPELINE AND DRAINAGE EASEMENT' means:

1. A full and free right for every person who is at any time entitled ^{to} any estate or interest in possession ~~in~~ the dominant tenement (called "the Dominant Tenement") with which the right shall be capable of enjoyment and every such person, surveyors, workmen and persons authorised by him from time to time and at all times hereafter if he or they should think fit to enter into an upon the lands shown as "~~Pipeline and Drainage~~ Easement 3.00m wide (SP 49131)" and "Pipeline and Drainage Easement 3.00m wide" on the Plan and there to construct, install, erect, repair, cleanse and amend any pump shed bore water mains for the purposes of carrying water over or under the said land to the Dominant Tenement or any part thereof and through all pipes which may hereafter be made or passing under through or along the said land without doing unnecessary damage to the said land;
2. A Right of Drainage over the "~~Pipeline and Drainage~~ Easement 3.00m wide (SP 49131)" and "Pipeline and Drainage Easement 3.00m wide" as shown on the Plan; and
3. A right for the owners for the time being of the Dominant Tenement to discharge sewerage and sullage there from through any sewer or sewers now existing or to be constructed in the strips of land marked "~~Pipeline and Drainage~~ Easement 3.00m wide (SP 49131)" and "Pipeline and Drainage Easement 3.00m wide" on the Plan and the right for the owner or owners and his of their surveyors and workmen from time to time and at all times hereafter if he or they should think fit to enter upon the said strip of land and to construct therein a new sewer or sewers in substitution for the existing sewer or sewers and to inspect, cause and maintain and amend any such existing or substituted sewer or sewers making good any damage done to the said strip of land.



R-Q-XIE
X.W.

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 168000</p>
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'PIPELINE EASEMENT' means the right for the owner of the Dominant Tenement to lay use and maintain forever pipes of such a size and number as shall from time to time be required for the purpose of carrying water over or under the "Pipeline Easement 2.00m wide" shown on the Plan and the right for his surveyors and workmen from time to time and at all times hereafter to enter into and upon the said Pipeline Easement or any part thereof bringing upon this said easement such materials, machinery and other things as he shall think fit and proper to inspect the condition of the said pipes and to repair, alter, amend and cleanse provided however that any damage occasions thereby shall be made good.

'INFRASTRUCTURE' means infrastructure owned or for which TasWater is responsible and includes but is not limited to:

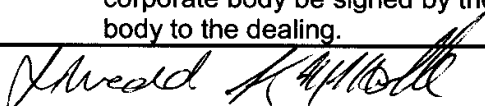
1. Sewer pipes and water pipes and associated valves;
2. Telemetry and monitoring devices;
3. Inspection and access pits;
4. Markers or signs indicating the location of the Easement Land, the Infrastructure or any warnings or restrictions with respect to the Easement Land or the Infrastructure;
5. Anything reasonably required to support, protect or cover any of the Infrastructure;
6. Any other infrastructure whether of a similar nature or not to the preceding which is reasonably required for the piping of sewage or water through the Easement Land or monitoring or managing that activity; and
7. Where the context permits, any part of the Infrastructure.

'RIGHT OF CARRIAGEWAY' means a full and free right for every person who is at any time entitled to an estate or interest in possession in the land herein indicated as the dominant tenement or any part thereof with which the right shall be capable of enjoyment, and every person authorised by him, to go, pass and repass at all times and for all purposes with or without animals or vehicles or both over that part of the strip of land shown on Sealed Plan 49131 marked 'Right of Way (Private) 10.06 wide' which is shown marked ABCD on the Plan annexed hereto and from the said dominant tenement or any such part thereof EXCEPT THAT notwithstanding the foregoing:

1. Such right shall not extend to members of the public purchasing goods or services from the Owner or Owners of the dominant tenement; and
2. Unless otherwise agreed to in writing by the Owner or Owners for the time being of the land herein indicated as the servient tenement, such right shall not include the use of vehicles requiring the driver thereof to hold a heavy rigid vehicle licence, a heavy combination vehicle licence, or a multi-combination vehicle licence as defined from time to time by the *Vehicle and Traffic (Driver Licensing and Vehicle Registration) Regulations 2000* or any regulations made in substitution therefore.

AND the Owner or Owners for the time being of the land herein indicated as the servient tenement covenant with the Owner or Owners for the time being of the land herein indicated as the dominant tenement to maintain the roadway passing through the said strip of land marked ABCD on the plan annexed hereto in good order repair and condition and to keep it unobstructed at all times (subject to the obligation of the Owner or Owners for the time being of the land herein indicated as the dominant tenement to pay one-tenth of the cost and expenses in relation to such upkeep and maintenance) AND the Owner or Owners for the time being of the land herein indicated as the dominant tenement covenant with the Owner or Owners for the time being of the land herein indicated as the servient tenement on demand a sum equal to one-tenth of the costs and expenses incurred by the Owner or Owners of the land herein indicated as the servient tenement in accordance with its obligation hereunder PROVIDED THAT notwithstanding the

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R. O. XIE
X.L

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 4 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 168000</p>
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foregoing the Owner or Owners for the time being of the land herein indicated as the dominant tenement shall be wholly liable for the cost of maintenance repair and restoration of the said roadway where such maintenance repair or restoration is necessitated solely as a consequence of the actions of the Owner or Owners of the said dominant tenement of the servants agents and invites thereof.

'PIPELINE EASEMENT A' means THE FULL RIGHT AND LIBERTY for TasWater at all times to:

1. Enter and remain upon the Easement Land with or without employees, contractors, agents and all other persons duly authorised by it and with or without machinery, vehicles, plant and equipment;
2. Investigate, take soil, rock and other samples, survey, open and break up and excavate the Easement Land for any purpose or activity that TasWater is authorised to do or undertake;
3. Install, retain, operate, modify, relocate, maintain, inspect, cleanse and repair the Infrastructure;
4. Remove and replace the Infrastructure;
5. Run and pass sewage and water through and along the Infrastructure;
6. Do all works reasonably required in connection with such activities or as may be authorised or required by any law:
 - (a) without doing unnecessary damage to the Easement Land; and
 - (b) leaving the Easement Land in a clean and tidy condition; and
7. If the Easement Land is not directly accessible from a highway, then for the purpose of undertaking any of the preceding activities TasWater may with or without employees, contractors, agents and all other persons authorised by it, and with or without machinery, vehicles, plant and equipment enter the Lot from the highway at any than existing vehicle entry and cross the Lot to the Easement Land; and
8. Use the Easement Land as a right of carriageway for the purpose of undertaking any of the preceding purposes on other land, TasWater reinstating any damage that it causes in doing so to any boundary fence of the Lot.

PROVIDED ALWAYS THAT:

9. The registered proprietors of the Lot in the folio of the Register ("the Owner") must not without the written consent of TasWater first had and obtained and only in compliance with any conditions which form the consent:
 - (a) alter, excavate, plough, drill or otherwise penetrate the ground level of the Easement Land;
 - (b) install, erect or plant any building, structure, fence, pit, well, footing, pipeline, paving, tree, shrub or other object on or in the Easement Land;
 - (c) remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land;
 - (d) do anything which will or might damage or contribute to damage to any of the Infrastructure on or in the Easement Land;
 - (e) in any way prevent or interfere with the proper exercise and benefit of the Easement Land by TasWater or its employees, contractors, agents and all other persons duly authorised by it; or
 - (f) permit or allow any action which the Owner must not do or acquiesce in that action.
10. TasWater is not required to fence any part of the Easement Land.
11. The Owner may erect a fence across the Easement Land at the boundaries of the Lot.
12. The Owner may erect a gate across any part of the Easement Land subject to these conditions:
 - (a) the Owner must provide TasWater with a key to any lock which would prevent the opening of the gate; and

J. Wedd *K. Noble* *R. Q. XTE* *x.w*

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 5 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 168000</p>
<p>SUBDIVIDER: J & L INDUSTRIAL PTY LTD (ACN 107 303 819), KENT MICHAEL NOBLE and ANITA LOUISE WEDD</p> <p>FOLIO REFERENCE: VOLUME 138093 FOLIOS 2, 4 and 5</p>	

- (b) if the Owner does not provide TasWater with that key or the key provided does not fit the lock, TasWater may cut the lock from the gate.
- 13. If the Owner causes damage to any of the Infrastructure, the Owner is liable for the actual cost to TasWater of the repair of the Infrastructure damaged.
- 14. If the Owner fails to comply with any of the preceding conditions, without forfeiting any right of action, damages or otherwise against the Owner, TasWater may:
 - (a) reinstate the ground level of the Easement Land; or
 - (b) remove from the Easement Land any building, structure, pit, well, footing, pipeline, paving, tree, shrub or other object; or
 - (c) replace anything that supported, protected or covered the Infrastructure.

EXECUTED by **J & L INDUSTRIAL PTY LTD**)
 (ACN 107 303 819) by authority of the Director)
 and Director/Secretary in accordance with Section)
 127 of the *Corporations Act 2001* and as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 138093 Folio)

SIGNATURE *x [Signature]*

FULL NAME: **WEI XIAO**

OFFICE HELD: **DIRECTOR AND SECRETARY**

SIGNATURE *x [Signature]*

FULL NAME: **RUIQUN XIE**

OFFICE HELD: **DIRECTOR**

SIGNED by **KENT MICHAEL NOBLE** as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 138093 Folio in the presence of:)

x [Signature]

WITNESS SIGNATURE *[Signature]*

WITNESS FULL NAME: **ROBERT REX WEDD**

WITNESS ADDRESS: **47 CORNWALL ST ROSE BAY 7015**

WITNESS OCCUPATION: **RETIRED**


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[Signatures] **R. Q. XIE** *x.w*

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 6 OF 6 PAGES</p>	<p>Registered Number</p> <p>SP 168000</p>
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SIGNED by ANITA LOUISE WEDD as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 138093 Folio in the presence of:)



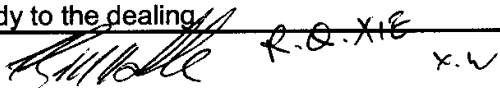
WITNESS SIGNATURE 

WITNESS FULL NAME ROBERT JOHN BRADENACH

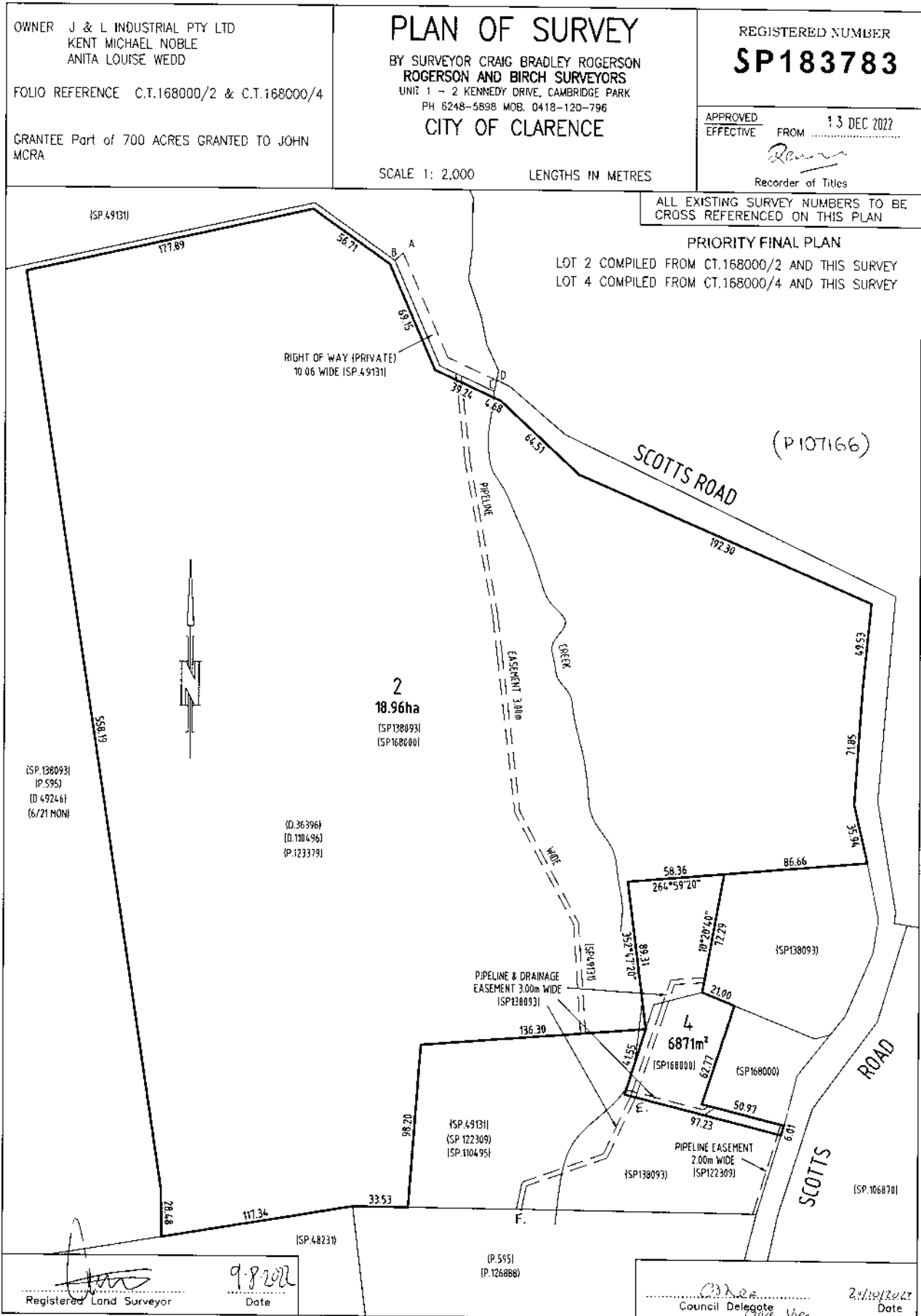
WITNESS ADDRESS 10 VICTORIA ST HOBART

WITNESS OCCUPATION SOLICITOR

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing



[Signature] R.O.X18 x.w



SEARCH OF TORRENS TITLE

VOLUME 183783	FOLIO 4
EDITION 1	DATE OF ISSUE 13-Dec-2022

SEARCH DATE : 21-Mar-2025

SEARCH TIME : 02.15 PM

DESCRIPTION OF LAND

City of CLARENCE
 Lot 4 on Sealed Plan 183783
 Derivation : Part of 700 Acres Gtd. to John McRa
 Prior CTs 168000/2 and 168000/4

SCHEDULE 1

C187513, D135861 & N106263 TRANSFER to KENT MICHAEL NOBLE and
 ANITA LOUISE WEDD Registered 13-Dec-2022 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP183783 EASEMENTS in Schedule of Easements
 SP183783 COVENANTS in Schedule of Easements
 SP110495, SP122309, SP138093 & SP168000 COVENANTS in Schedule
 of Easements
 SP138093 FENCING COVENANT in Schedule of Easements
 30390 BOUNDARY FENCES CONDITION in Transfer

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP 183783

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

EASEMENTS

Lot 2 is:-

- (a) Subject to a Pipeline and Drainage Easement over that part of the Lot marked "PIPELINE EASEMENT 3.00m WIDE" appurtenant to Lot 1 on SP49131.
- (b) Together with a Right of Carriageway (appurtenant to the land formerly comprised in Lot 1 on D49246) over the area marked "RIGHT OF WAY, 10.06m WIDE (SP49131)" marked ABCD on Diagram D49246 and as shown on the Plan. (PRIVATE)
- ~~(c) Subject to a Pipeline and Drainage Easement over that part of the Lot marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" appurtenant to Lot 6 on SP138093.~~
- (d) Together with a Pipeline Easement appurtenant to the that part of the Lot formerly comprising part of Lot 1 on Sealed Plan 123379 over the area marked "PIPELINE EASEMENT 2.00m WIDE (SP122309)" on the Plan.
- (e) Subject to a Pipeline Easement A (as hereinafter defined) over that part of the Lot marked "PIPELINE EASEMENT 3.00m WIDE" and ~~"PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)"~~ appurtenant to TasWater.
- (f) Subject to a Right of Drainage over that part of the Lot marked "PIPELINE EASEMENT 3.00m WIDE" and ~~"PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)"~~ appurtenant to Clarence City Council.

Lot 4 is:-

- (a) Subject to a Pipeline and Drainage Easement over that part of the Lot marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" appurtenant to Lot 5 on the SP168000.

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: J & L Industrial Pty Ltd (ACN 107 303 819) & Kent Michael Noble and Anita Louise Wedd FOLIO REF: 168000/2 & 4 SOLICITOR & REFERENCE: Murdoch Clarke (BRD 2100832)	PLAN SEALED BY: COUNCIL SEALING PLAN DATE: 24 th October 2022 SD-2021/023595 REF NO.C.Dhea..... Council Delegate Claire Shea
<p>NOTE: The Council Delegate must sign the Certificate for the purposes of identification.</p>	

Handwritten signatures and initials: J. Wedd, M. Clarke, C. Shea, etc.

<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 5 PAGES</p>	<p>Registered Number</p> <p>SP 183783</p>
<p>SUBDIVIDER: J & L Industrial Pty Ltd (ACN 107 303 819) & Kent Michael Noble and Anita Louise Wedd FOLIO REFERENCE: 168000/2 & 4</p>	

- (b) Subject to a Pipeline and Drainage Easement over that part of the Lot marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" appurtenant to Lot 6 on SP138093.
- (c) Subject to a Pipeline Easement over that part of the Lot marked "PIPELINE EASEMENT 2.00m WIDE (SP122309)" appurtenant to Lots 1, 2, 4, 5 and 6 on Sealed Plan ~~123379~~ ¹³⁸⁰⁹³, formerly comprising Lot 1 on Sealed Plan ~~138093~~ ¹²³³⁷⁹.
- (d) Together with a Pipeline Easement appurtenant to that part of the Lot formerly comprising part of Lot 1 on Sealed Plan 123379 over the area marked "PIPELINE EASEMENT 2.00m WIDE (SP122309)" on the Plan.
- (e) Together with a Pipeline and Drainage Easement appurtenant to that part of the Lot formerly comprising part of Lot 4 on Sealed Plan 138093 over the area marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)", on the Plan.
marked E.F.
- (f) Subject to a Pipeline Easement A (as hereinafter defined) over that part of the Lot marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" appurtenant to TasWater.
- (g) Subject to a Right of Drainage over that part of the Lot marked "PIPELINE EASEMENT 2.00m WIDE (SP122309)" and "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" appurtenant to Clarence City Council.

COVENANTS

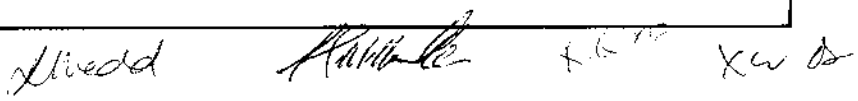
That part of Lot 4 on the Plan which formerly comprised part of Lot 1 on Sealed Plan 122309 ^{is} ~~are~~ burdened by the Restrictive Covenant fully set forth in Sealed Plans 110495, 122309 and 138093, namely:
that
"Not to site any habitable dwelling on ~~those parts~~ ^{is} of Lot 4 ~~and 5~~ which formerly comprised part of Lot 1 on Sealed Plan 122309 unless it is capable of connection to a Council sewer."

INTERPRETATION

"PIPELINE AND DRAINAGE EASEMENT" means:-

1. A full and free right for every person who is at any time entitled to any estate or interest in possession in the dominant tenement (called "the Dominant Tenement") with which the right shall be capable of enjoyment and every such person, surveyors, workmen and persons authorised by him from time to time and at all times hereafter if he or they should think fit to enter into an upon the lands shown as "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" and "PIPELINE EASEMENT 3.00m WIDE" on the Plan and there to construct, install, erect, repair, cleanse and amend any pump shed bore water mains for the purposes of carrying water over or under the said land to the Dominant Tenement or any part thereof and through all pipes which may hereafter be made or passing under through or along the said land without doing unnecessary damage to the said land;
2. A Right of Drainage over the areas marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" and "PIPELINE EASEMENT 3.00m WIDE" on the Plan; and

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 5 PAGES</p>	<p>Registered Number</p> <p>SP 183783</p>
<p>SUBDIVIDER: J & L Industrial Pty Ltd (ACN 107 303 819) & Kent Michael Noble and Anita Louise Wedd FOLIO REFERENCE: 168000/2 & 4</p>	

3. A right for the owners for the time being of the Dominant Tenement to discharge sewerage and sullage there from through any sewer or sewers now existing or to be constructed over the areas marked "PIPELINE & DRAINAGE EASEMENT 3.00m WIDE (SP138093)" and "PIPELINE EASEMENT 3.00m WIDE" on the Plan and the right for the owner or owners and his or their surveyors and workmen from time to time and at all times hereafter if he or they should think fit to enter upon the said strip of land and to construct therein a new sewer or sewers in substitution for the existing sewer or sewers and to inspect, cause and maintain and amend any such exiting or substituted sewer or sewers making good any damage done to the said strip of land.

"PIPELINE EASEMENT" means the right for the owner of the Dominant Tenement to lay use and maintain forever pipes of such a size and number as shall from time to time be required for the purpose of carrying water over or under the area marked "PIPELINE EASEMENT 2.00m WIDE (SP122309)" on the Plan and the right for his surveyors and workmen from time to time and at all times hereafter to enter into and upon the said Pipeline Easement or any part thereof bringing upon this said easement such materials, machinery and other things as he shall think fit and proper to inspect the condition of the said pipes and to repair, alter, amend and cleanse provided however that any damage occasions thereby shall be made good.

"RIGHT OF CARRIAGEWAY" means a full and free right for every person who is at any time entitled to an estate or interest in possession in the land herein indicated as the dominant tenement or any part thereof with which the right shall be capable of enjoyment, and every person authorised by him, to go, pass and repass at all times and for all purposes with or without animals or vehicles or both over that part of the strip of land shown on the "Sealed Plan 49131 marked 'Right of Way (Private) 10.06 wide" which is shown marked ABCD on the Plan and from the said dominant tenement or any such part thereof EXCEPT THAT notwithstanding the foregoing:

1. Such right shall not extend to members of the public purchasing goods or services from the Owner or Owners of the dominant tenement; and
2. Unless otherwise agreed to in writing by the Owner or Owners for the time being of the land herein indicated as the servient tenement, such right shall not include the use of vehicles requiring the driver thereof the hold a heavy rigid vehicle licence, a heavy combination vehicle licence, or a multi-combination vehicle licence as defined from time to time by the *Vehicle and Traffic (Driver Licensing and Vehicle Registration) Regulations 2000* or any regulations made in substitution therefore.

AND the Owner or Owners for the time being of the land herein indicated as the servient tenement covenant with the Owner or Owners for the time being of the land herein indicated as the dominant tenement to maintain the roadway passing through the said strip of land marked ABCD on the plan annexed hereto in good order repair and condition and to keep it unobstructed at all times (subject to the obligation of the Owner or Owners for the time being of the land herein indicated as the dominant tenement to pay one-tenth of the cost and expenses in relation to such upkeep and maintenance) AND the Owner or Owners for the time being of the land herein indicated as the dominant tenement covenant with the Owner or Owners for the time being of the land herein indicated as the servient tenement on demand a sum equal to one-tenth of the costs and expenses incurred by the Owner or Owners of the land herein indicated as the servient tenement in accordance with its obligation hereunder PROVIDED THAT notwithstanding the foregoing the Owner or Owners for the time being of the land herein indicated as the dominant tenement shall be wholly liable for the cost of maintenance repair and restoration of the said roadway where such maintenance repair or restoration is necessitated solely as a consequence of the actions of the Owner or Owners of the said dominant tenement of the servants agents and invites thereof.

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<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 4 OF 5 PAGES</p>	<p>Registered Number</p> <p>SP, 183783</p>
<p>SUBDIVIDER: J & L Industrial Pty Ltd (ACN 107 303 819) & Kent Michael Noble and Anita Louise Wedd FOLIO REFERENCE: 168000/2 & 4</p>	

“PIPELINE EASEMENT A” is defined as follows:-

FIRSTLY, THE FULL AND FREE RIGHT AND LIBERTY for TasWater and its employees, contractors, agents and all other persons duly authorised by it, at all times to:

- (1) enter and remain upon the Easement Land with or without machinery, vehicles, plant and equipment;
- (2) investigate, take soil, rock and other samples, survey, open and break up and excavate the Easement Land for any purpose or activity that TasWater is authorised to do or undertake;
- (3) install, retain, operate, modify, relocate, maintain, inspect, cleanse, repair, remove and replace the Infrastructure;
- (4) run and pass sewage, water and electricity through and along the Infrastructure;
- (5) do all works reasonably required in connection with such activities or as may be authorised or required by any law:
 - (a) without doing unnecessary damage to the Easement Land; and
 - (b) leaving the Easement Land in a clean and tidy condition;
- (6) if the Easement Land is not directly accessible from a highway, then for the purpose of undertaking any of the preceding activities TasWater may with or without employees, contractors, agents and any other persons authorised by it, and with or without machinery, vehicles, plant and equipment enter the Lot from the highway at any vehicle entry and cross the Lot to the Easement Land; and
- (7) use the Easement Land as a right of carriageway for the purpose of undertaking any of the preceding purposes on other land, TasWater reinstating any damage that it causes in doing so to any boundary fence of the Lot.

SECONDLY, the benefit of a covenant in gross for TasWater with the registered proprietor/s of the Easement Land and their successors and assigns not to erect any building, or place any structures, objects, vegetation, or remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land, without the prior written consent of TasWater to the intent that the burden of the covenant may run with and bind the servient land and every part thereof and that the benefit thereof may be annexed to the easement herein described.

Interpretation:

“Infrastructure” means infrastructure owned or for which TasWater is responsible and includes but is not limited to:

- (a) sewer pipes and water pipes and associated valves;
- (b) telemetry and monitoring devices;
- (c) inspection and access pits;
- (d) electricity assets and other conducting media (excluding telemetry and monitoring devices);
- (e) markers or signs indicating the location of the Easement Land or any other Infrastructure or
- (f) any warnings or restrictions with respect to the Easement Land or any other Infrastructure;
- (g) anything reasonably required to support, protect or cover any other Infrastructure;
- (h) any other infrastructure whether of a similar nature or not to the preceding which is
- (i) reasonably required for the piping of sewage or water, or the running of electricity, through
- (j) the Easement Land or monitoring or managing that activity; and
- (k) where the context permits, any part of the Infrastructure.

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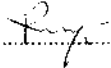
Wedd Noble Rennie Ken Or

ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 5 OF 5 PAGES	Registered Number SP 183783
SUBDIVIDER: J & L Industrial Pty Ltd (ACN 107 303 819) & Kent Michael Noble and Anita Louise Wedd FOLIO REFERENCE: 168000/2 & 4	

"TasWater" means Tasmanian Water & Sewerage Corporation Pty Ltd (ACN 162 220 653), its successors and assigns.

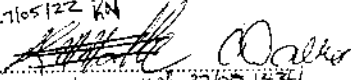
EXECUTED by **J & L INDUSTRIAL PTY LTD**)
 (ACN 107 303 819) by authority of the Director)
 and Director/Secretary in accordance with Section)
 127 of the *Corporations Act 2001* and as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 168000 Folio 2)

SIGNATURE .....
 FULL NAME: **WEI XIAO**
 OFFICE HELD: **DIRECTOR AND SECRETARY**

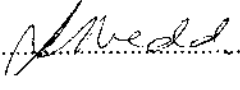
SIGNATURE .....
 FULL NAME: **RUIQUN XIE**
 OFFICE HELD: **DIRECTOR**

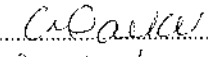
SIGNED by **KENT MICHAEL NOBLE** as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 168000 Folio 4 in the presence of:)

.....

WITNESS SIGNATURE .....
 WITNESS FULL NAME **Kent Michael Noble**
 WITNESS ADDRESS **25 Esplanade**
Midway Point

SIGNED by **ANITA LOUISE WEDD** as registered)
 Proprietor of the land comprised in Folio of the)
 Register Volume 168000 Folio 4 in the presence of:)

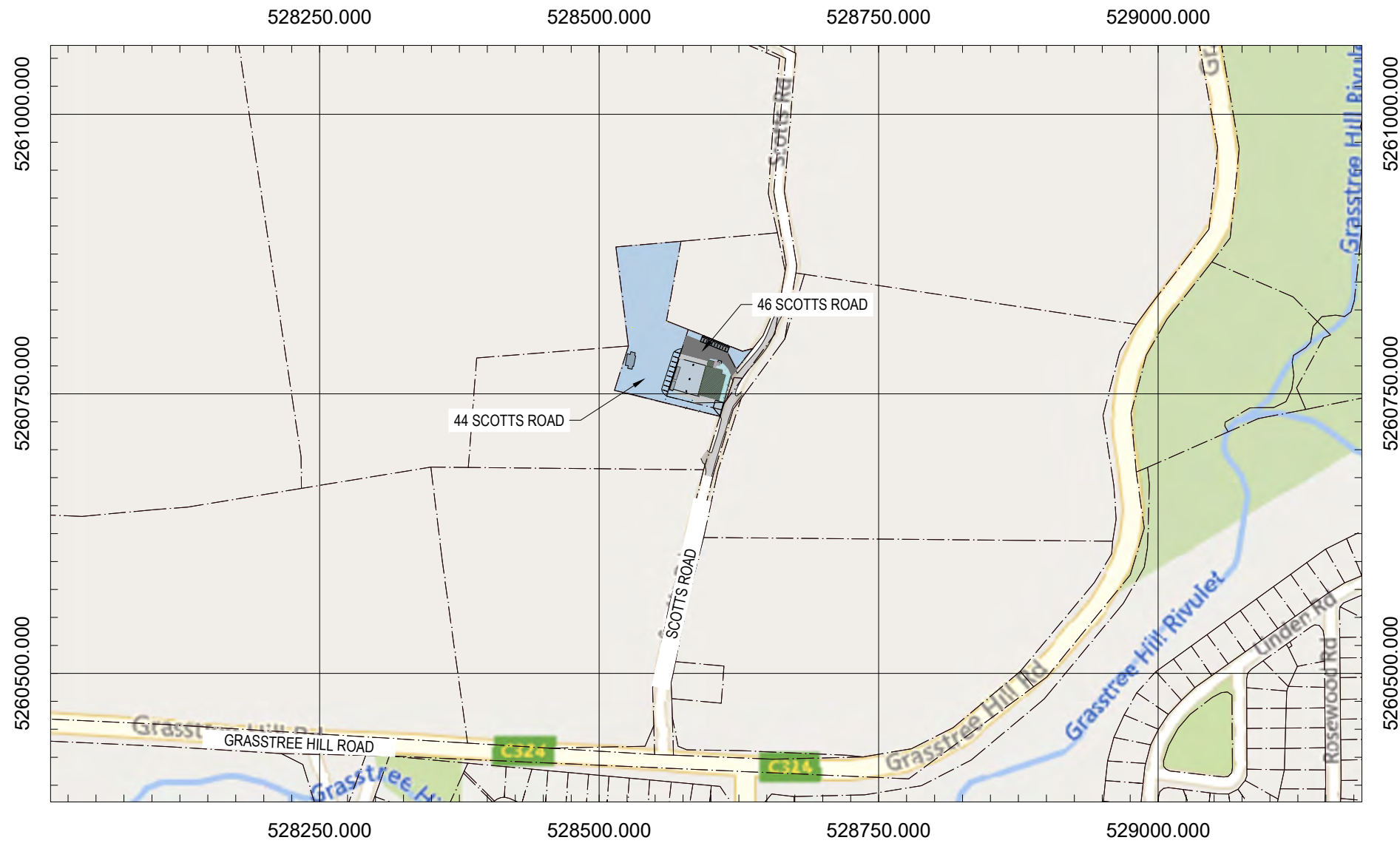
.....

WITNESS SIGNATURE .....
 WITNESS FULL NAME **Cindy Lee Walker**
 WITNESS ADDRESS **25 Esplanade**
Midway Point

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

R. G. XIE Xu CD

SITE ANALYSIS AND SITE PLANS FOR PROPOSED BOUNDARY ADJUSTMENT AND SITE ACCESS (RETROSPECTIVE)



LOCALITY MAP
SCALE 1:5000

DRAWING INDEX

	DRAWING NO.	REV.	DESCRIPTION
1	1008/100	E	COVER SHEET
2	1008/101	B	EXISTING SITE LAYOUT
3	1008/102	C	SITE DIMENSIONS AND SETBACKS
4	1008/103	B	EXISTING SERVICES AND EASEMENTS
5	1008/104	A	ADJOINING PROPERTIES AND USES
6	1008/105	A	CODE OVERLAY MAPS (TPS)
7	1008/200	B	PROPOSED BONDARY ADJUSTMENT
8	1008/201	D	PROPOSED DEVELOPMENT WORKS ON 44 & 46 SCOTTS ROAD
9	1008/202	E	STORMWATER MANAGEMENT PLAN
10	1008/203	B	STORMWATER LONGSECTION
11	1008/300	A	SWEPT PATH ANALYSIS - RIGID TRUCK
12	1008/301	A	SWEPT PATH ANALYSIS - PRIME MOVER AND SEMI-TRAILER
13	1008/302	B	PARKING BAY DESIGN
14	1008/400	A	PROPOSED STORAGE BINS EXTENSION
15	2640-03	-	PREPARED BY 'THE PRECASTERS P/L'
16	2640-11	-	PREPARED BY 'THE PRECASTERS P/L'

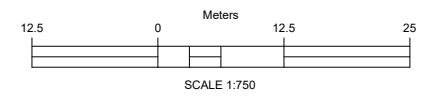
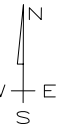
E	STORAGE BINS EXTENSION UPDATED	09/09/2025	CLIENT: NOBLE CONCRETE PUMPING PTY LTD
D	MINOR ADJUSTMENTS	17/07/2025	ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017
C	STORMWATER LONGSECTION UPDATED	19/06/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
B	MINOR ADJUSTMENT	25/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)

COVER SHEET

DRAWING NUMBER: 1008/100

A3





LEGEND AND DEFINITIONS

- GRASSED AREA
- TITLE BOUNDARIES
- FENCELINE
- MAJOR CONTOURS (5m)
- MINOR CONTOURS (1m)
- WATERCOURSE
- EXISTING CULVERT (COUNCIL)

- Ⓐ THE SHED
- Ⓑ CONTAINER STORAGE
- Ⓒ FUEL STORAGE
- Ⓓ SITE OFFICES
- Ⓔ LIGHT VEHICLE PARKING SLOTS
- Ⓕ STORMWATER ENTRY PITS



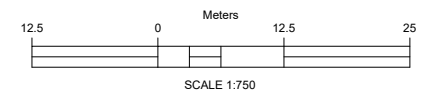
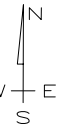
			CLIENT: NOBLE CONCRETE PUMPING PTY LTD
			ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017
B	COUNCIL CULVERT UPDATED	17/07/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	01/02/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)

**EXISTING SITE LAYOUT
(44 SCOTT'S RD AND 46 SCOTT'S RD)**

DRAWING NUMBER: 1008/101

A3

an Diemen CONSULTING

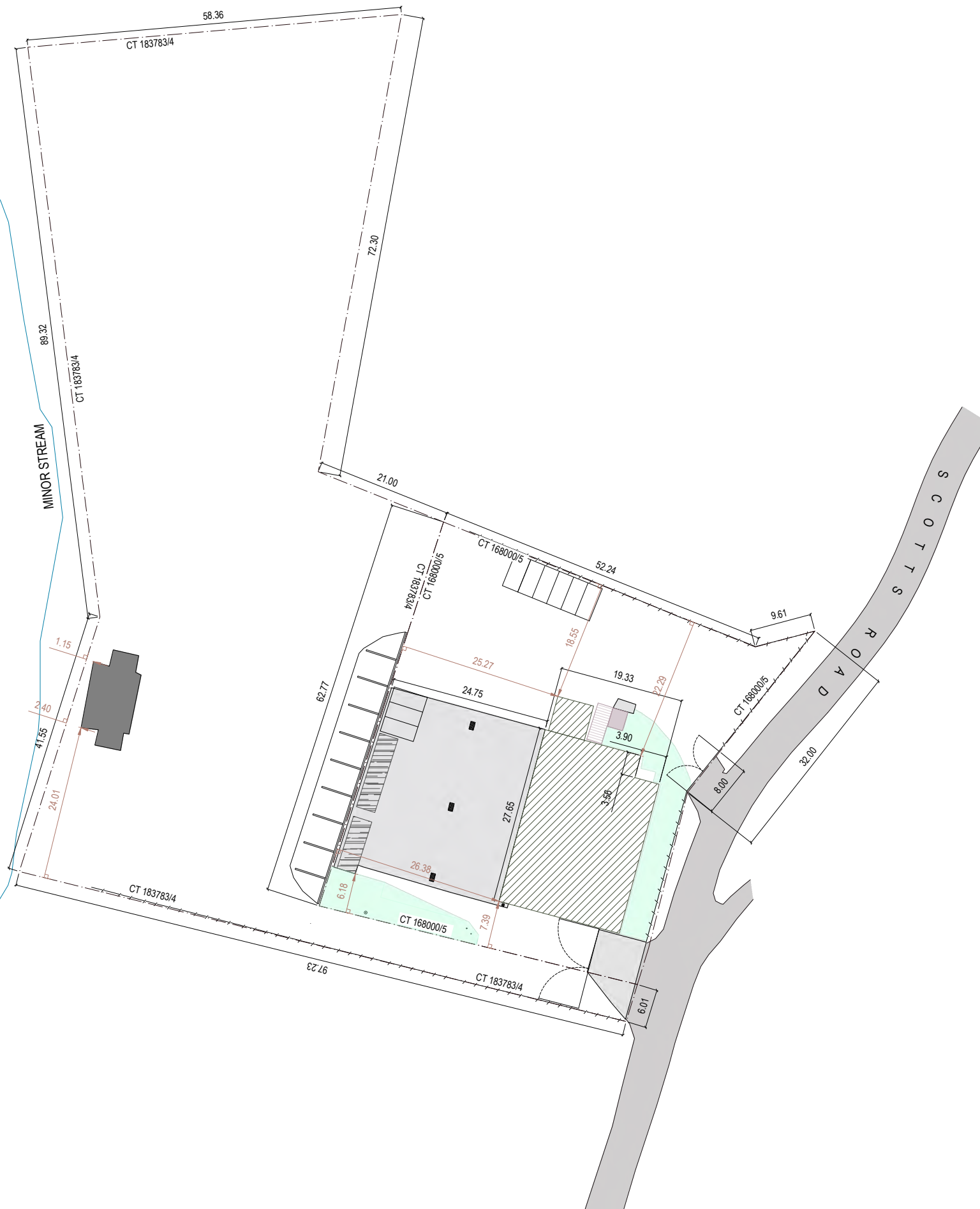


- PROPERTY SIZES (EXISTING)**
- 44 SCOTTS RD, RISDON VALE - 6871 m²
 - 46 SCOTTS RD, RISDON VALE - 3074 m²

LEGEND AND DEFINITIONS

- TITLE BOUNDARIES
- FENCELINE
- WATERCOURSE
- PERPENDICULAR SETBACKS

NOTE
NO STORMWATER STRUCTURES ARE SHOWN FOR THE CLARITY.

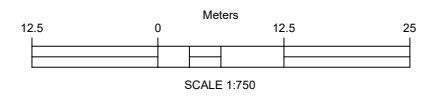
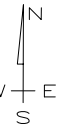


			CLIENT: NOBLE CONCRETE PUMPING PTY LTD
C	MINOR ADJUSTMENTS	17/07/2025	ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017
B	MINOR ADJUSTMENTS	25/03/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	01/02/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)

SITE DIMENSIONS AND SETBACKS

DRAWING NUMBER: 1008/102

A3



LEGEND AND DEFINITIONS

- TITLE BOUNDARIES
- FENCELINE
- WATERCOURSE
- WATER MAIN (AS PER DBYD)
- STORMWATER PIPE (AS PER DBYD)
- SEWER PIPE (AS PER DBYD)
- STORMWATER FLOW DIRECTION
- EASEMENT
- EXISTING CULVERT (COUNCIL)



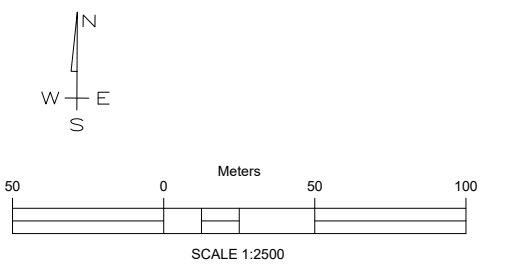
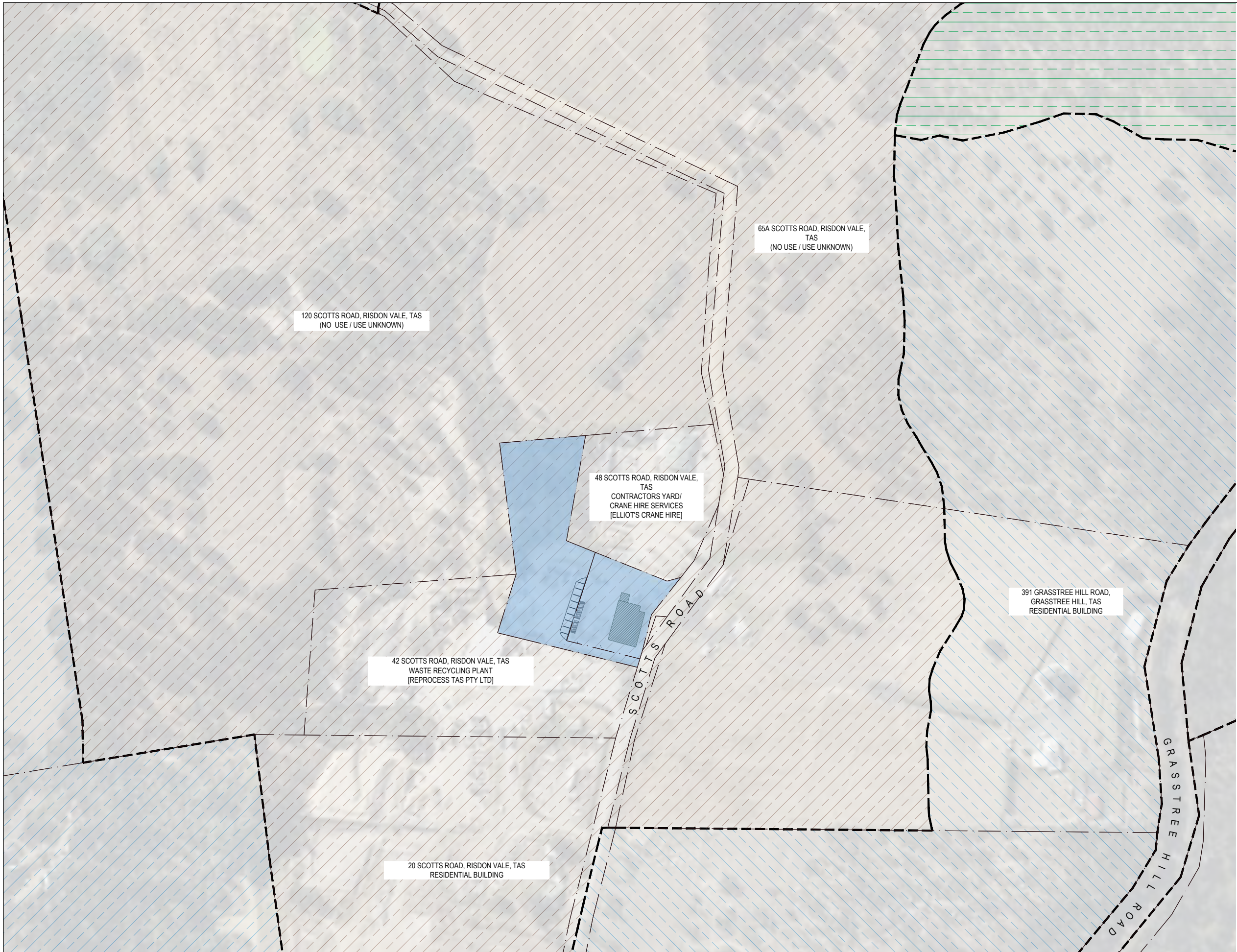
NOTES
 LOCATIONS OF THE UNDERGROUND SERVICES ARE INDICATIVE ONLY. WHERE REQUIRED, THE SERVICE LOCATIONS MUST BE IDENTIFIED THROUGH A CERTIFIED SERVICE LOCATOR.

		CLIENT: NOBLE CONCRETE PUMPING PTY LTD	
		ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017	
B	COUNCIL CULVERT UPDATED	17/07/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	01/02/2025	ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY		PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)	

EXISTING SERVICES AND EASEMENTS

DRAWING NUMBER: 1008/103

A3



- LEGEND**
- THE SITE
(44 SCOTT'S RD & 46 SCOTT'S RD)
 - RURAL ZONE
 - LIGHT INDUSTRIAL ZONE
 - LANDSCAPE CONSERVATION ZONE
 - TITLE BOUNDARIES
 - ZONE DELINEATION

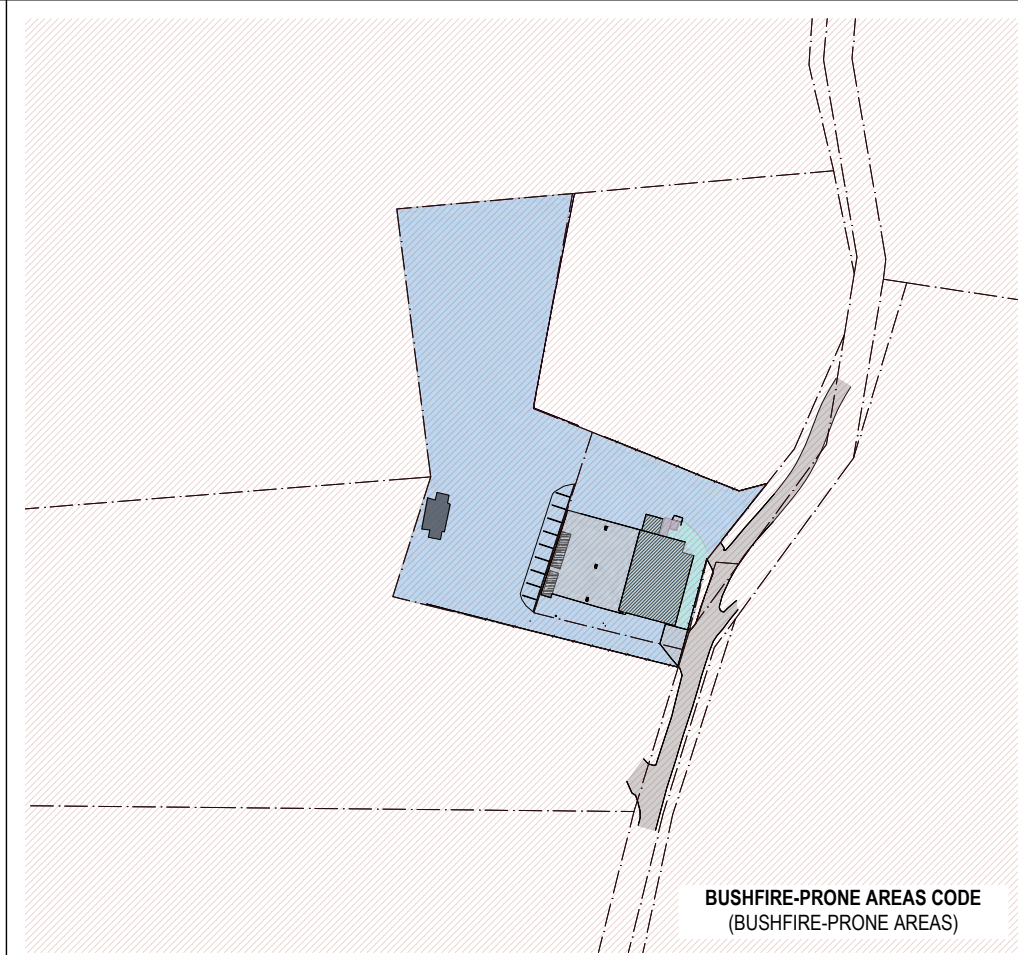
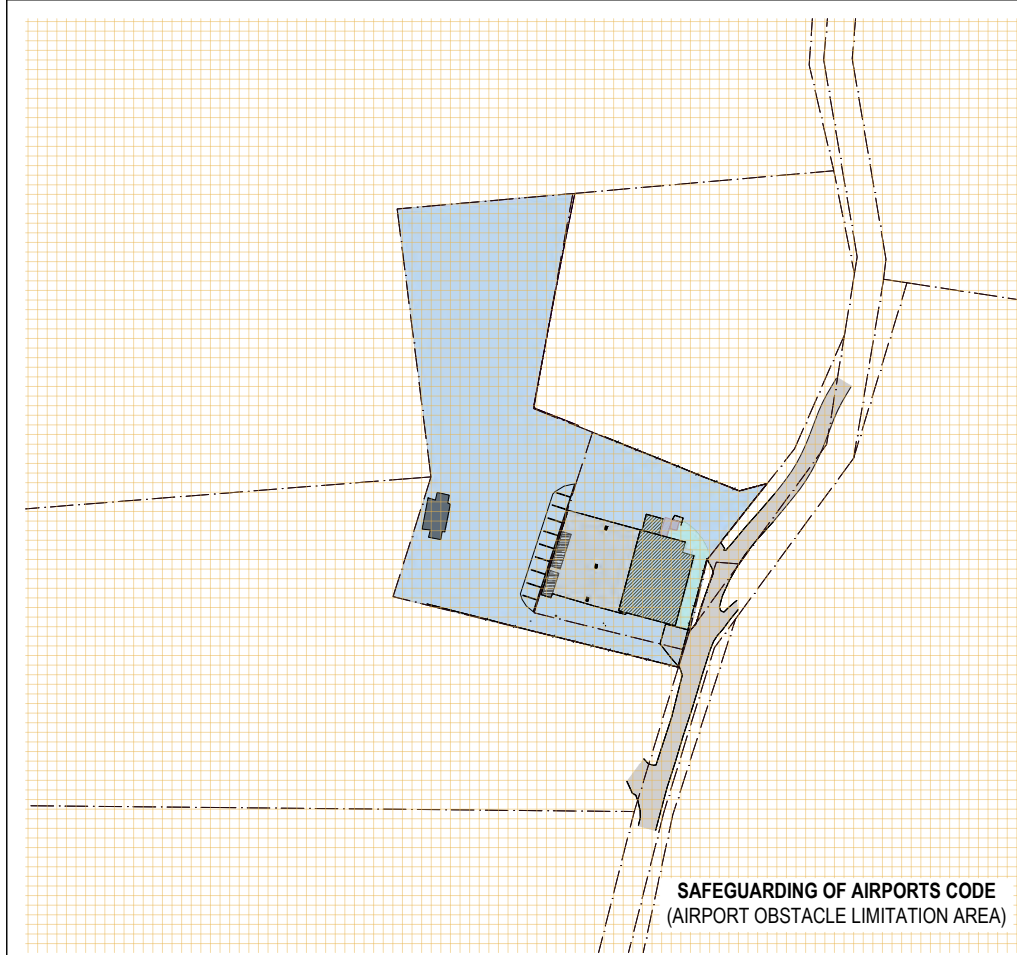
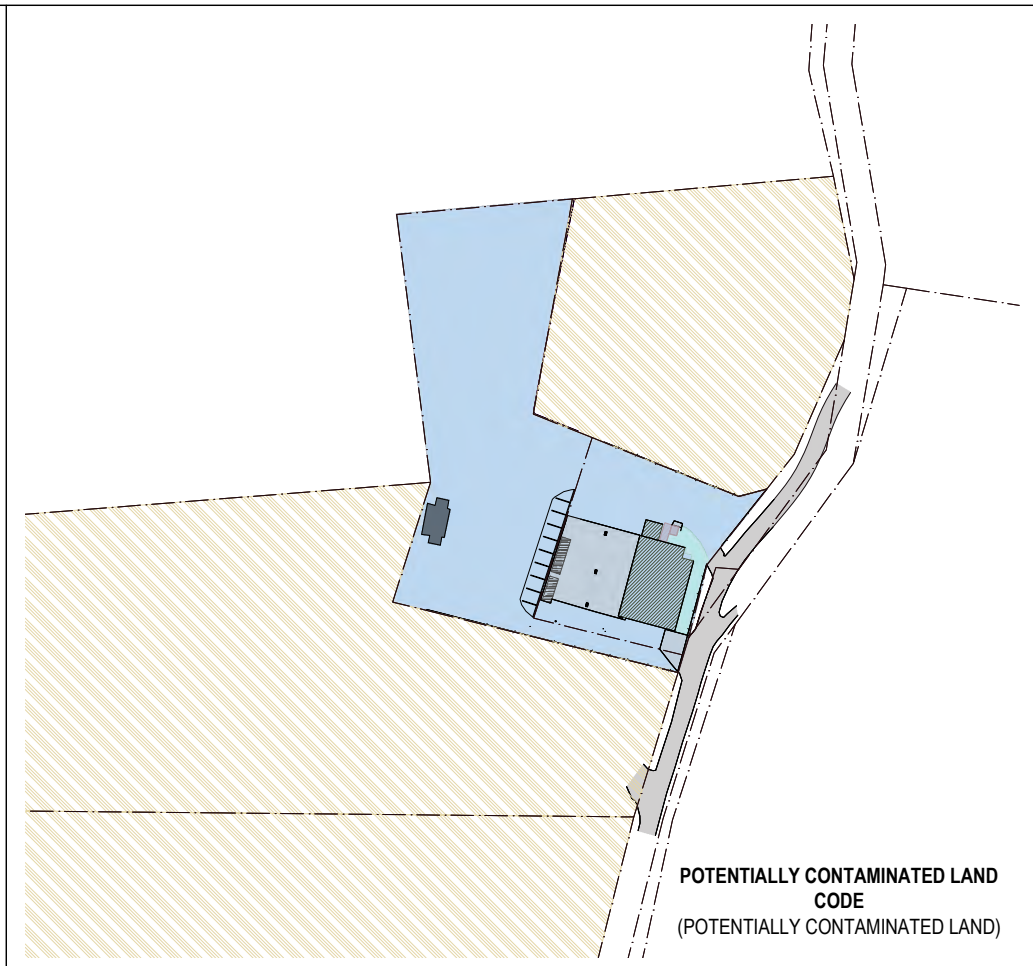
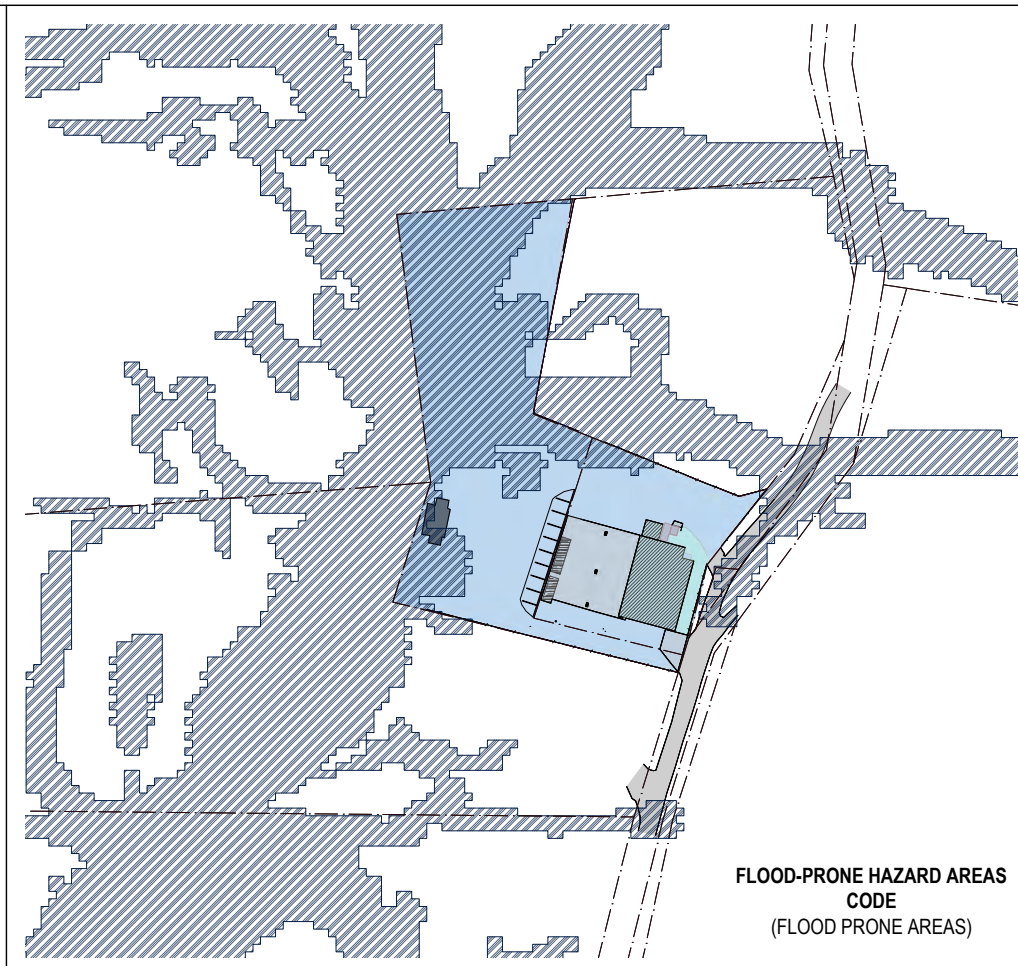
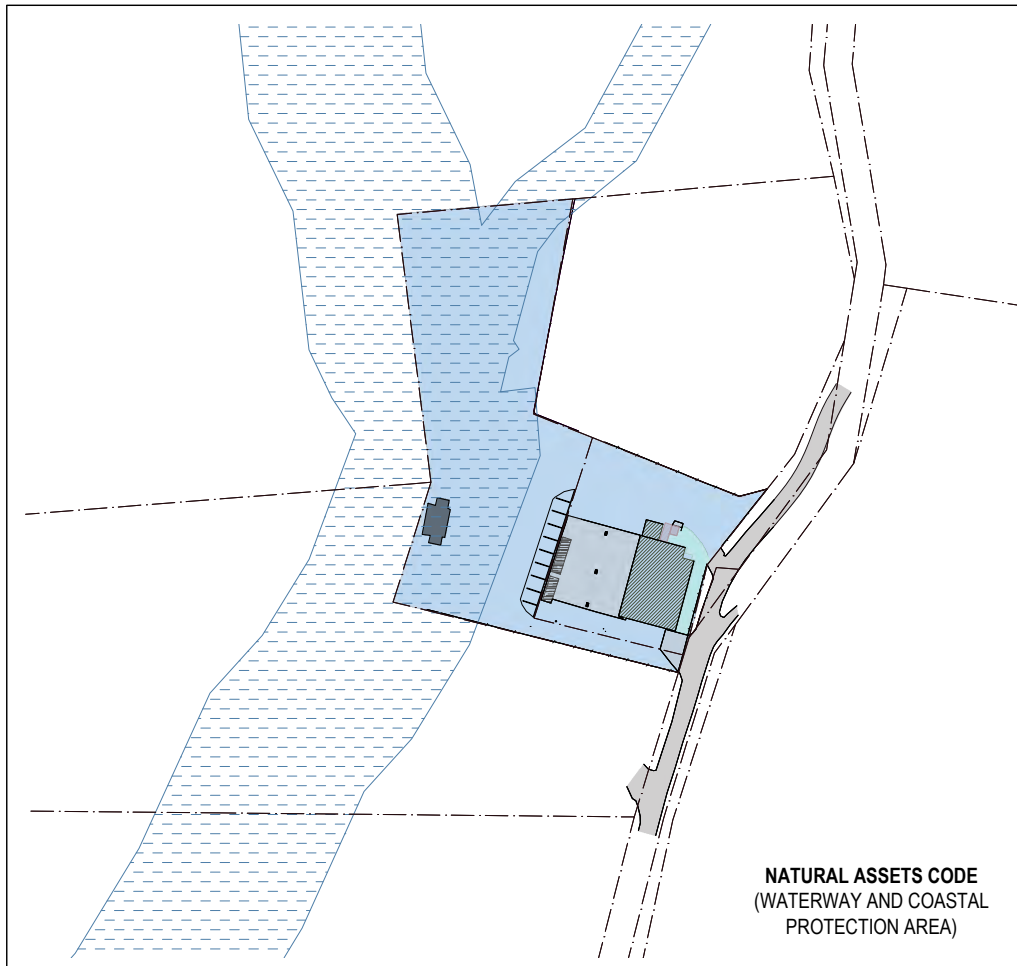
NOTE
ZONES ARE AS PER THE TASMANIAN PLANNING SCHEME

		CLIENT: NOBLE CONCRETE PUMPING PTY LTD
		ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017
		HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	01/02/2025
REVISION	DESCRIPTION	DATE
REVISION HISTORY		DO NOT SCALE FROM THE DRAWING.
		PREPARED BY: SANDUN INESHKA (MIE Aust, BSc(Eng.)Hons)

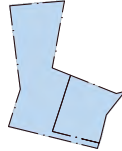






ADJOINING PROPERTIES AND USES

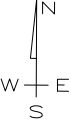
DRAWING NUMBER: 1008/104





LEGEND

-  THE SITE ENVELOPE
-  TITLE BOUNDARIES
-  SAFEGUARDING OF AIRPORTS CODE
-  BUSHFIRE-PRONE AREAS CODE
-  POTENTIALLY CONTAMINATED LAND CODE
-  FLOOD-PRONE HAZARD AREAS CODE
-  NATURAL ASSETS CODE (WATERWAY AND COASTAL PROTECTION AREA)



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		CLIENT: NOBLE CONCRETE PUMPING PTY LTD
		ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017
		HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	01/02/2025
REVISION	DESCRIPTION	DATE
REVISION HISTORY		DO NOT SCALE FROM THE DRAWING.
		PREPARED BY: SANDUN INESHKA (MIE Aust, B Sc(Eng.)Hons)

CODE OVERLAY MAPS
(TASMANIAN PLANNING SCHEME)

DRAWING NUMBER: 1008/105

A3

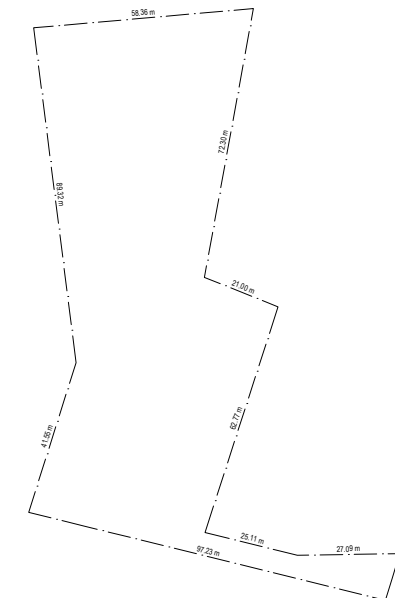




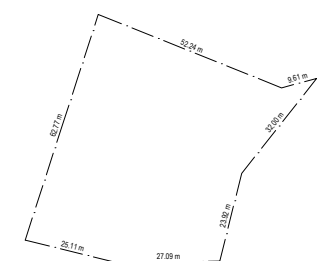
PROPERTY SIZE COMPARISON

	PROPERTY SIZE BEFORE THE BOUNDARY ADJUSTMENT	PROPERTY SIZE AFTER THE BOUNDARY ADJUSTMENT
44 SCOTT'S RD	6871 m ²	6961 m ²
46 SCOTT'S RD	3074 m ²	2984 m ²

44 SCOTT'S RD AFTER BOUNDARY ADJUSTMENT



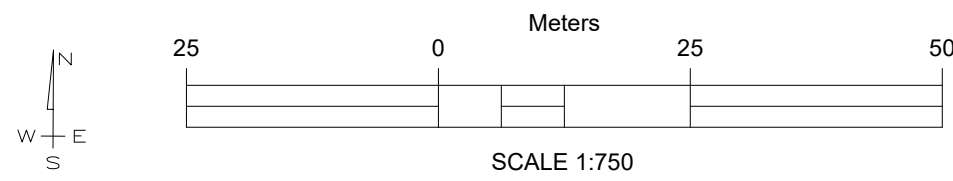
46 SCOTT'S RD AFTER BOUNDARY ADJUSTMENT



LOT 3 (PART OF CT 168000/5)
 APPROX. AREA = 90 m²
 THE PORTION TO BE TRANSFERRED
 FROM CT 168000/5 TO CT 183783/4

NOTES

- ALL MEASUREMENTS AND AREAS ARE SUBJECT TO THE FINAL SURVEY
- THIS PLAN IS ONLY FOR THE PURPOSE OF OBTAINING PRELIMINARY BOUNDARY ADJUSTMENT APPROVAL FROM THE LOCAL AUTHORITY.
- SOME SITE FEATURES ARE NOT SHOWN FOR THE CLARITY



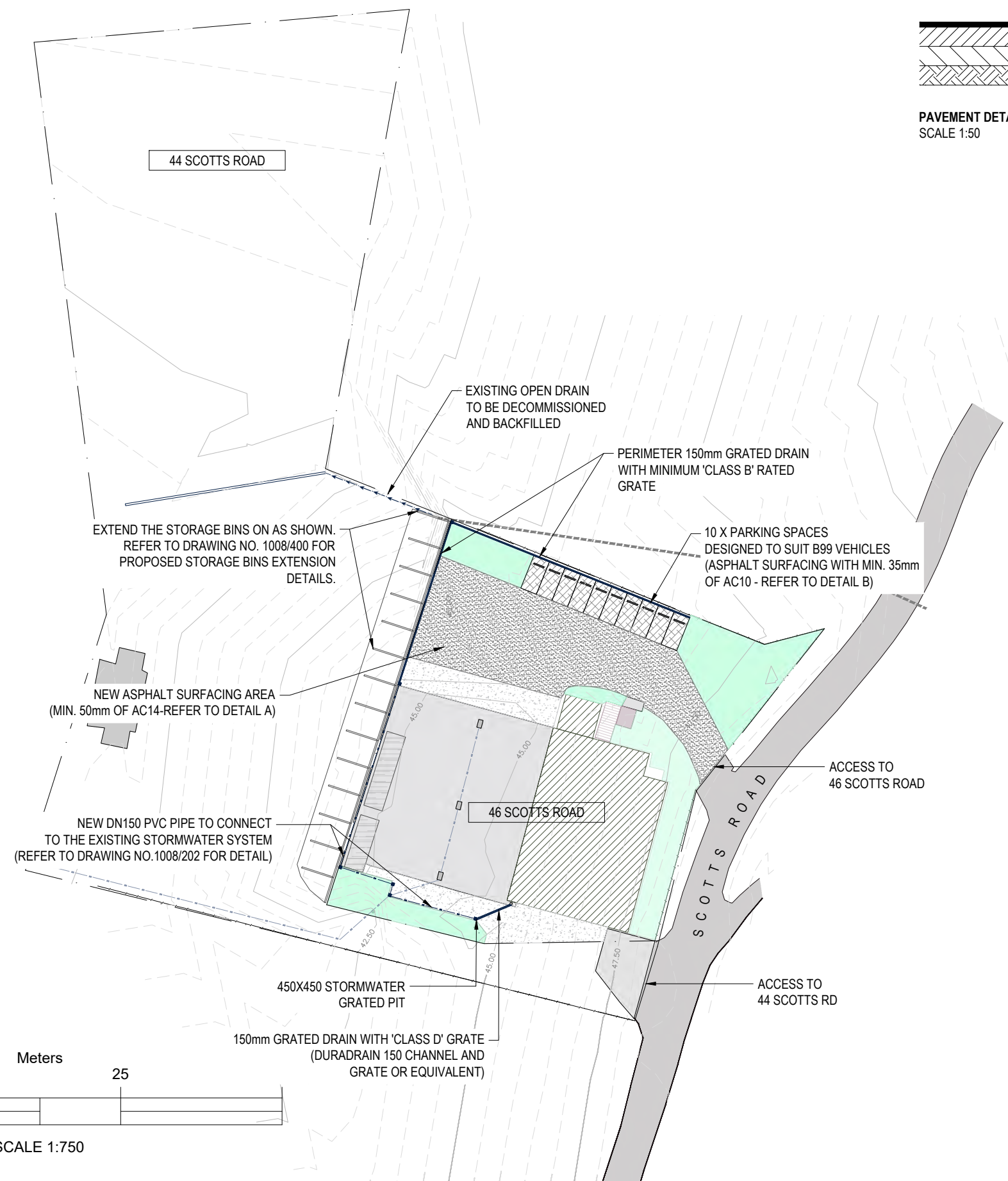
		CLIENT: NOBLE CONCRETE PUMPING PTY LTD	
		ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017	
B	MINOR ADJUSTMENT	25/03/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	15/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY		PREPARED BY: SANDUN INESHKA (MIE Aust, BSc(Eng.)Hons)	

PROPOSED BOUNDARY ADJUSTMENT

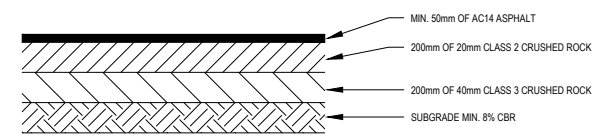
DRAWING NUMBER: 1008/200

A3

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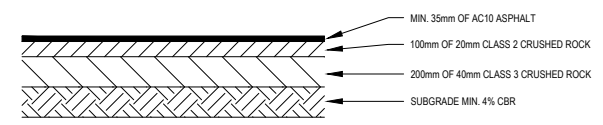


DETAIL A



PAVEMENT DETAILS
SCALE 1:50

DETAIL B



NOTES - CONCRETE

- ALL CONCRETE SHALL HAVE A STRENGTH OF MINIMUM CLASS N32 @ 28 DAYS.
- CONCRETE SHALL BE COMPACTED USING MECHANICAL VIBRATORS TO REMOVE AIR BUBBLES AND GIVE MAXIMUM COMPACTION WITHOUT SEGREGATION OF THE CONCRETE.
- SAW-CUT CONTRACTION JOINTS SHALL BE PROVIDED AT 4m MAXIMUM SPACING IN BOTH DIRECTIONS.
- EXPANSION JOINTS SHALL BE PLACED AT ALL SLAB TERMINATIONS OR AGAINST RIGID STRUCTURES.
- CURING OF CONCRETE SHALL BE COMMENCED AS SOON AS POSSIBLE AFTER PLACING AND SHALL BE IN ACCORDANCE WITH AS3600.
- MINIMUM SUBGRADE REQUIREMENT FOR CONCRETE PAVEMENT - 8%
- CONCRETE SLAB THICKNESS - 200mm

NOTES - STORMWATER

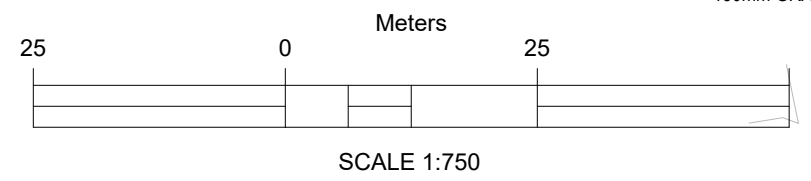
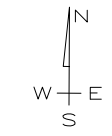
- REFER DRAWING NO.1008/202 FOR STORMWATER DETAILS.
- EXISTING STORMWATER CULVERT ENTERING FROM ROAD RESERVE TO BE CAPPED AT PROPERTY BOUNDARY.

NOTES - STORAGE BINS EXTENSION

- THE EXISTING STORAGE BINS AT 44 SCOTT'S ROAD ARE TO BE EXTENDED AS INDICATED ON THE DRAWINGS.
- THE EXISTING BINS ARE CONSTRUCTED FROM PRECAST CONCRETE PANELS AND WILL ACT AS A RETAINING STRUCTURE IN ADDITION TO THEIR STORAGE FUNCTION.
- THE EXISTING STORAGE BINS ARE APPROVED UNDER DEVELOPMENT APPLICATION D-2011/286.
- A CONCEPT LAYOUT OF THE PROPOSED STORAGE BINS IS PROVIDED IN DRAWING NO. 1008/400.
- FINAL DETAILS OF THE EXTENSION, INCLUDING TIE-IN WITH THE EXISTING PANELS AND REINFORCEMENT REQUIREMENTS, WILL BE DETERMINED DURING DETAILED ENGINEERING DESIGN.

LEGEND

- EXISTING CONCRETE HARDSTAND AREA
- NEW CONCRETE HARDSTAND AREA
- ASPHALT SURFACING AREA (AC14)
- ASPHALT SURFACING AREA (AC10) / PARKING SPACES
- GRASSED AREA
- THE SHED
- TITLE BOUNDARIES
- FENCELINE
- DN150 PVC STORMWATER LINE (EXISTING)
- DN150 PVC STORMWATER LINE (NEW)
- DN150 PVC STORMWATER LINE (NEW)
- EXISTING CULVERT (COUNCIL)



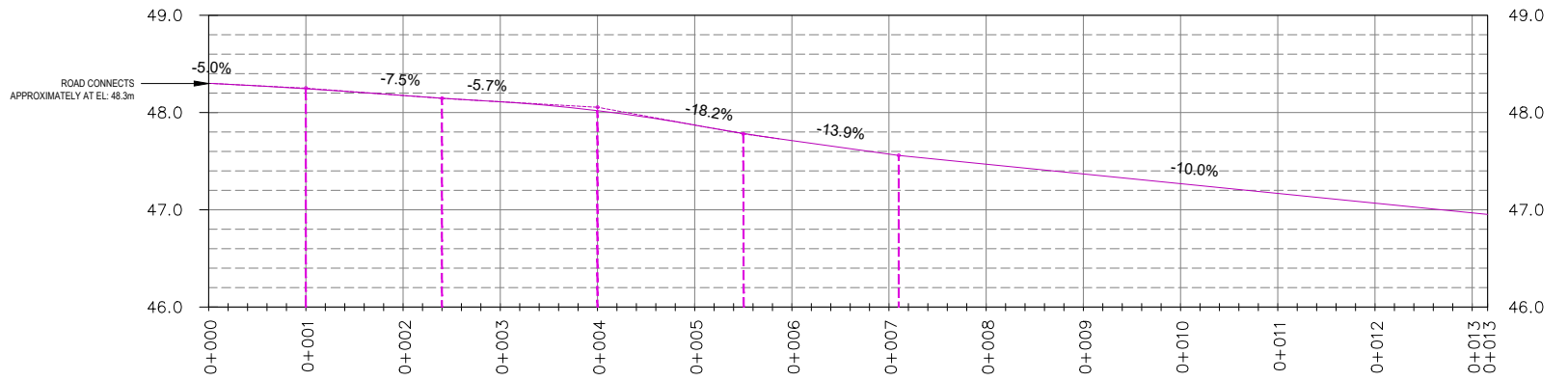
D	STORAGE BINS EXTENSION	06/09/2025	CLIENT: NOBLE CONCRETE PUMPING PTY LTD
C	COUNCIL CULVERT UPDATED	17/07/2025	ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017
B	STORWATER LINE UPDATED	17/06/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	15/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc.(Eng.)Hons)

**PROPOSED DEVELOPMENT
WORKS ON 44 & 46 SCOTT'S RD**

DRAWING NUMBER:1008/201

A3

DETAIL C
DRIVEWAY PROFILE



STORMWATER FLOW RATES - CATCHMENT AREA 01

DESCRIPTION	APPROX. AREA (m ²)	PEAK FLOW RATE (l/s)
IMPERVIOUS AREA (ROOF)	552.00	13.17
IMPERVIOUS AREA (NON-ROOF)	885.60	19.02
PEAK FLOW RATE REPORTING TO THE COUNCIL STORMWATER SYSTEM DURING A 1 IN 20 YEAR STORMWATER EVENT		32.19

STORMWATER FLOW RATES - CATCHMENT AREA 02

DESCRIPTION	APPROX. AREA (m ²)	PEAK FLOW RATE (l/s)
IMPERVIOUS AREA (ROOF)	0.00	0.00
IMPERVIOUS AREA (NON-ROOF)	912.16	19.59
PERVIOUS AREA (GRASS)	488.00	1.16
PEAK FLOW RATE REPORTING TO THE COUNCIL STORMWATER SYSTEM DURING A 1 IN 20 YEAR STORMWATER EVENT		20.75

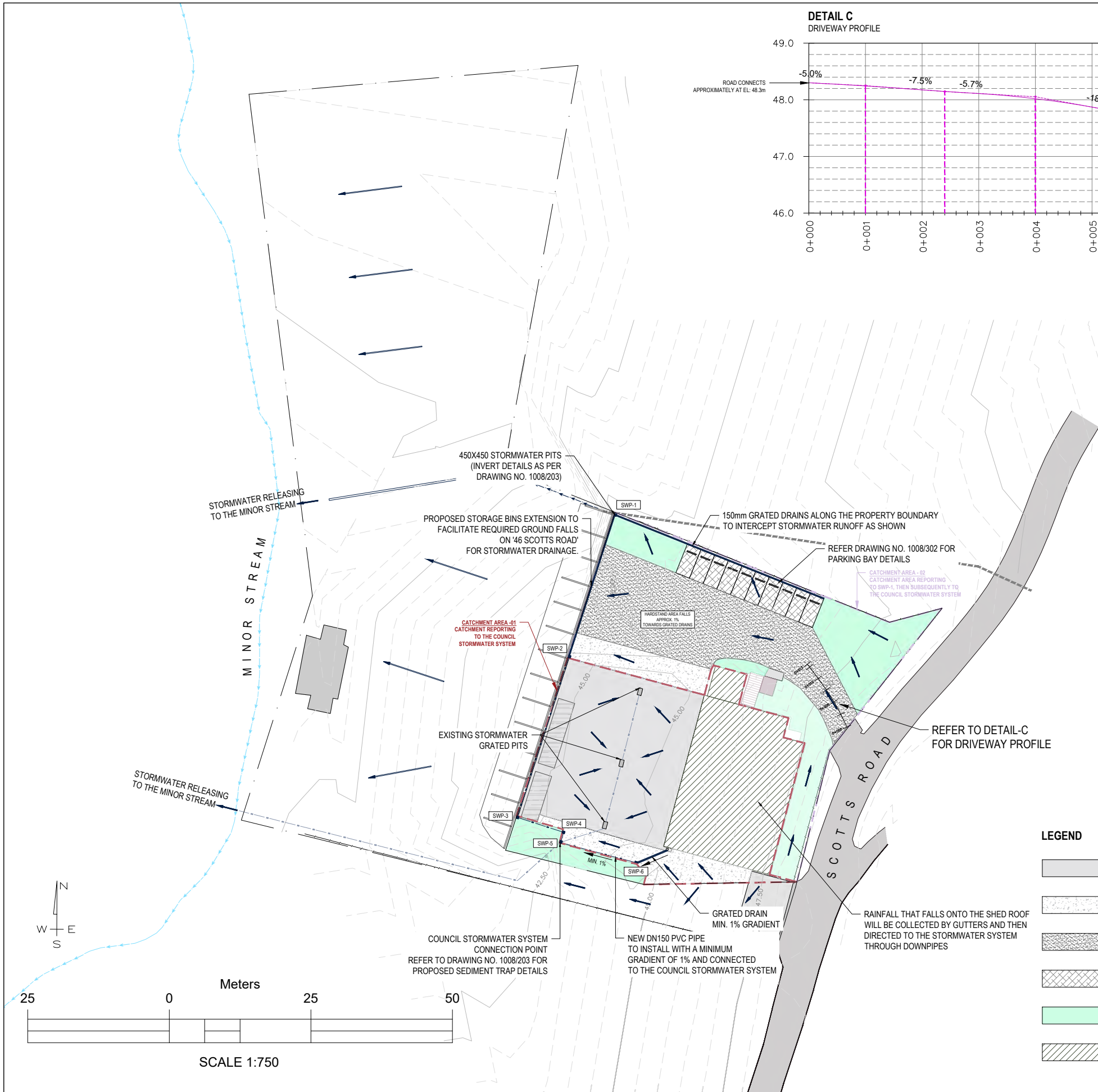
- ESTIMATED MAXIMUM CAPACITY OF 150mm GRATED DRAIN WITH 1% GRADIENT **30.6 l/s**
- ESTIMATED MAXIMUM CAPACITY OF DN150 PVC PIPE WITH 1.5% GRADIENT **25 l/s**
- ESTIMATED MAXIMUM CAPACITY OF EXISTING DN150 COUNCIL STORMWATER PIPE **55 l/s**
- ESTIMATED PEAK FLOW RATE FROM CATCHMENT 1 AND CATCHMENT 2 DURING A 1 IN 20 YEAR DESIGN STORMWATER EVENT **52.94 l/s**

NOTE

- ESTIMATED MAXIMUM CAPACITY OF THE EXISTING COUNCIL STORMWATER PIPE HAS BEEN CALCULATED BASED ON AN ASSUMED PIPE GRADIENT OF 7.5%. ACTUAL PIPE GRADIENTS MUST BE CONFIRMED ON SITE TO VERIFY SUFFICIENT CAPACITY WITHIN THE COUNCIL STORMWATER SYSTEM.
- LONGITUDINAL GRADIENT ALONG THE ACCESS DRIVEWAY IS IN ACCORDANCE WITH DETAIL C.
- THE VEHICLE MANEUVERING AREA (NEW ASPHALT SURFACE) IS GRADED AT 1% TOWARDS THE PERIMETER GRATED DRAINS, AS SHOWN ON THE DRAWINGS.

LEGEND

	EXISTING CONCRETE HARDSTAND AREA		TITLE BOUNDARIES
	NEW CONCRETE HARDSTAND AREA		FENCELINE
	ASPHALT SURFACING AREA (AC14)		CATCHMENT AREA 01
	ASPHALT SURFACING AREA (AC10) / PARKING SPACES		CATCHMENT AREA 02
	GRASSED AREA		STORMWATER FLOW DIRECTIONS
	THE SHED		STORMWATER PITS (450mm x 450mm)



E	STORAGE BINS UPDATE	06/09/2025	CLIENT: NOBLE CONCRETE PUMPING PTY LTD
D	COUNCIL CULVERT UPDATED	17/07/2025	ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017
C	GRATED DRAINS UPDATED	19/06/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
B	SPOON DRAIN CROSS SECTION UPDATED	25/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc.(Eng.)Hons)

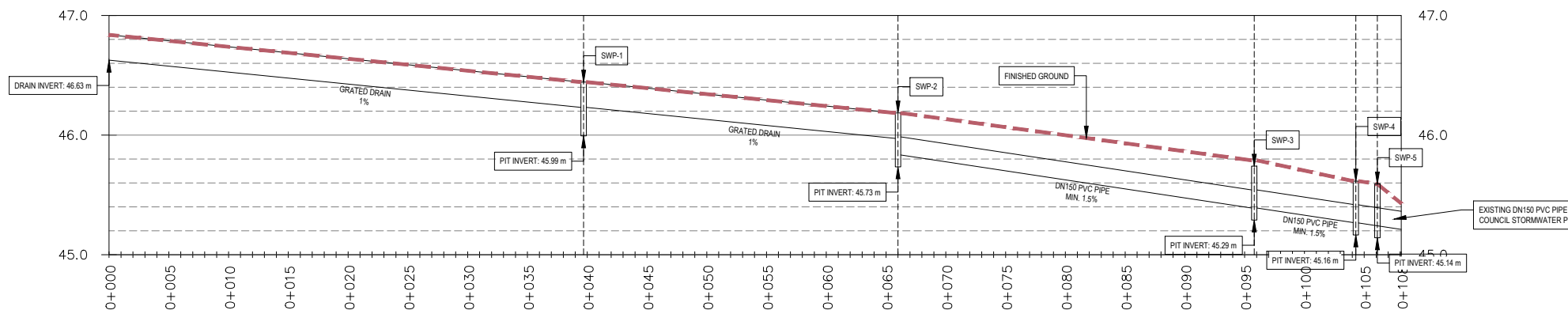
STORMWATER MANAGEMENT PLAN

DRAWING NUMBER: 1008/202

A3

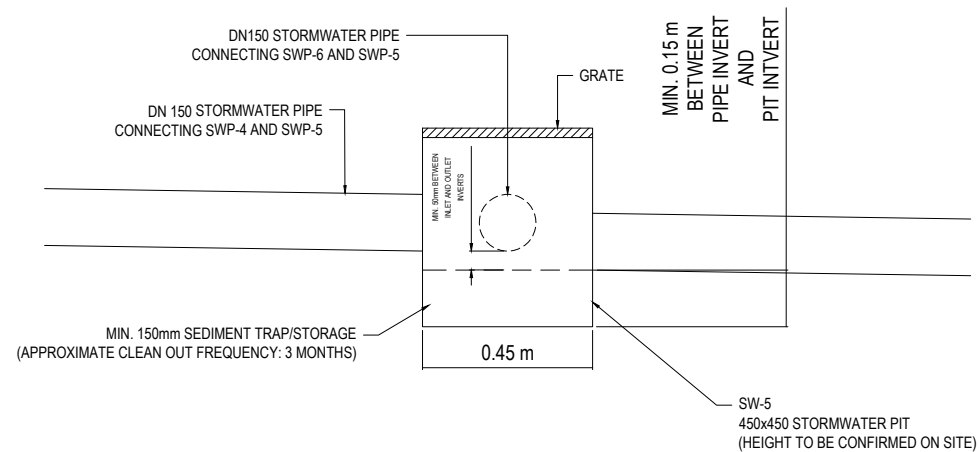


STORMWATER LONGSECTION



VERTICAL EXAGGERATION (HORIZONTAL/VERTICAL SCALE) 1:10

PROPOSED SEDIMENT TRAP AT SWP-5 SCALE 1:20



NOTES

- INVERT LEVEL OF EXISTING COUNCIL STORMWATER CONNECTION TO BE CONFIRMED ON SITE PRIOR TO CONSTRUCTION
- A MINIMUM OF LONGITUDINAL GRADIENT OF 1% SHALL BE MAINTAINED ALONG THE GRATED DRAIN
- A MINIMUM OF LONGITUDINAL GRADIENT OF 1.5% SHALL BE MAINTAINED ALONG THE CONNECTING PVC PIPEWORK.
- DRAIN INVERT LEVELS ARE INDICATIVE ONLY AND MAY VARY BASED ON SITE CONDITIONS. HOWEVER, MINIMUM DESIGN GRADIENTS MUST BE MAINTAINED.

		CLIENT: NOBLE CONCRETE PUMPING PTY LTD	
		ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017	
B	SEDIMENT TRAP UPDATED	06/09/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	19/06/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY		PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)	

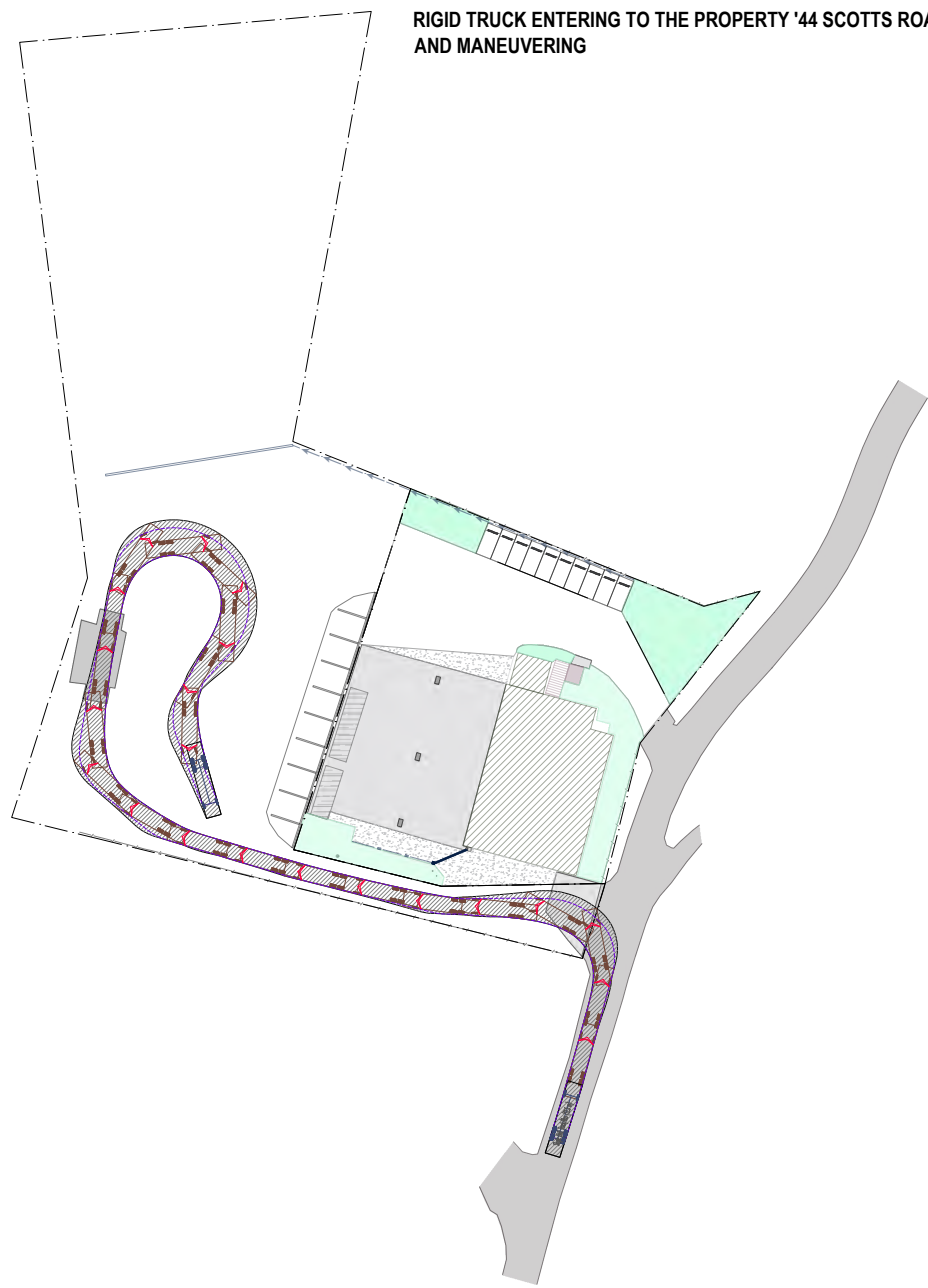
STORMWATER LONGSECTION

DRAWING NUMBER: 1008/203

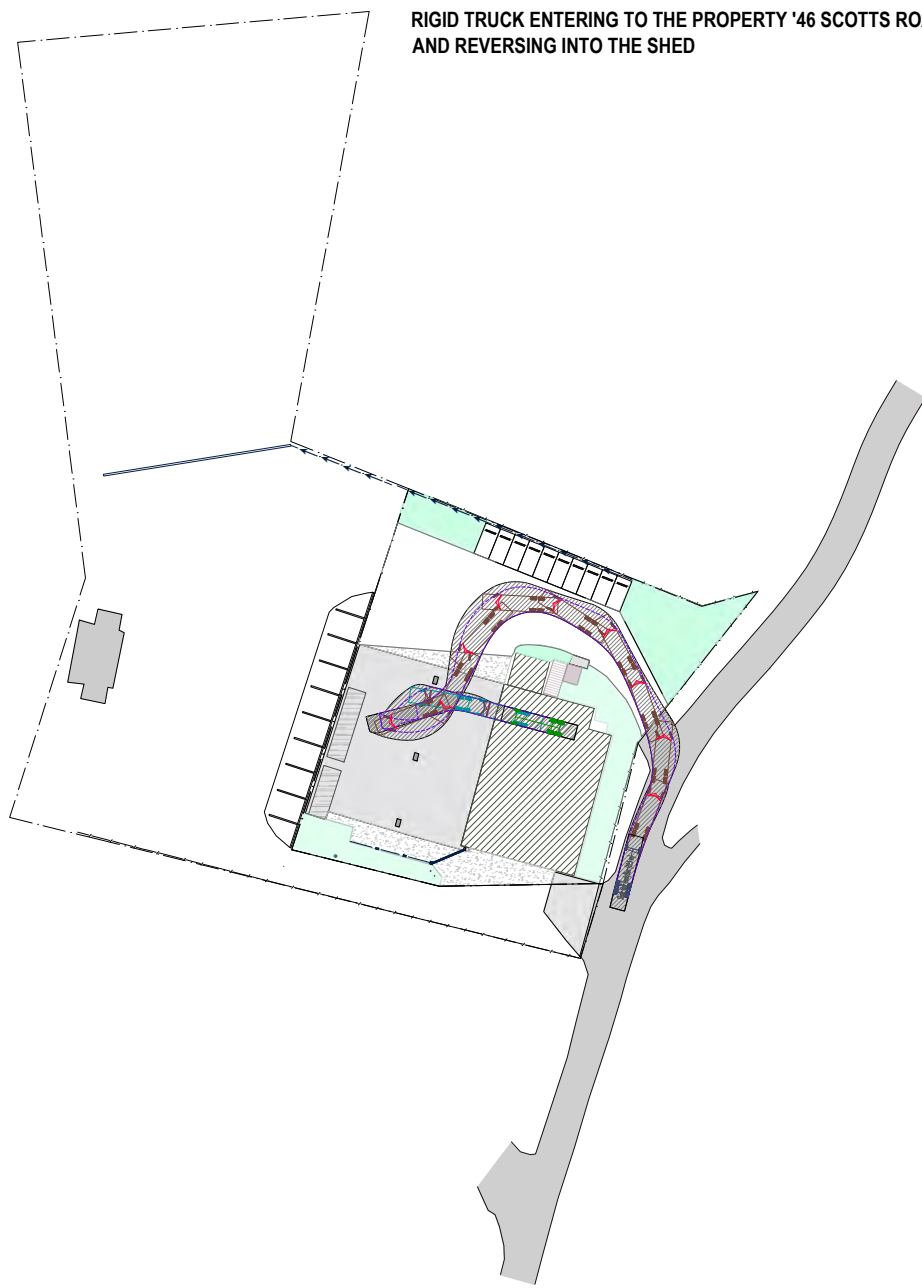
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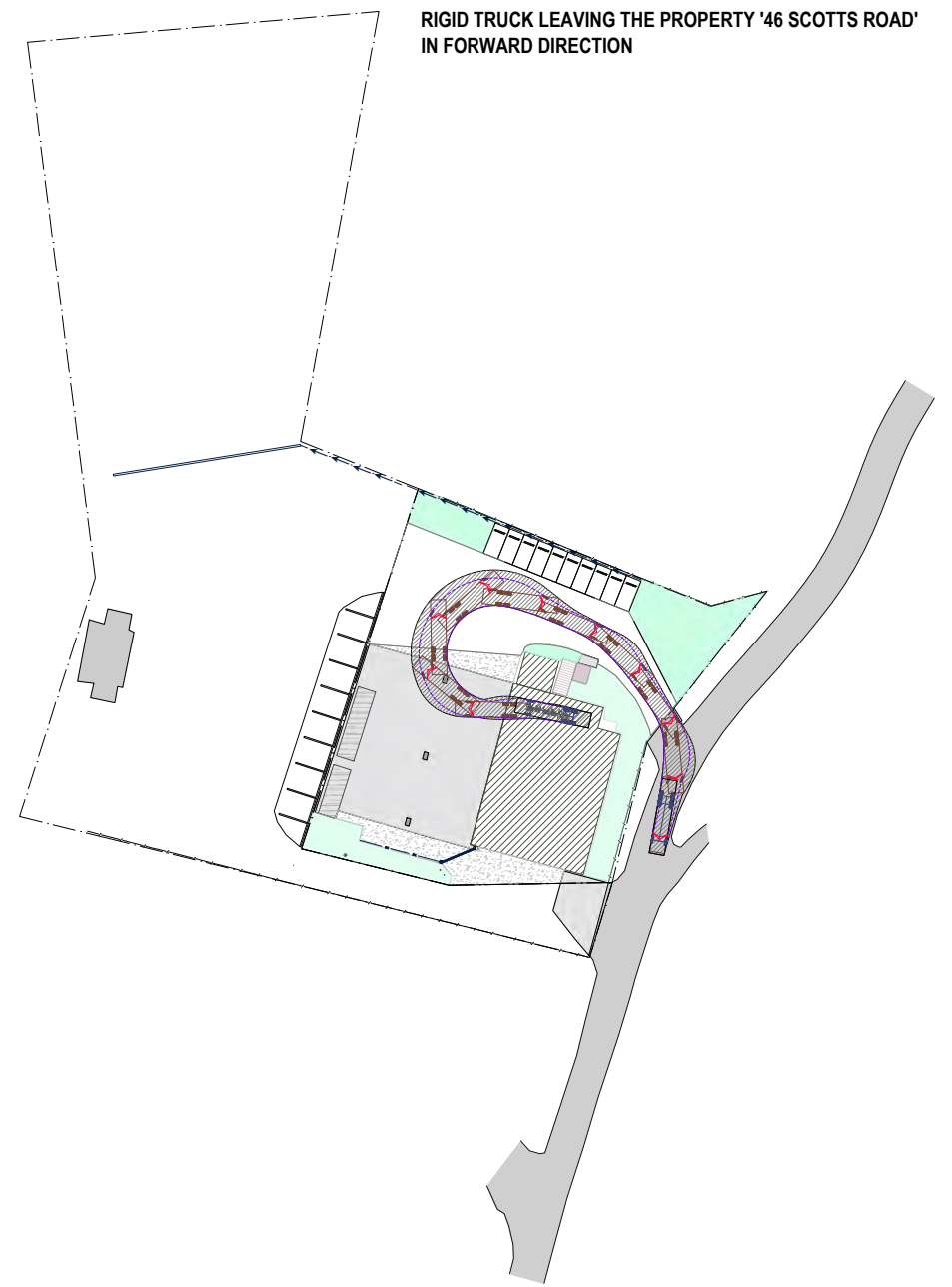
RIGID TRUCK ENTERING TO THE PROPERTY '44 SCOTT'S ROAD' AND MANEUVERING



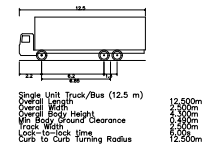
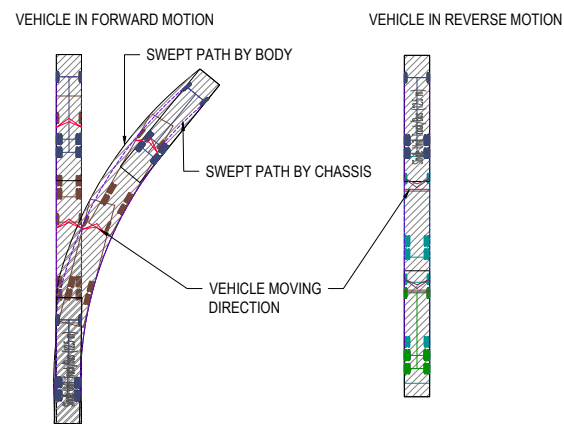
RIGID TRUCK ENTERING TO THE PROPERTY '46 SCOTT'S ROAD' AND REVERSING INTO THE SHED



RIGID TRUCK LEAVING THE PROPERTY '46 SCOTT'S ROAD' IN FORWARD DIRECTION



LEGEND



Single Unit Truck/Bus (12.5 m)
 Overall Length 12.500m
 Overall Width 2.500m
 Overall Height 3.500m
 Max. Body Height Clearance 2.500m
 Lock-to-Lock Time 6.000s
 Curb to Curb Turning Radius 12.500m

NOTES

- DESIGN VEHICLE: SINGLE UNIT RIGID TRUCK (12.5m)
- CURB TO CURB TURNING RADIUS: 12.5m
- LOCK TO LOCK TIME: 6s
- DESIGN SPEED: 10km/hr

			CLIENT: NOBLE CONCRETE PUMPING PTY LTD
			ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017
			HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	15/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY			PREPARED BY: SANDUN INESHKA (MIE Aust., B.Sc(Eng.)Hons)

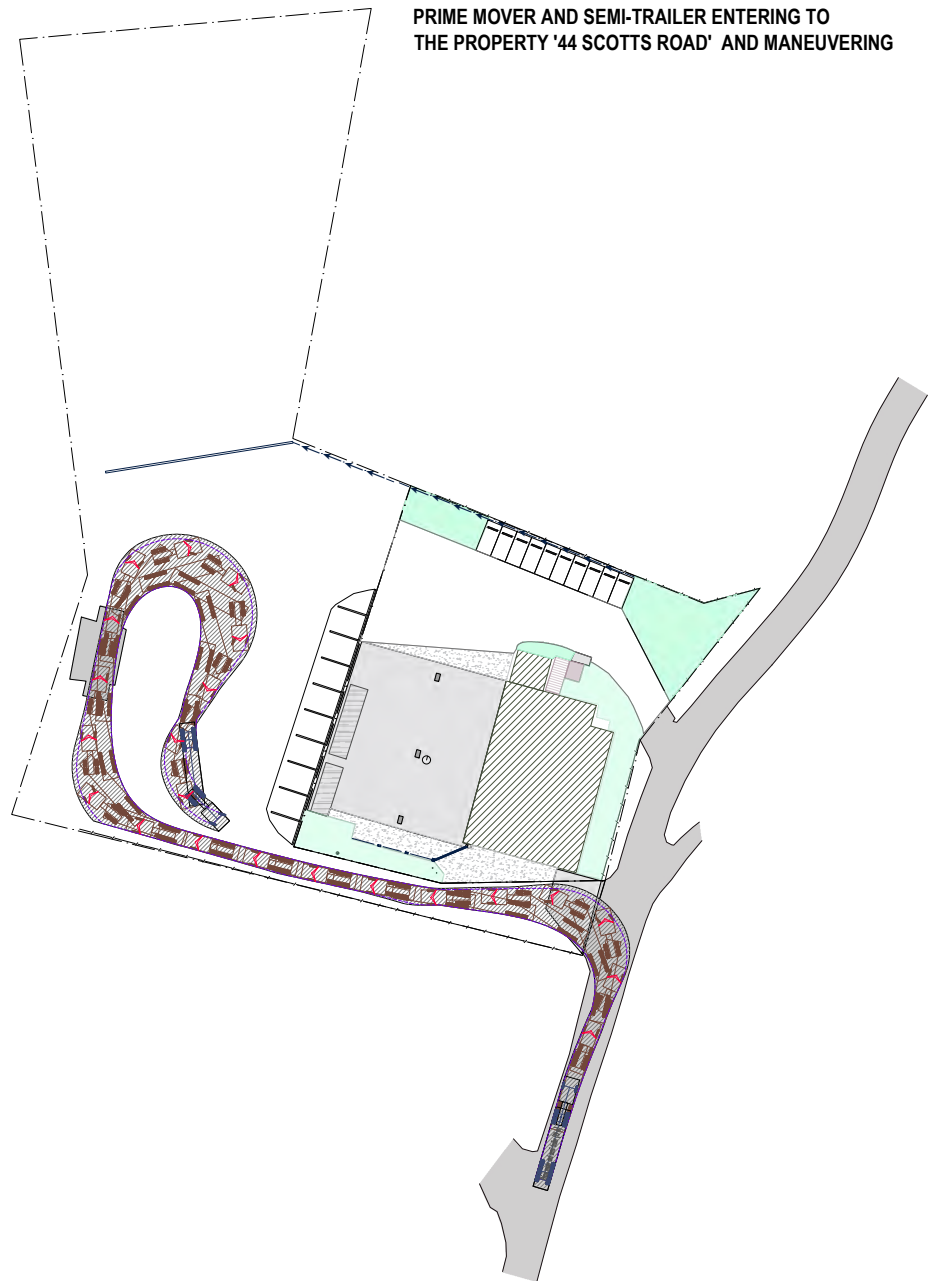
SWEPT PATH ANALYSIS - RIGID TRUCK

DRAWING NUMBER:1008/300

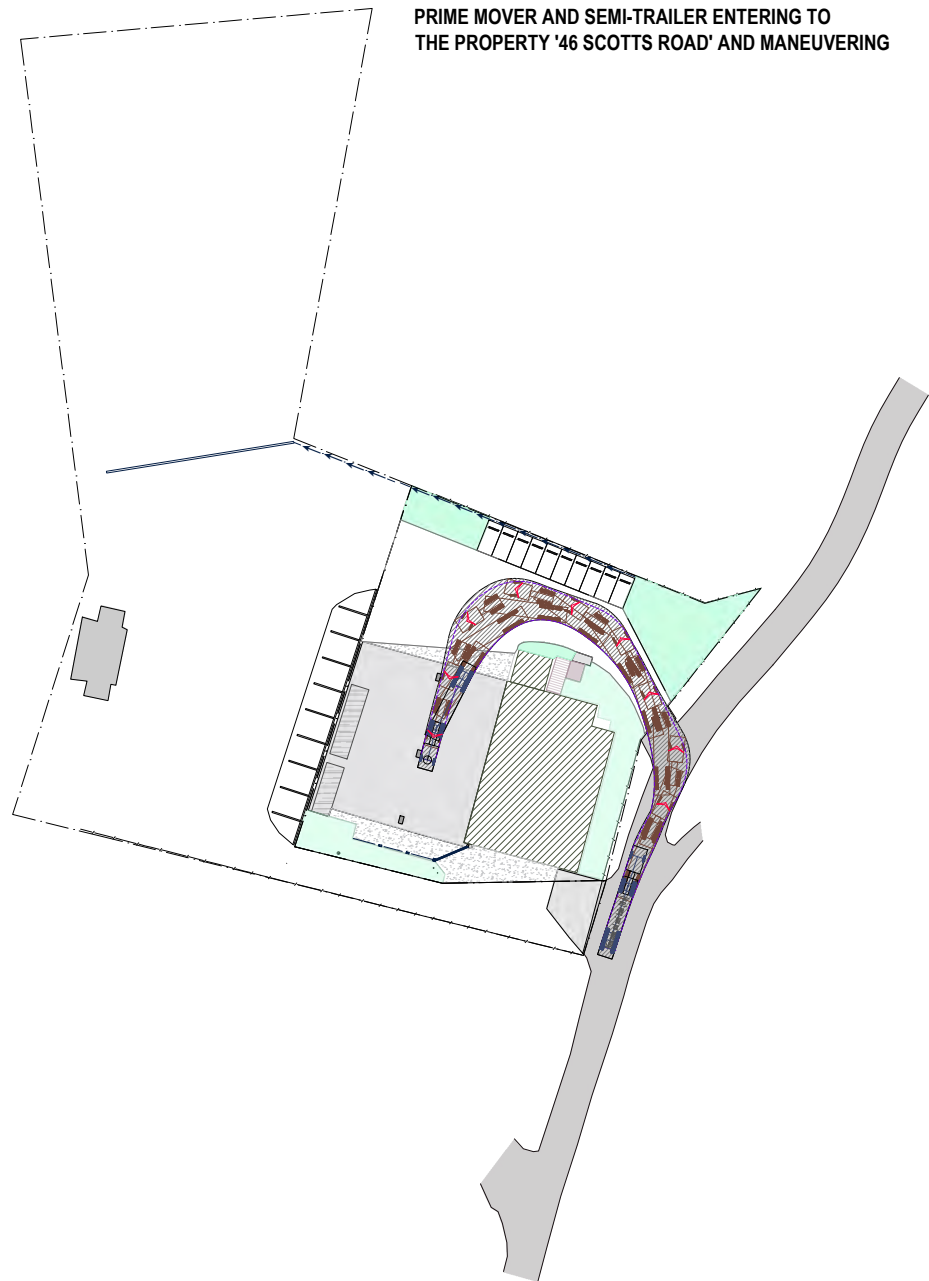
A3



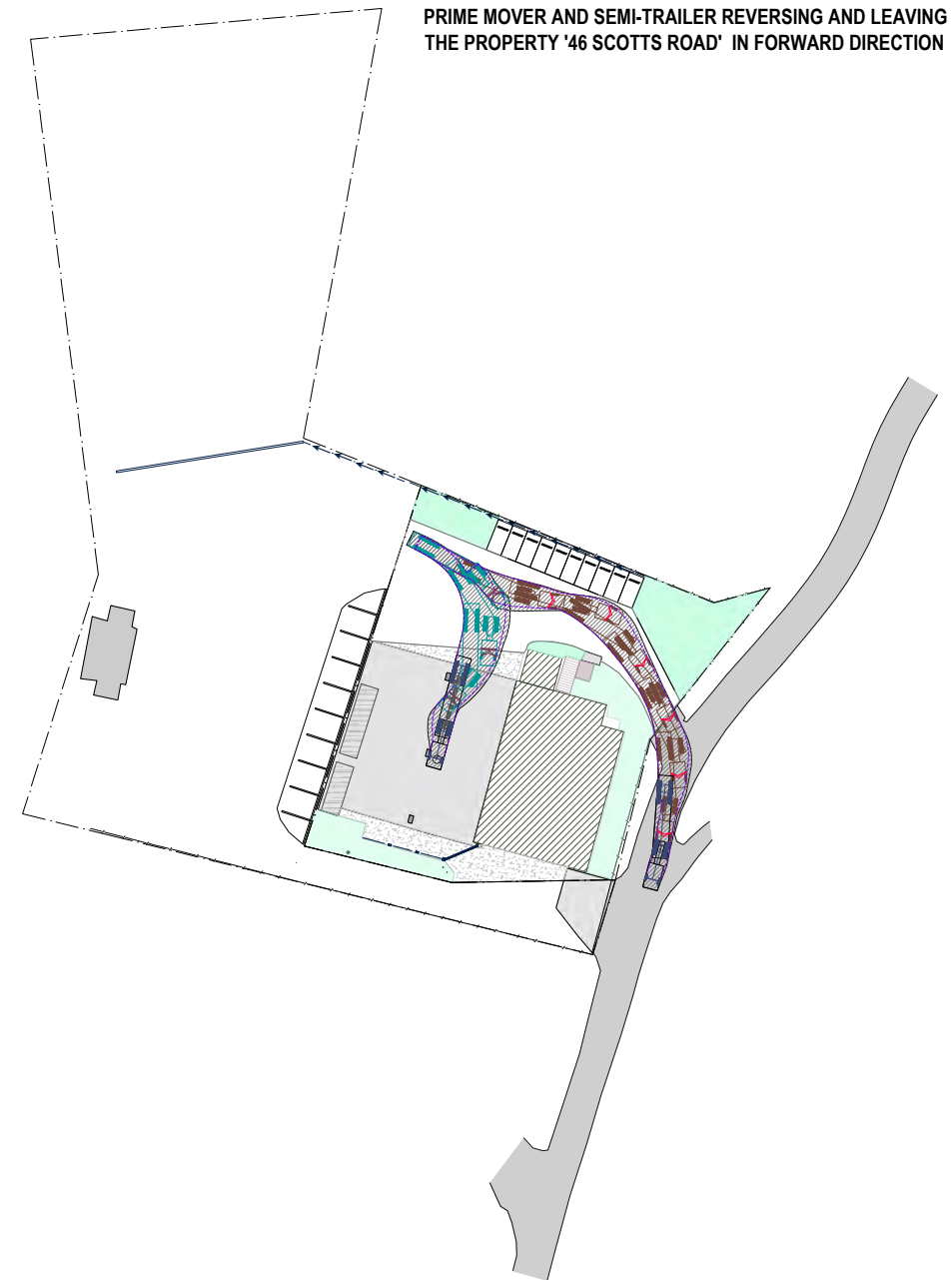
PRIME MOVER AND SEMI-TRAILER ENTERING TO THE PROPERTY '44 SCOTTS ROAD' AND MANEUVERING



PRIME MOVER AND SEMI-TRAILER ENTERING TO THE PROPERTY '46 SCOTTS ROAD' AND MANEUVERING



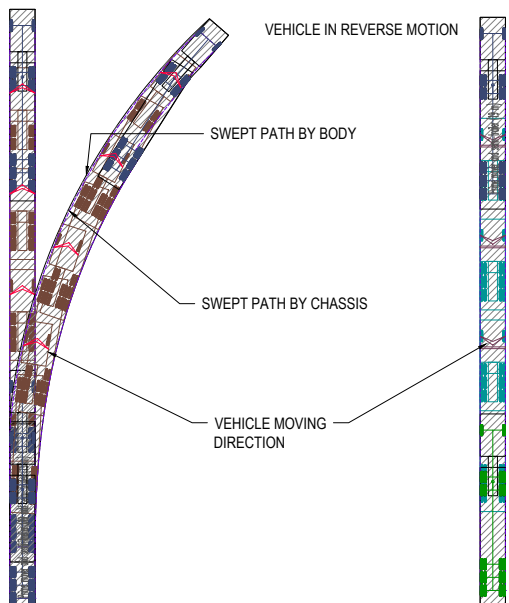
PRIME MOVER AND SEMI-TRAILER REVERSING AND LEAVING THE PROPERTY '46 SCOTTS ROAD' IN FORWARD DIRECTION



LEGEND

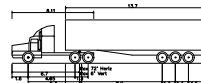
VEHICLE IN FORWARD MOTION

VEHICLE IN REVERSE MOTION



NOTES

- CHECKING VEHICLE: PRIME MOVER AND SEMI TRAILER (19.0m)
- CURB TO CURB TURNING RADIUS: 12.5m
- LOCK TO LOCK TIME: 6s
- DESIGN SPEED: 10km/hr
- PRIME MOVER WILL BE THE LARGEST VEHICLE TO ENTER IN TO THE FACILITY



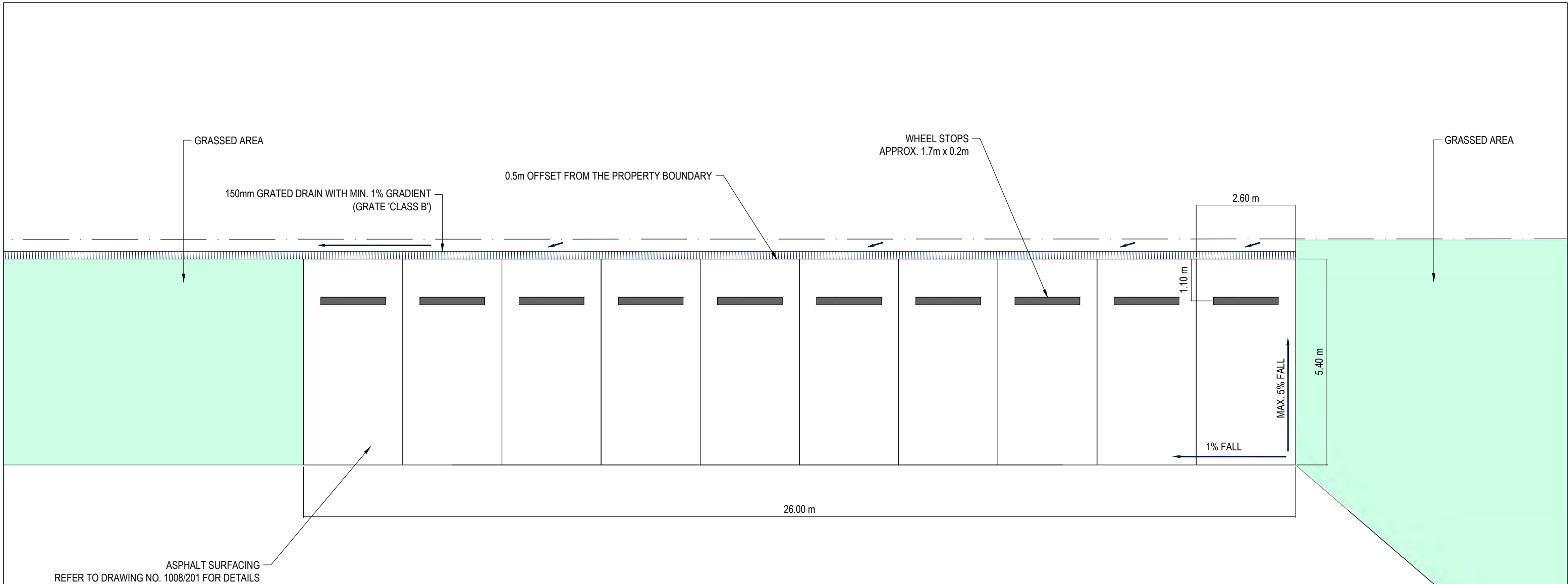
Prime mover and semi-trailer (19 m)
 Overall length 19.000m
 Overall width 2.500m
 Max Body Height 3.500m
 Min Body Ground Clearance 0.500m
 Lock-to-lock time 6.000s
 Curb to Curb Turning Radius 12.500m

REVISION	DESCRIPTION	DATE
A	INITIAL PLAN	15/03/2025

CLIENT: NOBLE CONCRETE PUMPING PTY LTD
 ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017
 HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
 ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
 DO NOT SCALE FROM THE DRAWING.
 PREPARED BY: SANDUN INESHKA (MIE Aust, BSc(Eng.)Hons)

SWEPT PATH ANALYSIS - PRIME MOVER (19.5m)
 DRAWING NUMBER: 1008/301

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ASPHALT SURFACING
REFER TO DRAWING NO. 1008/201 FOR DETAILS

NOTES - GENERAL

- PARKING SPACES ARE DESIGNED TO SUIT B99 VEHICLES
- DESIGN STANDARD: AS 2890.1:2004
- PARKING FACILITY USER CLASS: CLASS 01 AS PER AS2890.1:2004 (EMPLOYEE AND COMMUTER PARKING)
- PARKING ANGLE: 90 DEGREES

DEFINITIONS

- B99 VEHICLE: DEFINITION AS PER AS2890.1:2004 (THE DESIGN MOTOR CAR WHOSE PHYSICAL DIMENSIONS REPRESENT THE 99.8TH PERCENTILE CLASS OF ALL CARS AND LIGHT VANS ON THE ROAD.
- B85 VEHICLE: DEFINITION AS PER AS2890.1:2004 (THE DESIGN MOTOR CAR WHOSE PHYSICAL DIMENSIONS REPRESENT THE 85TH PERCENTILE CLASS OF ALL CARS AND LIGHT VANS ON THE ROAD.

REFERENCES

- AUSTRALIAN STANDARD AS 2890.1:2004 - PARKING FACILITIES; OFF STREET CAR PARKING
- AUSTRROADS GUIDE TO TRAFFIC MANAGEMENT PART 11: PARKING MANAGEMENT TECHNIQUES

NOTES - STORMWATER

- MAXIMUM GRADIENT MEASURED PARALLEL TO THE ANGLE OF PARKING WILL NOT EXCEED 5% (1 IN 20)
- GRADIENT MEASURED IN ANY DIRECTION WILL NOT EXCEED 6.25% (1 IN 16)
- STORMWATER RUNOFF FROM THE PARKING BAYS SHALL BE DIRECTED TOWARDS AND CAPTURED BY THE GRATED DRAIN.

		CLIENT: NOBLE CONCRETE PUMPING PTY LTD	
		ADDRESS/LOCATION: 44 SCOTTS ROAD & 46 SCOTTS ROAD, RISDON VALE, TAS 7017	
B	STORMWATER UPDATES	17/06/2025	HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	15/03/2025	ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE SPECIFIED
REVISION	DESCRIPTION	DATE	DO NOT SCALE FROM THE DRAWING.
REVISION HISTORY		PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)	

PARKING BAY DESIGN

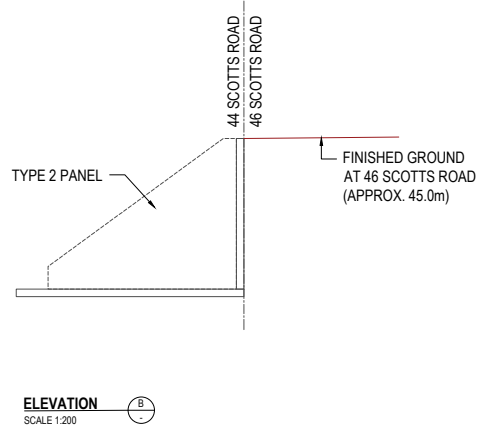
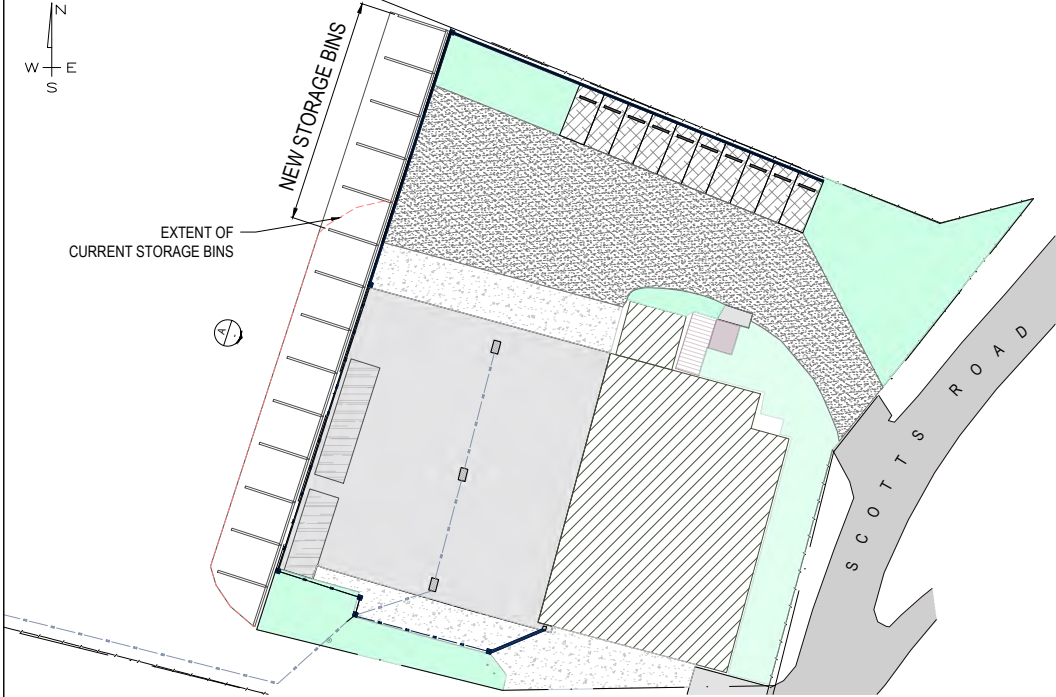
DRAWING NUMBER:1008/302

A3



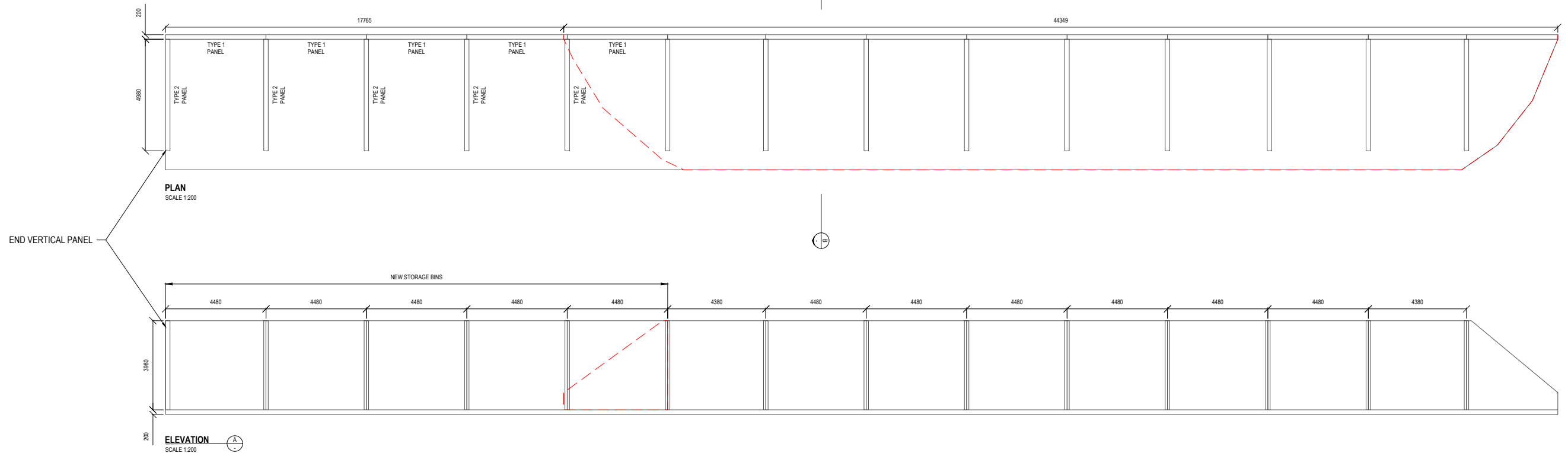
PROPOSED STORAGE BINS LOCATION

SCALE 1:750



NOTES

- THE PROPOSED STORAGE BINS EXTENSION IS TO BE CARRIED OUT BY EXTENDING THE PRECAST CONCRETE PANELS AS SHOWN.
- END PANEL CONNECTION BRACKET DETAILS ARE TO BE CONFIRMED DURING THE FINAL DESIGN STAGE.
- THE EXISTING STORAGE BINS' CONCRETE PAD FOOTING IS TO BE EXTENDED AS SHOWN.
- TYPE 1 PANEL REINFORCEMENT DESIGN IS TO BE IN ACCORDANCE WITH DRAWING NO. 2640-03 PREPARED BY THE PRECASTERS P/L.
- TYPE 2 PANEL REINFORCEMENT DESIGN IS TO BE IN ACCORDANCE WITH DRAWING NO. 2640-11 PREPARED BY THE PRECASTERS P/L.
- IT SHOULD BE NOTED THAT THE PROPOSED STORAGE BINS EXTENSION IS CONCEPTUAL ONLY. THE FINAL DESIGN WILL BE SUBJECT TO DETAILED ENGINEERING ANALYSIS TO SUIT THE EXISTING GROUND CONDITIONS.



		CLIENT: NOBLE CONCRETE PUMPING PTY LTD
		ADDRESS/LOCATION: 44 SCOTT'S ROAD & 46 SCOTT'S ROAD, RISDON VALE, TAS 7017
		HORIZONTAL/VERTICAL DATUM: MGA ZONE 55 GDA94 / AHD
A	INITIAL PLAN	09/09/2025
REVISION	DESCRIPTION	DATE
REVISION HISTORY		DO NOT SCALE FROM THE DRAWING.
		PREPARED BY: SANDUN INESHKA (MIEAust, B.Sc(Eng.)Hons)

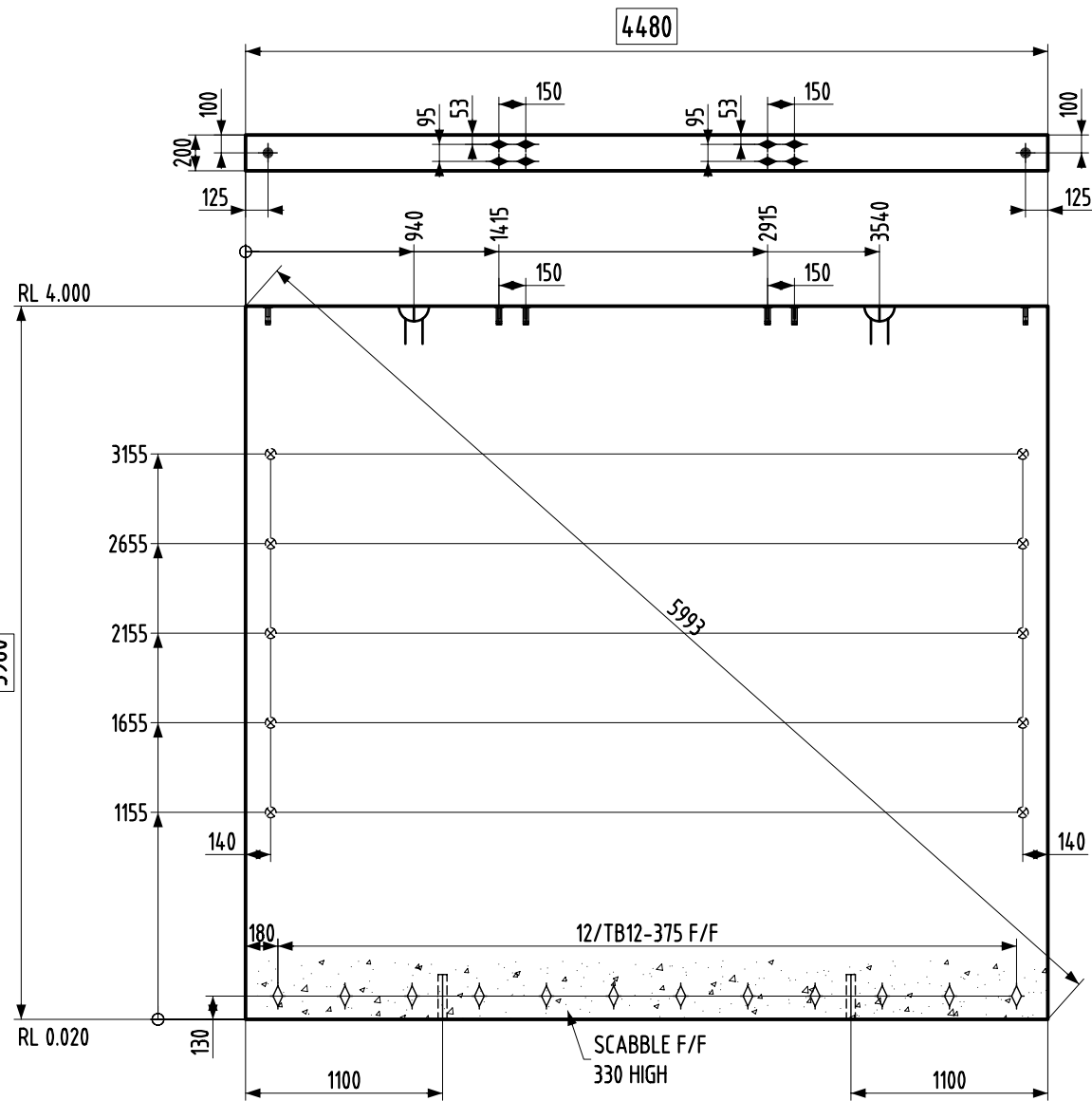
PROPOSED STORAGE BINS EXTENSION

DRAWING NUMBER:1008/400

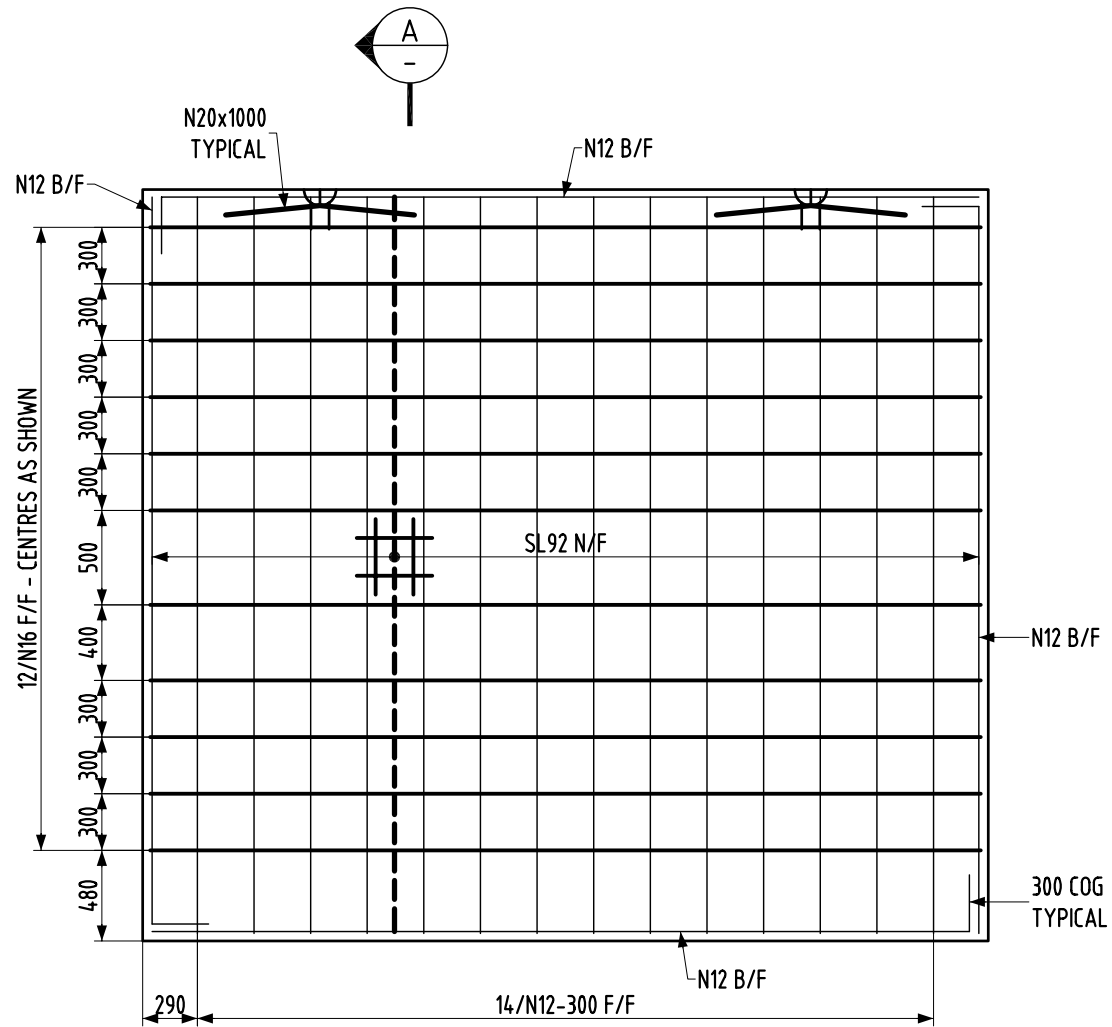
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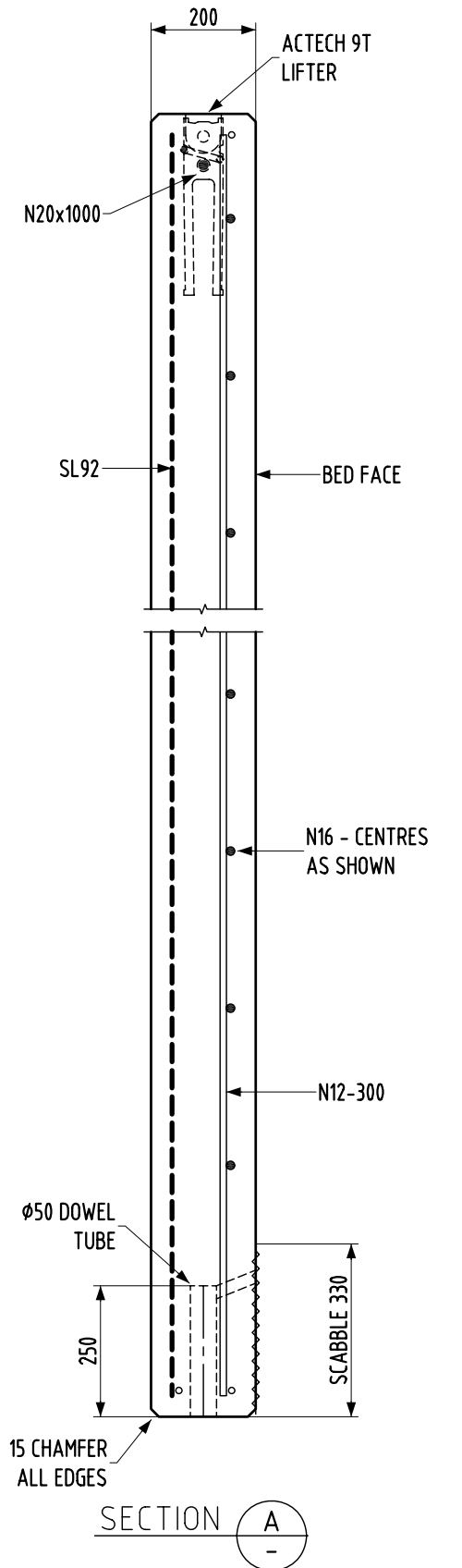
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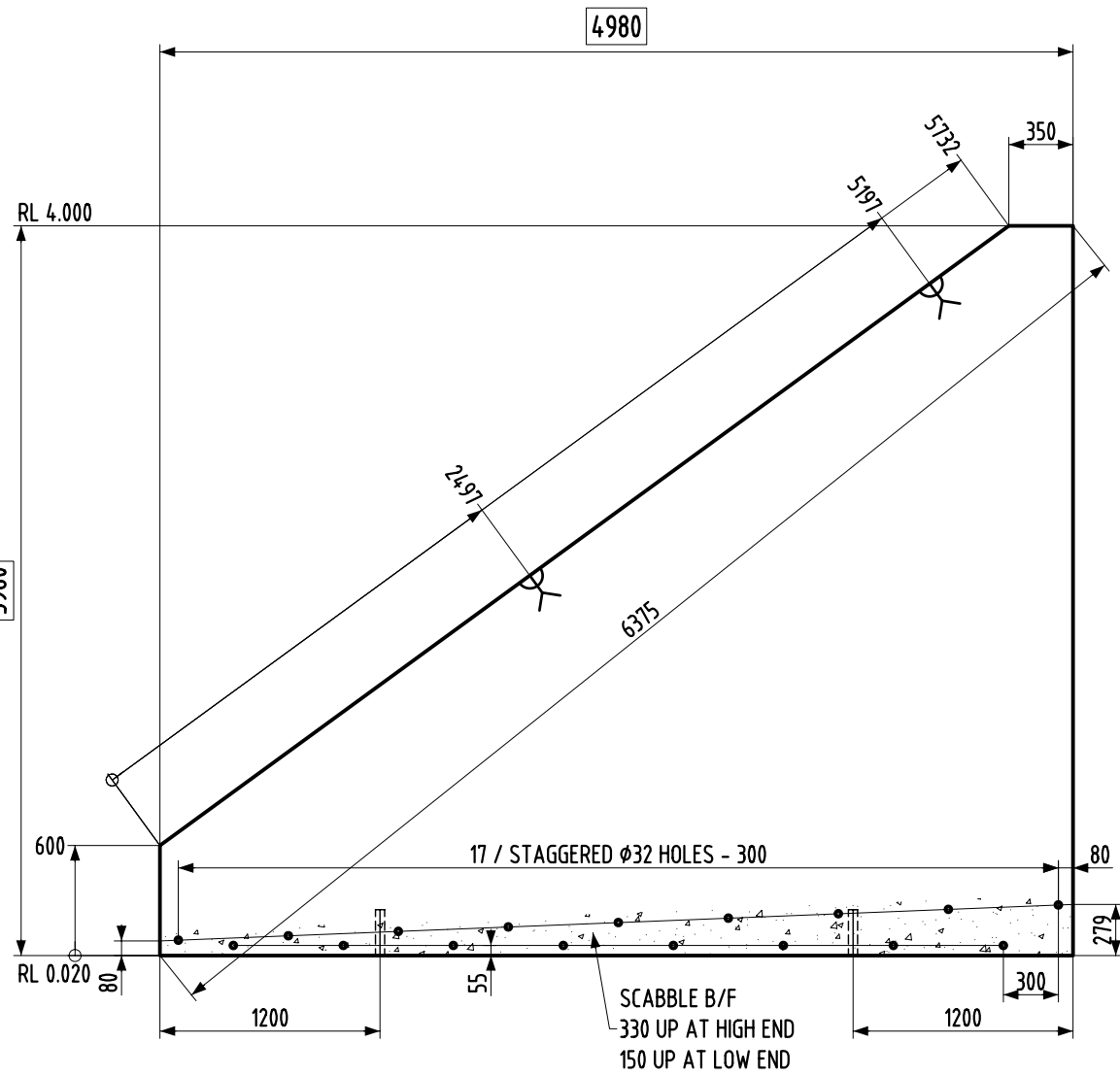
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WORKSHOP		2.5T FACE	2.5T EDGE	Ø20 FAR FACE	CONCRETE COLOUR GREY	LIFTER TYPE ACTECH	ABBREVIATIONS N/F=NEAR FACE F/F=FAR FACE B/F=BOTH FACES	Scale N.T.S.	Ph: (03) 6267 9261	Mark No
BOXED BY		5.0T FACE	5.0T EDGE	Ø16 NEAR FACE	COLOUR CODE	HAIRPIN	COVER	Drawn J. Long	Fax: (03) 6267 9810	No
DATE BOXED		9.0T FACE	9.0T EDGE	Ø16 FAR FACE	GOLD	28 DAY GRADE	40 Mpa	Reviewed -	W.B. 0427 67 9261	Remarks
STEELED BY		PROP POINT		Ø12 NEAR FACE		40 Mpa	40 mm	Plot Date 09:27:06	H.W. 0418 122 534	1
QA BY		NEAR FACE	FAR FACE	Ø12 FAR FACE				10/05/2013	E mail; hugh.winslow@theprecasters.com.au	AS DRAWN
POURED DATE									E mail; wayne.bell@theprecasters.com.au	DRG No 2640-03
									web; www.theprecasters.com.au	Rev 0

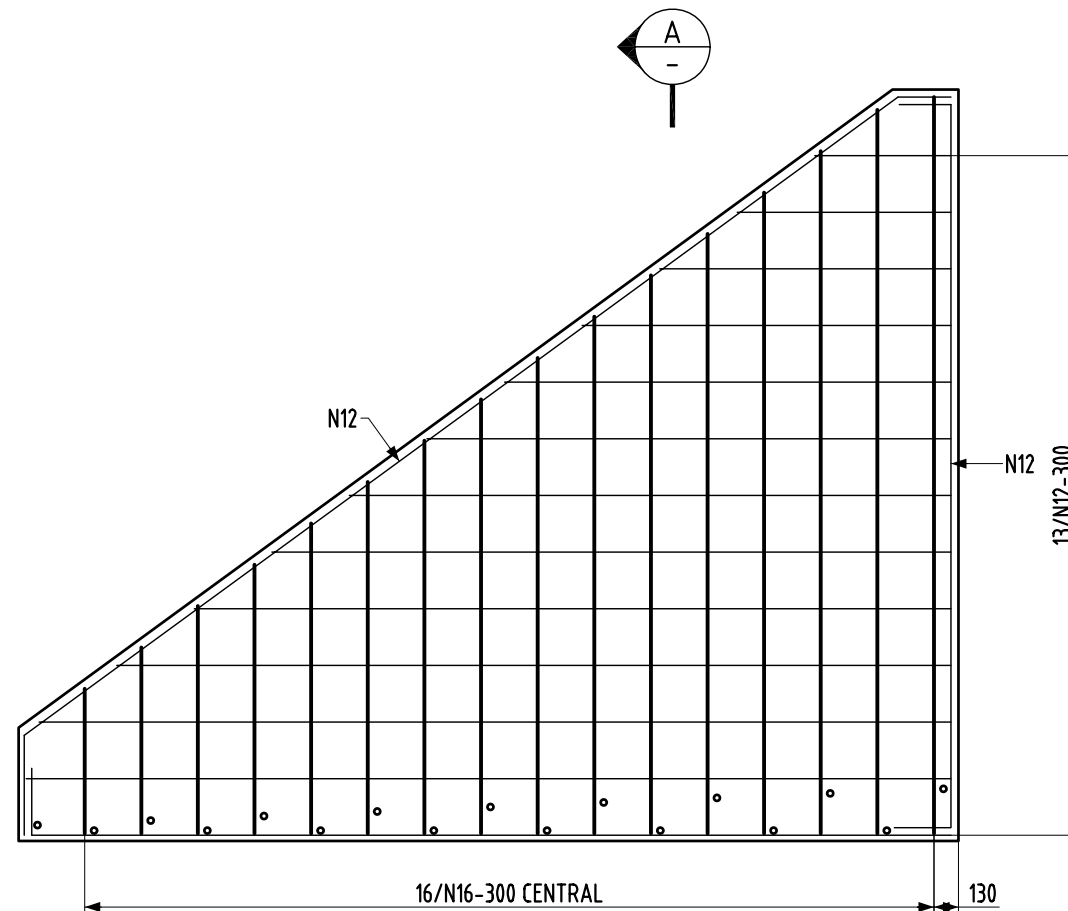
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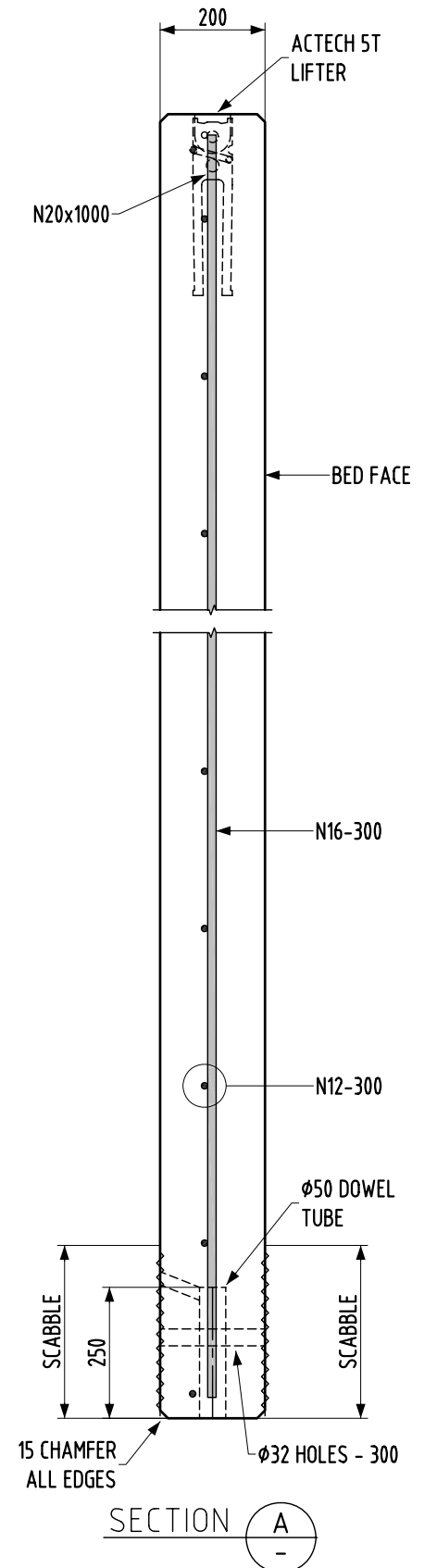
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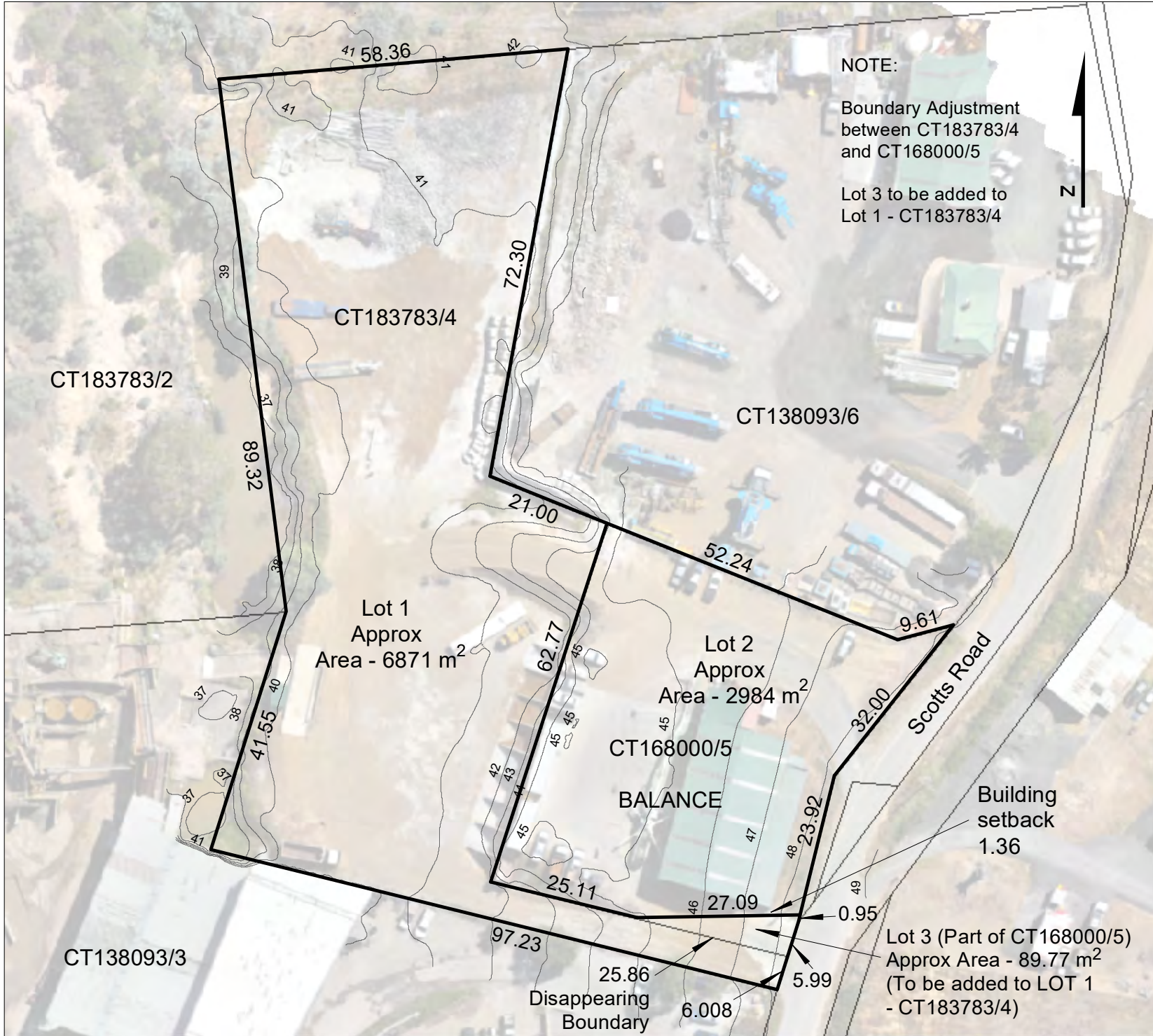
REINFORCEMENT REQUIRED



APPROVAL	10-5-13	FACE LIFTERS	EDGE LIFTERS	FERRULES	TOP FACE FINISH	VOLUME	MASS	NOTE; TOP FACE = N/F BED FACE = F/F	THE PRECASTERS P/L P.O. BOX 44 MARGATE, TAS. 7054	Proj. GRAVEL BINS 44 SCOTTS ROAD RISDON VALE
REINFORCEMENT	10-5-13	1.3T FACE	1.3T EDGE	Ø20 NEAR FACE	CLASS 2 VISIBLE	2.40 M ³	6.00 T			
WORKSHOP		2.5T FACE	2.5T EDGE	Ø20 FAR FACE	CONCRETE COLOUR	LIFTER TYPE	ABBREVIATIONS	Scale	Ph: (03) 6267 9261	Fax: (03) 6267 9810
BOXED BY		5.0T FACE	5.0T EDGE	Ø16 NEAR FACE	GREY	ACTECH HAIRPIN	N/F=NEAR FACE F/F=Far Face B/F=BOTH FACES	Drawn	W.B. 0427 67 9261	H.W. 0418 122 534
DATE BOXED		9.0T FACE	9.0T EDGE	Ø16 FAR FACE	COLOUR CODE	28 DAY GRADE	COVER	Reviewed	E mail; hugh.winslow@theprecasters.com.au	
STEELED BY		PROP POINT		Ø12 NEAR FACE	GOLD	40 Mpa	40 mm	Plot Date	E mail; wayne.bell@theprecasters.com.au	
QA BY		NEAR FACE	FAR FACE	Ø12 FAR FACE				10/05/2013	web; www.theprecasters.com.au	
POURED DATE									DRG No	Rev
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NOTE:
 Boundary Adjustment
 between CT183783/4
 and CT168000/5
 Lot 3 to be added to
 Lot 1 - CT183783/4



CT183783/4 &
 CT168000/5

PLAN OF
 SUBDIVISION

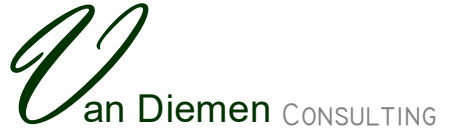
Figure 3: Plan
 of Subdivision

TASMAP:
 RICHMOND
 5226

LGA:
 CLARENCE

NOTE:
 SHOULD THIS PLAN BE UTILISED FOR
 CONTRACTUAL PURPOSES FINAL SURVEY
 DIMENSIONS SHALL PREVAIL. THIS PLAN HAS
 BEEN PREPARED ONLY FOR THE PURPOSE
 OF OBTAINING PRELIMINARY SUBDIVISION
 APPROVAL FROM THE LOCAL AUTHORITY AND
 IS SUBJECT TO THAT APPROVAL. ALL
 MEASUREMENTS ARE IN METRES AND SQUARE
 METRES UNLESS OTHERWISE STATED AND
 ARE SUBJECT TO FINAL SURVEY. DO NOT
 USE THIS PLAN FOR BUILDING DESIGN OR
 ANY OTHER PURPOSE APART FROM OBTAINING
 PRELIMINARY SUBDIVISION APPROVAL.

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DATUM:\n
 GDA94
 MGA Zone55
 CLIENT:
 KENT MICHAEL NOBLE
 ANITA LOUISE WEDD

DATE: 23 MAR 2025

TRAFFIC IMPACT ASSESSMENT

PROPOSED TITLE BOUNDARY ADJUSTMENT AND RETROSPECTIVE USE OF SITE ACCESS AT 46 SCOTTS ROAD

APPLICANT: NOBLE CONCRETE PUMPING PTY LTD

JUNE 2025

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Document Status

REV	Author	Review	Date
1	S Ineshka BSc Eng (Hons), MIEAust	Dr R Barnes BSc (Hons) PhD GDURP MPIA EIANZ MESA	09.04.2025
2	S Ineshka BSc Eng (Hons), MIEAust	R Barnes GDURP MPIA EIANZ MESA	19.06.2025

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ACRONYMS

RMS	Roads and Maritime Services, New South Wales Government
LUPAA	the <i>Land Use Planning and Approvals Act 1993</i>
TIA	Traffic Impact Assessment
SISD	Safe Intersection Sight Distance
VPD	Vehicles Per Day
AADT	Annual Average Daily Traffic (means the number of vehicles per day averaged over all days in a calendar year)

GLOSSARY OF TERMS

(the) Report	Traffic Impact Assessment Report
(the) Proponent	Noble Concrete Pumping Pty Ltd
Noble Concrete Pumping	Noble Concrete Pumping Pty Ltd
PLANNING SCHEME	Tasmanian Planning Scheme
A TRIP	A one-way vehicular movement from one point to another
VPD	Vehicles Per Day

1. INTRODUCTION

Noble Concrete Pumping Pty Ltd proposes to change the existing property access at '46 Scotts Road' to accommodate a proposed title boundary adjustment between '44 Scotts Road' and '46 Scotts Road'. The new access will be positioned further north along Scotts Road and will be a retrospective use.

This Traffic Impact Assessment (TIA) has been prepared to assist in the assessment of the proposed title boundary adjustment and to assess the suitability of proposed property access.

This TIA consists of the following.

- An evaluation of the current traffic using the road network and existing uses of the properties
- An examination of road and other related conditions along the Scotts Road and the vicinity
- An assessment of the suitability of proposed access (retrospective use) to '46 Scotts Road', including the site distances

The TIA has been prepared to meet the requirements outlined in Austroads Part 12: Integrated Transport Assessments for Developments and is based on Traffic Impact Assessment guidelines of the Department of State Growth, utilizing the following information and resources:

- Department of State Growth Crash database
- Land Information Database (LIST)
- Australian Standards AS2890.1_Off-street Car parking
- Updated Traffic Surveys, Guide to Traffic Generating Developments_Roads and Maritime Services_NSW

2. SITE DESCRIPTION

For the purpose of this TIA, the site encompasses two adjacent properties, 44 Scotts Road and 46 Scotts Road, respectively. Both properties are zoned as light industrial according to the LIST database and are owned by Noble Concrete Pumping. The site is situated approximately 325 meters from the intersection of Grasstree Hill Road and Scotts Road and is accessed via Scotts Road.



Figure 2.1: Locality Map

2.1. 44 Scotts Road

44 Scotts Road is a parcel spanning approximately 6,871 square meters, primarily used as a contractor's yard (storage) by Noble Concrete Pumping. Gravel and landscape supplies will be stored in open bins, and also firewood splitting at the north-western edge of the property. This property is usually accessed by single-unit trucks and light vehicles for deliveries and pickups. The largest vehicle that may access the site at any time will be a prime mover with a semi-trailer combination, although such occurrences are rare.

2.2. 46 Scotts Road

46 Scotts Road covers approximately 3,074 square meters and houses the Noble Concrete Pumping offices, parking facilities, and the main shed for concrete pumping trucks. This property is typically accessed by concrete pumping trucks and light vehicles. It may also occasionally receive visits from prime movers with a semi-trailer, but such instances are infrequent.

2.3. Property Accesses

The property accesses for 44 Scotts Road and 46 Scotts Road are located next to each other and currently use a shared gate; there is no noticeable delineation where the current property boundaries lie.



Figure 2.2: Current Property Accesses

3. DEVELOPMENT PROPOSAL

Noble Concrete Pumping proposes to adjust the title boundaries between 44 Scotts Road (CT 183783/4) and 46 Scotts Road (CT 168000/5), transferring approximately 90 square metres from 46 Scotts Road to 44 Scotts Road. Additionally, the property access for 46 Scotts Road will be shifted approximately 35 metres north. The new property access will utilise a retrospective approach for 46 Scotts Road.

However, there will not be any changes to the current uses of both properties.



Figure 3.1: Plan View of the Site

Figure 3.2 shows an image of the proposed access for 46 Scotts Road.



Figure 3.2: Proposed Property Access to 46 Scotts Road

4. TRAFFIC GENERATION BY THE DEVELOPMENT PROPOSAL

A trip in this report is defined as a one-way vehicular movement from one point to another, excluding the return journey. Consequently, a return trip to and from a property will be counted as two trips or two vehicular movements.

The proposal includes a title boundary adjustment between 44 Scotts Road and 46 Scotts Road, with the property access for 46 Scotts Road being shifted approximately 35 metres north. However, there are no proposed changes to the uses of the properties. Therefore, the volume of traffic generated by the properties after the development is expected to remain the same as before.

4.1. Current Trips generated by 44 Scotts Road

Typically, information about trips generated from a development is sourced from the RMS (Roads and Maritime Services) Guide to Traffic Generating Developments, which includes traffic survey records. However, these records do not contain information on a development or use similar to that at 44 Scotts Road. Therefore, the trip data for this property was requested by the director of Noble Concrete Pumping and is detailed in Table 4.1. The number of trips has been estimated based on past sales records and historical knowledge of the property's use.

Single-unit trucks visit the property to deliver or pick up gravel, landscape supplies, or firewood. The frequency of delivery trucks is typically no more than twice a month. In contrast, trucks collecting materials generally make three to four loads per day, resulting in six to eight trips daily. Additionally, a service truck may visit when heavy machinery, such as a front-end loader, requires servicing.

The maximum number of heavy vehicles anticipated on-site at any one time is not expected to exceed two. However, the parcel at 44 Scotts Road comprises over 6,600 m² of space available for vehicle manoeuvring and can safely accommodate up to five or six heavy vehicles simultaneously if required. That said, such an occurrence is considered highly unlikely under normal operating conditions. One employee may present at the site, with working hours from 7:30 AM to 4:30 PM during weekdays. The site remains non-operational on weekends and public holidays. Furthermore, light vehicles may occasionally visit for purposes such as weighbridge calibration, site maintenance, weed spraying, and other activities.

Vehicle Type	Worst case scenario		Based on average day	
	Daily	Peak Hour	Daily	Peak Hour
Heavy Vehicles	10	4	8	2
Light Vehicles	6	4	4	2
Total	16	8	12	4

Table 4.1: Current Trips Generated by 44 Scotts Road

4.2. Current Trips generated by 46 Scotts Road

46 Scotts Road operates with a maximum of six employees on-site at any given time, comprising four truck drivers and two office staff members. This staffing configuration typically results in 12 trips (twelve light vehicle movements) daily, accounting for the arrival and departure of each employee. Additionally, visitors occasionally contribute to the traffic flow, though their visits are not regular.

The site is home to four concrete pumping trucks, each operated by one of the drivers. These drivers depart the site around 6:30 AM and do not return until the end of the workday at approximately 5:30 PM, thus contributing to 8 trips daily (eight heavy vehicle movements). It is important to note that the trucks are away from the site throughout the day, reducing the potential for mid-day traffic.

Service trucks visit the site occasionally to perform maintenance and servicing on the concrete pumping trucks. The frequency of these visits can vary, but they are planned to minimize disruption and are not a daily occurrence.

Concrete trucks are parked inside the shed overnight and do not occupy any of the concrete hardstand area designated for vehicle manoeuvring. Typically, each truck parks in the shed upon returning at the end of the day, allowing subsequent trucks to enter and park safely. The maximum number of heavy vehicles anticipated on-site at any one time does not exceed two, except during morning departures and evening returns of the concrete trucks. No other heavy vehicle arrivals are expected during these peak movement periods. The existing concrete hardstand area is sufficiently large to accommodate at least two heavy vehicles simultaneously, without requiring access to the shed area. Refer to Drawing No. 1008/102 in the Attachments for detailed site dimensions.

Table 4.2 tabulated the estimated number of trips generated by 46 Scotts Road.

Vehicle Type	Worst case scenario		Based on average day	
	Daily	Peak Hour	Daily	Peak Hour
Heavy Vehicles	10	6	8	4
Light Vehicles	14	8	12	6
Total	24	14	20	10

Table 4.2: Current Trips Generated by 46 Scotts Road

4.3. Future Trips generated by 44 Scotts Road and 46 Scotts Road

There are no changes to the uses of both properties after the proposed development. Therefore, the traffic generation after the proposed development is expected to remain the same as detailed in Sections 4.2 and 4.3.

5. EXISTING ROAD NETWORK AND TRAFFIC CONDITIONS

All vehicles must use Grasstree Hill Road to access the development site, subsequently turning onto Scotts Road, which provides direct access to the two properties. Scotts Road is maintained by the local council (Clarence City Council) and serves as a connector to Grasstree Hill Road, which is classified as a category 5 state growth road. Grasstree Hill Road is designated as a category 5 road within the state road hierarchy, primarily facilitating local traffic flows and connecting routes to major roads. Grasstree Hill Road links with the East Derwent Highway, a category 3 road. Category 3 roads are crucial as they connect important towns to the primary and secondary roads in Categories 1 and 2, facilitating significant regional traffic movements.

5.1. Scotts Road Characteristics

Scotts Road extends approximately 920 metres from Grasstree Hill Road, terminating at a private property. The initial 420 metres are sealed with a 6-metre-wide carriageway, transitioning to an unsealed road for the remaining distance. The road is marked with a 50 km/h speed limit at its commencement. The alignment of Scotts Road is generally flat, although there is a slight slope at the junction with Grasstree Hill Road.



Figure 5.1: Typical Road Standard of Scotts Road

5.2. Grasstree Hill Road Characteristics

Grasstree Hill Road is classified as a category 5 road within the State Road Hierarchy, connecting Richmond Road and East Derwent Highway over a distance of approximately 12.8 km. The road features an 80 km/h speed limit at the intersection with Scotts Road. Constructed to high standards, Grasstree Hill Road has a sealed

carriageway that is 8 metres wide, which includes two 3-metre-wide traffic lanes and 1-metre-wide shoulders on both sides. This configuration facilitates safe and efficient traffic flow, accommodating both local and through traffic.



Figure 5.2: Typical Road Standard of Grasstree Hill Road

5.3. Junction of Grasstree Hill Road and Scotts Road

All traffic generated by the development site must turn at the junction of Grasstree Hill Road and Scotts Road. The Scotts Road intersects Grasstree Hill Road at approximately ninety degrees forming a standard T-junction. Traffic priority for Grasstree Hill Road motorists is enforced with the provision of a Give Way sign, supplemented with a marked holding line set back two metres from the edge of the through traffic lane.

The junction features an asphalt surface in good condition, with the junction throat widened to accommodate the swept path of heavy rigid vehicles. Scotts Road provides sufficient width to allow heavy rigid vehicles to enter and exit simultaneously.

The vertical grade at Scotts Road features a slight slope towards the north, and the road alignments are generally straight at this junction. The slope of Scotts Road is gentle enough not to adversely affect traffic movements.

Sight distances at the junction are approximately 185 metres towards the east and 700 metres towards the west, which meet the Safe Intersection Sight Distance (SISD) requirements for the posted speed limit of 80 km/h. These requirements are based on an observation time of 3 seconds and a reaction time of 2.0 seconds, equating to a necessary distance of 180 metres.

Additionally, a 110-metre section west and a 160-metre section east of the junction have been widened to facilitate a Channelised Right Turn (CHR) treatment to Sugarloaf Road, located about 75 metres east of the junction



Figure 5.3: Plan View of Scotts Road and Grasstree Hill Road Junction



Figure 5.4: Scotts Road and Grasstree Hill Road Junction

5.4. Traffic Conditions

Traffic data for Grasstree Hill Road was obtained from the Department of State Growth's traffic data portal. The closest monitoring station is located approximately 420 metres west of the junction between Scotts Road and Grasstree Hill Road. The most recent survey, conducted in 2023, recorded an Average Annual Daily Traffic (AADT) of 5,091 vehicles per day (vpd), with peak hours identified as 7:30 AM to 8:30 AM in the morning and 3:30 PM to 4:30 PM in the evening. During these peak periods, traffic volume can reach up to 584 vehicles per hour. Outside of peak hours, traffic volume varies between 300 and 400 vehicles per hour.

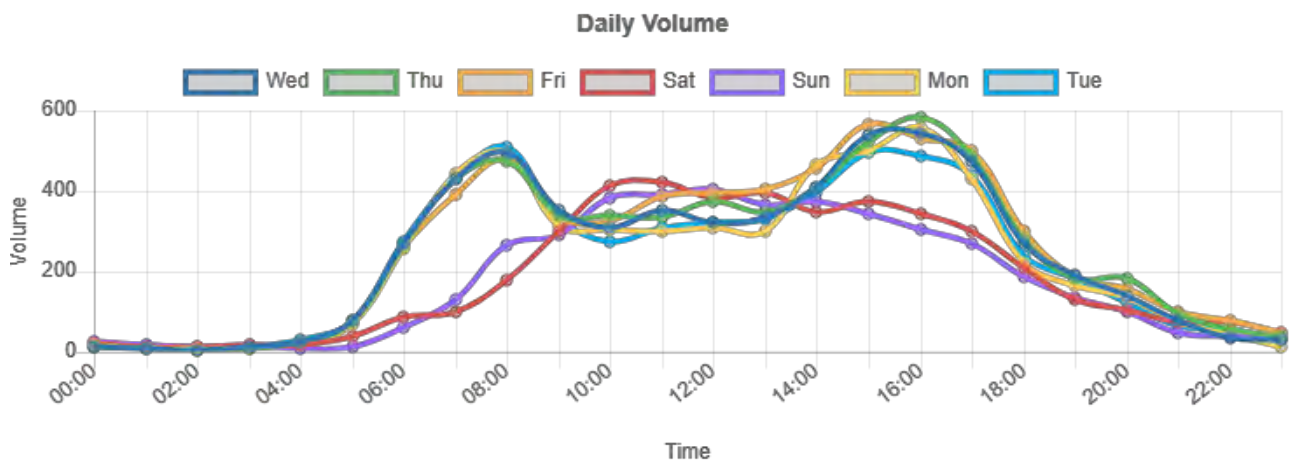


Figure 5.5: Daily Traffic Variation at Grasstree Hill Road

No publicly available traffic data exists for Scotts Road. Given that the proposed development does not alter traffic generation from existing uses, it is determined that there is no impact on the junction. Consequently, a manual traffic count was deemed unnecessary.

5.5. Impact on Scotts Road and Grasstree Hill Road Junction from the proposed development

As discussed in Section 4 of this report, there will be no change in traffic generation from and after the proposed development works. Therefore, it can be concluded that there will be no adverse impacts on the Scotts Road and Grasstree Hill Road junction due to the proposed development.

5.6. Crash Records

Information was requested from the Department of State Growth regarding reported crashes along Scotts Road and its vicinity. Over the past five years, four crash incidents have been reported, although none occurred directly on Scotts Road. Three of these crashes took place at the junction of Sugarloaf Road and Greentree Hill, with two occurring during nighttime.

Additionally, one crash was recorded at the junction of Scotts Road and Greentree Hill Road, involving a light vehicle veering off the carriageway into a parked vehicle or object.

None of the recorded crashes has been related to the operations at the development site.

6. RESTROSPECTIVE ASSESSMENT OF PROPOSED DRIVEWAY LOCATION

6.1. Safe Intersection Sight Distance

Investigations were conducted on Scotts Road to evaluate the adequacy of the available sight distances at the proposed driveway location. Measurements were taken from a point 3 metres back from the edge of the traffic lane, showing sight distances of approximately 105 metres to the north and 250 metres to the south. These distances are consistent for both directions, applying to vehicles waiting to turn right into the access road.

The speed limit on Scotts Road is 50 km/h, enforced from a sign at the road's commencement. According to Austroads Guide to Road Design Part 4A, the required sight distance for a design speed of 50 km/h, assuming an observation time of 3 seconds and a reaction time of 2 seconds, is 97 metres.

Therefore, the sight distances at the proposed driveway location on Scotts Road are determined to be more than sufficient for safe vehicle operations.



Figure 6.1: Available Sight Distance to the left



Figure 6.2: Available Sight Distance to the right

6.2. Swept Path Analysis at the driveway

Swept paths of vehicles entering and exiting the site were analysed using AutoCAD Vehicle Tracking to evaluate the suitability of the proposed driveway. The analysis was undertaken for both a rigid vehicle (design vehicle) and a prime mover with a semi-trailer (checking vehicle), assuming a 6-second lock to lock time, a 10 km/h design speed, and a 12.5m kerb-to-kerb turning radius.

It was determined that the vehicles can safely manoeuvre in and out through the proposed driveway. Refer to Attachment 1 for detailed diagrams.

6.3. Conclusion of Retrospective Assessment

Currently, the two driveways for 44 Scotts Road and 46 Scotts Road are located next to each other and share the space for entering and exiting the properties. Given that both properties are frequented by heavy vehicles, there is a potential risk of traffic collisions due to the limited space. Swept path analysis conducted at the existing driveways indicates that heavy vehicles must encroach on adjacent properties to safely enter the sites. The proposed title boundary adjustment and the relocation of the driveway for 46 Scotts Road are expected to eliminate the risk of vehicle collisions and provide adequate room for safely manoeuvring into the driveway at 44 Scotts Road. It was also noted that there are sufficient Safe Intersection Sight Distances (SISDs) for the new driveway location at 46 Scotts Road from both directions. Additionally, it should be noted that Scotts Road serves only two more properties to the north beyond 46 Scotts Road, which minimises the number of southbound vehicles at the proposed driveway.

Therefore, it can be concluded that relocating the driveway of 46 Scotts Road will enhance both the safety and efficiency of vehicle movements in and out of the properties.

7. OTHER CONSIDERATIONS

7.1. Car Parking

Ten (10) car parking spaces have been proposed to construct as a part of the development. Refer to the Drawing No. 1008/302 in the Attachment 1 for detailed information.

8. PLANNING SCHEME

According to Table 6.2 in the Tasmanian Planning Scheme (TPS), the designated uses of both properties are categorized as 'storage'. The property at '44 Scotts Road' is currently used for the storage of gravel, landscape materials, and firewood. In contrast, '46 Scotts Road' is primarily used as a contractor's yard for concrete pumping trucks and includes an office.

However, the proposed development works are focused solely on 46 Scotts Road only. Apart from the title boundary adjustment, there are no changes to the use or proposed works on 44 Scotts Road.

Therefore, the proposed development works on 46 Scotts Road have been assessed below in accordance with Code 2.0- Parking and Sustainable Transport Code, and Code 3.0- Road and Railway Assets Code, as outlined under the TPS.

8.1. Parking and Sustainable Transport Code- C2.5 Use Standards

<p>C2.5.1 Car Parking Numbers</p> <p>Objective: That an appropriate level of car parking spaces are provided to meet the needs of the use.</p>	
<p>PERFORMANCE CRITERIA P1.1</p> <p>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the availability of off-street public car parking spaces within reasonable walking distance of the site; (b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> (i) variations in car parking demand over time; or (ii) efficiencies gained by consolidation of car parking spaces; (c) the availability and frequency of public transport within reasonable walking distance of the site; (d) the availability and frequency of other transport alternatives; (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping; (f) the availability, accessibility and safety of on street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; (g) the effect on streetscape; and (h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development. 	<p>ASSESSMENT</p> <p>The maximum number of employees at any time is limited to six, and there are no changes to this number anticipated in the foreseeable future. Ten parking spaces are proposed, which includes four extra spaces for visitors, although visits by third parties are infrequent.</p> <p>There is no dedicated off-street car parking available in the vicinity; however, such a necessity has never arisen before.</p> <p>Therefore, ten parking spaces are sufficient for the current use and will not cause any parking overflow outside of the property.</p>
<p>C2.5.2 Bicycle Parking Numbers</p> <p>Objective: That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.</p>	
<p>ACCEPTABLE SOLUTION A1</p> <p>Bicycle parking spaces must:</p>	<p>ASSESSMENT</p>

<p>(a) be provided on the site or within 50m of the site; and (b) be no less than the number specified in Table C2.1.</p>	<p>There is no requirement of bicycle parking spaces as per Table C2.1 in TPS.</p>
<p>C2.5.3 Motorcycle Parking Numbers Objective: That the appropriate level of motorcycle parking is provided to meet the needs of the use. NOT APPLICABLE</p>	
<p>C2.5.4 Loading Bays Objective: That adequate access for goods delivery and collection is provided, and to avoid unreasonable loss of amenity and adverse impacts on traffic flows.</p>	
<p>PERFORMANCE CRITERIA P1 Adequate space for loading and unloading of vehicles must be provided, having regard to: (a) the type of vehicles associated with the use; (b) the nature of the use; (c) the frequency of loading and unloading; (d) the location of the site; (e) the nature of traffic in the surrounding area; (f) the area and dimensions of the site; and (g) the topography of the site; (h) the location of existing buildings on the site; and (i) any constraints imposed by existing development.</p>	<p>ASSESSMENT The main heavy vehicle type entering and leaving the site are Concrete Pumping Trucks. The existing main shed is used to park concrete pumping trucks overnight. Even though loading and unloading is not major part of the nature of the operation, there is approximately 25mx26m concrete hardstand area is available for any unloading or loading purposes. Therefore, an adequate space for any loading/unloading purposes are available on site.</p>
<p>C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone Objective: To: (a) facilitate the reuse of existing non-residential buildings within the General Residential Zone and Inner Residential Zone; and (b) to not cause an unreasonable impact on residential amenity by the car parking generated by that reuse. NOT APPLICABLE</p>	

8.2. Parking and Sustainable Transport Code- C2.6 Development Standards

<p>C2.6.1 Construction of Parking Areas</p> <p>Objective: That parking areas are constructed to an appropriate standard.</p>	
<p>ACCEPTABLE SOLUTION A1</p> <p>All parking, access ways, manoeuvring and circulation spaces must:</p> <p>(a) be constructed with a durable all weather pavement;</p> <p>(b) be drained to the public stormwater system, or contain stormwater on the site; and</p> <p>(c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.</p>	<p>ASSESSMENT</p> <p>(a) The parking spaces are constructed with an asphalt surfacing, ensuring a durable all-weather pavement as required.</p> <p>(b) Stormwater collected on the parking area is directed into an adjacent spoon drain, which subsequently flows into the public stormwater system, ensuring proper drainage as stipulated.</p> <p>(c) The parking areas are surfaced with a minimum of 35mm thick AC10 asphalt layer. This surfacing meets the requirements to restrict abrasion from traffic and minimise the entry of water into the pavement.</p>
<p>C2.6.2 Design and Layout of Parking Areas</p> <p>Objective: That parking areas are designed and laid out to provide convenient, safe and efficient parking.</p>	
<p>ACCEPTABLE SOLUTION A1.1</p> <p>Parking, access ways, manoeuvring and circulation spaces must either:</p> <p>(a) comply with the following:</p> <p>(i) have a gradient in accordance with Australian Standard AS 2890- Parking facilities, Parts 1- 6;</p> <p>(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;</p> <p>(iii) have an access width not less than the requirements in Table C2.2;</p> <p>(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;</p> <p>(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;</p> <p>(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and</p> <p>(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or</p> <p>(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6.</p>	<p>ASSESSMENT</p> <p>Parking facilities are designed as per AS2890 – Parking Facilities, Parts 1.</p>

<p>ACCEPTABLE SOLUTION A1.2</p> <p>Parking spaces provided for use by persons with a disability must satisfy the following:</p> <p>(a) be located as close as practicable to the main entry point to the building;</p> <p>(b) be incorporated into the overall car park design; and</p> <p>(c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities</p>	<p>ASSESSMENT</p> <p>Dedicated parking space for a person with a disability deemed unnecessary for this development. Therefore, no disability parking spaces are provided.</p>
<p>C2.6.3 Number of Accesses for Vehicles</p> <p>Objective:</p> <p>That:</p> <p>(a) access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;</p> <p>(b) accesses do not cause an unreasonable loss of amenity of adjoining uses; and</p> <p>(c) the number of accesses minimise impacts on the streetscape.</p>	
<p>ACCEPTABLE SOLUTION A1</p> <p>The number of accesses provided for each frontage must:</p> <p>(a) be no more than 1; or</p> <p>(b) no more than the existing number of accesses, whichever is the greater.</p>	<p>ASSESSMENT</p> <p>(a) The number of accesses to the property will be one</p> <p>(b) The number of existing accesses to the property is one. The development proposal is to relocate the access to the property '46 Scotts Road' so that the safety and the efficiency will be increased</p>
<p>ACCEPTABLE SOLUTION A2</p> <p>Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.</p>	<p>NOT APPLICABLE</p>
<p>C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone</p> <p>Objective: That parking and vehicle circulation roads and pedestrian paths within the General Business Zone and Central Business Zone, which are used outside daylight hours, are provided with lighting to a standard which:</p> <p>(a) enables easy and efficient use;</p> <p>(b) promotes the safety of users;</p> <p>(c) minimises opportunities for crime or anti-social behaviour; and</p> <p>(d) prevents unreasonable light overspill impacts.</p> <p>NOT APPLICABLE</p>	

The property is zoned as light industrial. However, the operational times are generally between 6.30 AM to 5.30 PM, with works not occurring outside of these time frames. Security and surveillance lights are in place to cover the parking spaces and the access.

C2.6.5 Pedestrian Access

Objective: That pedestrian access within parking areas is provided in a safe and convenient manner.

PERFORMANCE CRITERIA P1

Safe and convenient pedestrian access must be provided within parking areas, having regard to:

- (a) the characteristics of the site;
- (b) the nature of the use;
- (c) the number of parking spaces;
- (d) the frequency of vehicle movements;
- (e) the needs of persons with a disability;
- (f) the location and number of footpath crossings;
- (g) vehicle and pedestrian traffic safety;
- (h) the location of any access ways or parking aisles; and
- (i) any protective devices proposed for pedestrian safety.

ASSESSMENT

- (a) The site is primarily flat, facilitating easy pedestrian movement. The existing infrastructure ensures clear line of sights for both pedestrians and drivers.
- (b) The site is primarily used to park Concrete Pumping Trucks overnight and undertake minor services inside the shed.
- (c) Ten car parking spaces are provided in close proximity to the shed and office area.
- (d) Vehicular movements are higher at the start and end of the day, generally due to Concrete Trucks leaving in the morning and returning in the evening. During the rest of the day, vehicular movements are typically fewer than 2 trips per hour.
- (e) Currently, there are no designated parking spaces for persons with disabilities; this is because the site does not receive public traffic and is primarily an operational industrial area.
- (f) Due to the size of the property and the available clear line of sights, no dedicated footpaths are provided
- (g) No pedestrians are expected to enter the site by foot. All personnel are expected to arrive by vehicle and will report to the site office, which is approximately 25 metres from the parking area. The site has ample space to safely manoeuvre vehicles without adversely impacting pedestrian safety..
- (h) Parking spaces are strategically placed to facilitate easy navigation for vehicles while ensuring sufficient space for pedestrian access and emergency egress.
- (i) Surveillance cameras and adequate lighting are installed to enhance safety during hours of darkness.

The parking area design at the site effectively meets the requirements set forth in Performance Criteria P1, ensuring safe and convenient pedestrian access. While the current configuration adequately addresses the operational needs of the site, it also

	ensures the safety and convenience of all site personnel.
<p>C2.6.6 Loading Bays</p> <p>Objective: That the area and dimensions of loading bays are adequate to provide safe and efficient delivery and collection of goods.</p> <p>NOT APPLICABLE</p> <p>Loading bays are not considered necessary for this site and ample area is available for any loading or unloading activity as described under C2.5.4.</p>	
<p>C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone</p> <p>Objective: That parking for bicycles are safe, secure and convenient, within the General Business Zone and Central Business Zone.</p> <p>NOT APPLICABLE</p> <p>The site is zoned as light industrial.</p>	
<p>C2.6.8 Siting of parking and turning areas</p> <p>Objective: That the siting of vehicle parking and access facilities in an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone or Central Business Zone does not cause an unreasonable visual impact on streetscape character or loss of amenity to adjoining properties.</p> <p>NOT APPLICABLE</p>	

8.3. Road and Railway Assets Code- C3.5 Use Standards

<p>C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction</p> <p>Objective: To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.</p>	
<p>PERFORMANCE CRITERIA P1</p> <p>Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</p> <ul style="list-style-type: none"> (a) any increase in traffic caused by the use; (b) the nature of the traffic generated by the use; (c) the nature of the road; (d) the speed limit and traffic flow of the road; (e) any alternative access to a road; (f) the need for the use; (g) any traffic impact assessment; and (h) any advice received from the rail or road authority. 	<p>ASSESSMENT</p> <ul style="list-style-type: none"> (a) There is no increase in traffic caused by the proposed development works. The number of traffic generated by the use will remain same (b) The site generates both heavy and light vehicle movements. However, heavy vehicle movements are generally limited to morning and evening times, corresponding with Concrete Pumping Trucks leaving in the morning and returning in the evening. (c) The site is accessed via a sealed council road with a carriageway width of approximately 6 metres. All vehicles must use Grasstree Hill Road, which is a Category 5 road, to access the site. (d) The speed limit on Scotts Road is posted at 50 km/h. (e) There are no alternative accesses to any other road. (f) The use of the site is existing and ongoing.

	<p>(g) This independent Traffic Impact Assessment found no reason for this development not to proceed based on traffic impacts.</p> <p>(h) Aware of none.</p>
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CONCLUSION

This Traffic Impact Assessment was conducted in accordance with the relevant traffic engineering principles and guidelines, as detailed in Section 1 of this report.

Key findings include:

- The proponent proposes a title boundary adjustment between 44 Scotts Road and 46 Scotts Road and plans to shift the existing access to 46 Scotts Road further north.
- The development proposal does not change the use of the property; therefore, there is no anticipated change in traffic generation from either property.
- The nearest major road, Grasstree Hill Road, shows no adverse impacts at the junction with Scotts Road due to the proposed development.
- Over the past five years, there have been four reported crashes in the vicinity of the property, with only one occurring at the junction of Grasstree Hill Road and Scotts Road. This incident, caused by a light vehicle veering off the carriageway into a parked object, is not related to the existing uses of the properties.
- A retrospective assessment of the proposed access to 46 Scotts Road determined that the location meets Safe Intersection Sight Distance (SISD) requirements, and swept path analysis confirmed that the access is suitable for the safe manoeuvring of heavy vehicles, including prime movers with semi-trailer combinations.
- The proposal to relocate the access to 46 Scotts Road is expected to enhance vehicle safety by reducing the potential for collisions, given the existing road configuration.
- Ten car parking spaces are proposed, featuring asphalt surfacing to ensure a durable all-weather pavement.

In conclusion, the proposed development works are not expected to increase traffic generation but will enhance the safety and efficiency of traffic entering and leaving the site.