

PLEASE QUOTE

Your Ref:

Our Ref: DA 2025/55

Enquiries: Planning Department

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.au

We value your feedback on our service.
Tell us about it at www.burnie.tas.gov.au/feedback



NOTICE OF APPLICATION FOR LAND USE PERMIT

(Section 57(3) Land Use Planning and Approvals Act 1993)

Advice to Adjoining Land Owner or Occupier

Application No: - DA 2025/55
Development Site: - 100-106, 110 & 112 Wilson Street, 81, 83 & 85-89 Mount Street, & 23-25 Ladbroke Street BURNIE - CT: 187122/1, 102930/1, 121419/1, 23763/6, 127144/2. 127144/4, 102928/2 & 53017/1
Proposal: - Court Complex

Notice of the above application is served on you as an adjoining land owner or occupier.

The application may be viewed at -

**Burnie City Council Customer Services Counter
Ground Floor, City Offices,
80 Wilson Street, Burnie**

Between the hours of 8.45 am – 4.45 pm Monday to Friday inclusive (excluding public holidays) or on Council's website at www.burnie.tas.gov.au/permits

You are entitled to make representation in writing on any aspect of the proposal addressed to: -

**General Manager,
Burnie City Council,
PO Box 973, Burnie 7320**

or burnie@burnie.tas.gov.au by no later than 5.00 pm on **27 October 2025**. Council must have regard to any written representation received during the exhibition period when considering its decision on the application.

All persons who make representation will be notified within seven (7) days of the Council's decision. Any persons who made representation and is not satisfied with the Council decision may, under Section 61(5) of the *Land Use Planning and Approvals Act 1993*, lodge an appeal against that decision within fourteen (14) days of the date of that notice to: -

**The Tasmanian Civil and Administrative Tribunal,
GPO Box 1311,
HOBART TAS 7001.**

Should you have any enquiries regarding this development proposal, please do not hesitate to contact the Planning Department on (03) 6430 5700.

Troy McCarthy

PRINCIPAL PLANNER

Date of Notice: - **11 October 2025**

Land Use Planning and Approvals Act 1993

Tasmanian Planning Scheme

PERMIT APPLICATION

Office use only

Application No _____

Date Received _____

Permit Pathway - *Permitted/Discretionary*

Use or Development Site:

Street Address

100-106 Wilson Street, Burnie
81 Mount Street, 83 Mount Street, 85-89 Mount Street, 110 Wilson Street, 112 Wilson Street, 23-25 Ladbroke Street, Burnie.

Certificate of Title Reference

187122-1, 102930-1, 121419-1, 23763-6, 127144-2, 127144-4, 102928-2, 53017/1

Applicant

First Name

Andrew

Second Name

Surname

McCreary

Postal Address:

1st Floor, 125 Collins Street, Hobart

Phone No:

6224 9370

Mobile:

Email Address:

I/we consent for all giving of information and the serving of notices in relation to this application to be delivered electronically to the above email address?

YES



NO



Applicants Signature:

A. McCreary

Owner (note – if more than one owner, all names must be indicated)

First Name

Kristy

Second Name

Surname

Bourne

Postal Address:

Phone No:

Instruction for making a permit application

a) *Use or development?*

The application must provide a full description of the proposed use and/or development and of the manner in which the use and/or development is to operate.

“Use” is the purpose or manner for which land is utilised. “Development” is any site works (including any change in natural condition or topography of land and the clearing or conversion of vegetation), and the construction, alteration, or removal of buildings, structures and signs, required in order to prepare a site for use or to change existing conditions within a site. Subdivision is development.

Clause 6.2 Tasmanian Planning Scheme provides the use classes by which all use or development must be described. Development must be categorised by reference to the use class it is to serve.

b) *Required Information*

Adequate statements, plans and specifications must be included within the permit application to address and demonstrate compliance with all applicable requirements of the planning scheme, including any site analysis, impact report and recommendation, and advice, consent or determination required from a State agency or utility entity.

The application must clearly identify the documents relied upon for determination.

Section 51(1AC) *Land Use Planning and Approvals Act 1993* provides that a permit application is not valid unless it includes all of the information required by a planning scheme. Clause 6.1 Tasmanian Planning Scheme prescribes the minimum information that is necessary in order to complete a valid permit application.

S54 *Land Use Planning and Approvals Act 1993* provides that the planning authority may require the applicant to supply further information before it considers a permit application. If the planning authority requires further information to more particularly address one or more of the applicable requirements of the Tasmanian Planning Scheme, the statutory period for determination of a permit application does not run until that information is answered to the satisfaction of the planning authority

c) *Applicable Provisions and Standards*

The permit application must be assessed against the applicable provisions and standards of the Tasmanian Planning Scheme. The application is to identify by reference the clauses it relies upon to demonstrate compliance. (eg *clause 8.4.3 (A1 – A4, and P5)*)

d) *Discretionary Permits*

If a permit is discretionary the permit application must be notified for a period of 14 days to allow opportunity for any interested person to consider the proposed use and/or development and to provide comment on the discretionary matter.

If a permit application relies on performance criteria to satisfy an applicable standard or is discretionary under another provision of the interim planning scheme, the permit is discretionary only with respect to that standard.

The Council must have regard to all representations received during the notification period on a discretionary matter when determining whether to grant or refuse a permit.

e) *If the applicant is not the landowner*

If the applicant is not the owner of the land in the use or development site, the applicant is required to notify all of the owners either prior to or within 7 days from the date of making the permit application.

The permit application must identify all of the landowners; and the applicant must sign the application form to acknowledge the obligation to advise such landowners that the permit application has been made.

If the site includes land owned or administered by the Burnie City Council or by a State government agency, the consent in writing from the Council or the Minister responsible for Crown land must be provided at the time of making the application.

f) *Applicant declaration*

It is an offence for a person to do any act that is contrary to a compliance requirement created under the section 63 *Land Use Planning and Approvals Act 1993*. The applicant is required to complete a declaration that the information given in the permit application is true and correct.

g) *Payment of Fees*

The Council is not required to take any action on the permit application until all the relevant fees have been paid.

Permit Information

(NB If insufficient space, please attach separate document)

Proposed Use:**Use Class** Business and Professional Services**Documents included with the permit application to describe the Use**

Architectural Drawings
Civil Report and Building Services Drawings
Traffic Impact Assessment
Landscape Drawings

Proposed Development**Use class to which the development applies** Business and Professional Services**Documents included with the permit application to describe the Development**

Architectural Drawings
Civil Report and Building Services Drawings
Traffic Impact Assessment
Landscape Drawings

Provisions and Standards relied upon for grant of a Permit

Tasmanian Planning Scheme

Value of use and/or development

Notification of Landowner/s

If land is not in applicant's ownership

I, **Andrew McCreary**, declare that the owner/each of the owners of the land has been notified of the intention to make this permit application.

Signature of Applicant



Date 17/07/25

If the permit application involves land owned or administered by the BURNIE CITY COUNCIL

Burnie City Council consents to the making of this permit application.

General Manager (Signature)

Date

If the permit application involves land owned or administered by the CROWN

On behalf of the Minister responsible for the land, consent to the making of this permit application.

Secretary (Signature)



Date 24 July 2025

Applicant Declaration

I, **Andrew McCreary** declare that the information I have given in this permit application to be true and correct to the best of my knowledge.

Signature of Applicant



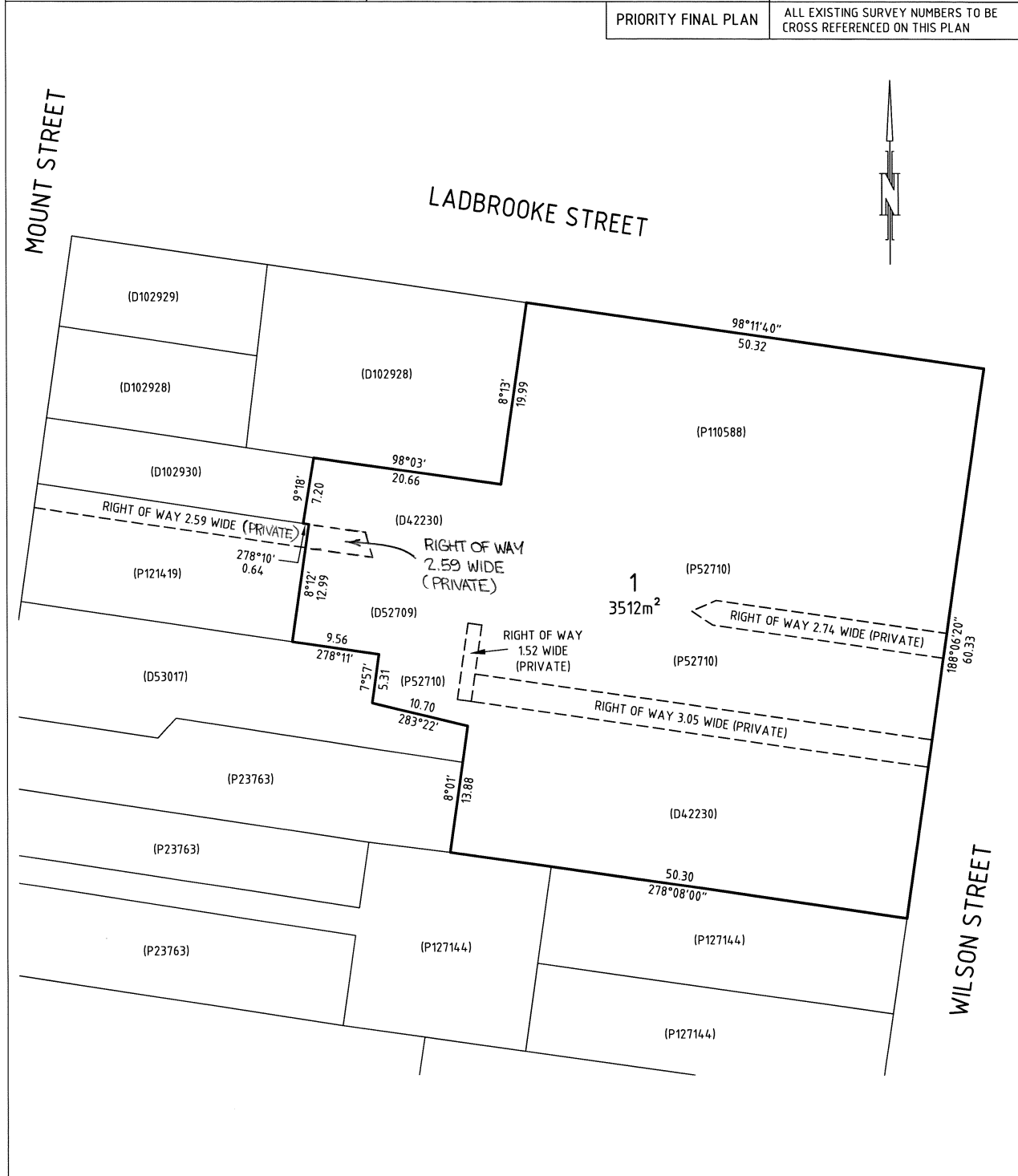
Date 17/07/25

Office use only



OWNERS: The Crown FOLIO REFERENCES: 110588/1, 110588/3, 42230/2, 52709/1, 52710/1, 52710/2, 52710/3, 52710/4, 52710/5, 52710/6, 52710/7, 42230/1 GRANTEE: Part of 50,000 acres Granted to the Van Diemens Land Company.	PLAN OF SURVEY BY SURVEYOR: JOHN E W MAGEE PDA SURVEYORS, ENGINEERS & PLANNERS	REGISTERED NUMBER SP187122
	LOCATION: CITY OF BURNIE SCALE 1: 400 LENGTHS IN METRES	APPROVED EFFECTIVE 27 AUG 2024 FROM Recorder of Titles

PRIORITY FINAL PLAN ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN



 Registered Land Surveyor	3/05/2024 Date	52348 Surveyor Ref	_____ Council Delegate	_____ Date
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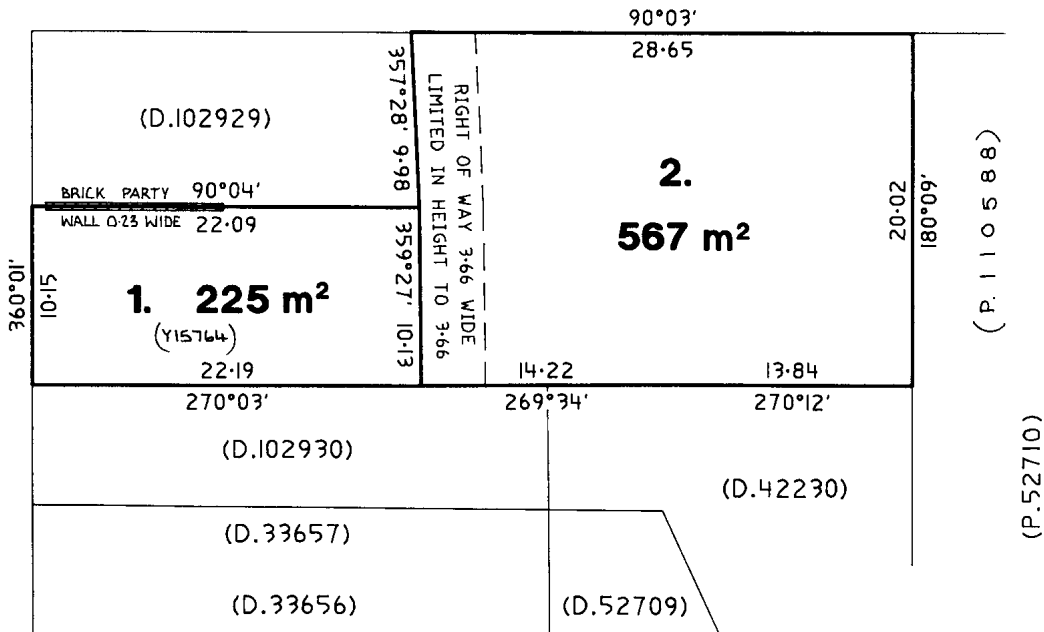
Owner: L.T.ACT 1980	<p>PLAN OF TITLE of land situated in the CITY OF BURNIE</p> <p>COMPILED FROM 32/3D.O.</p> <p>SCALE 1:300 MEASUREMENTS IN METRES</p>	Registered Number: D.102928
Title Reference: Y.15764		Approved: - 5 FEB 1993
Grantee: PART OF 50000-0-0 GTD. TO V.D.L. Co.		 Recorder of Titles

TASMAP MUNICIPAL CODE NO. 42	LAST TASMAP UPI NO. 3857
ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN	



LADBROOKE STREET

MOUNT STREET



M.S.B.
4.2.93

APPROVED - 5 FEB 1993 <i>Michael Dine</i> RECORDER OF TITLES	CONVERSION PLAN CONVERTED FROM 55/9026	REGISTERED NUMBER D.102930
FILE NUMBER Y.15766	GRANTEE: PART OF 50000-0-0 GTD. TO V.D.L. Co.	DRAWN M.S.B. 3.2.93

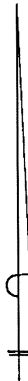
06-K 2002

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF **BURNIE**
LAND DISTRICT OF
PARISH OF

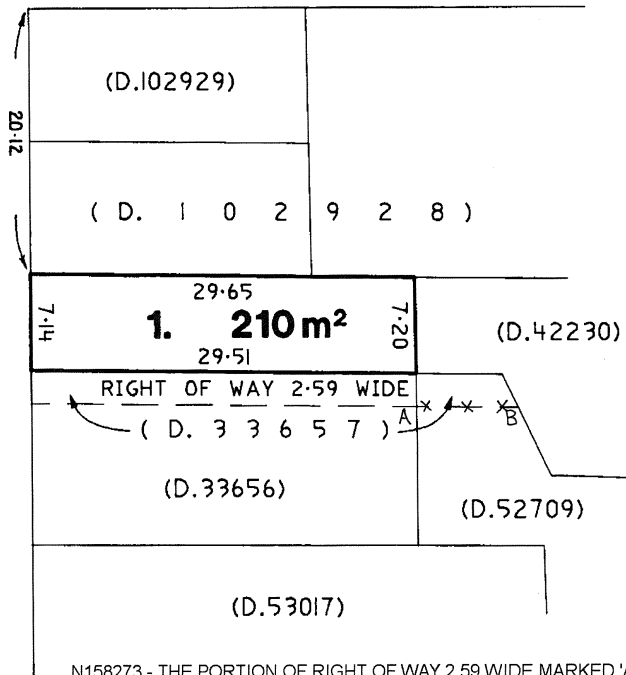
LENGTHS ARE IN METRES. NOT TO SCALE.
~~LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.~~

TASMAR MUNICIPAL CODE NO. 42	LAST TASMAR UPI NO. 3858
ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN	



LADBROOKE STREET

MOUNT STREET



N158273 - THE PORTION OF RIGHT OF WAY 2.59 WIDE MARKED 'AB' IS HEREBY
RELEASED PURSUANT TO SECTION 108 OF THE LAND TITLES ACT 1980

Renwick
RECORDER OF TITLES

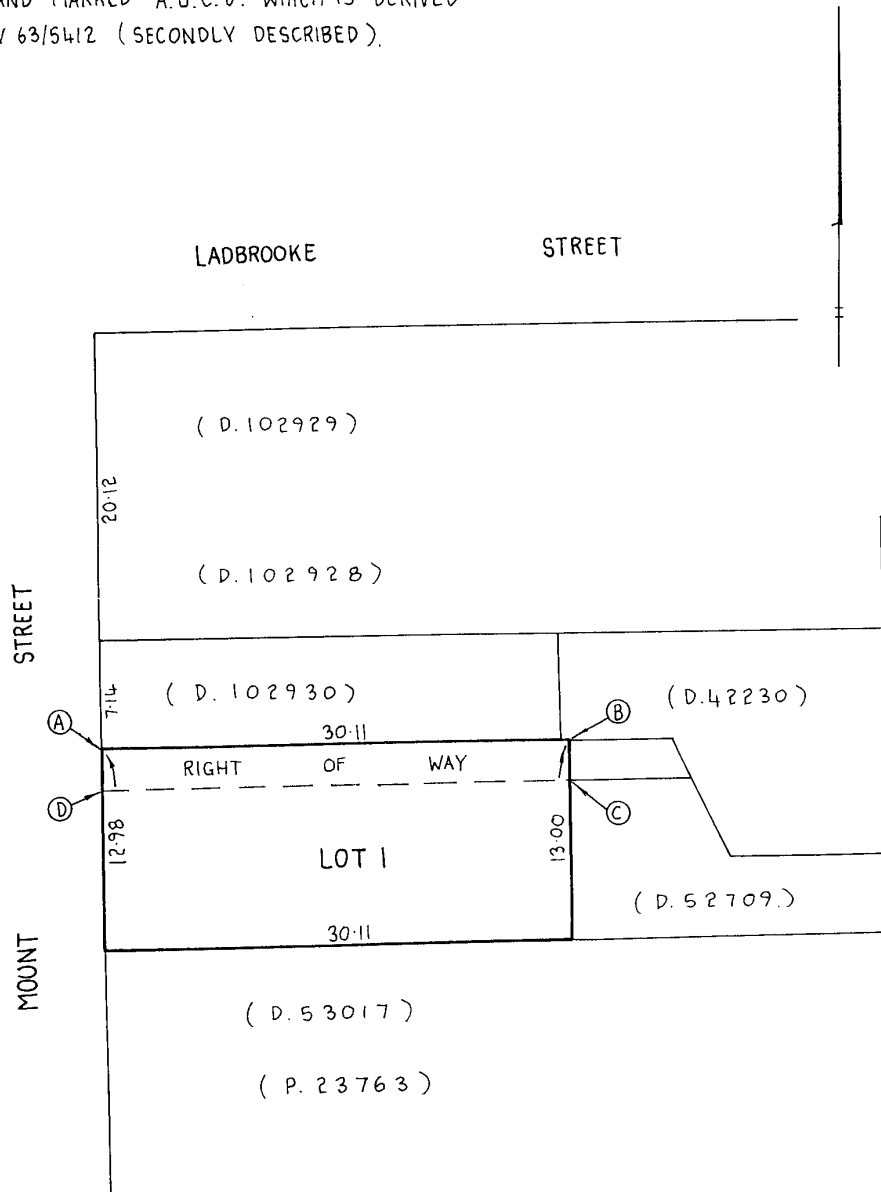
28/11/2023
DATE

OWNER		PLAN OF TITLE		REGISTERED NUMBER
FOLIO REFERENCE CT 33656/1 CT 33657/2				LOCATION
GRANTEE		CITY OF BURNIE		APPROVED 21 NOV 1995
		FIRST SURVEY PLAN No. 12/3 D.O.		<i>Michael Simi</i> Recorder of Titles
		COMPILED BY LTO		
		SCALE 1: 300		LENGTHS IN METRES

MAPSHEET MUNICIPAL CODE No. 103	LAST UPI No 4203859	LAST PLAN No. D.33656 & D.33657	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN
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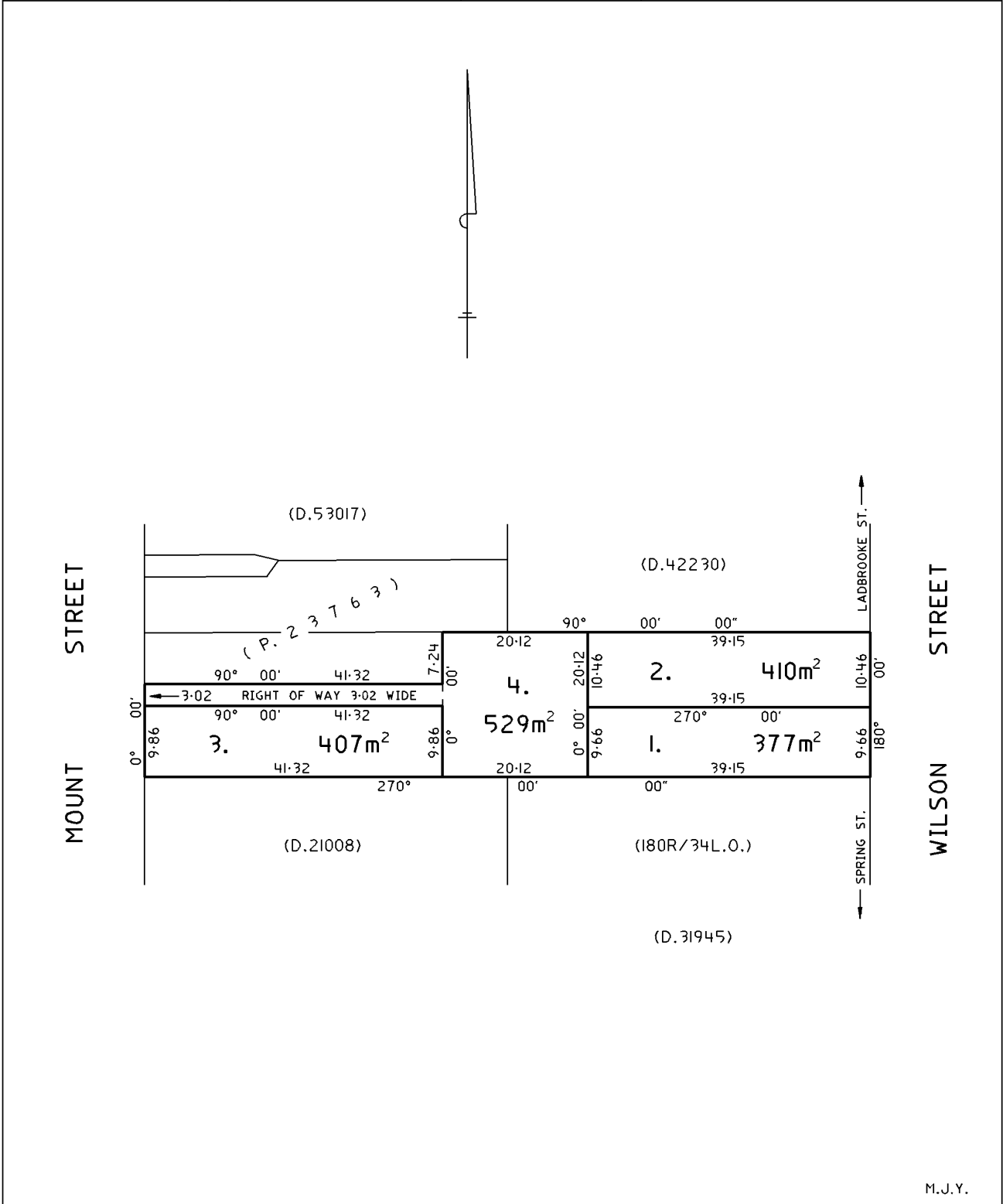
SKETCH BY WAY OF ILLUSTRATION ONLY AS RELATES TO THE LAND MARKED A.B.C.D. WHICH IS DERIVED FROM CONV 63/5412 (SECONDLY DESCRIBED).

BALANCE PLAN



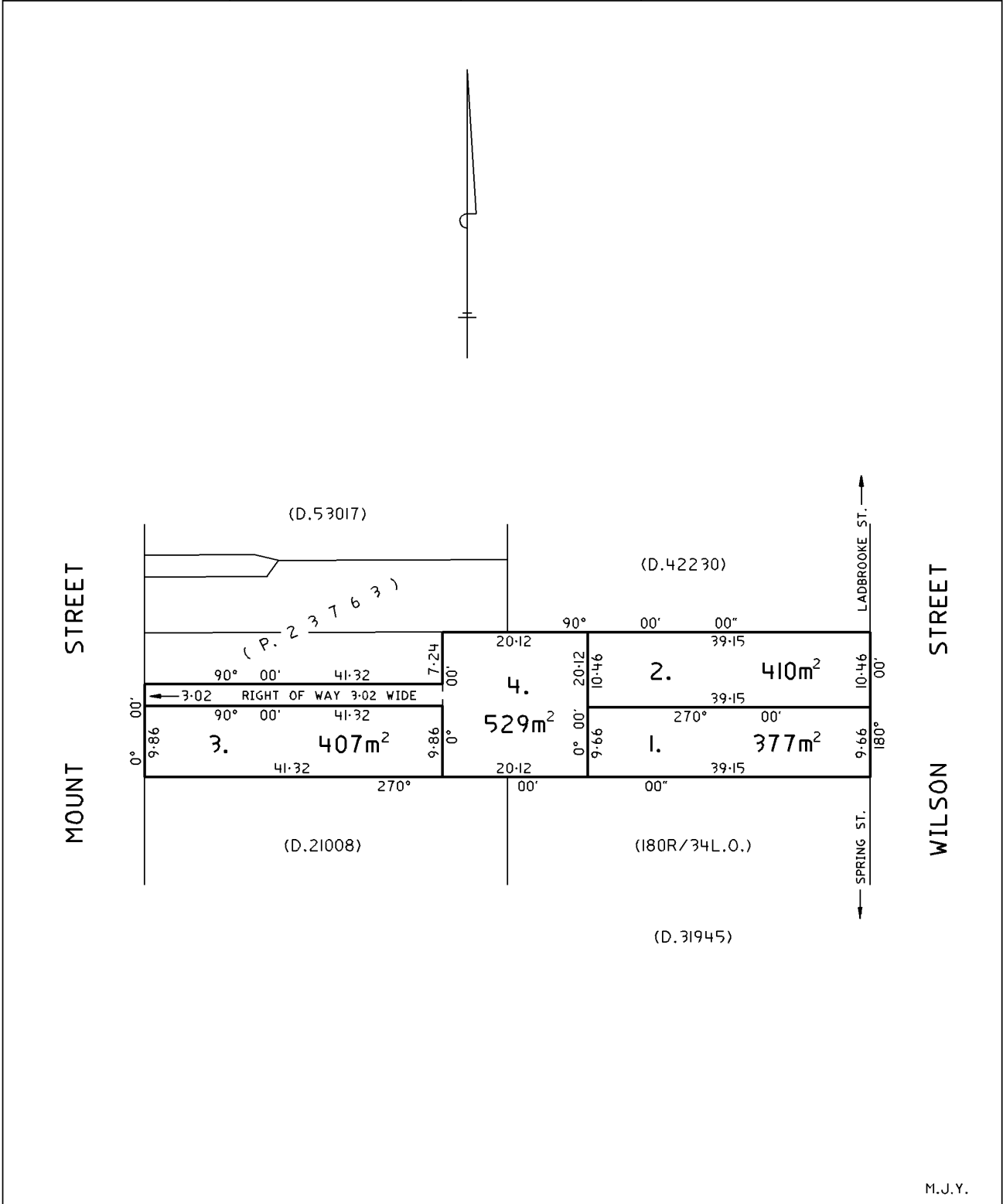
J.G.
A-143

OWNER L.T.ACT 1980		PLAN OF TITLE		Registered Number	
FOLIO REFERENCE W.4782				P.127144	
GRANTEE PART OF 50000-0-0 GTD TO THE V.D.L. Co		LOCATION CITY OF BURNIE		APPROVED 5 MAR 1997	
		FIRST SURVEY PLAN No. P.348 D.O.		 Recorder of Titles	
		COMPILED BY			
		SCALE 1: 500		LENGTHS IN METRES	
MAPSHEET MUNICIPAL CODE No. 103 (4045-325)	LAST 4203864 UPI No 4203873-75 INCL.	LAST PLAN No. P.23763	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN		



M.J.Y.

OWNER L.T.ACT 1980		PLAN OF TITLE		Registered Number	
FOLIO REFERENCE W.4782				P.127144	
GRANTEE PART OF 50000-0-0 GTD TO THE V.D.L. Co		LOCATION CITY OF BURNIE		APPROVED 5 MAR 1997	
		FIRST SURVEY PLAN No. P.348 D.O.		 Recorder of Titles	
		COMPILED BY			
		SCALE 1: 500		LENGTHS IN METRES	
MAPSHEET MUNICIPAL CODE No. 103 (4045-325)	LAST 4203864 UPI No 4203873-75 INCL.	LAST PLAN No. P.23763	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN		



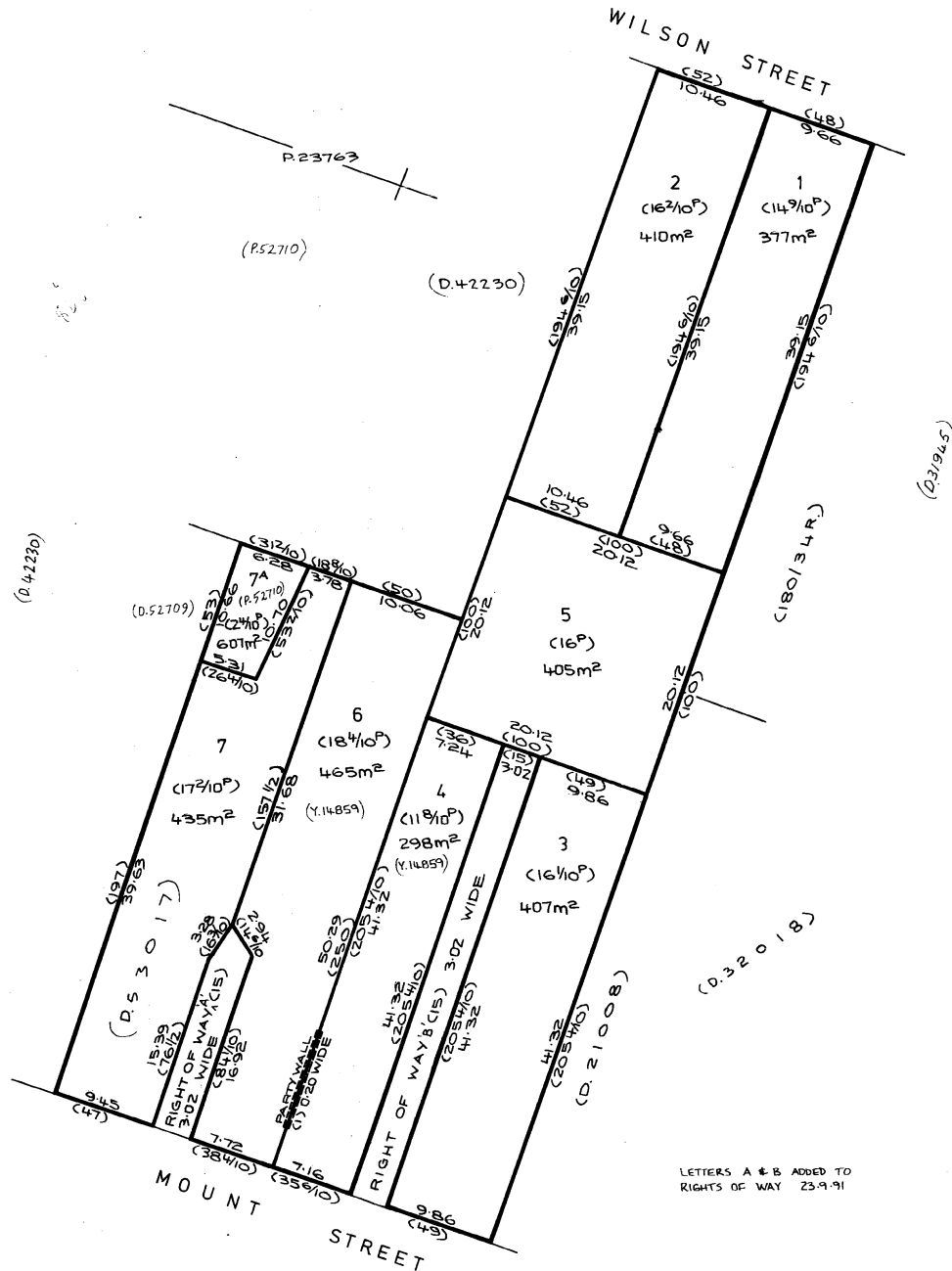
M.J.Y.

APPROVED FROM 13 AUG 1984 <i>E. R. Thorp</i> RECORDER OF TITLES	CONVERSION PLAN	REGISTERED NUMBER P. 23763
FILE NUMBER	GRANTEE: PART OF 50,000 ACRES DIEMENS LAND COMPANY	GTD. TO THE VAN 5.1.9-8-84



SKETCH BY WAY OF ILLUSTRATION ONLY

~~CITY/TOWN OF BURNIE~~
LAND DISTRICT OF
PARISH OF
LENGTHS ARE IN METRES. NOT TO SCALE.
LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.

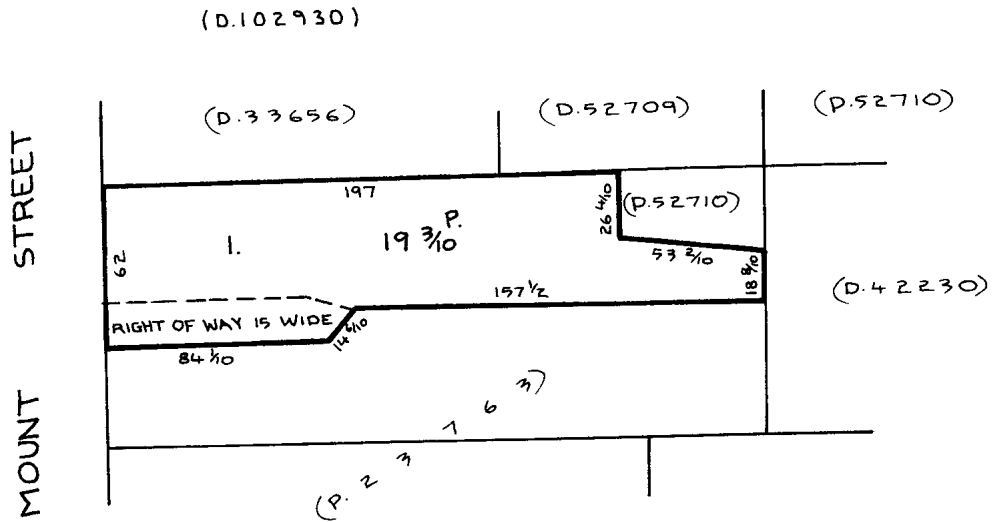


APPROVED 28 JAN 1992 <i>[Signature]</i> RECORDER OF TITLES	CONVERSION PLAN CONVERTED FROM 52/5592	REGISTERED NUMBER D.53017
FILE NUMBER Y.14860	GRANTEE PART OF 50,000-0-0 GTD TO THE VAN DIEMENS LAND COMPANY	DRAWN P. PERRI 10-1-92

05-K 2002

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF BURNIE
~~LAND DISTRICT OF~~
~~PARISH OF~~
 LENGTHS ARE IN METRES. NOT TO SCALE.
 LENGTHS IN BRACKETS IN LINKS/FEET & INCHES.



SEARCH OF TORRENS TITLE

VOLUME 187122	FOLIO 1
EDITION 1	DATE OF ISSUE 27-Aug-2024

SEARCH DATE : 29-Aug-2024
 SEARCH TIME : 01.27 PM

DESCRIPTION OF LAND

City of BURNIE
 Lot 1 on Sealed Plan 187122
 Derivation : Part of 50,000 Acres Gtd. to The Van Diemens Land Company
 Prior CTs 42230/1, 42230/2, 52709/1, 52710/1, 52710/2, 52710/3, 52710/4, 52710/5, 52710/6, 52710/7, 110588/1 and 110588/3

SCHEDULE 1

N161674 & N162998 TRANSFER to THE CROWN

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 SP187122 EASEMENTS in Schedule of Easements
 6/2059 & 14/5063 CONVEYANCE Made Subject to Fencing Condition
 6/2059 & 14/5063 CONVEYANCE Made Subject to Exceptions And Reservations in favour of The V.D.L. Co.
 16/6557 & 26/5332 CONVEYANCE Made Subject to Boundary Fences Condition

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 102928	FOLIO 2
EDITION 1	DATE OF ISSUE 27-Sep-1995

SEARCH DATE : 18-Jul-2025

SEARCH TIME : 10.32 AM

DESCRIPTION OF LAND

City of BURNIE

Lot 2 on Diagram [102928](#)

Derivation : Part of 50,000 acres granted to the Van Diemen's Land Company

Derived from W3988

SCHEDULE 1

DE SANTIS NOMINEES PROPRIETARY LIMITED

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
[26/0567](#) CONVEYANCE: Burdening Easement: Right to pass and
 repass (appurtenant to Lot 1 on diagram number
[102928](#)) over the Right of Way shown on the said
 diagram as passing through the said land within
 described subject to the conditions described therein

[15/5132](#) CONVEYANCE Made Subject to Exceptions And
 Reservations in favour of The V.D.L. Co.

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 102930	FOLIO 1
EDITION 3	DATE OF ISSUE 28-Nov-2023

SEARCH DATE : 18-Jul-2025

SEARCH TIME : 10.29 AM

DESCRIPTION OF LAND

City of BURNIE

Lot 1 on Diagram [102930](#)

Being the land described in Conveyance 55/9026

Derivation : Part of 50,000 acres Granted to V.D.L. Company

Derived from Y15766

SCHEDULE 1

ANTONIOS PAPASTAMATIS and EUGENIA PAPASTAMATIS as tenants in
common in equal shares

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

[21/2327](#) CONVEYANCE: Benefiting Easement: Right to pass and
repass over the right of way shown on Diagram [102930](#)

[C679855](#) CAVEAT by The Australian Broadcasting Corporation
Registered 13-Dec-2005 at noon

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 121419	FOLIO 1
EDITION 11	DATE OF ISSUE 09-Dec-2022

SEARCH DATE : 18-Jul-2025

SEARCH TIME : 10.35 AM

DESCRIPTION OF LAND

City of BURNIE
 Lot 1 on Plan [121419](#)
 Being the land described in Conveyance No. 63/5412
 Derivation : Part of 50,000 Acres Gtd to Van Diemens Land
 Company
 Prior CTs [33656/1](#) and [33657/2](#)

SCHEDULE 1

[M958427](#) TRANSFER to COMMUNITY CARE NESB INC Registered
 09-Dec-2022 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 BURDENING EASEMENT: A right to go pass and repass as is more
 fully set forth in Conveyance No. [31/1468](#) for Percy
 Nothrop his heirs executors administrators and
 assigns and the owners or occupiers for the time
 being of the balance of one Rood of land known as
 Allotment No. 96 Burnie now or formerly belonging to
 the said Percy Nothrop and of 2.4/10 perches of land
 described in Conveyance No. [19/3269](#) along over and
 upon the strip of land marked A.B.C.D on Plan No.
[121419](#)

BURDENING EASEMENT: A right to go pass and repass as is more
 fully set forth in Conveyance No. [20/1241](#) for Alfred
 George Brooks his heirs and assigns and the owners or
 occupiers for the time being of portion of 1 rood
 known as Allotment No. 96 Burnie containing 18.1/10
 perches and being the land comprised in the said
 Conveyance No. [20/1241](#) along over and upon the strip
 of land marked A.B.C.D on Plan No. [121419](#)

[B829153](#) ADHESION ORDER under Section 110 of the Local
 Government (Building and Miscellaneous Provisions)
 Act 1993 Registered 05-Dec-1995 at noon

[E324563](#) MORTGAGE to Westpac Banking Corporation Registered
 09-Dec-2022 at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 127144	FOLIO 2
EDITION 2	DATE OF ISSUE 28-Aug-2012

SEARCH DATE : 18-Jul-2025

SEARCH TIME : 10.40 AM

DESCRIPTION OF LAND

City of BURNIE

Lot 2 on Plan [127144](#)

Derivation : Part of 50,000 Acres Granted to The V D L Co.

Derived from A24512

SCHEDULE 1

[D64421](#) ASSENT to BRIAN ARTHUR HODGKINSON Registered
28-Aug-2012 at 12.01 PM

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 127144	FOLIO 4
EDITION 5	DATE OF ISSUE 04-Jul-2025

SEARCH DATE : 21-Aug-2025

SEARCH TIME : 11.44 AM

DESCRIPTION OF LAND

City of BURNIE

Lot 4 on Plan [127144](#)

Derivation : Part of 50,000 Acres Gtd to the Van Diemens Land Company

Derived from W4782

SCHEDULE 1

[N261371](#) TRANSFER to 4D INVESTMENTS (TAS) PTY LTD Registered
04-Jul-2025 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

[32/8940](#) BURDENING EASEMENT: Right of Way (appurtenant to Lot 3 on Plan [348](#) and Lot 4 on Plan No. 23763) over the land marked "Right of Way 3.02 wide" on Plan No. [127144](#) (subject to maintenance expenses)

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 23763	FOLIO 6
EDITION 3	DATE OF ISSUE 04-Jul-2025

SEARCH DATE : 21-Aug-2025

SEARCH TIME : 04.12 PM

DESCRIPTION OF LAND

City of BURNIE

Lot 6 on Plan [23763](#)

Being the land described in conveyance No. 45/4684

Derivation : Part of 50,000 Acres Granted to the Van Diemens

Land Company

Prior CT [4852/42](#)

SCHEDULE 1

[N261371](#) TRANSFER to 4D INVESTMENTS (TAS) PTY LTD Registered
04-Jul-2025 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

SEARCH OF TORRENS TITLE

VOLUME 53017	FOLIO 1
EDITION 3	DATE OF ISSUE 04-Jul-2025

SEARCH DATE : 21-Aug-2025

SEARCH TIME : 11.42 AM

DESCRIPTION OF LAND

City of BURNIE
 Lot 1 on Diagram [53017](#)
 Being the land described in Conveyance No. 52/5592
 Derivation : Part of 50,000 Acres Granted to the Van Diemens
 Land Company
 Prior CT [4852/41](#)

SCHEDULE 1

[N261371](#) TRANSFER to 4D INVESTMENTS (TAS) PTY LTD Registered
 04-Jul-2025 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
[15/5774](#) CONVEYANCE Made Subject to Exceptions And
 Reservations in favour of The V.D.L. Co.
[15/5774](#) CONVEYANCE Made Subject to Boundary Fences Condition

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations

Troy McCarthy
Principal Planner
Burnie City Council
80 Wilson Street
Burnie TAS 7320
planning@burnie.tas.gov.au



27 August 2025

Dear Troy,

**Re: Planning Application Submission – Burnie Courts Complex Relocation Project,
106 Wilson Street, Burnie**

Thank you for your correspondence regarding the proposed built form of the Burnie Courts Complex relocation project and the invocation of discretion under Clause 16.4.2 of the Tasmanian Planning Scheme.

We acknowledge that not all elements of the proposed built form are constructed to the street frontage. In accordance with your request, we provide the following submission addressing the performance criteria set out in Clause 16.4.2 of the State Planning Provisions.

Clause 16.4.2 – Setbacks

Objective:

That building setback:

- is compatible with the streetscape;
- does not cause an unreasonable loss of amenity to adjoining residential zones; and
- minimises opportunities for crime and anti-social behaviour through setback of buildings.

Performance Criteria P1:

Buildings must have a setback from a frontage that is compatible with the streetscape and minimises opportunities for crime or anti-social behaviour, having regard to:

- providing small variations in building alignment to break up long building façades;
- providing variations in building alignment appropriate to provide a forecourt space for public use, such as outdoor dining or landscaping;
- the avoidance of concealment spaces;
- the ability to achieve passive surveillance; and
- the availability of lighting.

Response to Performance Criteria

The Burnie Courts Complex relocation project proposes a **public forecourt** at the street frontage, which is a deliberate design feature aligned with the performance

HOBART
1st Floor
125 Collins Street
Hobart Tasmania 7000
t: 03 6224 9370

LAUNCESTON
Suite 4, 1st Floor
39 Paterson Street
Launceston Tasmania 7250
t: 03 6334 9539

e: admin@xsa.net.au

Peter Scott FRAIA - Director
m. 0400 530 306
Alex Newman RAIA - Director
m. 0437 356 641

www.xsa.net.au



criteria of Clause 16.4.2. This forecourt is intended to serve both **court users and the local community**, offering a landscaped public space that:

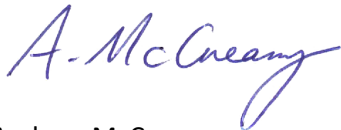
- **Provides a waiting area** external to the building, reducing congestion on the public footpath and enhancing pedestrian flow;
- **Breaks up the building façade**, introducing variation in alignment and avoiding a monolithic frontage;
- **Supports passive surveillance**, with clear sightlines from the street and adjacent buildings, contributing to public safety;
- **Avoids concealment spaces**, with open landscaping and lighting designed to deter anti-social behaviour;
- **Enhances streetscape compatibility**, with materials, scale, and landscaping that reflect the civic character of the precinct.

The forecourt is not only a functional space but also a civic gesture, reinforcing the public nature of the courthouse and its role within the community. It contributes positively to the urban fabric and aligns with the planning scheme's intent to encourage active and safe public spaces.

We trust that this submission demonstrates the project's compliance with the performance criteria of Clause 16.4.2 and supports the exercise of discretion in this instance.

Please do not hesitate to contact us should you require further clarification or supporting documentation.

Yours sincerely,

A handwritten signature in blue ink that reads 'A. McCreary'.

Andrew McCreary
Associate, Xsquared Architects

CONCEPT SERVICES REPORT

BURNIE COURTS COMPLEX

XSQUARED ARCHITECTS
JULY 2025



XSG

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Appendix D - Water and Sewer Demand Calculations

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JMG Project No. J230912BS								
Document Issue Status								
Ver.	Issue Date	Description	Originator		Checked		Approved	
1	15/07/2025	Development Application	JFB		BHL		CJM	

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1. Introduction

JMG has been engaged by the Department of Justice, as a subconsultant to XSquared Architects, to undertake engineering services design for the proposed Burnie Courts Complex project proposed for the corner of Ladbroke Street and Wilson Street, Burnie.

This concept services report has been prepared to support the Development Application (DA) submission and outlines the proposed servicing strategy for sanitary drainage, water supply, and stormwater management, in accordance with relevant regulatory requirements and design standards.

The site has an area of approximately 3,495 m² and is currently occupied by existing buildings, a sealed carpark, and a gravel driveway and parking area. The site previously comprised four separate titles, which have recently been amalgamated into a single title (CT 187122/1). The site is accessible from both Ladbroke Street and Wilson Street and generally falls topographically towards Wilson Street.

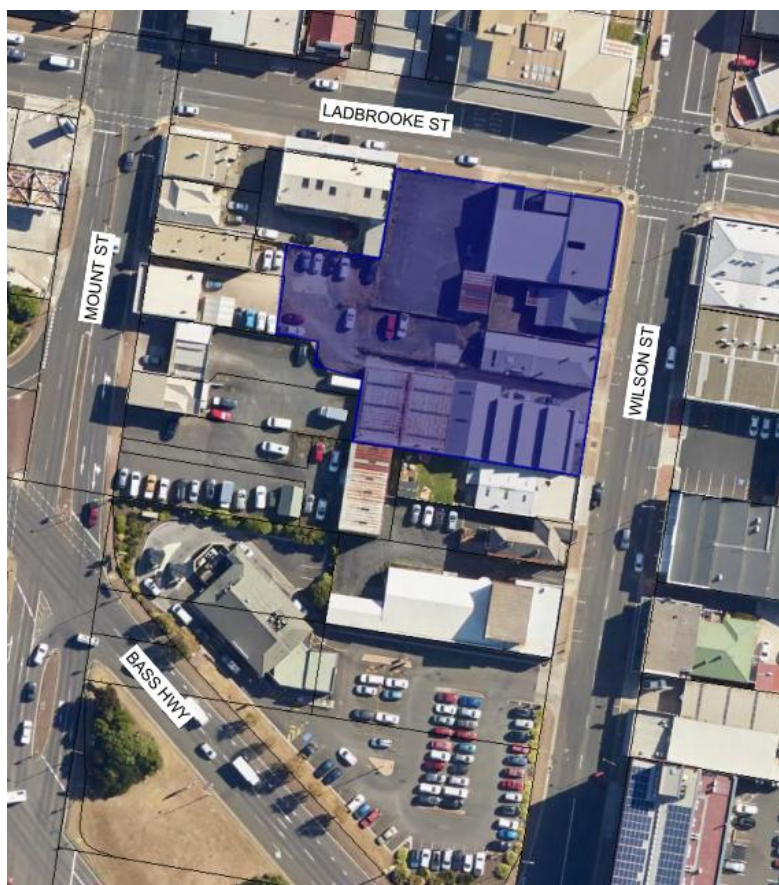


Figure 1: Site Locality Plan

Demolition of the existing buildings and sealing of redundant service connections is being undertaken under a separate approved permit (DA2025/31).

2. Sanitary Drainage System

2.1 Existing System

There are four known sewer property connections servicing the site - one for each of the original titles. All four property connections discharge to TasWater's DN150 uPVC sewer main (Asset ID: A214988) located in Wilson Street to the east of the site. These connections are shown in Figure 2.

An existing TasWater DN150 sewer main (Asset IDs: A214902 and A215043) also traverses the western portion of the site within a TasWater easement, servicing upstream properties.

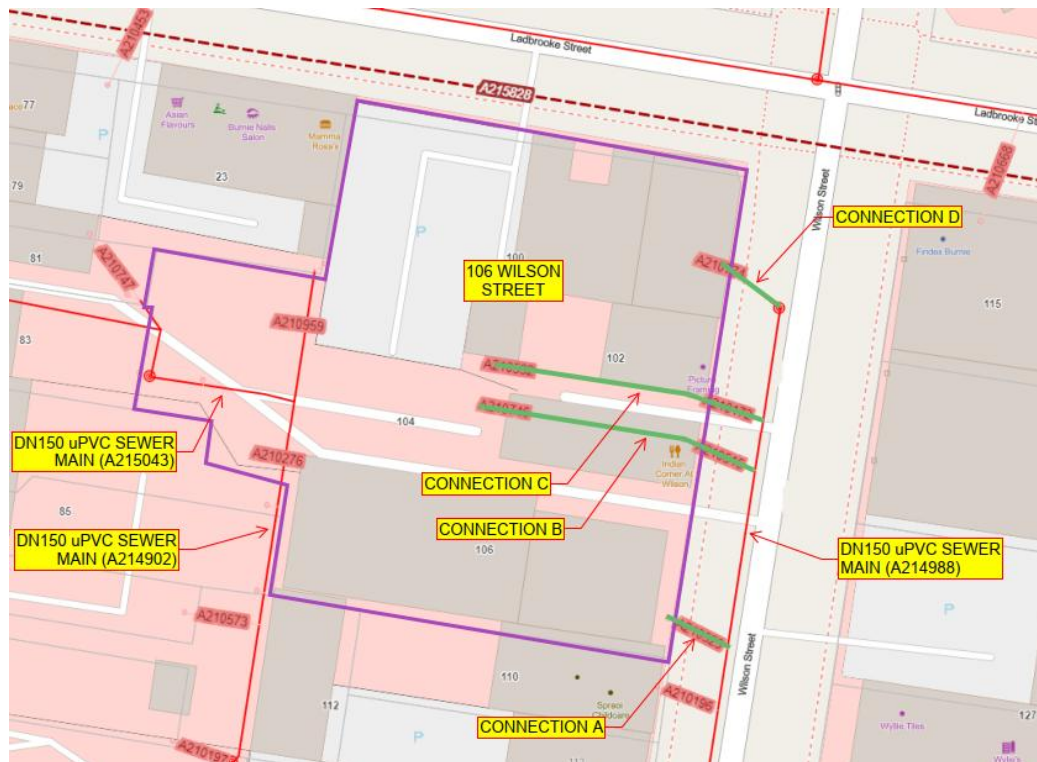


Figure 2: Existing TasWater sewer infrastructure and site sewer connections

2.2 Applicable Design Standards

The sanitary drainage system for the site will be designed in accordance with:

- AS 3500.2 - *National Plumbing and Drainage Code - Sanitary Plumbing and Sanitary Drainage*.
- TasWater Development Technical Standards.

2.3 Proposed System

The four existing sewer property connections will be decommissioned in accordance with TasWater's requirements under a separate planning Permit No. DA2025/31. A new single connection to service the proposed development will be constructed under this development application.

Based on AS 3500.2 Section 6.3, the estimated total fixture unit load for the development is 256. According to AS 3500.1 Table 3.3.1, this corresponds to a minimum DN150 sewer. Therefore, a new DN150 property connection will be constructed and connected to the existing TasWater gravity sewer main (A214988) in Wilson Street.

Sewerage flows from the development have been estimated using TasWater’s supplement to the *Sewerage Code of Australia (WSA 02)*. A summary of the calculated flows is provided in Table 1, with detailed calculations included in Appendix D.

Table 1: Sewer Flow Calculations Summary

	Value	Units
Equivalent Tenements	19	-
Average Dry Weather Flow	0.0993	L/s
Peak Dry Weather Flow	0.994	L/s
Total Design Flow	1.55	L/s

Levels of existing services in Wilson Street have been reviewed using available BYDA information. This preliminary assessment indicates that the proposed property connection to the existing sewer main is feasible, subject to confirmation of service locations and depths during detailed design. Refer to the preliminary sewer profile in Figure 3.

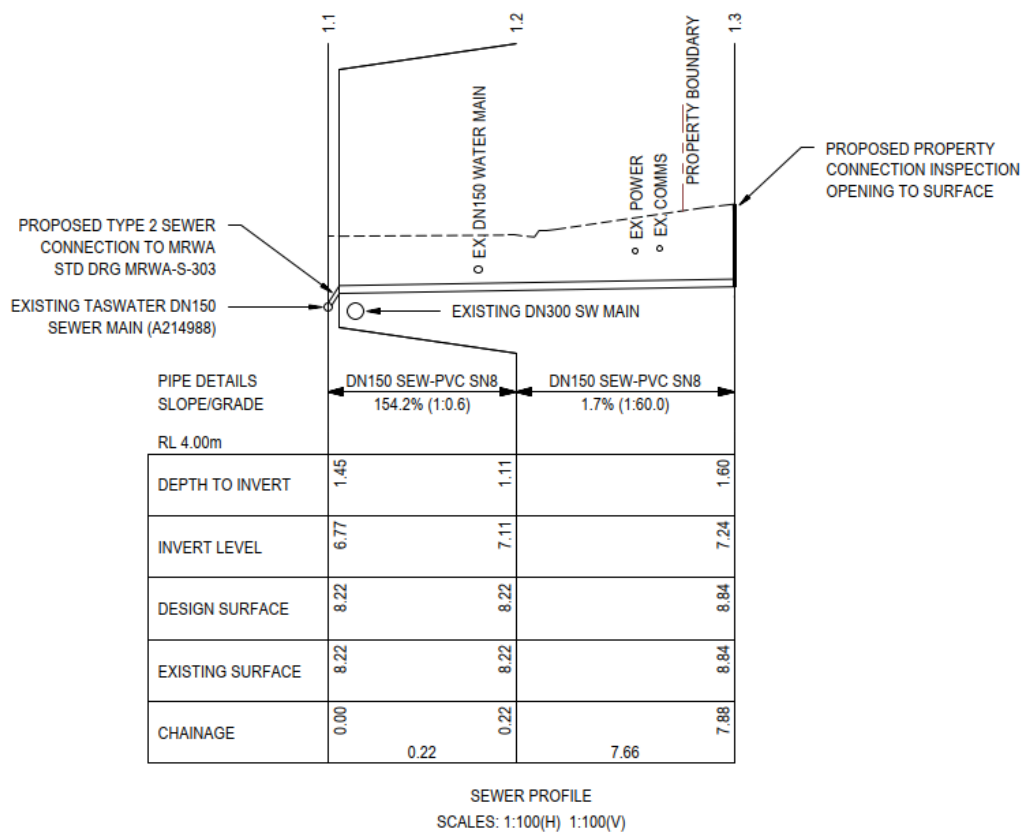


Figure 3: Proposed TasWater Sewer Connection. Note all existing services in Wilson Street are from BYDA records and are therefore approximate only

3. Water

3.1 Existing System

The site is currently serviced by four known water connections, one for each of the previous titles along with a DN100 fire service connection for 106 Wilson Street shown as 'Connection 2' in Figure 4. The four other connections are DN20 in size with below-ground meters and connect to TasWater's DN150 CICL water main (Asset ID A115971) in Wilson Street to the east of the site. These connections are shown in Figure 4.

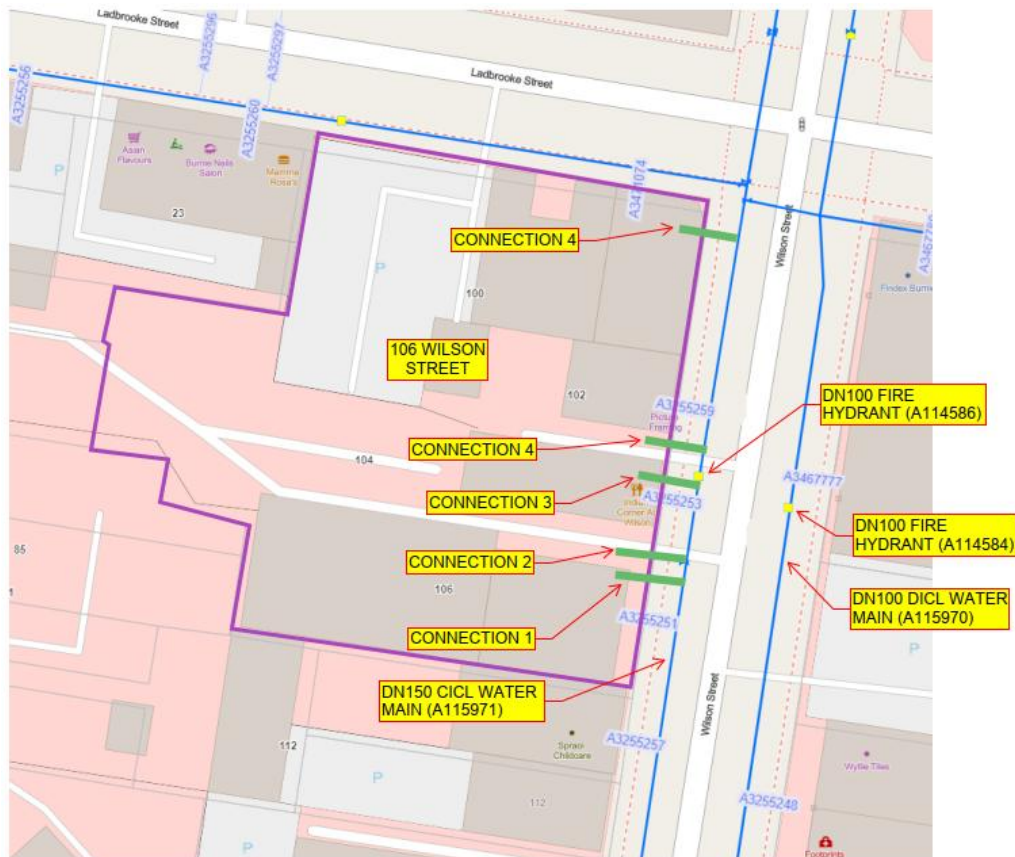


Figure 4: Existing TasWater water infrastructure and site water connections

3.2 Applicable Design Standards

The water reticulation system for the development will be designed in accordance with the following standards and authority requirements:

- AS 3500.1 - National Plumbing and Drainage Code: Water Supply.
- TasWater's Water Metering & Guidelines - for metering requirements.
- TasWater's Water Boundary Backflow Containment Selection Guidelines - for backflow prevention.
- AS 2419.1 - Fire Hydrant Installations, for fire coverage compliance.
- AS 2118.1 - Automatic Fire Sprinkler Systems.

3.3 Proposed System

All four existing water connections and the DN100 fire service connection from Wilson Street will be decommissioned and sealed in accordance with TasWater’s requirements under a separate Planning Permit No. DA2025/31.

A new above-ground domestic water meter sized in accordance with AS3500.1 and a DN150 fire water meter assembly will be installed within the property boundary, meeting TasWater’s metering and backflow guidelines. Under the NCC Volume 3 Plumbing Code of Australia specification 41 Cross Connections Hazards, the proposed class 5 building is deemed to be MEDIUM Hazard backflow rating.

The new connection is proposed to be made to the existing DN150 TasWater main (A115971) in Wilson Street with the domestic and fire assemblies to be installed in a recessed enclosure in the front façade of the building. Clearances to all pipework being in accordance with TasWater’s metering guidelines.

New internal water supply pipework will be installed throughout the development in accordance with AS 3500.1 and connected to the new metered supplies.

Water demand has been estimated using TasWater’s supplement to the MRWA Water Supply Code. The results are summarised in Table 2.

Table 2: Water Demand Calculations as per AS3500.1 and TasWater’s Supplement

	Value	Units
Equivalent Number of Tenements	12.7	(-)
Probable Simultaneous Demand	2.01	(L/s)
Average Water Demand per day	8708	(L/day)

Fire hydrant protection for the development will be designed in accordance with AS 2419.1, Table 2.2.5(b), which specifies for the proposed development that two hydrants must be capable of delivering a minimum flow of 10 L/s at a residual pressure of 350 kPa at each hydrant simultaneously.

Fire sprinkler protection for the development will be designed in accordance with AS 2118.1, which specifies for the proposed development the flowrate required for the activation of 10 sprinkler heads is 10 L/s at a minimum pressure of 700 kPa at the booster assembly.

Based on the fire head loss calculations (refer to Appendix E), the combined demand from the most disadvantaged hydrant and the internal sprinkler system of 30 L/s results in a minimum required pressure at the proposed connection point to the DN150 TasWater main (A115971) of 715 kPa.

TasWater have completed flow and pressure testing on two fire hydrants on the DN150 water main (Asset ID: A115971) in Wilson Street. The results from this test are shown in Figure 5. Based on these results and the 820 kPa static pressure at the connection point, the required flowrate of 30 L/s at 750 kPa can be achieved.

Non-Standard Pressure & Flow Test									
Date:	12/03/2025	Time:	06:00AM-06:30AM	Reservoir Level (if known):		Other notes:			
Static Gauge (Pressure only)		FP 1		FP 2		Project: DEPARTMENT OF JUSTICE			
Asset Number	A3255253 (20mm property connection)	A114586		A114575		PID: 9244153			
test	Pressure kPa	Pressure kPa	Flow L/s	Pressure kPa	Flow L/s	Total Flow L/s	SG	operator name:	Anthony Cengia
(a)	840	840	0	800	0	0		Phone#:	
(b)	840	725	5	800	0	5			
(c)	840	725	5	790	5	10	FP 1	operator name:	Greg French
(d)	840	450	10	790	5	15		Phone#:	
(e)	830	400	10	700	10	20			
(f)	830	0	15	700	10	25	FP 2	operator name:	Mitch Ferguson
(g)	820	0	15	600	15	30		Phone#:	
(h)	820	0	17	0	32	49			
(i)						0			
(j)						0			

Figure 5: Fire Hydrant Flow and Pressure Test Results

4. Stormwater

4.1 Applicable Planning Scheme Requirements

The Tasmanian Planning Scheme does not impose specific requirements regarding stormwater quality or quantity management. However, Burnie City Council has adopted a Stormwater Management Policy that aligns with the *Tasmanian Stormwater Standards for New Developments*. This development has been assessed in accordance with the requirements outlined in Council's policy standards.

4.2 Applicable Design Standards

The private stormwater reticulation system will be designed in compliance with

- AS3500.3 National Plumbing and Drainage Code - Stormwater Drainage.
- Burnie City Council Stormwater Management Policy.
- Australian Rainfall and Runoff (ARR) Guidelines.

Onsite private stormwater detention sizing has been determined in consultation with Burnie City Council's Development Engineers, resulting in the adoption of specific runoff coefficients and methodologies detailed below.

Burnie City Council's policy mandates that stormwater detention calculations utilise a predevelopment runoff coefficient of 0.55, reflecting a catchment that is 54% impervious. Considering the site is currently nearly entirely impervious, a runoff coefficient of 0.55 was deemed highly conservative. Recognising the minimal impact on the downstream stormwater network due to negligible changes in runoff, an alternative solution was proposed and approved by Council. The adopted runoff coefficients were 0.83 for the predevelopment scenario and 0.9 for post-development. The detention volume calculated adopts the 10% Annual Exceedance Probability (AEP) event for Permissible Site Discharge (PSD) calculations and the 5% AEP event for Design Site Discharge (DSD) calculations.

Rainfall intensities for the 1% AEP overland flow calculations have been increased by 66%, reflecting projected climate impacts under the SSP3-7.0 scenario as per the Australian Rainfall and Runoff (ARR) Guidelines and Council's requirements.

4.3 Proposed Systems

4.3.1 Stormwater System Design Requirements

The site currently comprises four separate properties (100, 102, 104 and 106 Wilson Street) with each property draining to separate stormwater connections in Wilson Street. All of the existing stormwater connection will be decommissioned and a new consolidated connection for the combined site will be constructed under a separate Planning Permit No. DA2025/31.

Under this development application the stormwater property connection inspection opening from the previous Planning Permit (DA2025/31) will be replaced with a grated pit at the property boundary.

Site stormwater will be captured and discharged to the existing gravity main running along Wilson Street, meeting the detention and treatment requirements.

Detention storage volumes have been assessed adopting runoff coefficients of 0.83 predevelopment and 0.9 post-development resulting in a calculated PSD of 89 L/s and a DSD of 120 L/s, requiring a detention volume of 10.0 m³.

Table 3: Summary of Site Stormwater Calculations

	Permissible Site Discharge	Design Site Discharge
Time of Concentration	5 mins	5 mins
AEP Rainfall Event	10% AEP	5% AEP
Runoff Coefficient	0.83	0.9
Runoff Flow	89 L/s	120 L/s

4.3.2 On-Site Detention

To limit the DSD of 120 L/s to the PSD of 89 L/s, a detention volume of 10.0 m³ will be provided via a precast concrete tank connected to an orifice pit upstream of a baffle plate, as shown in the stormwater design drawings (Appendix B).

The stormwater outlet orifice will be sized to meet the required flow restriction.

Detailed detention system calculations are provided in Appendix C.

4.3.3 Stormwater Quality Method Requirements

Stormwater quality targets, as stipulated by the Burnie City Council Stormwater Management Policy, will be met through the implementation of a proprietary treatment device integrated within the Onsite Detention (OSD) tank and litter baskets installed in grated pits across vehicular pavement areas.

Compliance with stormwater quality targets has been verified using a MUSIC model, with the schematic and results presented in Figure 6 and Figure 7.

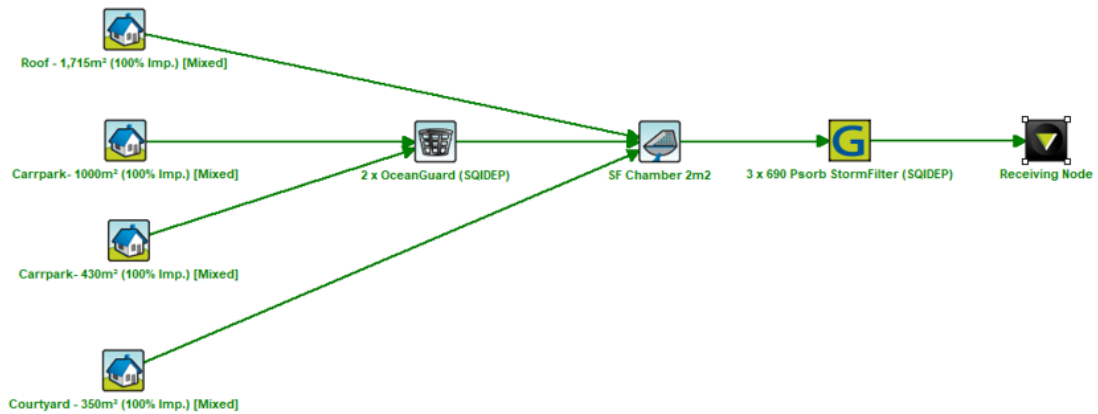


Figure 6: MUSIC Model of Proposed Treatment System

	Sources	Residual Load	% Reduction
Flow (ML/yr)	1.8	1.8	0
Total Suspended Solids (kg/yr)	312	19.9	93.6
Total Phosphorus (kg/yr)	0.608	0.0754	87.6
Total Nitrogen (kg/yr)	4.13	1.39	66.2
Gross Pollutants (kg/yr)	66.7	0	100

Figure 7: MUSIC Model Results

4.4 Overland Flow

Overland flows from the site are currently concentrated along the existing concrete access laneway between 106 and 108 Wilson Street, discharging into Wilson Street as shown in Figure 8.



Figure 8: Existing Site Access Laneway from Wilson Street

The development site is located downstream of 81 and 83 Mount Street, which have been identified as contributing overland runoff from the west. This runoff enters the site via an existing right of way that provides legal access between the subject site and the upstream properties. These overland flows occur due to inadequate stormwater drainage infrastructure on the upstream lots, resulting in surface water discharging into the gravel carpark area of the site, as shown in Figure 9.

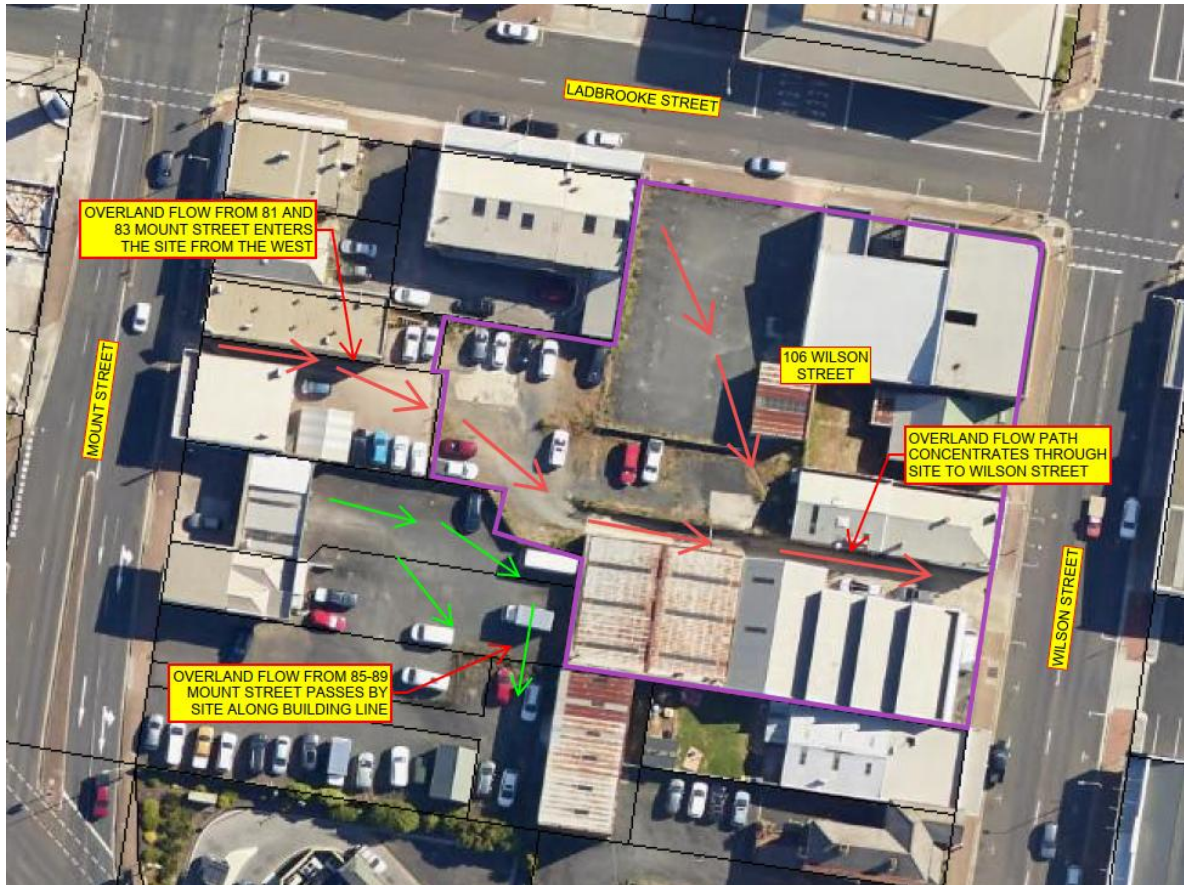


Figure 9: Existing Site Overland Flow Paths

The proposed development will intercept and cut off the existing overland flow path through the concrete access laneway on the project site due to construction of the new Burnie Courts Complex building. To manage both upstream and on-site runoff, the proposed carpark and access driveway off Ladbroke Street have been graded to direct flows away from the new building envelope. Runoff from the carpark will be split, with the majority of flows conveyed via the access driveway to Ladbroke Street. Remaining flows will be directed toward the southern corner of the rear carpark.

Runoff directed to the southern corner of the rear carpark will be captured in an oversized grated pit located in the low point and conveyed via an oversized pipe sized to accommodate flows up to the 1% Annual Exceedance Probability (AEP) storm event. This pipe will discharge to the stormwater detention tank to the east near Wilson Street. In events exceeding the 5% AEP, the stormwater piped system will surcharge at the grated pit stormwater property connection at the property boundary and discharge as overland flow into the Wilson Street road reserve. Refer to Figure 10 for the proposed site overland flow paths and design measures.

The proposed overland flow paths will be designed to accommodate the 1% Annual Exceedance Probability (AEP) storm event, with consideration of the SSP3-7.0 climate change scenario. These design measures will ensure that stormwater generated during a 1% AEP event is safely managed and conveyed away from existing and proposed buildings, discharging to the public stormwater infrastructure in both Ladbroke and Wilson Streets, in accordance with Burnie City Council stormwater policy and best practice design.

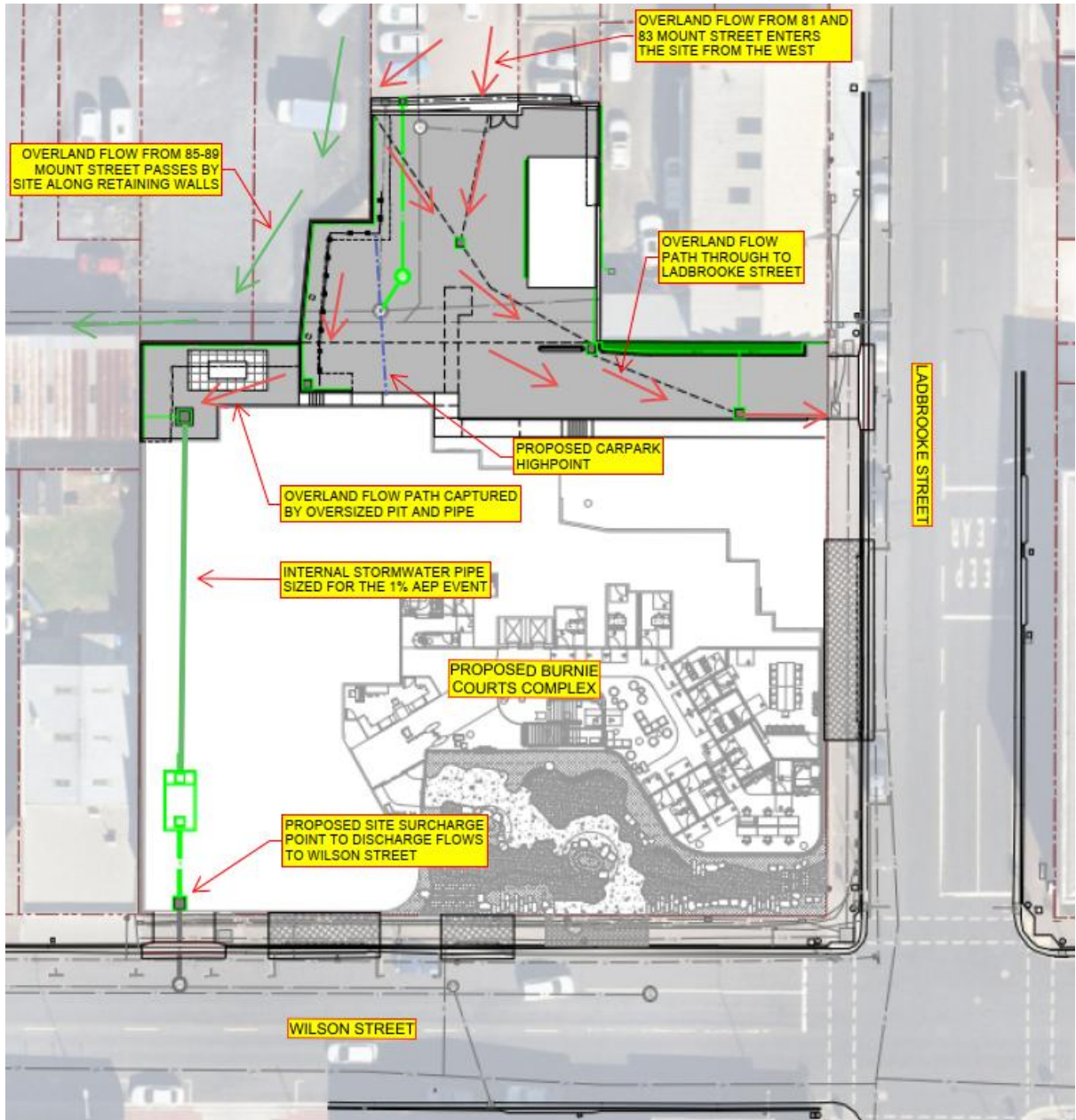


Figure 10: Proposed Site Overland Flow Paths

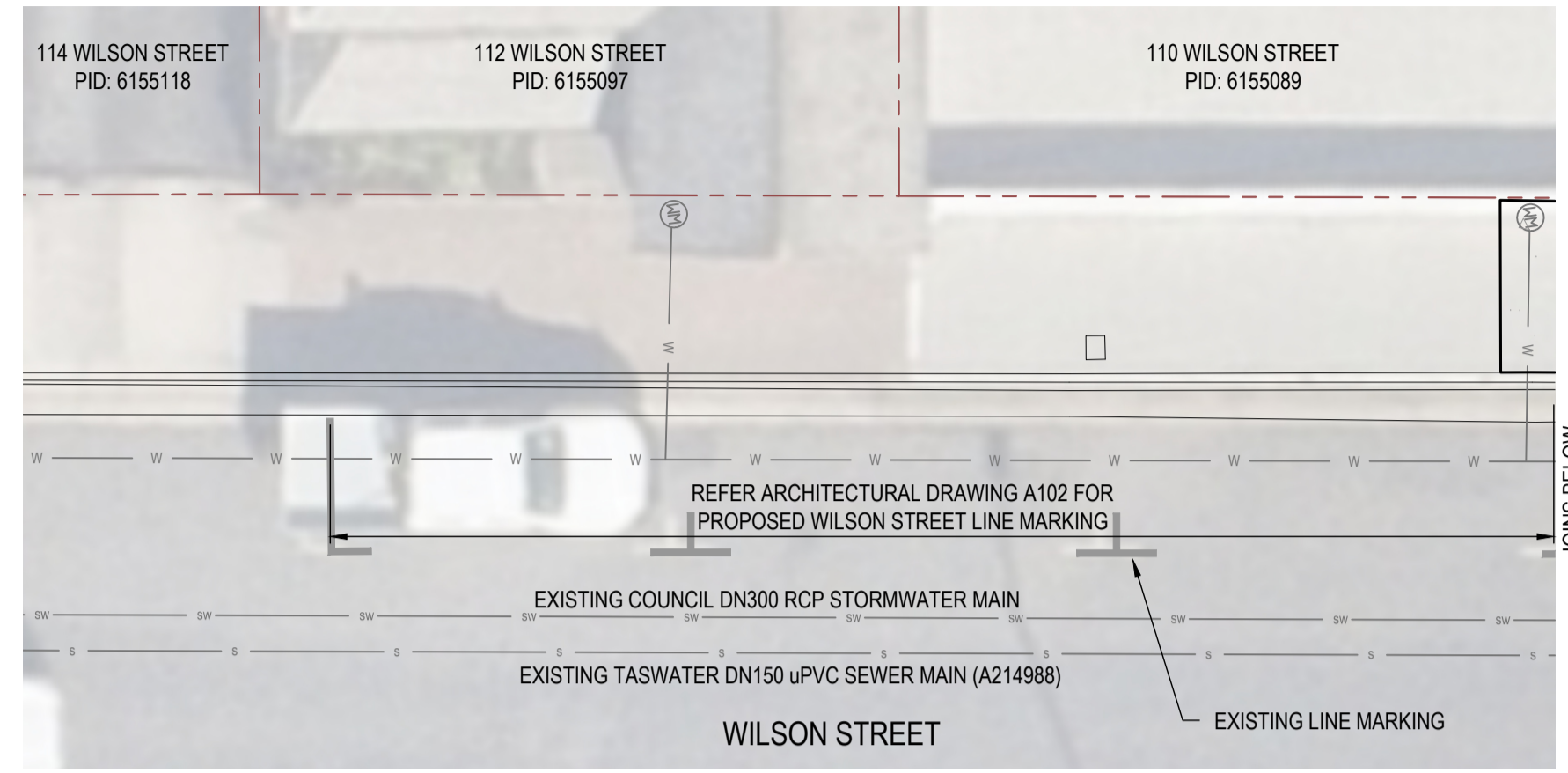
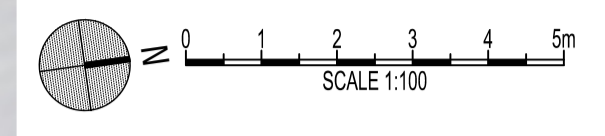
APPENDIX A

Architectural Plans
(under separate cover)

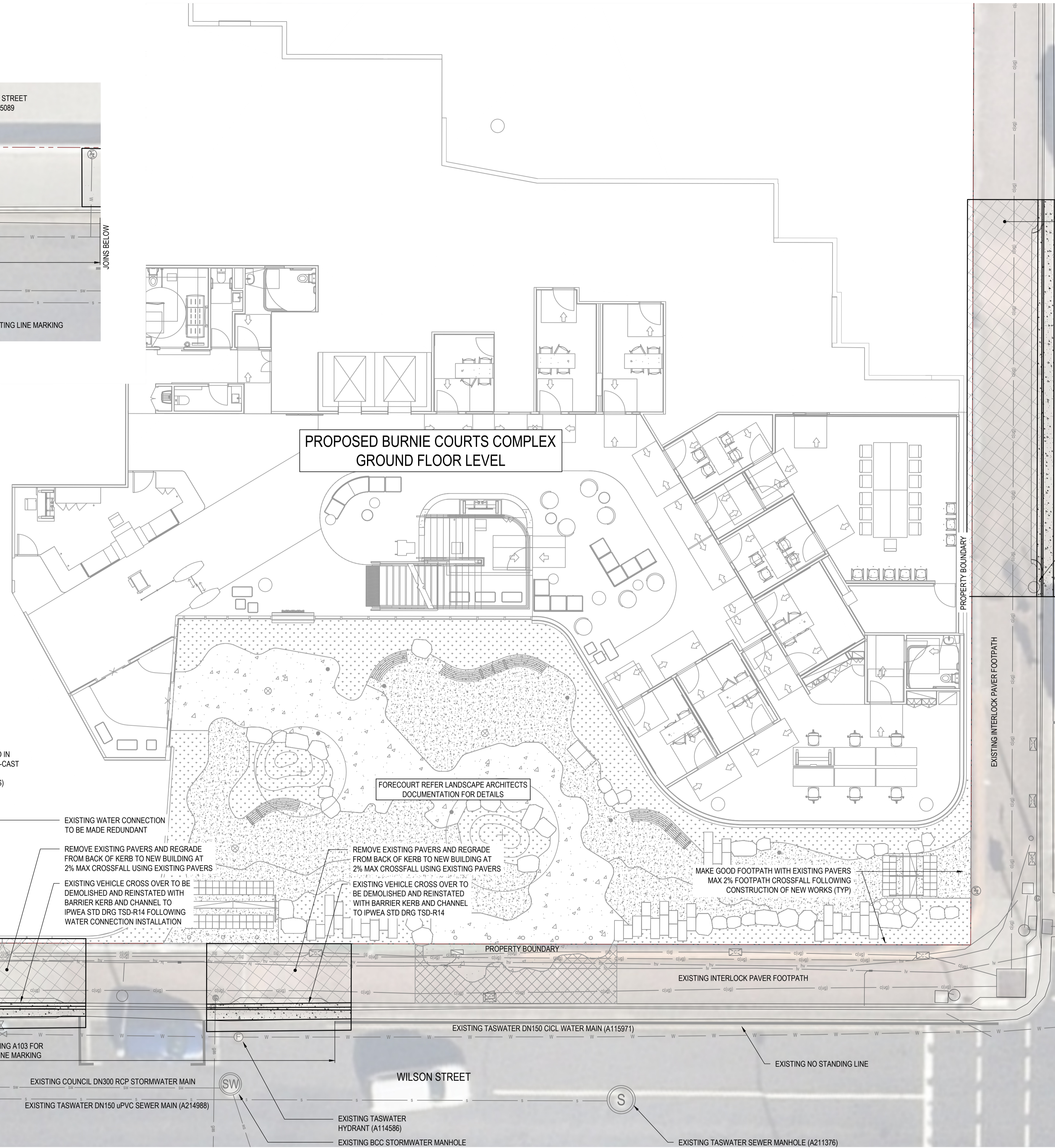
APPENDIX B

JMG Design Drawings 230912BS - C01 to C04

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WILSON STREET LINE MARKING
 SCALE: 1:100



REMOVE EXISTING PAVERS AND REGRADE FROM BACK OF KERB TO NEW BUILDING AT 2% MAX CROSSFALL USING EXISTING PAVERS

EXISTING VEHICLE CROSS OVER TO BE DEMOLISHED AND REINSTATED WITH BARRIER KERB AND CHANNEL TO IPWEA STD DRG TSD-R14

EXISTING COIN OPERATED PARKING METER

EXISTING INTERLOCK PAVER FOOTPATH

EXISTING TASWATER DN150 DICL WATER MAIN (A119539)

EXISTING TASWATER DN150 uPVC SEWER MAIN (A214988)

EXISTING TASWATER DN150 DICL WATER MAIN (A115971)

EXISTING TASWATER SEWER MANHOLE (A211376)

EXISTING TASWATER HYDRANT (A114586)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

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EXISTING BCC STORMWATER MANHOLE (A211376)

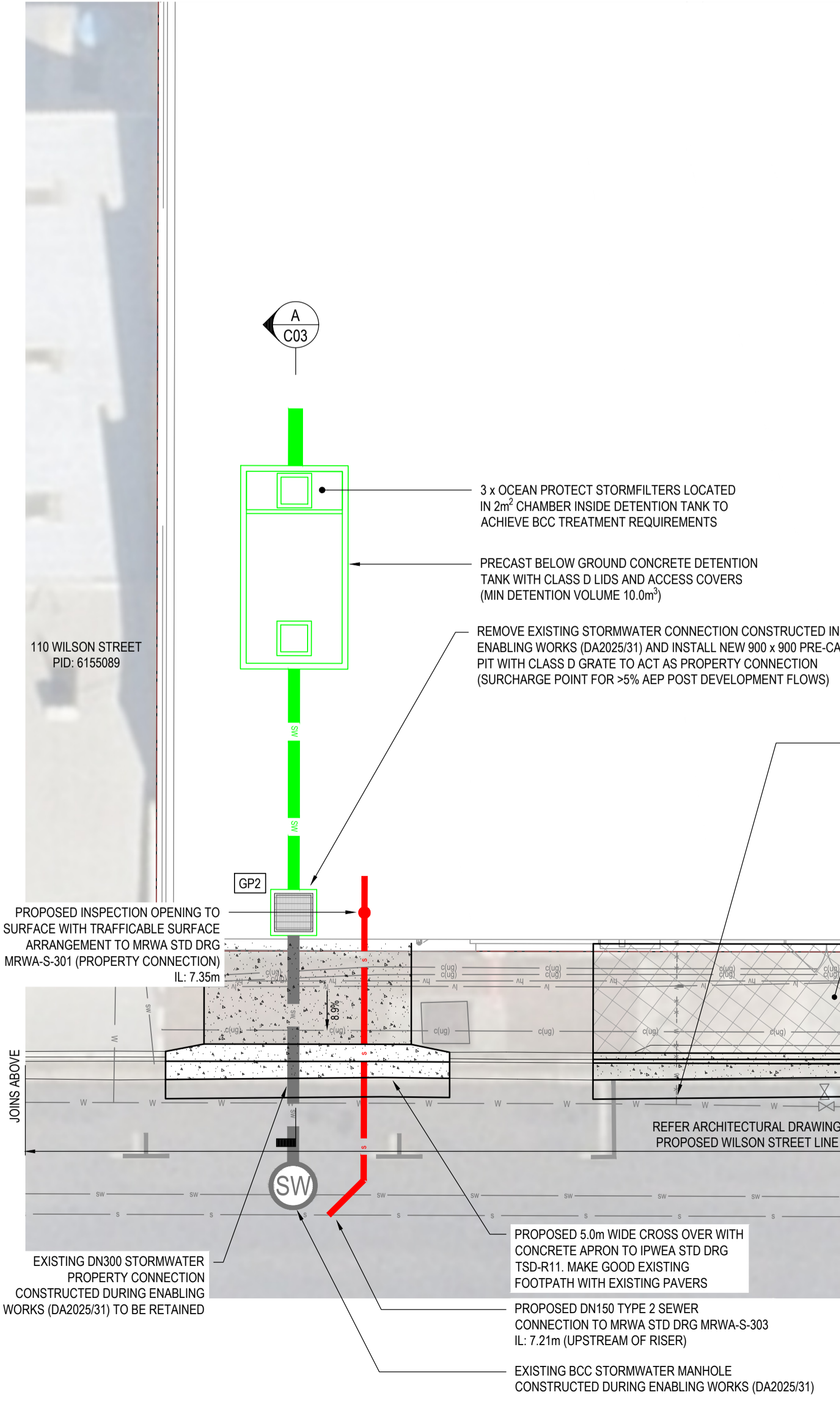
EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)

EXISTING BCC STORMWATER MANHOLE (A211376)



3 x OCEAN PROTECT STORMFILTERS LOCATED IN 2m² CHAMBER INSIDE DETENTION TANK TO ACHIEVE BCC TREATMENT REQUIREMENTS

PRECAST BELOW GROUND CONCRETE DETENTION TANK WITH CLASS D LIDS AND ACCESS COVERS (MIN DETENTION VOLUME 10.0m³)

REMOVE EXISTING STORMWATER CONNECTION CONSTRUCTED IN ENABLING WORKS (DA2025/31) AND INSTALL NEW 900 x 900 PRE-CAST PIT WITH CLASS D GRATE TO ACT AS PROPERTY CONNECTION (SURCHARGE POINT FOR >5% AEP POST DEVELOPMENT FLOWS)

EXISTING WATER CONNECTION TO BE MADE REDUNDANT

REMOVE EXISTING PAVERS AND REGRADE FROM BACK OF KERB TO NEW BUILDING AT 2% MAX CROSSFALL USING EXISTING PAVERS

EXISTING VEHICLE CROSS OVER TO BE DEMOLISHED AND REINSTATED WITH BARRIER KERB AND CHANNEL TO IPWEA STD DRG TSD-R14 FOLLOWING WATER CONNECTION INSTALLATION

REMOVE EXISTING PAVERS AND REGRADE FROM BACK OF KERB TO NEW BUILDING AT 2% MAX CROSSFALL USING EXISTING PAVERS

EXISTING VEHICLE CROSS OVER TO BE DEMOLISHED AND REINSTATED WITH BARRIER KERB AND CHANNEL TO IPWEA STD DRG TSD-R14

MAKE GOOD FOOTPATH WITH EXISTING PAVERS MAX 2% FOOTPATH CROSSFALL FOLLOWING CONSTRUCTION OF NEW WORKS (TYP)

REFER ARCHITECTURAL DRAWING A103 FOR PROPOSED WILSON STREET LINE MARKING

PROPOSED 5.0m WIDE CROSS OVER WITH CONCRETE APRON TO IPWEA STD DRG TSD-R14. MAKE GOOD EXISTING FOOTPATH WITH EXISTING PAVERS

PROPOSED DN150 TYPE 2 SEWER CONNECTION TO MRWA STD DRG MRWA-S-303 IL: 7.21m (UPSTREAM OF RISER)

EXISTING BCC STORMWATER MANHOLE CONSTRUCTED DURING ENABLING WORKS (DA2025/31)

EXISTING BCC STORMWATER MANHOLE CONSTRUCTED DURING ENABLING WORKS (DA2025/31)

EXISTING BCC STORMWATER MANHOLE CONSTRUCTED DURING ENABLING WORKS (DA2025/31)

DA 125/07/2025	DESIGN APPLICATION ISSUE	
P 22/05/2025	DESIGN DEVELOPMENT ISSUE	
P 16/05/2025	DD REVIEW ISSUE	
REV	DATE	REMARK
SAFETY IN DESIGN REPORT PER WHS REGULATIONS		
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PROJECT
 100-106 WILSON STREET
 BURNIE COURTS COMPLEX

TITLE
 CIVIL WORKS
 GENERAL ARRANGEMENT
 SHEET 1

Accepted C MALES (Discipline Head)	Date
Accepted N STOLP (Team Leader)	Date
Approved N STOLP (Principal)	Date

SCALES @ A1	DESIGNED BY	DRAWN BY
1:100	BHL	JFB
	PLOT DATE	25/07/2025

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PLOT DETAILS J230912BS - GENERAL ARRANGEMENT

PROJECT NO. **230912BS**
 DWG NO. **C01** REVISION **DA1**

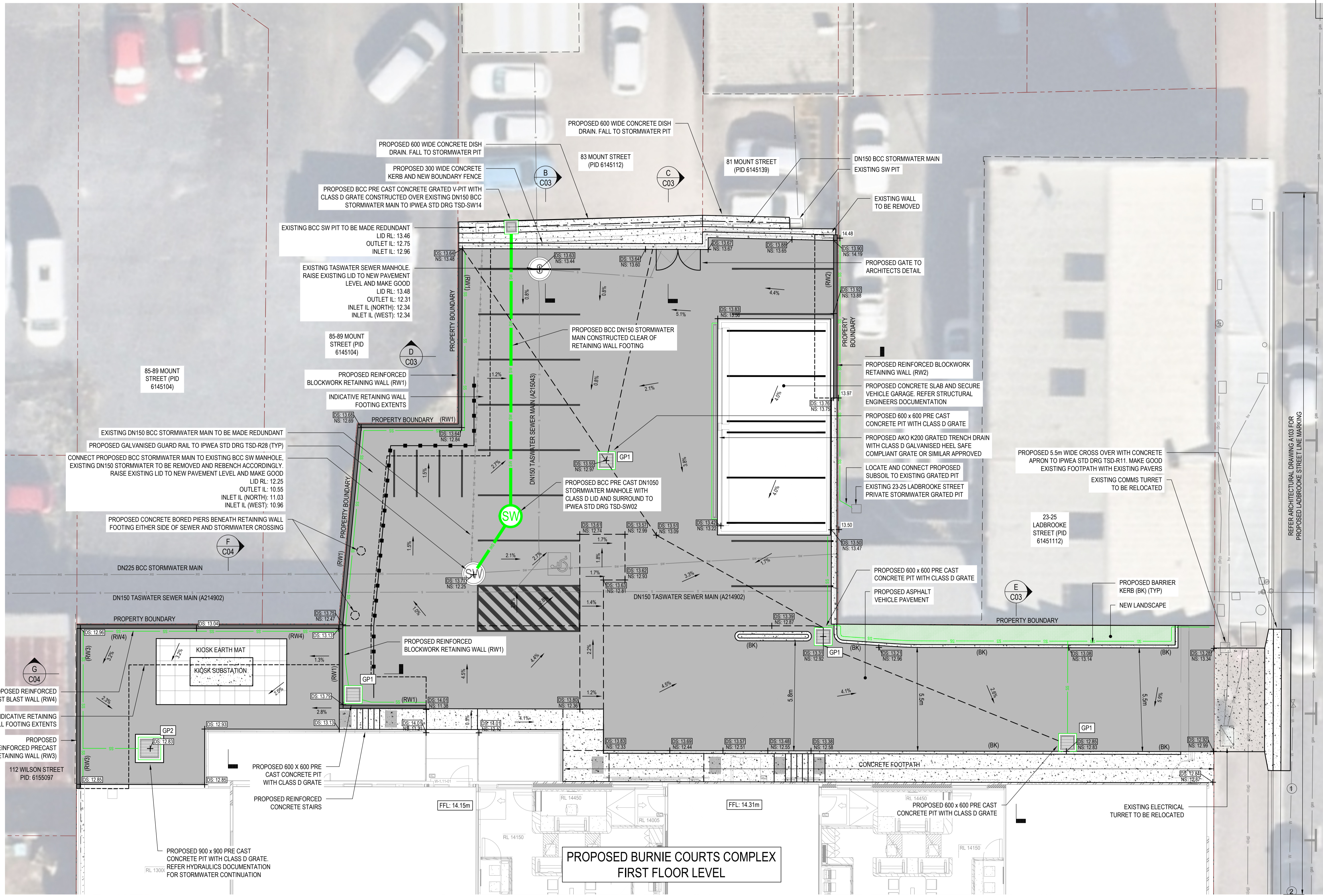
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REFER ARCHITECTURAL DRAWING A103 FOR PROPOSED LADBROOKE STREET LINE MARKING

LADBROOKE STREET

DA 12/07/2025 DESIGN APPLICATION ISSUE
P2 22/05/2025 DESIGN REVIEW ISSUE
P1 16/05/2025 DD REVIEW ISSUE
REV DATE REMARK

SAFETY IN DESIGN REPORT PER WHS REGULATIONS

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Ground Floor, 73 Paterson Street, Launceston (03) 6334 5548
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PROJECT
100-106 WILSON STREET
BURNIE COURTS COMPLEX

TITLE
CIVIL WORKS
GENERAL ARRANGEMENT
SHEET 2

Accepted C MALES (Discipline Head)	Date
Accepted N STOLP (Team Leader)	Date
Approved N STOLP (Principal)	Date

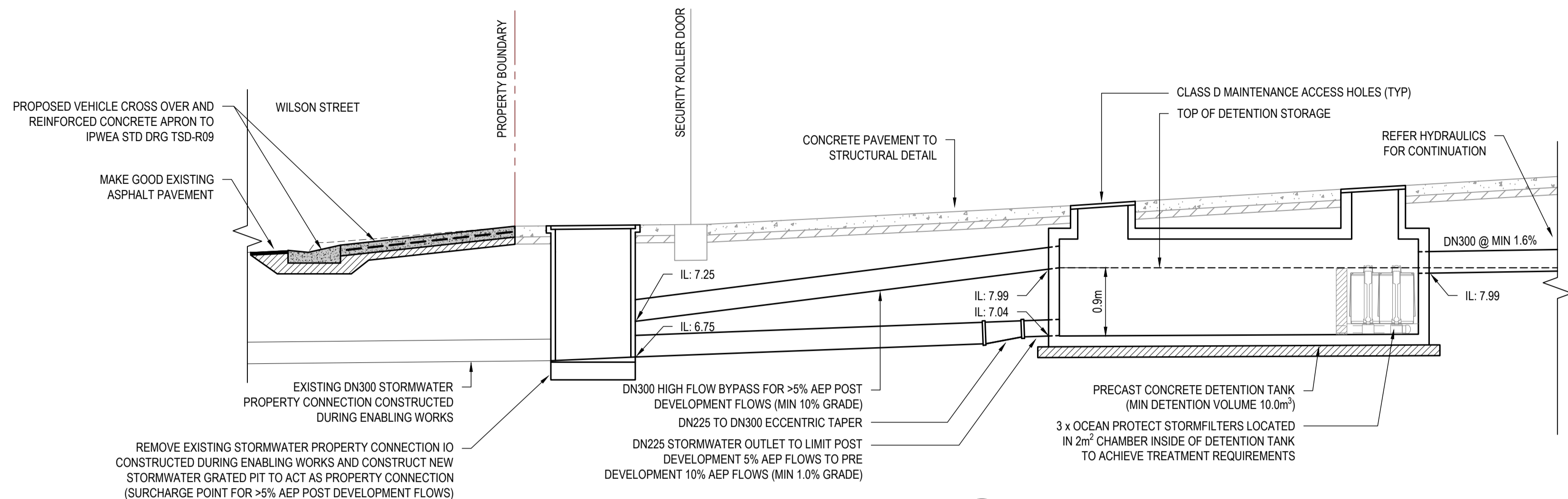
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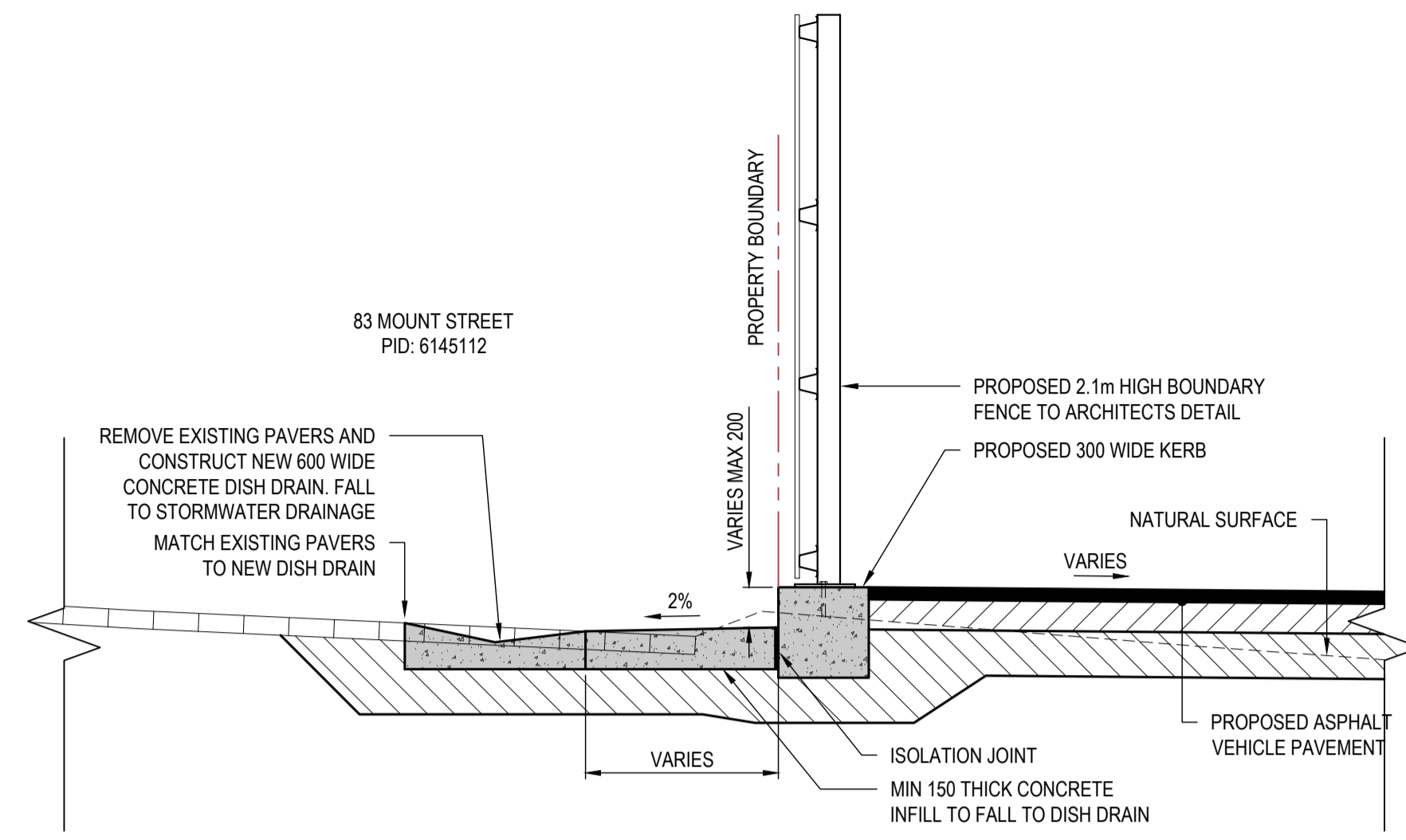
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PROJECT NO. 230912BS
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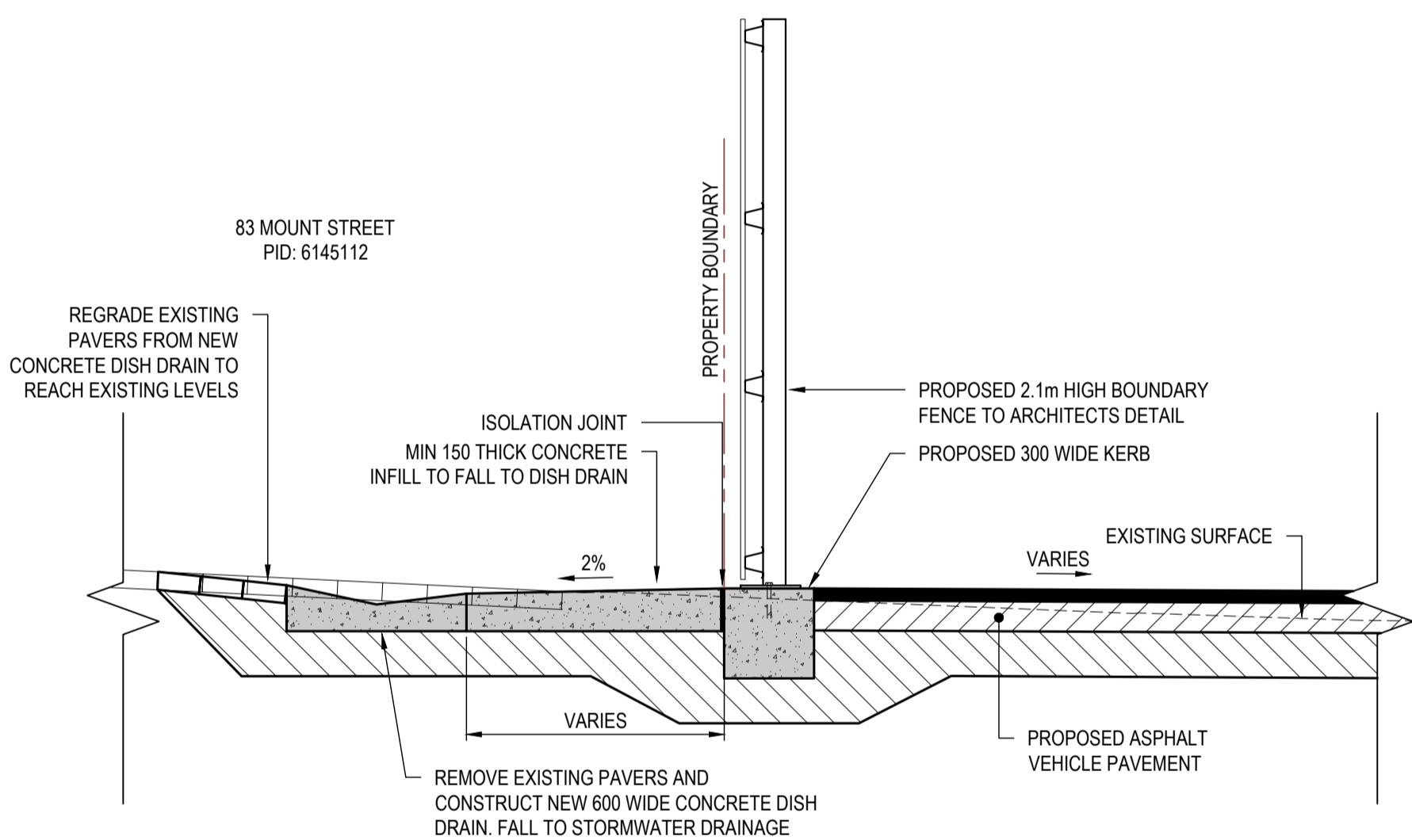
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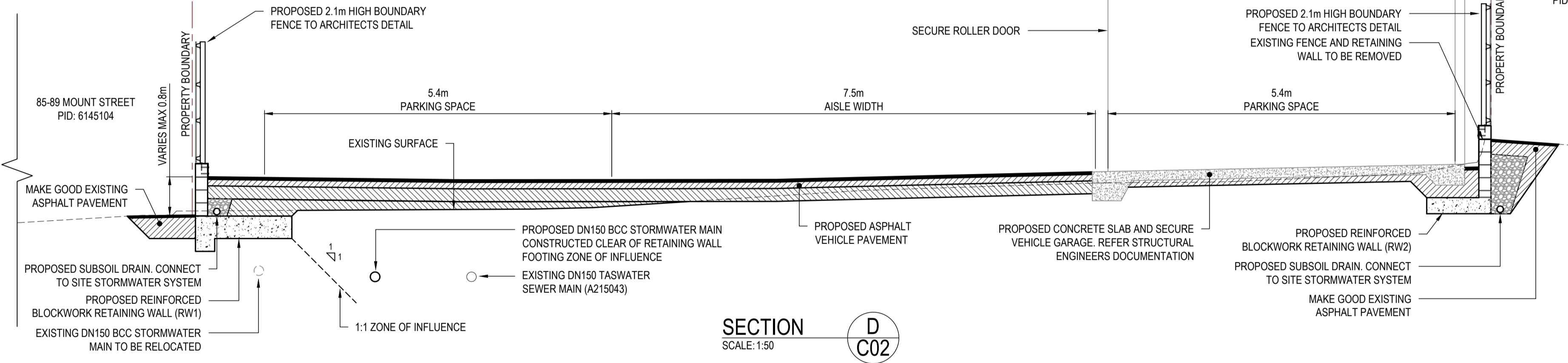
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C01



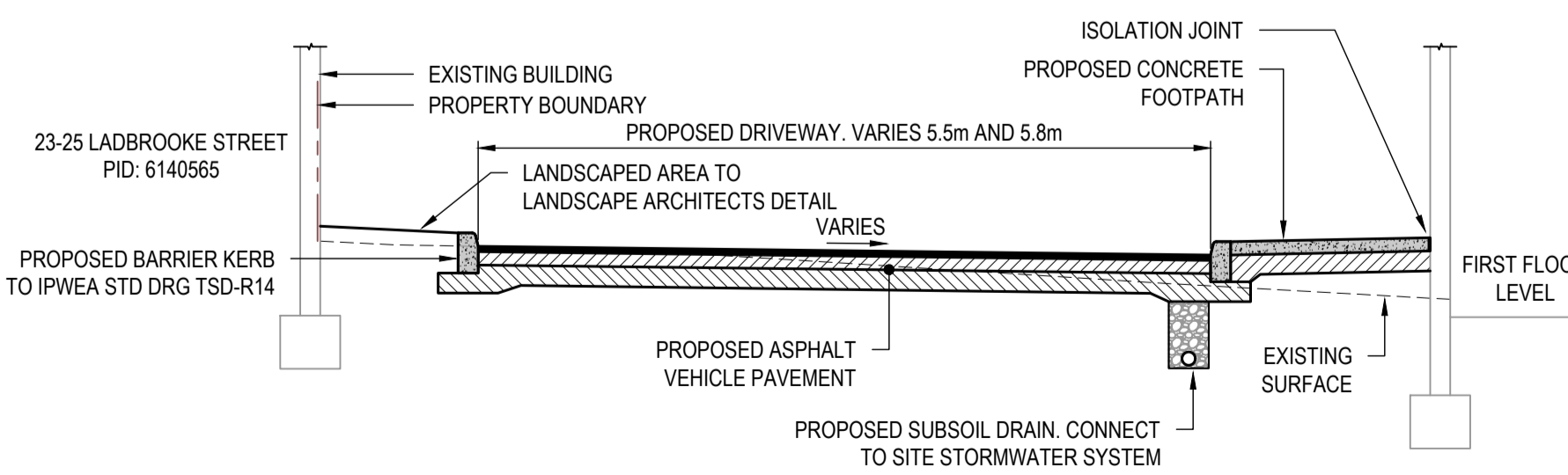
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SECTION C
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C02



SECTION D
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SECTION E
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PROJECT
 100-106 WILSON STREET
 BURNIE COURTS COMPLEX

TITLE
 CIVIL WORKS
 TYPICAL SECTIONS
 SHEET 1

Accepted C MALES (Discipline Head)	Date
Accepted N STOLP (Team Leader)	Date
Approved N STOLP (Principal)	Date

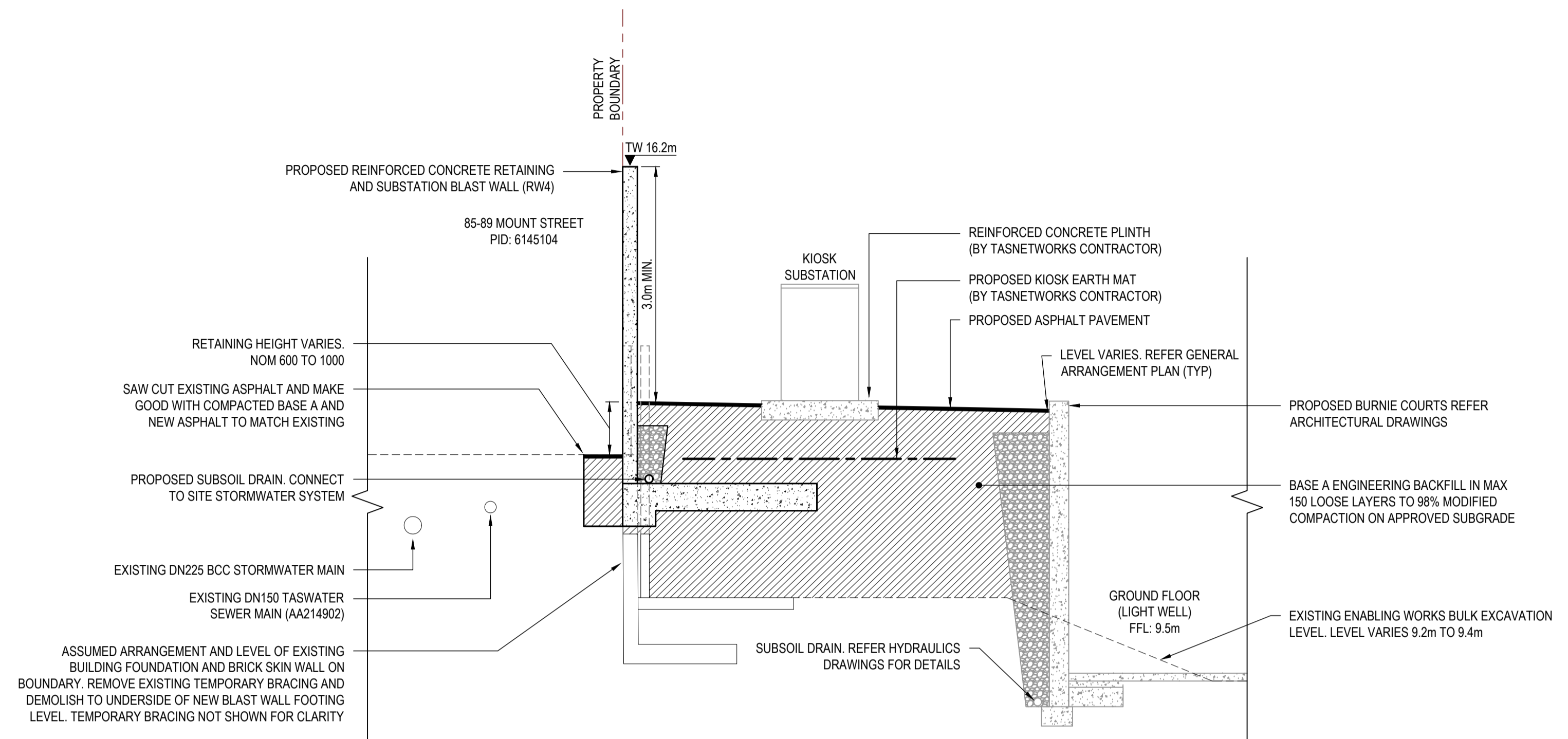
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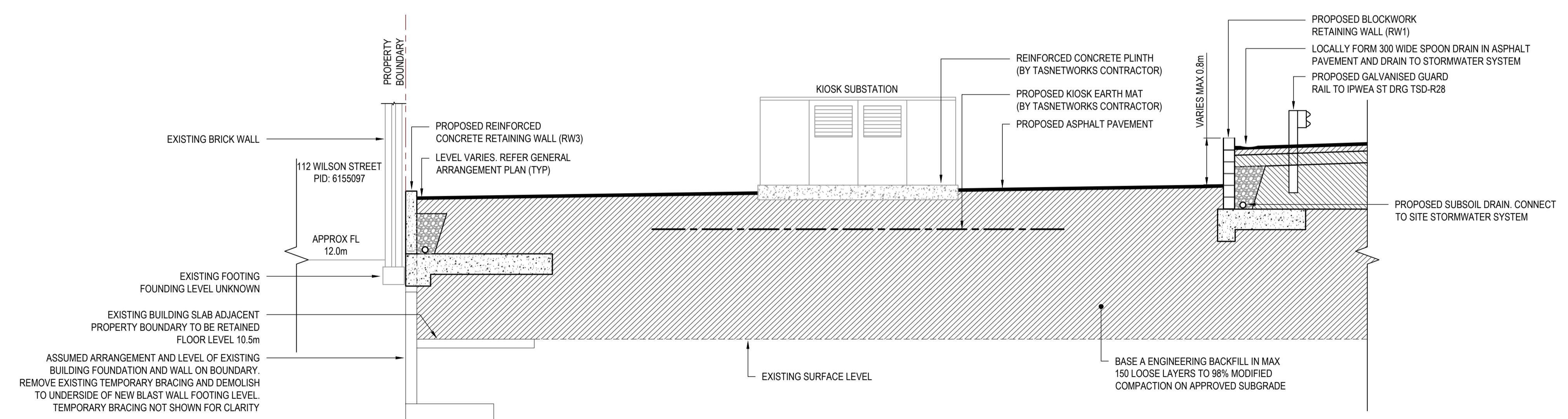
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PROJECT NO. 230912BS
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PROJECT
100-106 WILSON STREET
BURNIE COURTS COMPLEX

TITLE
CIVIL WORKS
TYPICAL SECTIONS
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Approved N. STOLP (Principal)	Date

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APPENDIX C

Site Catchment Flow Analysis

STORMWATER CALCULATIONS



PROJECT DESCRIPTION:	Burnie Courts Complex
PROJECT ADDRESS:	106 Wilson Street
PROJECT NUMBER:	230912BS
REVISION:	1

DATE:	26/06/2025
DESIGNED:	JFB
REVIEWED:	BHL

SITE PARAMETERS

	Pre Development		Post Development	
Site Area	3550	m ²	3550	m ²
Effective Impervious Area	3200	m ²	3550	m ²
Percentage Impervious	90	%	100	%
Runoff Coefficient	0.83	(-)	0.90	(-)
Time of Concentration	5	mins	5	mins

PEAK CATCHMENT FLOWS FOR GIVEN AEP AT T.O.C.

AEP	Pre Development		Post Development	
	I _{tc,Y} (mm/h)	Flow (L/s)	I _{tc,Y} (mm/h)	Flow (L/s)
10%	109.0	88.92	109.0	96.8
5%	129.0	110.50	129.0	120.3
2%	157.0	147.30	157.0	160.4
1%	179.0	175.24	179.0	190.8

Allowable Site Discharge: 88.9 L/s

DETENTION VOLUME AND ORIFICE SIZING

Detention			
Storm Duration (min)	Tank Inflow (L/s)	Tank Outflow (L/s)	Storage (m3)
5	120.2	88.9	9.97

Orifice		
Head (Above Orifice):	0.9	m
No. of Orifice:	1	(-)
Total Orifice Flow:	88.1	L/s
Orifice Diameter:	220	mm

APPENDIX D

Water and Sewer Demand Calculations

WATER ET CALCULATIONS

PROJECT DESCRIPTION:	Burnie Courts Complex
PROJECT ADDRESS:	106 Wilson Street
PROJECT NUMBER:	230912BS
REVISION:	1



DATE:	26/06/2025
DESIGNED:	JFB
REVIEWED:	BHL

EQUIVALENT TENEMENT RATES

Development Number	Development Type	Rate	Units	Value	ETs
Development 1	Office	0.004	GBFA (m ²)	3178	12.712
				Total ETs	12.712

WATER DESIGN FLOW CALCULATIONS



PROJECT DESCRIPTION:	Burnie Courts Complex
PROJECT ADDRESS:	106 Wilson Street
PROJECT NUMBER:	230912BS
REVISION:	1

DATE:	26/06/2025
DESIGNED:	JFB
REVIEWED:	BHL

DESIGN FLOWS

Variable	Formula	Calculated Value	Units	Comments
ETs	-	12.712	-	From Previous Sheet
Average Day Demand (AD)	$685 \text{ L/ET/day} \times \text{ETs}$	8708	L/day	Section 2.3.1 TasWater Supp
Mean Day Max Month (MDMM) demand	$1.5 \times \text{AD}$	13062	L/day	Section 2.3.4.1 TasWater Supp
Peak Day Demand (PD)	$2.25 \times \text{AD}$	19592	L/day	Section 2.3.4.2 TasWater Supp
Peak Hour Demand (PH)	$2.0 \times \text{PD}$	39185	L/hr	Section 2.3.4.3 TasWater Supp
Probable Simultaneous Demand (PSD)	$0.03 \times \text{ETs} + 0.4554 \times \text{SQRT}(\text{ETs})$	2.005	L/s	AS/NZS 3500.1:2003 Table 3.2.3
Peak Day Flow Rate	$\text{PD} / (\text{day/hour/minute})$	0.227	L/s	-

SEWER ET CALCULATIONS



PROJECT DESCRIPTION:	Burnie Courts Complex
PROJECT ADDRESS:	106 Wilson Street
PROJECT NUMBER:	230912BS
REVISION:	1

DATE:	26/06/2025
DESIGNED:	JFB
REVIEWED:	BHL

SITE PARAMETERS

Site Area	0.3495	ha
I(1,2) 1 hr duration rainfall intensity, ARI 2 years	17.6	mm/hr

EQUIVALENT TENEMENT RATES

Development Number	Development Type	Rate	Units	Value	ETs
Development 1	Office	0.006	GBFA (m ²)	3178	19.068
				Total ETs	19.068

SEWER DESIGN FLOW CALCULATIONS



PROJECT DESCRIPTION:	Burnie Courts Complex
PROJECT ADDRESS:	106 Wilson Street
PROJECT NUMBER:	230912BS
REVISION:	1

DATE:	26/06/2025
DESIGNED:	JFB
REVIEWED:	BHL

DESIGN FLOWS				
Variable	TasWater Formula	Calculated Value	Units	Comments
ETs	-	19.068	-	From Previous Sheet
ADWF	450 L/ET/day x ETs	0.0993	L/s	Appendix C TasWater Supp
d	$0.01*(\text{LOG}(A))^4 - 0.19*(\text{LOG}(A))^3 + 1.4*(\text{LOG}(A))^2 - 4.66*\text{LOG}(A) + 7.57$	10.01	-	Appendix C WSA02
PDWF	d x ADWF	0.994	L/s	Section 2.3.4.2 TasWater Supp
GWl	0.025 X A X Portion(wet)	0.00611625	L/s	Section 5.5.5.2 TasWater Supp
A _{eff}	A for Density > 150 EP/Ha	0.350	m ²	Appendix C WSA02
C	Saspect + Naspect	1.4	-	Section 5.5.5.2 TasWater Supp
Factor (size)	$(40/A)^{0.12}$	1.77	-	Appendix C WSA02
X	log(ARI)	0.699	-	Section 5.5.5.2 TasWater Supp
Factor (containment)	$0.77 \times (10^{(0.43X)}) / (10^{(0.14X^2)})$	1.3	-	Appendix C WSA02
I	$I_{1,2} \times \text{Factor}_{\text{size}} \times \text{Factor}_{\text{containment}}$	40.4	-	Appendix C WSA02
RDI	0.028 x Aeff x C x I	0.554	L/s	Appendix C WSA02
Design Flow	PDWF + GWl + RDI	1.55	L/s	Appendix C WSA02

APPENDIX E

Fire Head Loss Calculations

AS2419.1 2021 Table 2.2.5(B) NCC Building classification all classes not more than 25m height, fire compartment >1000m²

∴ Number of hydrant outlets= 2

AS2419.1 2021 Table 2.2.5(B) Minimum unassisted hydrant pressure & flow external or internal attack hydrant 10L/s @ 350kPa residual pressure each

Elevation loss from meter to hydrant on roof

Wilson Street surface level approx 9.5m AHD (proposed booster location)

Proposed building roof hydrant outlet= 24.0m AHD + 1m riser

∴ Δ elevation= 24.0m - 9.5m = 14.5m

Fitting loss from meter to hydrant splitting point

	k	No.	
90 degree bend	0.75	6 (assumption)	Sum k= 4.5

∴ Loss = $4.5 \times Vm/s^2 / 2g$
 $= 4.5 \times 2.54m/s^2 / (2 \times 9.81)$
 $= 0.58m$

flow rate to m³/s

10 L/s = 0.01 m³/s

Pipe friction loss from meter to hydrant splitting point

Assumption for pipe length: 45m

Assume: c=140, Q= 20L/s, d= 100mm, L=45m

Using Hazen Williams online head loss calculator= 26.9kPa= 2.69m

Area = $\pi \times r^2 = \pi \times (0.05)^2 \approx 0.00785 m^2$

Velocity = $\frac{0.01}{0.00785} \approx 1.27 m/s$

Fitting loss from hydrant splitting point to roof hydrant

	k	No.	
90 degree bend	0.75	12 (assumption)	Sum k= 9
Discharge	1	1	

∴ Loss = $10 \times Vm/s^2 / 2g$
 $= 10 \times 1.27m/s^2 / (2 \times 9.81)$
 $= 0.65m$

Pipe friction loss from hydrant splitting point to roof hydrant

Assumption for pipe length: 45m

Assume: c=140, Q= 10L/s, d= 100mm, L=45m

Using Hazen Williams online head loss calculator= 7.44kPa= 0.744m

Total head loss from meter to roof hydrant

Δ elevation= 14.5m

k(fittings)= 0.58m + 0.65m

pipe= 2.69m + 0.744m

∴ total= 20.164m= 201.64kPa

fire demand at meter

Hydrant= 20L/s

Sprinkler= 10L/s (from separate calculations)

$$= 30\text{L/s}$$

Elevation loss from connection at TasWater main to meter

I.L. TasWater main in Campbell Street = 8.0m AHD (assumed)

RL booster= 9.5m

$$\therefore \Delta \text{ elevation} = 9.5\text{m} - 8.0\text{m} = 1.5\text{m}$$

Pipe friction loss from connection at TasWater main to meter

Assumption for pipe length:

7m horizontal + 1.0m vertical= say 8m

Assume: $c=140$, $Q=30\text{L/s}$, $d=150\text{mm}$, $L=8\text{m}$

Using Hazen Williams online head loss calculator= 1.41kPa= 0.14m

Fitting loss from connection at TasWater main to meter

	k	No.	Sum k= 3
90 degree bend	0.75	4 (assumption)	

$$\therefore \text{Loss} = 4$$

$$= 4 \times V\text{m/s}^2 / 2g$$

$$= 4 \times 1.7 \text{ m/s}^2 / (2 \times 9.81)$$

$$= 0.26\text{m}$$

Fitting through detector check

Flow rate= 30L/s

From Emerson Valvcheq backflow online specification, loss= 75kPa= 7.5m

Total head loss from connection at TasWater main to meter Δ elevation= 1.5m

k(fittings)= 0.26m

pipe= 0.14m

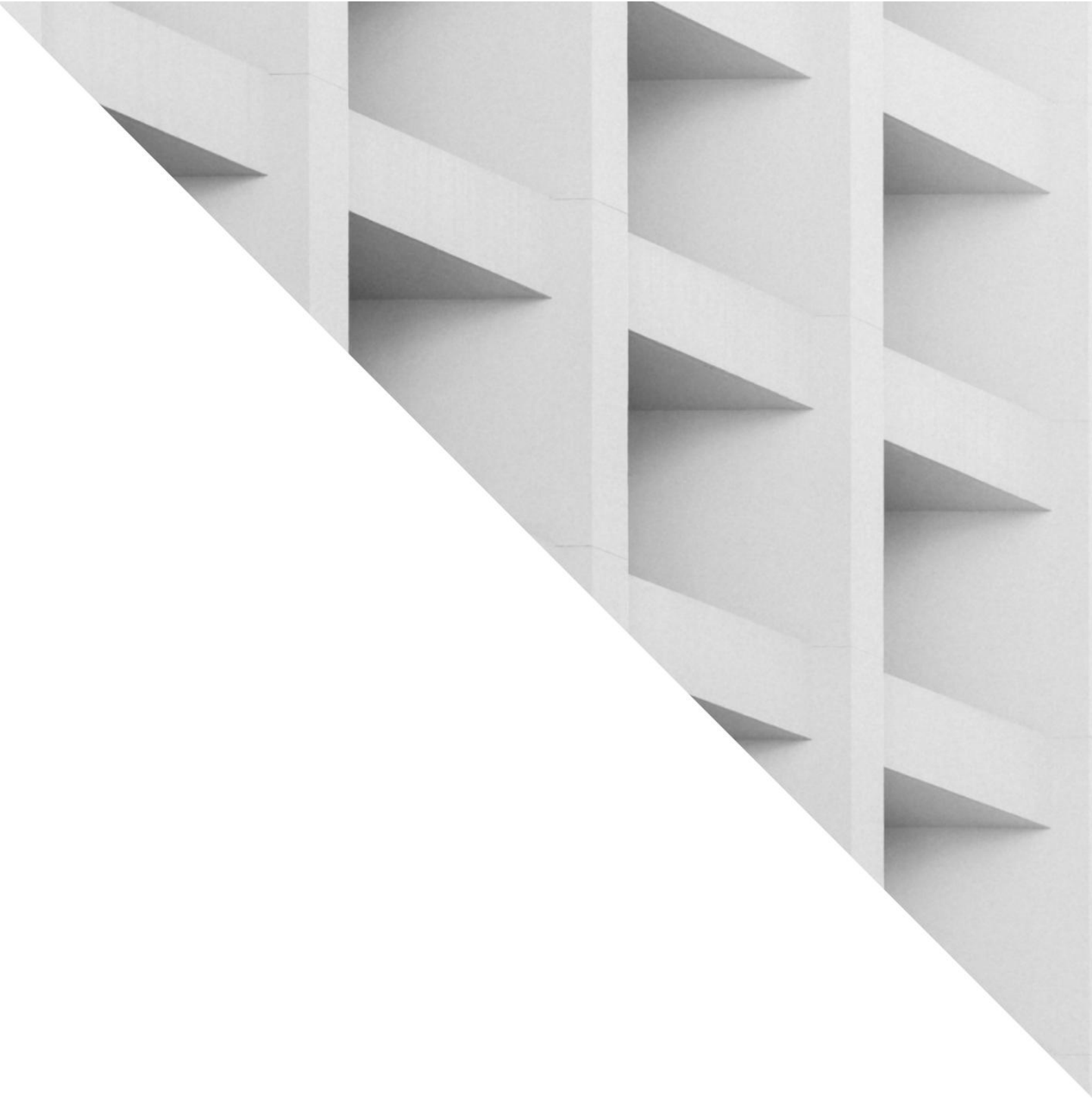
detector check= 7.5m

$$\therefore \text{total} = 9.40\text{m} = 94.0\text{kPa}$$

Required flow and pressure at connection to TasWater main in Wilson Street required to service development:

Demand at meter= 30L/s @ 621.64-kPa

Demand at connection to TasWater main= 30L/s @ 715.64kPa, say, 715kPa



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X Squared Architects
Burnie Court Complex
Traffic Impact Assessment
May 2025



CELEBRATING 15 YEARS
2008 - 2023

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1. Introduction

1.1 Background

Midson Traffic were engaged by X-Squared Architects to prepare a traffic impact assessment for a proposed new Burnie Court Complex development at 100-106 Wilson Street, Burnie. The proposed development is part of an \$86.5 million Tasmanian Government commitment to design and construct contemporary court facilities that improve operational requirements of modern courts.

The development comprises a purpose-built justice facility that will consolidate both Supreme and Magistrates Court functions within a single integrated complex. The facility will incorporate a Supreme Court, two Magistrate Courts, and a flexible alternate Court, including all required supporting accommodation and ancillary functions. The development will replace the existing Burnie Courts Complex at 38-40 Alexander Street, which no longer provides the essential functional and operational requirements of a modern court facility.

The facility necessitates specialised traffic and parking considerations due to its operational requirements, including:

- Secure vehicular access for judicial staff, court personnel, and correctional services.
- Dedicated access for custody transport vehicles.
- Controlled circulation paths that separate public, restricted staff, and secure prisoner movements.
- Interface with existing CBD transport infrastructure.
- Multiple user groups with varying parking and access needs

The proposed development is shown in Figure 1.

Figure 1 Proposed Development



1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management*, Part 12: *Integrated Transport Assessments for Developments*, 2020.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses of C2.0, *Parking and Sustainable Parking Code*, and C3.0, *Road and Railway Assets Code*, of the Tasmanian Planning Scheme – Burnie, 2021.

1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 29 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004
- Bachelor of Civil Engineering, University of Tasmania, 1995
- Engineers Australia: Fellow (FIEAust); Chartered Professional Engineer (CPEng); Engineering Executive (EngExec); National Engineers Register (NER)

1.4 Project Scope

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

1.5 Subject Site

The subject site is located at 100 - 106 Wilson Street, Burnie. The site currently accommodates commercial premises and a small retail component, but parts of the site are currently unused. The total area of the subject site is approximately 3,500 m². The previous uses of the subject site comprise of the following:

- 100 Wilson Street Vacant lot.
- 102 Wilson Street Picture framing shop with an estimated floor area of 130 m².
- 104 Wilson Street Restaurant with an estimated floor area of 215 m².
- 106 Wilson Street Vacant lot.

The subject site is zoned 'Southern Service Precinct' as defined in the Burnie City Master Plan.

The subject site and surrounding road network is shown in Figure 2.

Figure 2 Subject Site & Surrounding Road Network

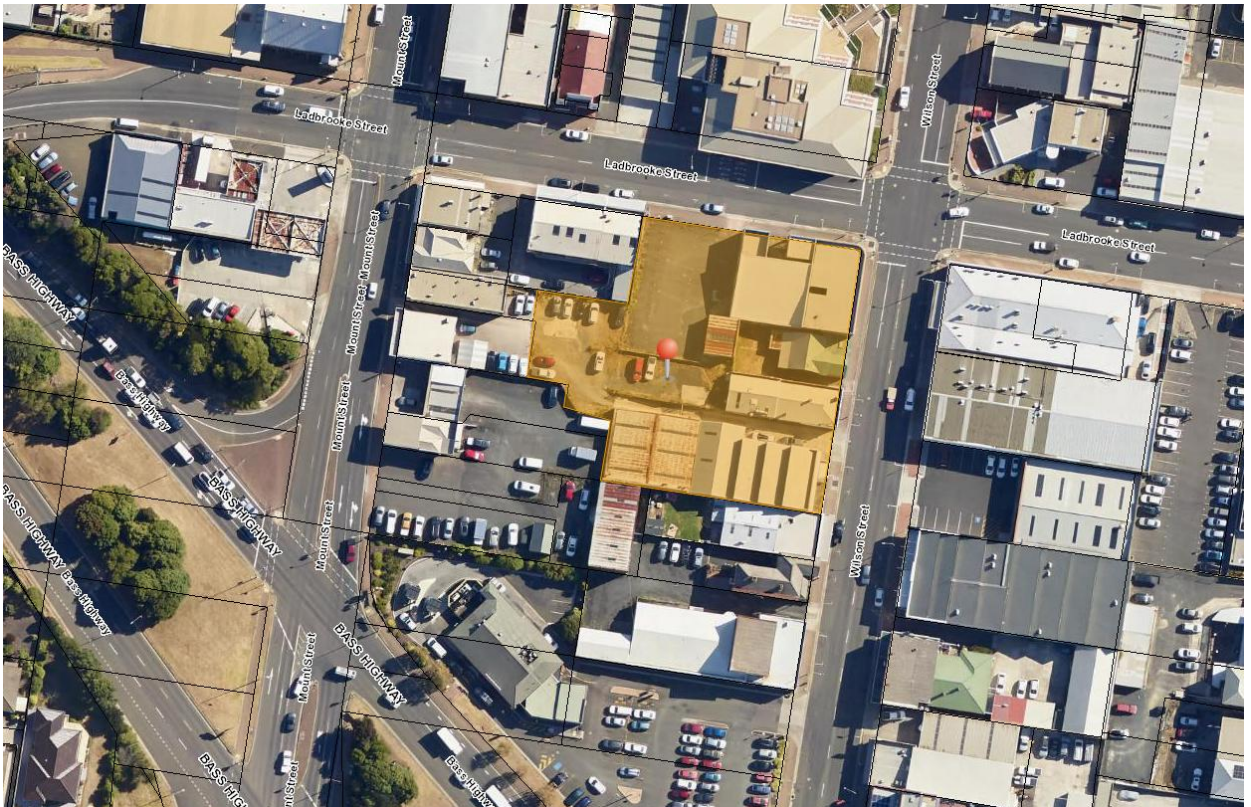


Image Source: LIST Map, DPIPWE

1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Tasmanian Planning Scheme – Burnie, 2021 (Planning Scheme)
- Austroads, *Guide to Traffic Management, Part 12: Integrated Transport Assessments for Developments*, 2020
- Austroads, *Guide to Road Design, Part 4A: Unsignalised and Signalised Intersections*, 2021
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Transport NSW, *Guide to Traffic Impact Assessment*, 2024 (TfNSW Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1)
- Australian Standards, AS2890.2, *Off-Street Parking, Commercial Facilities*, 2018 (AS2890.2)

2. Existing Conditions

2.1 Transport Network

For the purposes of this report, the transport network consists of Wilson Street and Ladbrooke Street.

2.1.1 Wilson Street

Wilson Street is a key CBD connector and will serve as the primary public entrance to the Burnie Courts Complex, accommodating pedestrian flows during peak court operational hours.

Wilson Street is classified as a local collector road managed by the Burnie City Council. It connects the Bass Highway in the north to Wellington Street in the south. It is one of the primary roads facilitating movement within the Burnie City Centre.

Wilson Street has a sealed pavement width of approximately 13.5 metres, with a total traffic lane width of 8 metres. On-street car parking is available on both sides of the road. Peak traffic volumes on Wilson Street are approximately 600 vehicles per hour during the AM peak and 750 vehicles per hour during the PM peak. Wilson Street connects to the Bass Highway at a signalised intersection. Pedestrian refuges are also provided between Cattley Street and North Terrace.

2.1.2 Ladbrooke Street

Ladbrooke Street will provide secondary secure access to the court facility, primarily serving staff parking and services.

Ladbrooke Street is local collector road managed by Burnie City Council. It connects between Marine Terrace to the east and View Road to the west, traversing over the Bass Highway. The street provides additional connectivity between the subject site and adjacent areas.

It has a kerb-to-kerb width of approximately 13.5 metres with an 8-metre-wide trafficable lane. On-street parking is available on both sides.

During peak periods, Ladbrooke Street carries approximately 250 vehicles per hour during the AM peak and 300 vehicles per hour during the PM peak.

2.2 Public Transport

The subject site is located in a CBD environment that has good public transport accessibility. Wilson Street includes a Metro bus stop near the subject site, servicing Route 192. Other nearby bus routes in Cattley Street and Mount Street that service most areas.

2.3 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5-year period between 1st January 2020 to 31st December 2024 for Wilson Street between Cattley Street to Spring Street, and the full length of Ladbroke Street.

The findings of the crash data is summarised as follows:

Wilson Street

- A total of 28 crashes were reported during this time.
- Severity. 1 crash resulted in a fatality; 1 crash resulted in minor injury; 1 crash involved first aid at the scene; 25 crashes resulted in property damage only.
- Time of day. 27 crashes were reported between 7:00 and 7:00pm. 1 crash was reported after 8:00pm. There was a relatively even distribution of crashes during the morning and afternoon periods.
- Day of week. No clear crash patterns were noted by day of week. 6 crashes were reported on Saturdays and Wednesdays; 5 crashes were reported on Fridays; 4 crashes were reported on Thursdays; 3 crashes were reported on Mondays and Tuesdays; 1 crash was reported a Sunday.
- Crash types. No clear crash trends were noted by crash type. 4 crashes involved 'emerging from driveway or lane'; 3 crashes involved 'left near' collisions at intersections; 4 crashes involved parking manoeuvres; 2 crashes involved 'left turn side swipe' collisions; and various other crash types with no clear trend.
- Crash locations. 6 crashes were reported at the Spring Street/ Wilson Street junction; 4 crashes were reported at the Cattley Street/ Wilson Street junction; 1 crash was reported at the Ladbroke Street/ Wilson Street junction; 17 crashes were reported at midblock locations; the crash locations are shown in Figure 3.
- Vulnerable road users. 2 crashes involved pedestrians ('near side' collision at 12:30pm, 4th January 2020 at the intersection of Cattley Street/ Wilson Street resulting in fatality; 'other pedestrian' collision at 2:35pm, 4th June 2021 at the intersection of Cattley Street/ Wilson Street resulting in property damage only); and 1 crash involved a motorcycle ('out of control' single vehicle crash at 6:30pm, 15th February 2024 at a midblock location resulting in minor injury).

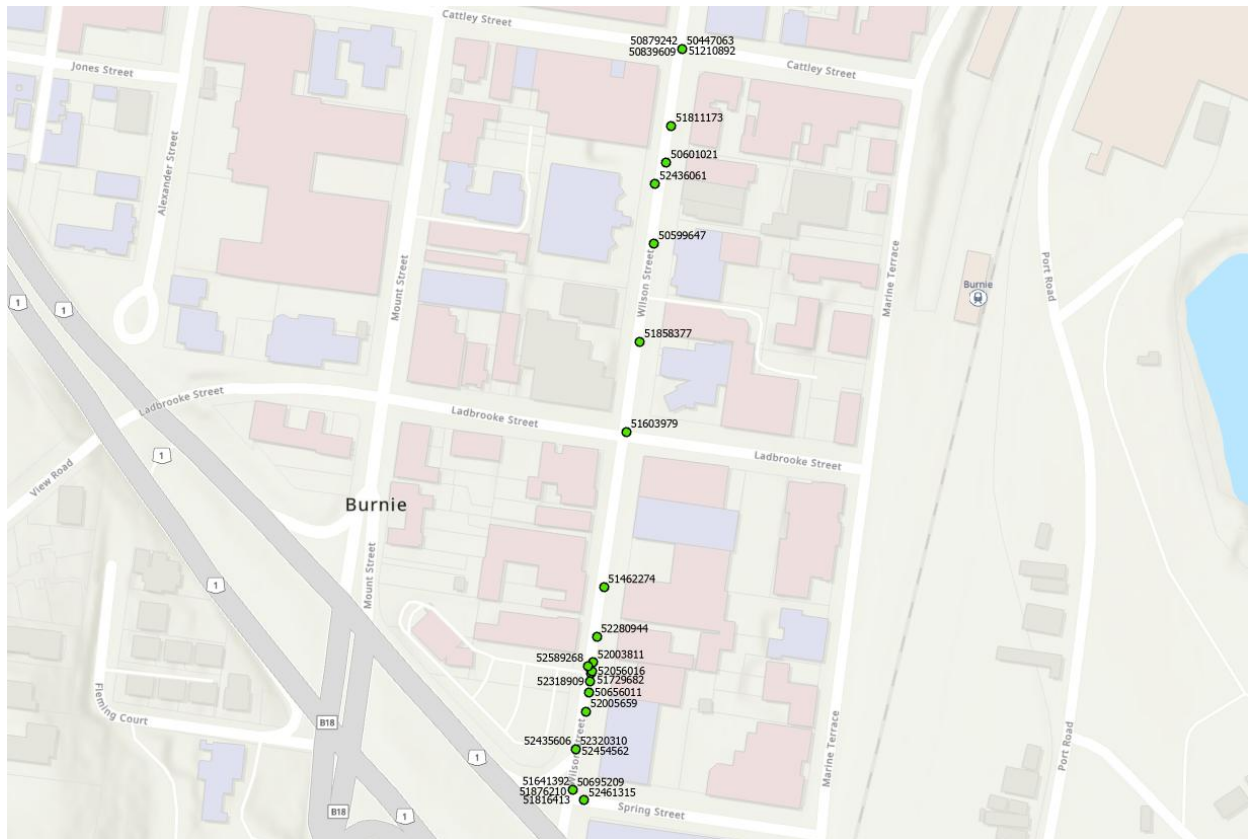
Ladbroke Street

- A total of 16 crashes were reported during this time.
- Severity. 2 crashes resulted in minor injury; 1 crash resulted in first aid at the scene; 13 crashes resulted in property damage only.

- Time of day. 12 crashes were reported between 7:30am and 6:00pm. 4 crashes were reported after 6:00pm. Afternoon crashes were dominant, with a total of 7 crashes reported between 12:00pm and 6:00pm.
- Day of week. Weekday crashes were dominant. 4 crashes were reported on Tuesdays and Thursdays; 3 crashes were reported on Wednesdays; 2 crashes were reported on Saturdays; and 1 crash was reported on a Monday, Friday and Sunday.
- Crash types. 4 crashes involved 'cross-traffic' collisions at intersections; 3 crashes involved 'emerging from driveway or lane'; 2 crashes involved 'rear end' collisions; 2 crashes involved 'right through' collisions at intersections; 2 crashes involved parked vehicles; and various other crash types with no clear trend.
- Crash locations. 10 crashes were reported at the Ladbroke Street/ Mount Street intersection; 6 crashes were reported at midblock locations. The crash locations are shown in Figure 4.
- Vulnerable road users. 1 crash involved a motorcycle ('right through' collision at 8:19pm, 23rd July 2020 at the Mount Street/ Ladbroke Street intersection resulting in minor injury). 1 crash involved a bicycle ('emerging from driveway' at 3:53pm, 2nd January 2024 at a midblock location resulting in first aid at the scene).

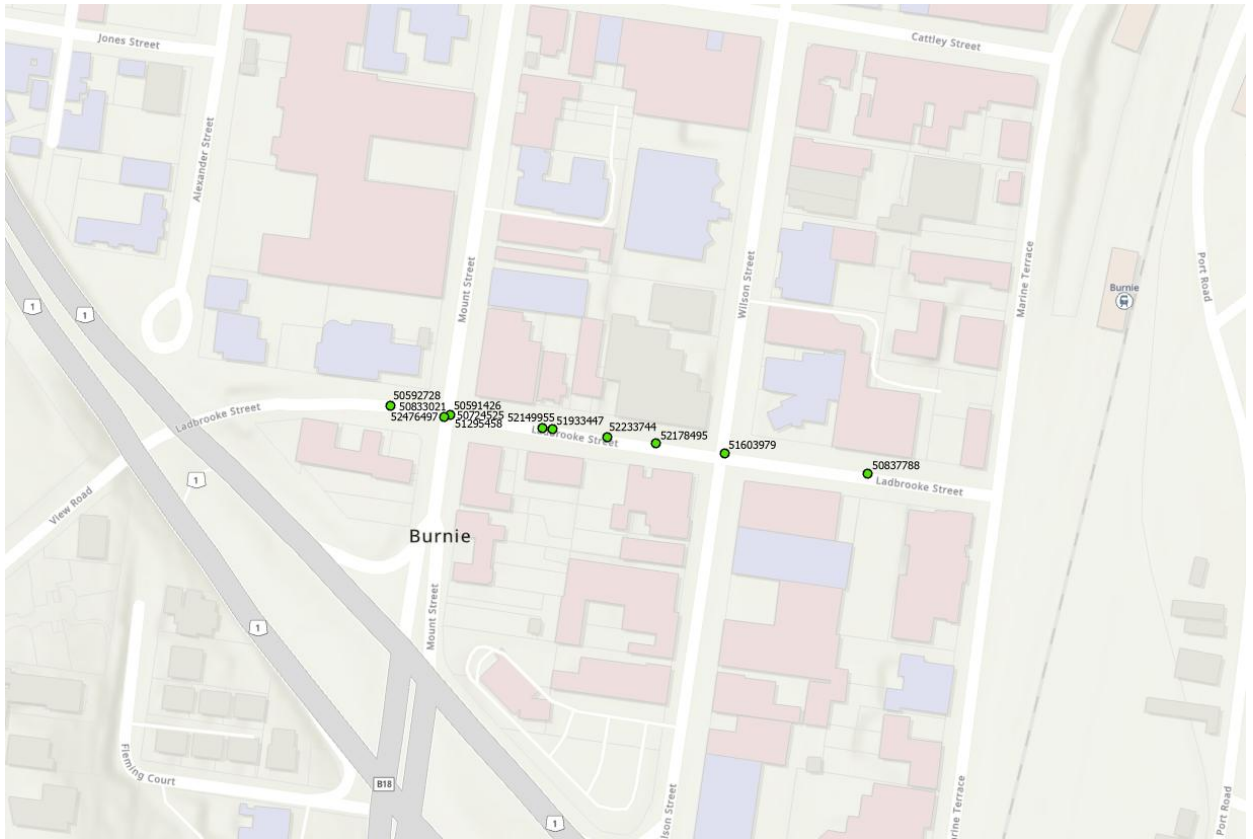
The crash history is considered to be relatively typical of a CBD environment. The crash rates at key intersections is considered to be a function of the high traffic flows on each approach. The fatal pedestrian crash is of concern, however it is noted that vulnerable road users generally have an increased crash severity and this type of crash severity outcome is relatively rare.

Figure 3 Wilson Street Crash Locations



Source: Department of State Growth

Figure 4 Ladbrooke Street Crash Locations



Source: Department of State Growth

3. Proposed Development

3.1 Development Proposal

The proposed development involves the demolition of the existing building structures on the site and the construction of a new court facility with frontage onto Wilson Street and Ladbrooke street. The development is part of the Tasmanian Government's \$86.5 million commitment to provide contemporary, innovative court facilities at 100-106 Wilson Street, Burnie.

The development comprises approximately 5,700 square meters of purpose-built justice facilities including:

- One Supreme Court (approx. 150m²) designed to accommodate multiple defendants (up to five).
- Two Magistrates Courts (approx. 120m² each) with secure dock facilities for up to three accused persons.
- One flexible alternate courtroom (approx. 150m²) built to Supreme Court standards but available for various judicial functions.
- Associated functional spaces including mediation rooms, waiting areas, interview rooms, administrative offices, chambers for judges and magistrates, and registry facilities.
- Specialised vulnerable witness facilities with separate secure access.
- Custody cells with direct secure connections to courtrooms, including purpose-built juvenile areas.

The site's circulation has been designed based on three separate zones:

- Public zone: Primary access via Wilson Street with forecourt area.
- Restricted zone: Staff and judiciary access with controlled entry points.
- Secure zone: Custody transport and custody circulation with completely separate access systems.

Secure staff parking provides 14 spaces accessed via Ladbrooke Street, with access controlled by secure gate facility. This includes:

- Four secured garage spaces (fully enclosed with individual doors).
- One accessible parking space.
- Eight staff parking spaces.
- Secure bicycle storage facility

Custody transport facility accessed via Wilson Street provides:

- Space for up to 4 custody transport vehicles.

- Secure area designed to accommodate specialized prisoner transport vehicles (up to 11 tonnes, 7.4m length).
- Direct secure access to custody areas

The proposed development significantly reduces the number of vehicular access points to the site (from four existing accesses to two controlled access points) and incorporates security measures that limit vehicle movements to authorized personnel only.

The proposed development is shown in Figure 5 and Figure 6.

Figure 5 Proposed Development Plans – Site Layout Plan

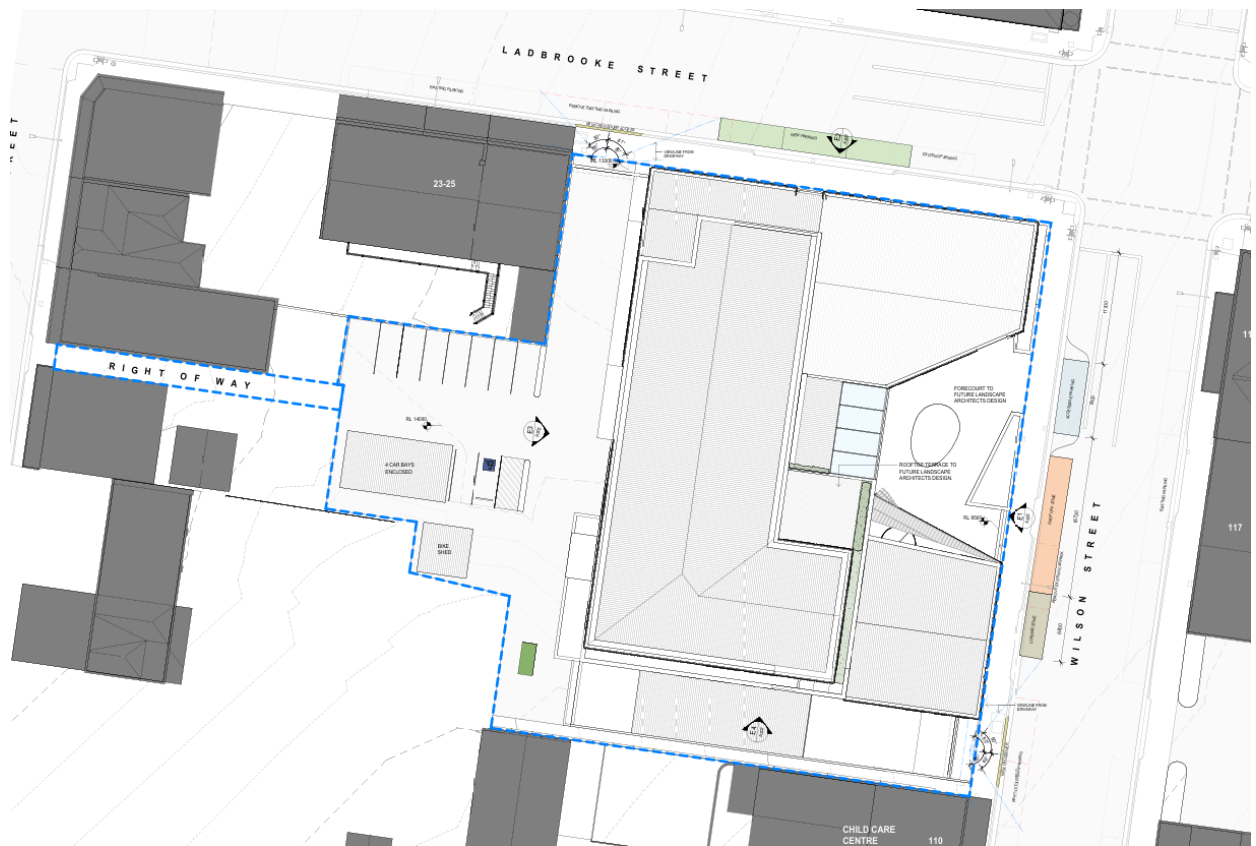
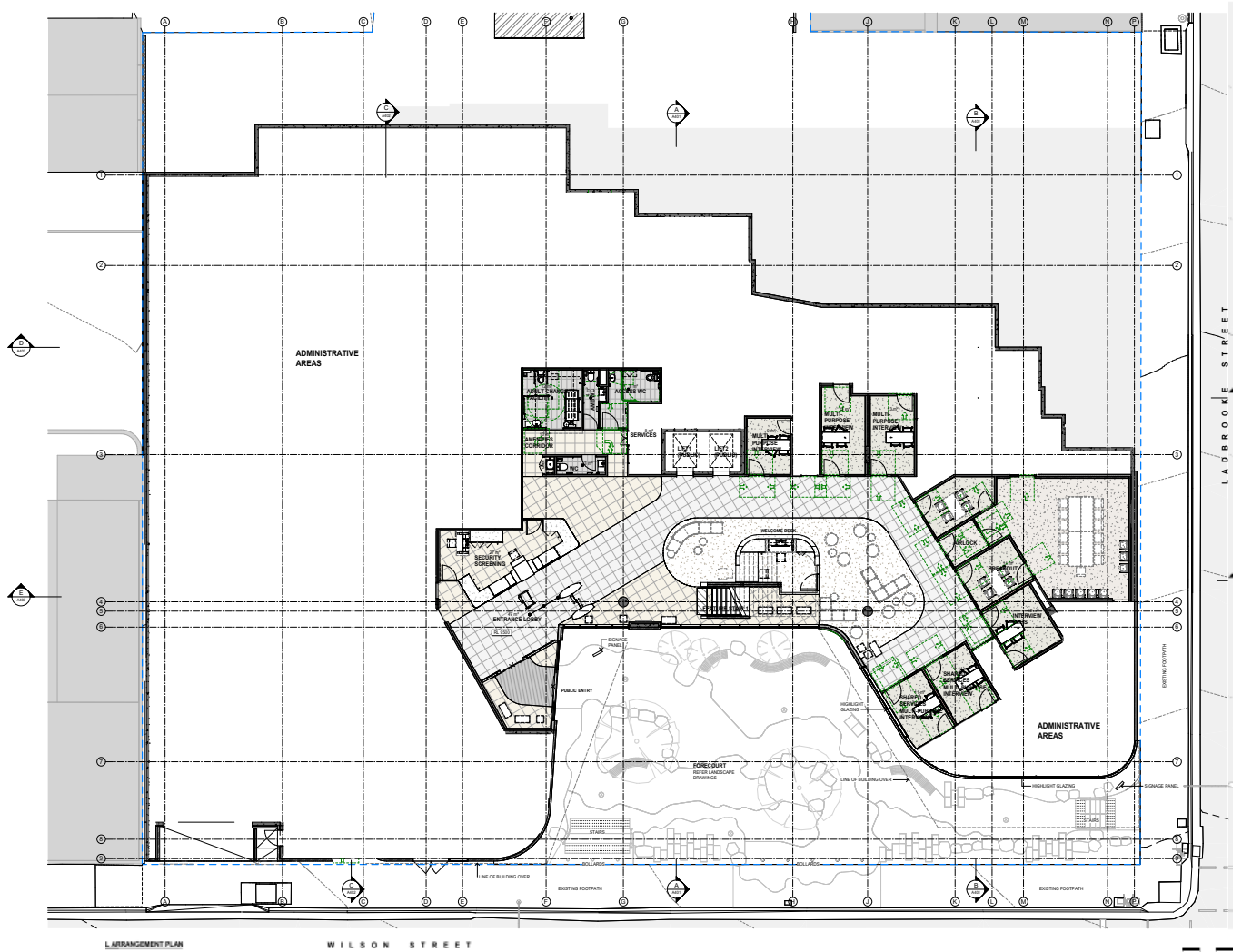


Figure 6 Proposed Development Plans – Ground Floor



4. Traffic Impacts

4.1 Trip Generation

4.1.1 Existing Site Traffic Generation

The traffic generation of the previous uses of the site is unknown. The traffic generation of the site is limited by the quantity of on-site car parking. In this case the parking for the site is limited to approximately 52 spaces. The existing parking is a mix of short-term and commuter parking and would have likely generated around 300 to 400 vehicles per day, with a peak of approximately 30 to 40 vehicles per hour.

The majority of traffic generation for the existing site occurs at the Ladbroke Street access.

4.1.2 Proposed Development Traffic Generation

Based on the traffic generation associated with similar court facilities¹, the peak generation of the proposed development will be as follows:

- AM peak generation 20 vehicles per hour
- PM peak generation 240 vehicles per hour

The AM peak is relatively low due to staggered court session arrivals and administrative activities. The PM peak is considered higher due to the dispersal of staff, court attendees, and other users at the end of the business day. It is noted that the parking on the site is not accessible for the general public. The majority of the traffic generation will therefore occur within the surrounding road network as on-street parking and parking within public car parking stations within Burnie CBD.

The traffic generation at the proposed development's accesses is estimated to be as follows:

- Ladbroke Street access 30 vehicles per day/ peak 10 vehicles per hour
- Wilson Street access 12 vehicles per day/ peak 2 vehicles per hour

The traffic generation at the site's accesses will therefore be lower than the previous use of the site, primarily due to the reduced number of parking spaces, and the nature of the parking spaces.

¹ Pitt & Sherry traffic assessment of 16-20 Mooreville Rd, February 2022

4.2 Trip Assignment

The majority of vehicle trips are expected to access the site via the Bass Highway intersections with Wilson Street and Mount Street. A smaller portion of vehicle trips may utilize alternative routes like Ladbrooke Street and View Road.

4.3 Access Impacts

The subject site has four existing vehicular accesses:

- One large crossover (approximately 14 metres wide) on Ladbrooke Street.
- Two crossovers on Wilson Street.
- An access via a formal right-of-way (R.O.W.) easement connecting to Mount Street.

The proposed development will removal all four existing vehicular accesses and provide two new accesses:

- A new access located at the western end of the Ladbrooke Street boundary frontage (secure car park access).
- A new access located at the southern end of the Wilson Street boundary frontage (secure custody transport access).

4.3.1 New Accesses Assessment

The Acceptable Solution A1.2 of Clause C3.5.1 of the Planning Scheme states "*For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority*".

In this case Council (as road authority) have not provided written consent, therefore the Acceptable Solution A1.2 of Clause C3.5.1 of the Planning Scheme is not met.

The Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme states:

"Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:

- (a) any increase in traffic caused by the use;*
- (b) the nature of the traffic generated by the use;*
- (c) the nature of the road;*
- (d) the speed limit and traffic flow of the road;*
- (e) any alternative access to a road;*
- (f) the need for the use;*

- (g) any traffic impact assessment; and
- (h) any advice received from the rail or road authority”.

The following is relevant with respect to the two proposed accesses:

- a. Increase in traffic. The proposed development will result in traffic generation being disbursed within the Burnie CBD network. The traffic generation at the two accesses to the site will be lower than the existing traffic generation of the existing (previous) use of the site due to the reduced number of on-site car parking spaces.
- b. Nature of traffic. Traffic generation at the site’s accesses will be securely controlled by gated entry and exit. Only authorised vehicles will be granted access to the site. The nature of traffic accessing the site will be a mix of staff and magistrate vehicles (Ladbroke street access) and custody transport vehicles (Wilson Street access).
- c. Nature of road. Both Wilson Street and Ladbroke Street are CBD roads that provide commercial property frontage access. The access requirements of the proposed development is consistent with the nature of the road.
- d. Speed limit and traffic flow. The general urban speed limit of 50-km/h is applicable to both Wilson Street and Ladbroke Street.
- e. Alternative access. The site has frontages to both Wilson Street and Ladbroke Street, providing ideal conditions for separating public access from secure operational movements, a critical requirement for modern court facilities.
- f. Need for use. The proposed development represents essential state infrastructure providing court services to the Burnie region. The access arrangements are necessary for the operation of a modern court facility that requires separation of public, staff, and secure custody movements.
- g. Traffic impact assessment. This report documents the findings of a traffic impact assessment.
- h. Road authority advice. Council (as road authority) require a TIA to be prepared for the development proposal.

Based on the above assessment, the proposed access arrangements satisfy the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.

4.3.2 Traffic Generation Assessment

The Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme states “ *Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than the amounts in Table C3.1*”. Whilst the accesses to the site are not technically ‘existing’ accesses (both accesses are located in new locations from existing site driveways), they are located in similar locations to existing driveways.

Table C3.1 specifies a maximum increase of:

- Vehicles up to 5.5m. 20% or 40 vehicles per day (whichever is greater).
- Vehicles over 5.5m. 20% or 5 vehicle movements per day (whichever is greater)

Based on the traffic generation analysis in Section 4.1.2, the proposed development will generate:

- Ladbroke Street access: 30 vehicles per day.
- Wilson Street access: 12 vehicles per day.

Breakdown by Vehicle Size:

Vehicles up to 5.5m in length:

- Ladbroke Street access: 30 vehicles per day (staff and judiciary vehicles).
- Wilson Street access: 7 vehicles per day (service and administrative vehicles).
- Total: 37 vehicles per day.

Vehicles over 5.5m in length:

- Wilson Street access: 5 vehicles per day (custody transport vehicles).
- Total: 5 vehicles per day.

The custody transport vehicles (Eight Seat Iveco at 6.6m length and 16 Seat Isuzu at 7.4m length) exceed 5.5m and are used for prisoner transport to court sessions as required.

Therefore, the proposed vehicle movements satisfy the requirements of Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme.

4.4 Sight Distance

4.4.1 Vehicle Sight Distance

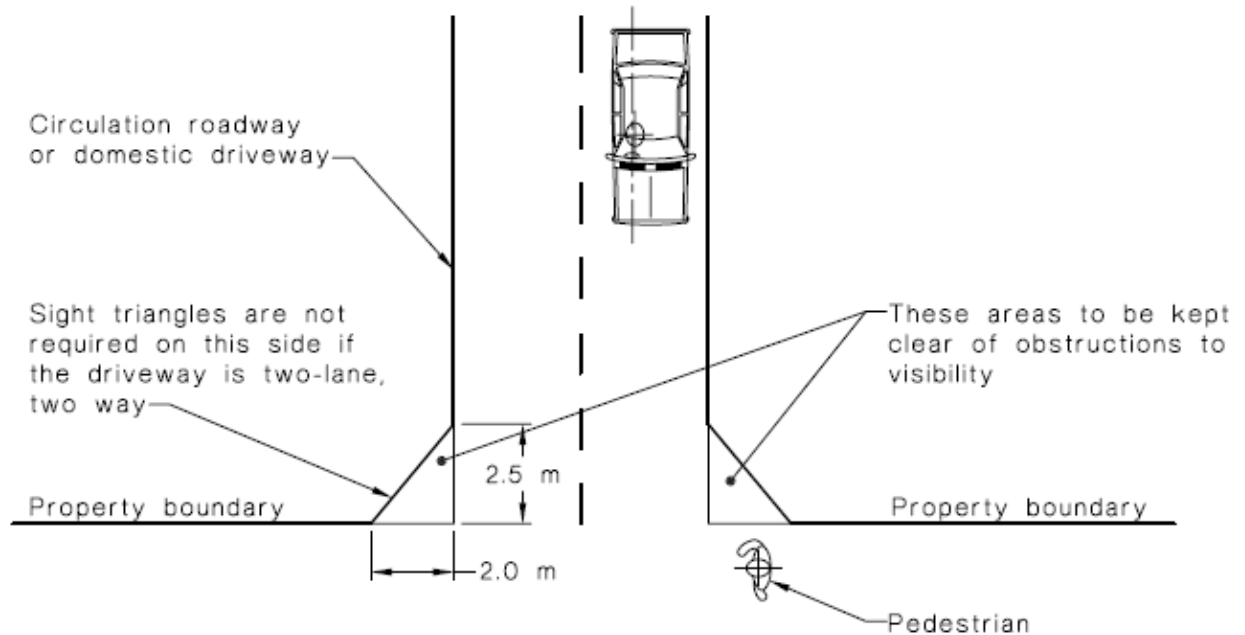
Australian Standards, AS2890.1, provide the sight distance requirements for commercial driveways. Sight distance requirements are lower for driveways compared to road junctions.

The minimum sight distance requirements for a commercial driveway access in a 50-km/h frontage road is 45 metres (the desirable sight distance is 69 metres). The available sight distance exceeds the minimum requirements in both directions at both the Wilson Street and proposed Ladbroke Street accesses and therefore satisfies the requirements of AS2890.1.

4.4.2 Pedestrian Sight Distance

AS2890.1 specifies a pedestrian sight triangle provision at driveway accesses that connect adjacent to a footpath. This is reproduced in Figure 7.

Figure 7 Pedestrian Sight Distance Requirements



The provision of a sight triangle is available at the Wilson Street driveway, but not at the Ladbroke Street access (noting that the sight triangle is only required on the exit side of the access due to the two-way flow).

It is noted that the Ladbroke Street access is accessible via a secure gate system. The operation of the gate when vehicles are exiting the site will provide warning for pedestrians that a vehicle will be exiting the site. A warning device (such as a warning light) should be considered at the access. The warning system can be linked to the operation of the gate.

4.5 Public Transport Impacts

The proposed development is located within a CBD environment and is likely to generate demand for public transport use. Court facilities typically attract diverse users including staff, legal professionals, jury members, witnesses, and members of the public attending hearings, many of whom may rely on public transportation.

4.5.1 Existing Public Transport Services

Public bus services operate on a regular basis along Wilson Street, Mount Street and Ladbroke Street providing connectivity with all regions within the greater Burnie area. Specifically:

- Metro Tasmania Route 192 services Wilson Street directly adjacent to the development site, providing connections to Burnie's suburbs.
- Additional routes operate along Cattley Street (approximately 100 metres north of the site) and Mount Street (approximately 200 metres west of the site).
- The central Burnie bus interchange is located approximately 400 metres northeast of the site, providing access to regional services.

4.5.2 Public Transport Accessibility

The site's CBD location provides several advantages for public transport users:

- Bus stops are located within 100 to 200 metres walking distance from the subject site's main entrance on Wilson Street.
- The walking routes between bus stops and the site entrance are along well-established footpaths with pedestrian crossing facilities.
- The forecourt design on Wilson Street provides suitable waiting space for court users transferring to public transport.
- The site is positioned within the walkable catchment of most Burnie CBD public transport routes.

The proposed development is expected to generate public transport demand throughout the day, with particular concentration during court session start and end times. Based on the anticipated staffing and visitor numbers, the existing public transport services should have sufficient capacity to accommodate this demand without requiring service upgrades.

For staff working regular business hours, the existing public transport timetables align well with typical court operational hours. For court attendees with specific appointment times, the regular frequency of services during business hours provides reasonable accessibility.

The development's emphasis on improving accessibility to the justice system aligns with the provision of good public transport options, particularly for vulnerable court users who may not have access to private vehicles.

4.6 Pedestrian Impacts

A relatively high standard of pedestrian infrastructure is provided on all roads connecting to the site.

The proposed development will generate a moderately high level of pedestrian activity, with the pedestrian activity focussed at the Wilson Street frontage.

The development includes a forecourt area that connects to Wilson Street that will facilitate a high level of service for pedestrian movements, with pedestrians able to congregate (or queue if necessary) within the forecourt without spilling onto the Wilson Street footpath.

4.7 Road Safety Impacts

No significant adverse road safety impacts are foreseen for the proposed development, as the predicted future peak traffic generation is not significant enough to generate any road safety deficiencies based on the following:

- The proposed development will reduce the number of vehicular accesses on the frontages of Wilson Street and Ladbrooke Street. This will reduce the associated conflict risk at the interface with the road network.
- The proposed development will reduce the number of on-site car parking spaces within the site. The available parking spaces will be securely controlled, thus only enabling permitted access for authorised vehicles (ie. removing unnecessary access to the site).
- The crash history does not provide an indication that there are any pre-existing road safety issues that may be exacerbated by traffic generated by the proposed development.
- Pedestrian access is via a wide forecourt that enables pedestrians to congregate (and queue if necessary) within the site without spilling onto the Wilson Street footpath.

5. Parking Assessment

5.1 Parking Provision

The proposed development provides a total of 14 on-site parking spaces accessed via Ladbrooke Street, and 4 custody transport vehicles accessed via Wilson Street.

The parking layout associated with these areas is shown in Figure 8 and Figure 9.

Figure 8 Car Parking Layout

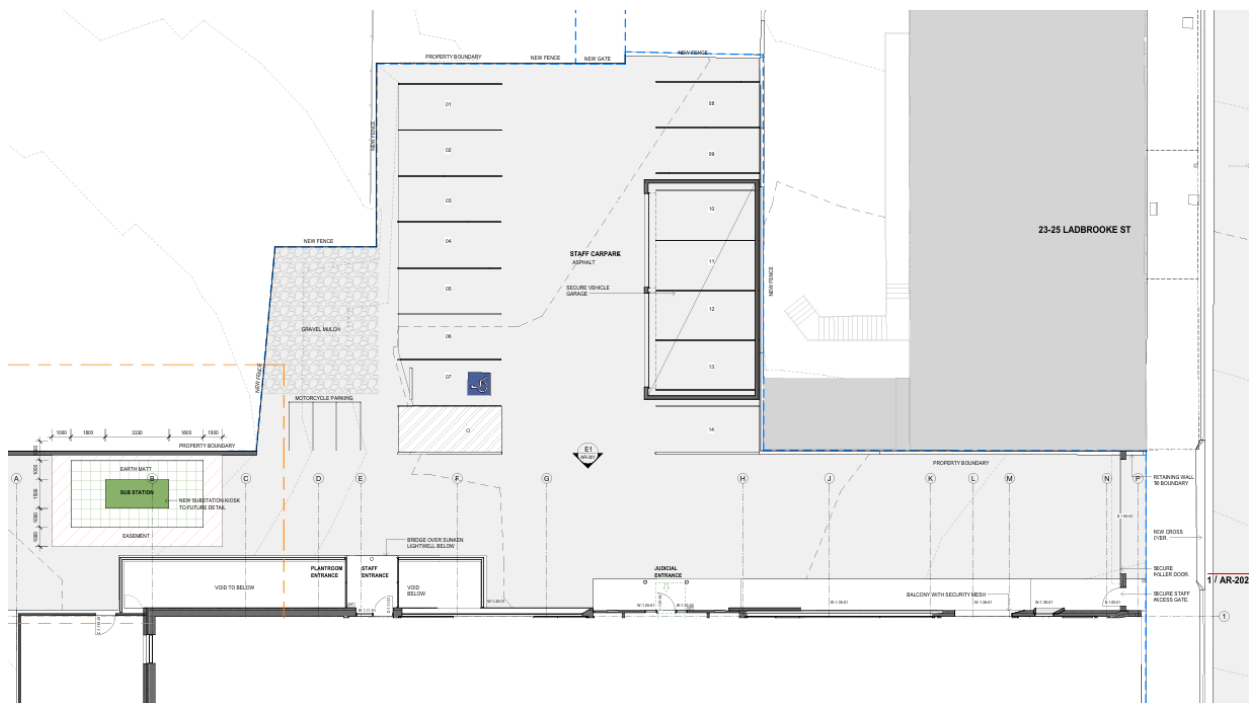
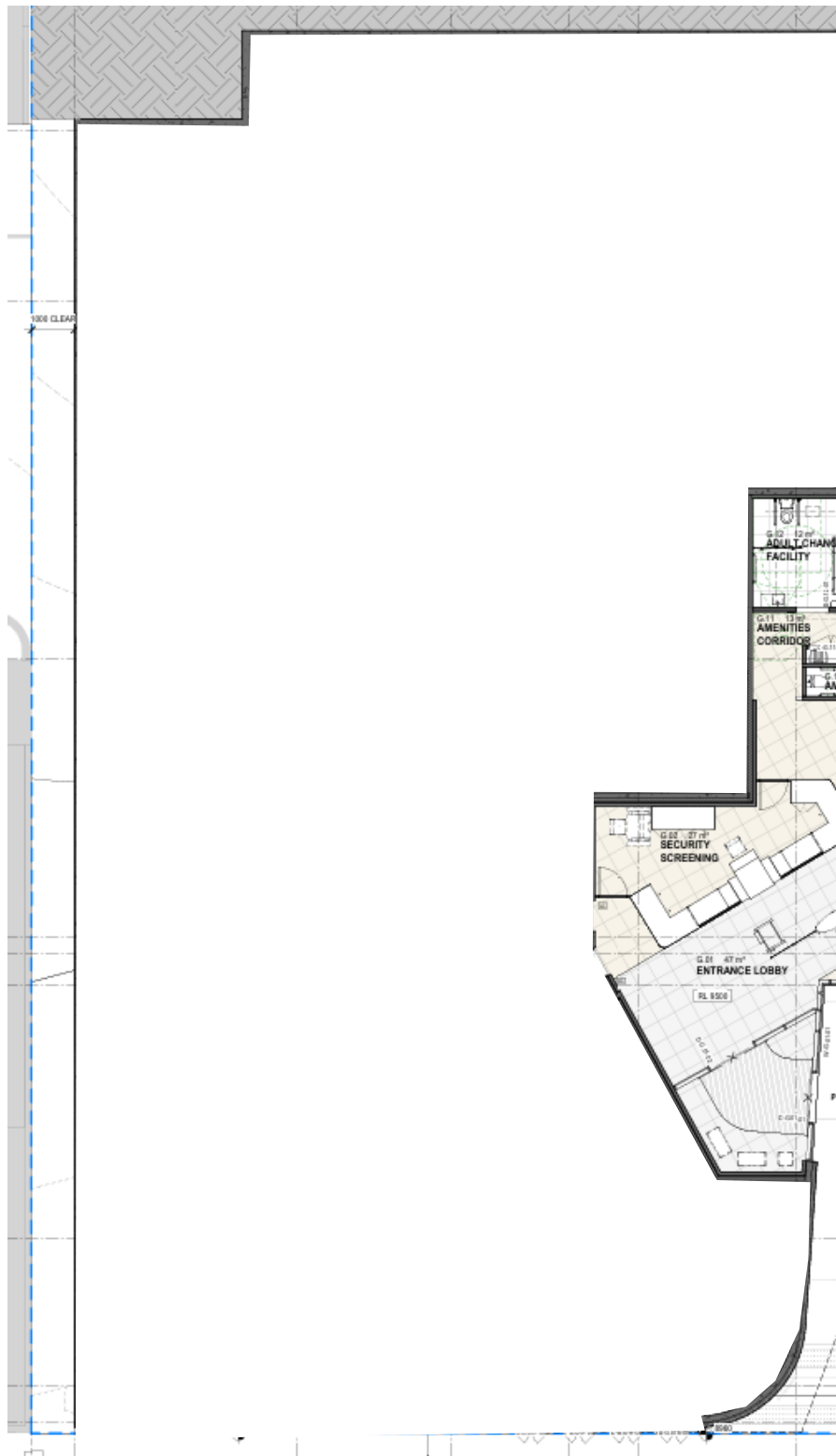


Figure 9 Car Parking Layout – Custody Transport Area



5.2 Planning Scheme Requirements

The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme states:

"The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:

(a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;

(b) the site is contained within a parking precinct plan and subject to Clause C2.7;

(c) the site is subject to Clause C2.5.5; or

(d) it relates to an intensification of an existing use or development or a change of use where:

(i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or

(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:

$$N = A + (C - B)$$

N = Number of on-site car parking spaces required

A = Number of existing on site car parking spaces

B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1

C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1".

In this case the site is located within the Burnie Town Centre Parking Specific Area Plan and therefore A1(b) is applicable.

The Acceptable Solution A1 of Clause C2.7.1 states *"within a parking precinct plan, on-site car parking must (a) not be provided; or (b) not be increased above existing parking numbers"*.

In this case the parking provision of 13 spaces is less than the existing provision of parking across the subject site and therefore the Acceptable Solution A1 of Clause C2.7.1 of the Planning Scheme is met. It follows that the Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme is also met.

5.3 Car Parking Layout

The car parking layouts of the proposed development are shown in Figure 8 and Figure 9.

The Acceptable Solution A1.1 of Clause C2.6.2 of the Planning Scheme states:

"Parking, access ways, manoeuvring and circulation spaces must either:

(a) comply with the following:

(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;

(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;

(iii) have an access width not less than the requirements in Table C2.2;

(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;

(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;

(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and

(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or

(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6".

The parking layout was assessed against the requirements of AS2890.1, A1.1(b) for the Ladbrooke Street access and AS2890.2 for the Wilson Street access in the following sections.

5.3.1 Ladbrooke Street Car Parking Assessment

The internal car park accessed via Ladbrooke Street has capacity for 14 spaces as shown in Figure 8. Four of these parking spaces are secured garage spaces, whereby the individual spaces are only accessible via a garage door and are fully enclosed. The parking will not be accessible to the public. Access to the car park will be controlled by a secure gate facility.

The car parking area includes one disabled parking space. The assessment of the disabled parking space is provided in Section 5.5.

Parking Space Dimensions

AS2890.1 defines the parking as User Class 1A ("residential, domestic and employee parking"). The dimensional requirements for 90-degree parking for User Class 1A are as follows:

- Space length 5.4 metres
- Space width 2.4 metres

- Aisle width 5.8 metres

The parking dimensions are typically as follows:

- Space length 5.4 metres
- Space width 2.4 metres (normal spaces), 2.6 metres (enclosed spaces)
- Aisle width 7.4 metres

The parking space dimensions therefore comply with AS2890.1 requirements.

Parking Grade

Section 2.4.6 of AS2890.1 states that the maximum grades within a car park shall be:

- Measured parallel to the angle of parking 1 in 20 (5%)
- Measured in any other direction 1 in 16 (6.25%)

The grades of the parking spaces are effectively level, thus complying with the AS2890.1 grade requirements.

The parking grades are shown in Figure 10.

Access Width Requirements

AS2890.1 defines the access as 'Category 1' access facility (Class 1A parking with less than 25 spaces fronting onto a local road). The AS2890.1 minimum driveway width requirement for a Category 1 access is 3.0 metres.

The width of the access driveway (6.4 metres at the roller door) complies with this requirement.

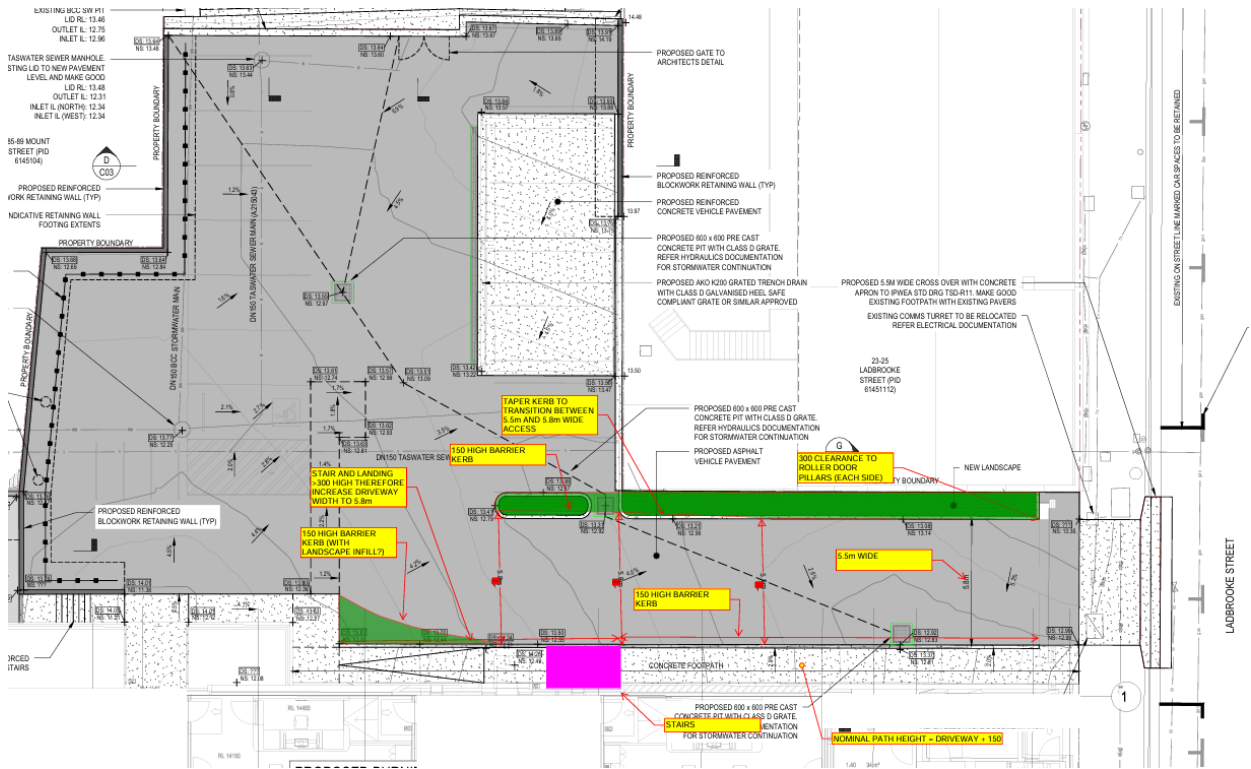
Circulation Roadway Width

AS2890.1 requires a minimum circulation roadway of 5.5 metres. Where a circulation roadway is adjacent to a vertical wall or structure, an additional clearance of 0.3 metres is required (5.8 metres minimum).

The access measures 5.5 metres width driveway, tapering to 5.8 metres at the steps where the obstruction is greater than 150mm high. The access design complies with the requirements of AS2890.1.

The access dimensions is provided in Figure 10.

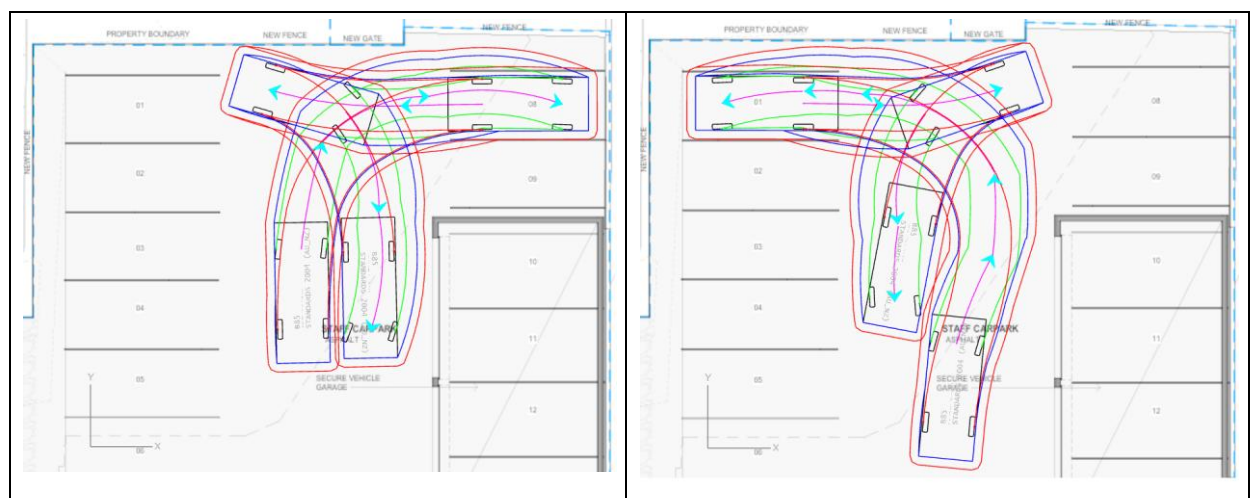
Figure 10 Car Parking Layout

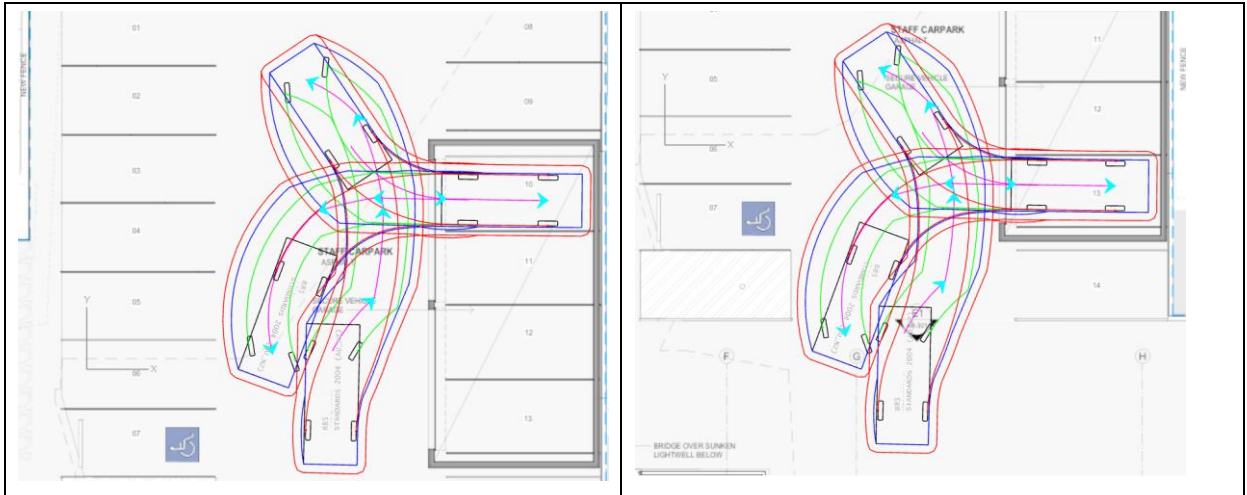


On-site Turning

The car parking layout enables on-site turning such that all vehicles can enter and exit the site in a forward direction. The car parking layout was tested using B85 vehicle swept paths as shown in Figure 11.

Figure 11 B85 Vehicle Swept Path Assessment





5.3.2 Wilson Street Parking Assessment

A secure custody transport parking facility is proposed, with access from Wilson Street. This area provides access to four parking spaces designed for a specific transport vehicle.

The secure parking area has been designed to accommodate the following custody transport vehicles as specified in the development brief:

- Eight Seat Iveco: 7.2 Tonnes, 6.6m Length, 2.45m Width, 3.1m Height.
- 16 Seat Isuzu: 11 Tonnes, 7.4m Length, 2.45m Width, 3.1m Height.

These vehicles fall into the classification of Service Vehicles (SRV) under AS2890.2, specifically designed for prisoner transport with specialized security requirements.

AS2890.2 provides requirements for commercial vehicle facilities. For the proposed custody transport facility, the following standards apply:

- Loading Bay Dimensions (parking): The parking bays provided are 7.5m in length and 3.5m in width, exceeding the minimum dimensions required for SRV vehicles under AS2890.2. This additional space accommodates the specialized nature of prisoner transfer operations where doors may need to be fully opened in a secure environment.
- Vertical Clearance: A minimum of 3.5m vertical clearance is provided throughout the facility, exceeding the 3.1m height of the tallest vehicle to ensure adequate overhead space for vehicle operations.

The Wilson Street access incorporates the following design features:

- Access Width: The access driveway exceeds 6.0m width at the property boundary, complying with AS2890.2 requirements for commercial facilities accommodating two-way movements.
- Security Gate: A secure roller door is provided at the entrance, allowing complete access control to the sally port area.
- Sightlines: Adequate sightlines are maintained for vehicles exiting onto Wilson Street, with more than 45m visibility in both directions.
- Gradients: The internal gradients are less than 1:20 (5%), complying with AS2890.2 requirements for service areas.

Turning Movements

Swept path analysis has been conducted for the custody transport vehicles as shown in Figure 12. The analysis demonstrates that:

- All vehicles can enter and exit in a forward direction.
- Adequate turning area is provided within the sally port to allow vehicles to manoeuvre without requiring multiple-point turns.
- Circulation within the sally port allows vehicles to park in any of the four designated spaces.
- The design accommodates the turning requirements of the largest expected vehicle.

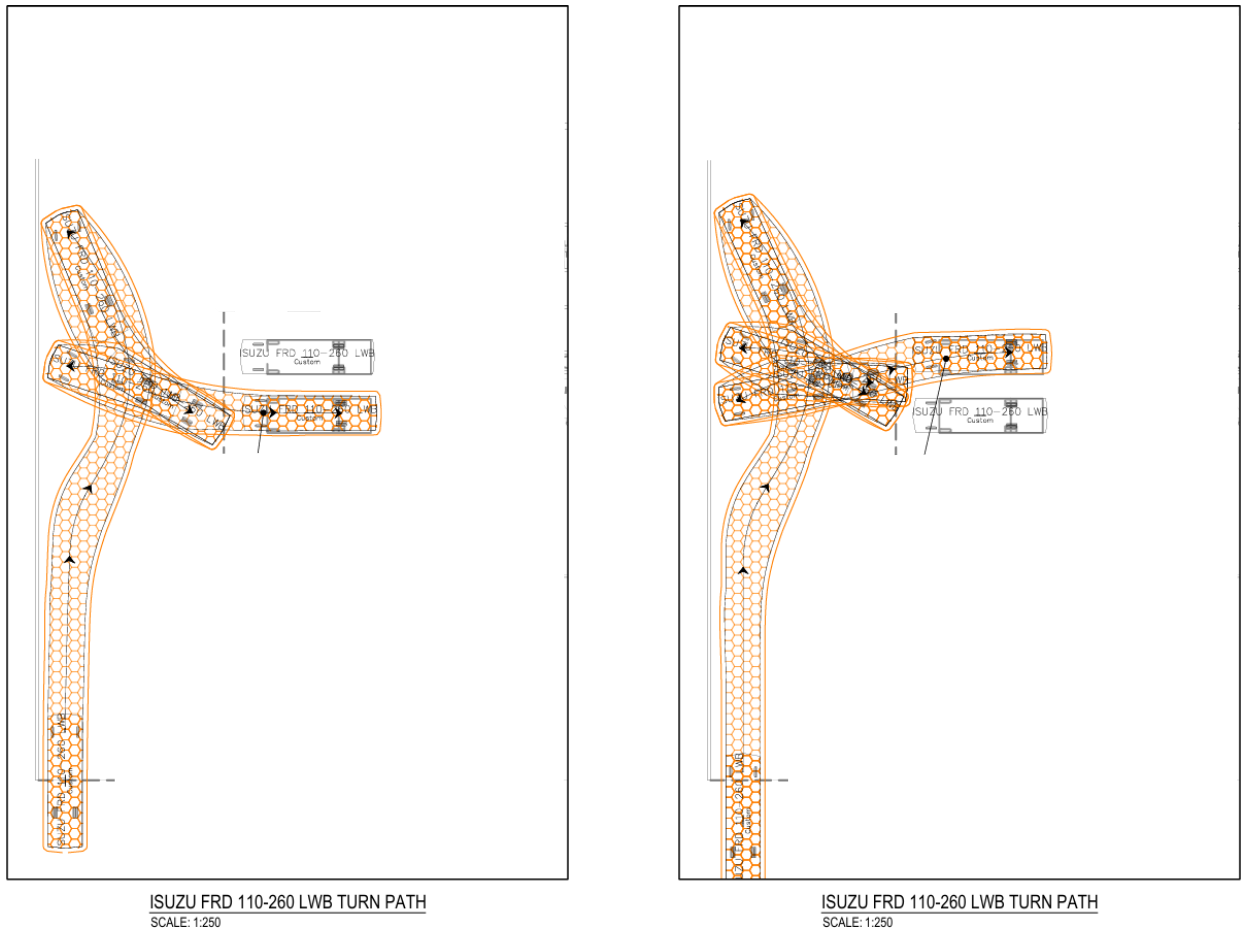
The turning paths demonstrate that vehicles can execute the required movements within the confines of the secure area without creating conflicts or requiring reverse entry from Wilson Street.

Compliance Summary

The custody transport parking area complies with the dimensional, access, and circulation requirements specified in AS2890.2 for commercial vehicle facilities. The design provides additional space beyond the minimum requirements to accommodate the specialized security needs of prisoner transport operations.

The secure nature of the facility, with controlled access and complete separation from public areas, aligns with best practice for court custody facilities while maintaining compliance with relevant Australian Standards.

Figure 12 Custody Transport Swept Paths



5.3.3 Parking Layout Summary

The car parking layout complies with the requirements of AS2890.1 and AS2890.2, thus satisfying the requirements of Acceptable Solution A1.1(b) of Clause C2.6.2 of the Planning Scheme.

5.4 Bicycle Parking

The subject site is located within the Burnie Town Centre Parking Specific Area Plan and therefore the provision of bicycle parking is not required under the Planning Scheme.

The proposed development provides a secure bicycle parking facility for the staff use. The bicycle parking facility is located just inside the staff entrance on Wilson Street.

5.5 Disabled Parking

The Acceptable Solution A1.2 of Clause C2.6.2 of the Planning Scheme states:

"Parking spaces provided for use by persons with a disability must satisfy the following:

(a) be located as close as practicable to the main entry point to the building;

(b) be incorporated into the overall car park design; and

(c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities".

The following is relevant with respect to the development proposal:

- a. Location. One disabled parking space is located immediately adjacent to an entrance to the building accessed from the car park via the Ladbrooke Street access.
- b. Car park design. The disabled parking space has been incorporated into the design of the car park.
- c. Disabled space design. The disabled parking space complies with the dimensional requirements in AS2890.6, including the adjacent shared area.

The disabled parking provision therefore satisfies the requirements of Acceptable Solution A1 of Clause 2.6.2 of the Planning Scheme.

5.6 On-Street Parking

Some changes to the on-street parking layout are proposed. This includes the provision of an accessible parking space, a loading zone and a drop off zone. Future discussions with Council will consider the replacement of some on-street car parking with a taxi, drop off/pickup zone outside the facility on Wilson Street.

This will reduce left turn lane of Wilson Street to 11.7 metres. The peak traffic flow on Wilson Street is approximately 600 vehicles per hour during the AM peak and 750 vehicles per hour during the PM peak (two-way flow). The capacity of the southern approach to the intersection will therefore not be compromised by the reduction of the left lane on the approach to the intersection.

Any changes to on-street parking arrangements will require approval from Council as the road authority

Figure 13 On-Street Parking Changes



6. Conclusions

This traffic impact assessment (TIA) investigated the traffic and parking impacts of a proposed new Burnie Courts Complex facility at 100 Wilson Street, Burnie. The key findings of the TIA are summarised as follows:

- The traffic generation of the development at its two access locations is likely to be as follows:
 - Ladbroke Street access 30 vpd/ peak 10 vph
 - Wilson Street access 12 vpd/ peak 2 vph
- The new accesses on Ladbroke Street and Wilson Street satisfy the requirements of Acceptable Solution A1.2 of Clause C3.5.1 of the Planning Scheme. The traffic generation at each of these accesses satisfy the requirements of Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme.
- The proposed development provides a total of 14 secure on-site car parking spaces accessed via Ladbroke Street. On-site parking is also provided for four custody transport vehicles with access via Wilson Street. The subject site is located within the Burnie Town Centre Parking Specific Area Plan and the parking provision satisfies the requirements of Acceptable Solution A1 of Clause C2.7.1 of the Planning Scheme.
- The car parking layout for both the staff parking area (Ladbroke Street access) and custody transport area (Wilson Street access) comply with the requirements of AS2890.1 and AS2890.2 respectively, satisfying Acceptable Solution A1.1(b) of Clause C2.6.2 of the Planning Scheme.
- Sight distance at both access points exceeds the minimum requirements of AS2890.1 for commercial driveways in a 50-km/h zone.
- A pedestrian warning system (such as warning lights) should be installed at the Ladbroke Street access where the required pedestrian sight triangle cannot be provided. This system should be linked to the secure gate operation to alert pedestrians when vehicles are exiting the facility.
- The proposed development will reduce the number of vehicular access points to the site from four existing access points to two controlled access points, which represents an improvement to road safety along both Wilson Street and Ladbroke Street frontages.
- The development's pedestrian infrastructure, particularly the forecourt area on Wilson Street, will provide a high level of service for pedestrian movements and effectively manage pedestrian activity without impacting the Wilson Street footpath.
- Public transport accessibility is excellent due to the site's CBD location, with bus services operating on Wilson Street and nearby roads providing good connectivity to all regions of the Burnie area.
- Access to the site will be controlled by secure entrances, with only authorized vehicles permitted, further enhancing the safety of the road network in the vicinity of the development.

Based on the findings of this report the proposed development is supported on traffic and parking grounds.

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Document Status

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	21 May 2025
1	Keith Midson	Zara Kacic-Midson	26 May 2025

JMG Ref: 230912BS
Client Ref: DA 2025/55

29 August 2025

Andrew McCreary
Xsquared Architects
125 Collins Street
Hobart TAS 7000

Dear Andrew,

RE: External Lighting at Burnie Courts including Signage and Flagpole Lighting

The applicable standard for the signage is AS/NZS 4282 - Control of the obtrusive effects of outdoor lighting. Based on our interpretation, the project falls under Environmental Zone A3. Please refer to the extract below for further detail:

Table 3.1 — Environmental zones

Environmental zones	Ambient light conditions	Descriptions/ Examples
A0	Intrinsically dark	UNESCO Starlight Reserve. IDA: Dark Sky Parks, Reserves or Sanctuaries Major optical observatories Other accreditations for dark sky places for example astrotourism, heritage value, astronomical importance, wildlife/ecosystem protection Lighting for safe access may be required
A1	Dark	Relatively uninhabited rural areas (including terrestrial, marine, aquatic and coastal areas) Generally roadways without streetlighting through rural areas
A2	Low district brightness	Sparsely inhabited rural and semi-rural areas Generally roadways without streetlighting through suburban, rural or semi-rural areas other than intersections
A3	Medium district brightness	Suburban areas in towns and cities Generally roadways with streetlighting through suburban, rural or semi-rural areas
A4	High district brightness	Town and city centres and other commercial areas Residential areas abutting commercial areas Industrial and Port areas Transport Interchanges
TV	High district brightness	Vicinity of major sport and event stadiums during TV broadcasts

NOTE Zones A0 and A1 would normally have a minimum area of 50 ha.(0.5 km²). There may be smaller environmentally sensitive areas.

Currently there are no external signage which is internally illuminated but if we decide to add internally illuminated signs, we need to comply with the below Upward Light Ratio (ULRL):

3.3.3.4 Control of upward waste light

The upward light impact of lighting included under [Clause 3.3.3](#) shall be assessed as individual items as follows:

- (a) Internally illuminated signs and other internally illuminated objects shall have a ULR_L of ≤ 0.50 .
- (b) Digital signs shall have a ULR_L of ≤ 0.45 .
- (c) Externally lit signs and billboards shall be lit from the top and shall have a ULR_L or ULR_S no greater than that specified in [Table 3.2](#).

For other lit surfaces not included in (a) to (c), e.g. façade lighting, walls, and trees, the lighting system shall include measures which mitigate upward waste light. See [Appendix A](#) for guidance.

Compliance will depend on the type of sign specified, and the sign supplier should be able to provide supporting documentation to demonstrate conformity with the standard. Additionally, LED signage often includes dimming functionality, rather than switching the sign off completely after curfew hours, dimming allows sufficient legibility and contrast while minimising visual impact.

Further to the above, we need to comply with the requirement for externally illuminated surfaces of signs, facades, or artwork. Please see requirement below:

Table 3.4 — Maximum average luminance of surfaces (cd/m²)

Application conditions	Environmental zones				
	A0	A1	A2	A3	A4
See Clause 3.3.3	0.1	50	150	250	350

We will ensure compliance with the specification by implementing the following for the façade lighting:

- Luminaires will be directed straight down with no tilt.
- External wall wash/grazing lights to be installed behind façade ensuring that it is not directly visible from the street.
- Luminaires will be selected with optics that emits no light above the horizontal plane.
- Use Luminaires with a correlated colour temperature of 3000K.

According to AS 4282 Section 3.3.3.5, flags and banners should be lit from ambient light from other sources or luminaires mounted above the top of the flag or banner and shall have a ULR of than 0.4.

Alternatively, a ground mounted Luminaire can be used and where possible, it should be located so that the angle on incidence of the light to the fully unfurled flag is less than 60 degrees.

Regards,
JOHNSTONE MCGEE & GANDY PTY LTD



De Wet Wentzel
Senior Electrical Engineer
Building Services Provider
License No: 163706070



DEVELOPMENT APPLICATION DRAWING SCHEDULE

2324	A000	B	COVERPAGE AND DRAWING SCHEDULE
2324	A101	A	SITE PLAN - EXISTING
2324	A102	B	SITE PLAN - PROPOSED
2324	A105	A	SHADOW DIAGRAMS
2324	A108	A	SHADOW DIAGRAMS
2324	A201	A	GROUND FLOOR PLAN
2324	A202	A	FIRST FLOOR PLAN
2324	A203	A	FIRST FLOOR - CARPARK PLAN
2324	A204	A	SECOND FLOOR PLAN
2324	A205	A	THIRD FLOOR PLAN
2324	A208	A	ROOF PLAN
2324	A300	B	STREET ELEVATIONS
2324	A301	C	ELEVATIONS
2324	A302	B	ELEVATIONS
2324	A303	B	ELEVATIONS
2324	A401	A	SECTIONS
2324	A402	A	SECTIONS
2324	A403	A	SECTIONS
2324	A801	A	3D STREET VIEWS
2324	A802	A	3D VIEWS

BURNIE COURTS COMPLEX RELOCATION PROJECT

106 WILSON ST, BURNIE TAS
DEPARTMENT OF JUSTICE

SEP 2025

DEVELOPMENT APPLICATION



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LAUNCESTON
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LEGEND
 [Grey Box] EXISTING BUILDING
 [Blue Dashed Line] TITLE BOUNDARY

NOTE:
 ENABLING WORKS INCLUDED
 DEMOLITION AND SITE
 EXCAVATION APPROVED UNDER
 DA 2025/31

DEVELOPMENT APPLICATION

REVISIONS
 A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
 SITE PLAN - EXISTING

PROJECT TITLE
 BURNIE COURTS COMPLEX
 RELOCATION PROJECT

PRINCIPAL
 DEPARTMENT OF JUSTICE

PROJECT ADDRESS
 100 - 106 WILSON ST, BURNIE
 TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:200	DRAWN MM
	CHECKED AM

A101 A



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- LEGEND**
- EXISTING BUILDING
 - TITLE BOUNDARY
 - SUBSTATION KIOSK
 - ACCESSIBLE CARPARK
 - DROP OFF ZONE
 - NEW CARPARKING
 - LOADING ZONE/5 MIN COURT USE ONLY PARK
 - DRIVEWAY SIGHT LINES
 - NEW CROSSOVER
 - REMOVE CARPARK
 - EXISTING CARPARK
 - NEW FENCE

DEVELOPMENT APPLICATION

REVISIONS
 A 1905205 DEVELOPMENT APPLICATION
 B 2708205 DEVELOPMENT APPLICATION #11

DRAWING
 SITE PLAN - PROPOSED

PROJECT TITLE
 BURNIE COURTS COMPLEX
 RELOCATION PROJECT

PRINCIPAL
 DEPARTMENT OF JUSTICE

PROJECT ADDRESS
 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE SEP 2025
SCALE @ A1 1:200	DRAWN MM
	CHECKED AM

A102 B

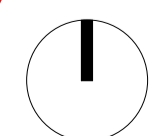
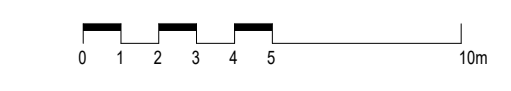


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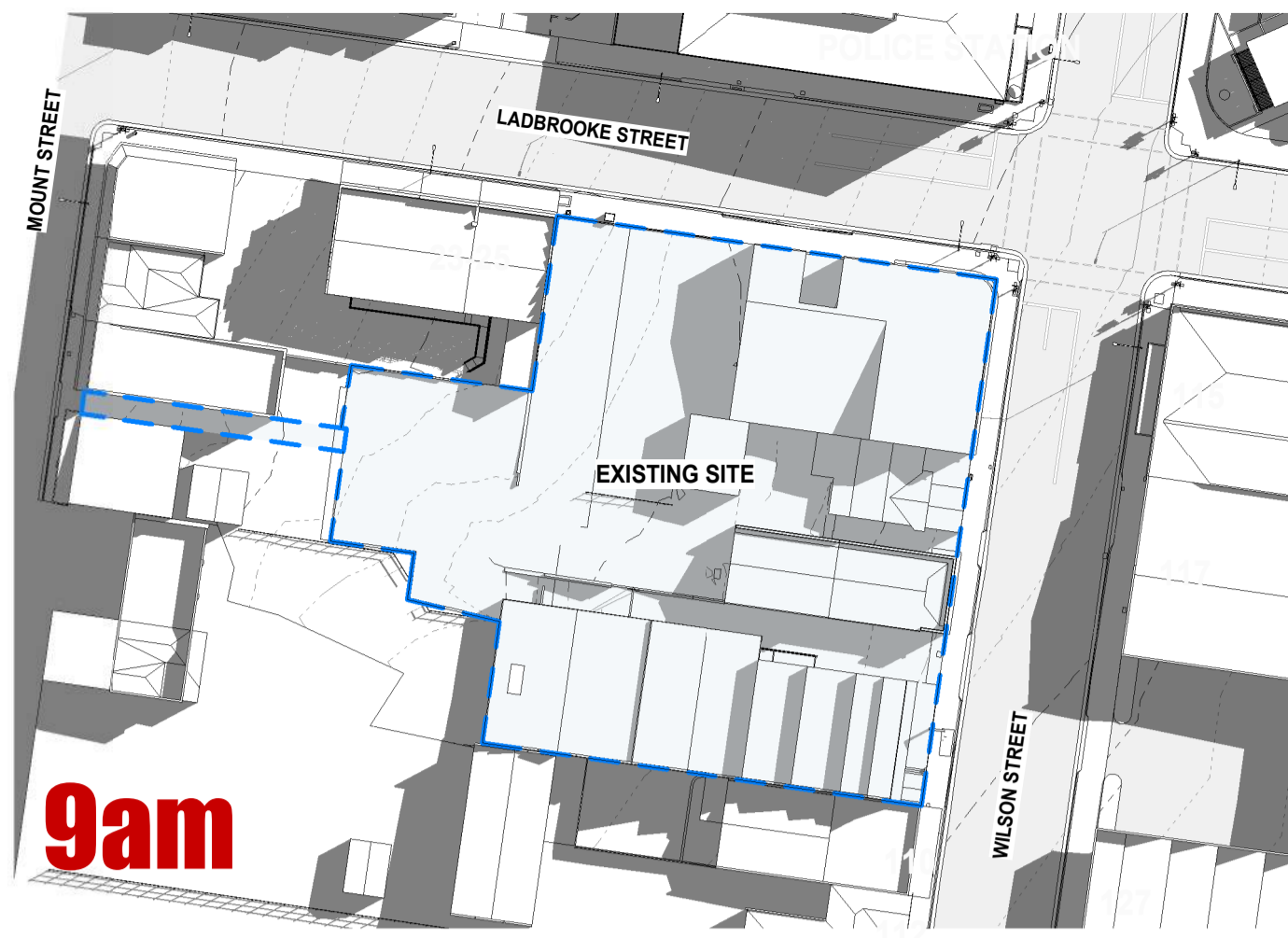
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AUTUMN EQUINOX: 20 MARCH



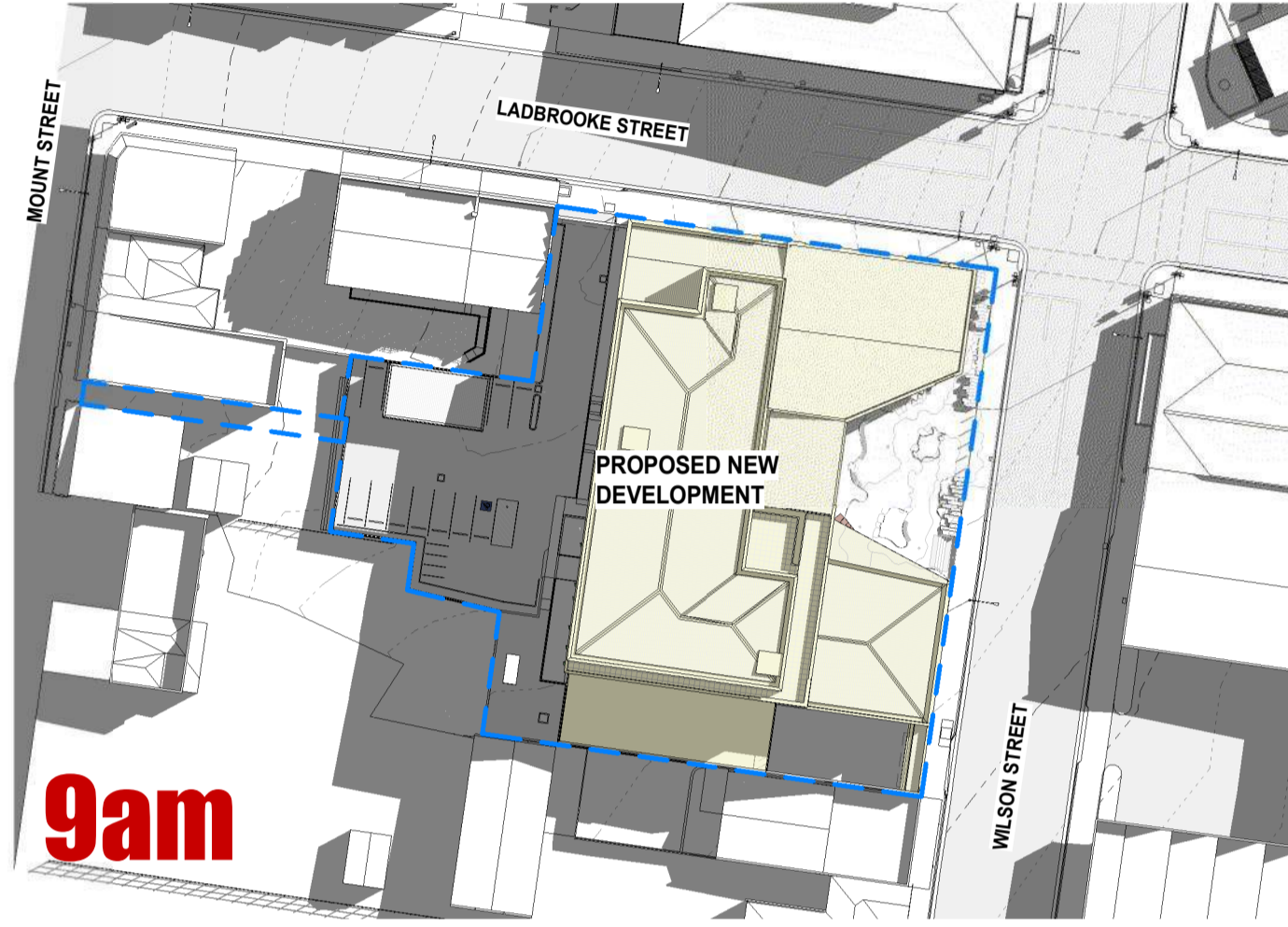
9am



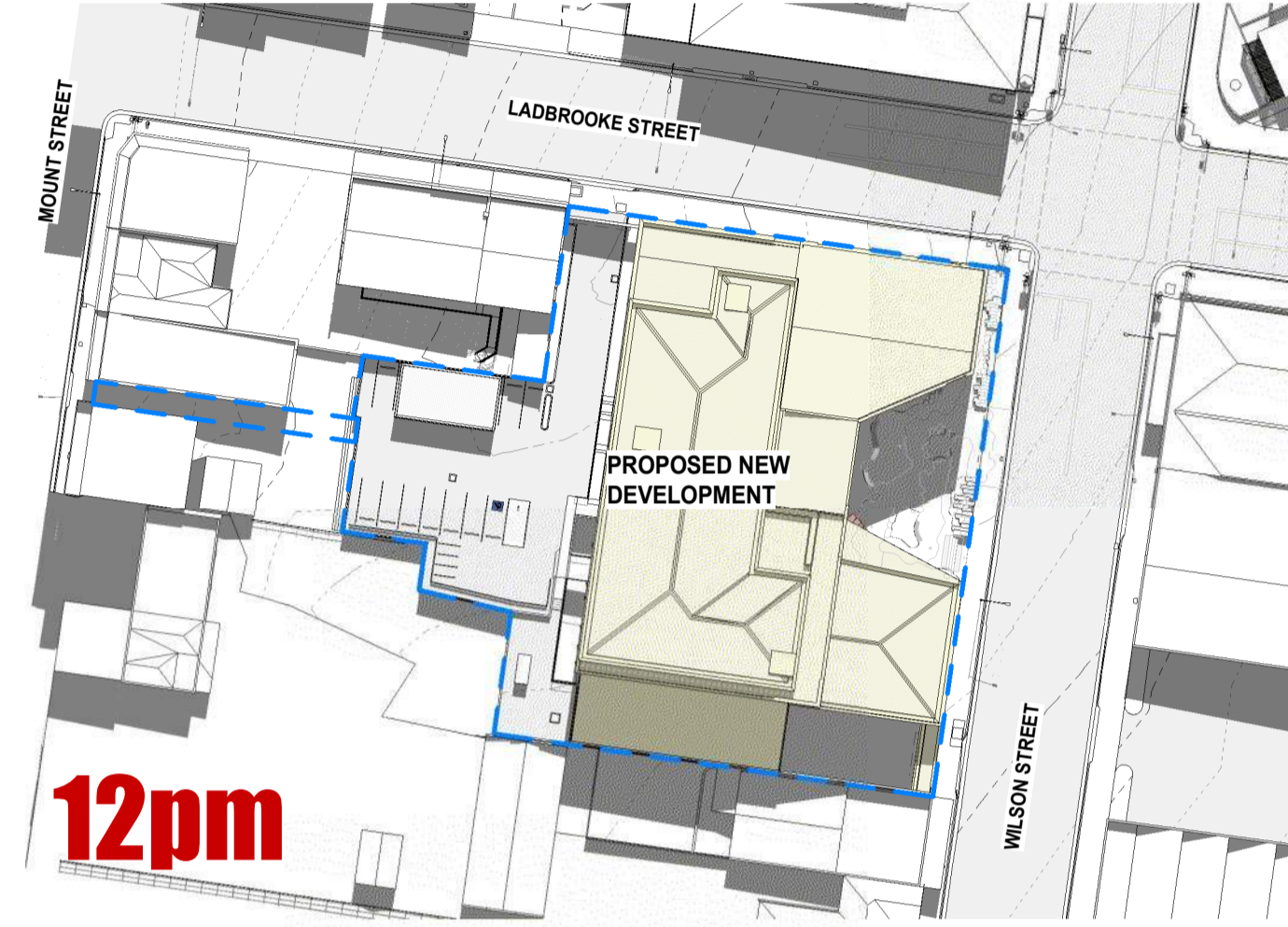
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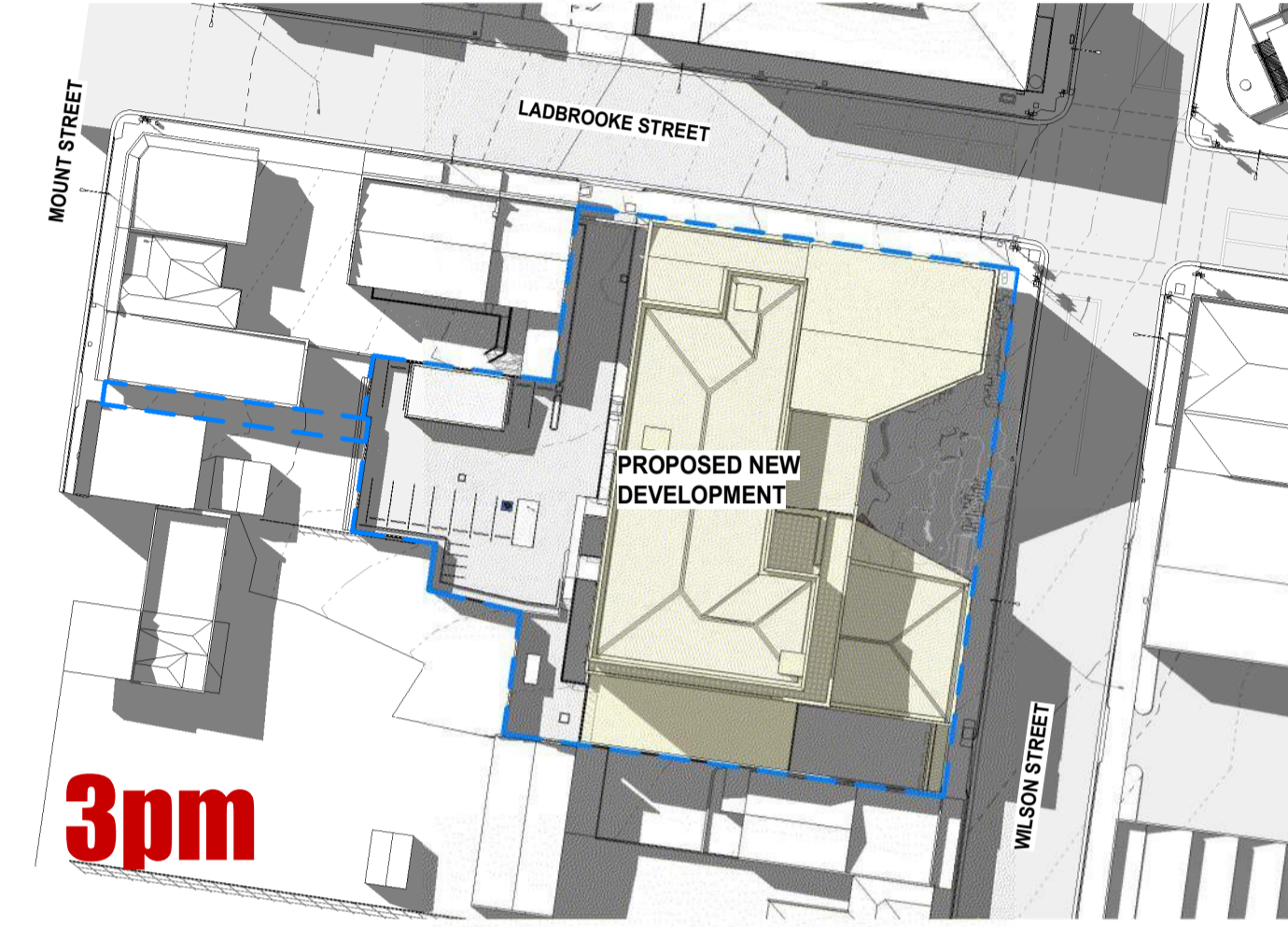
3pm



9am

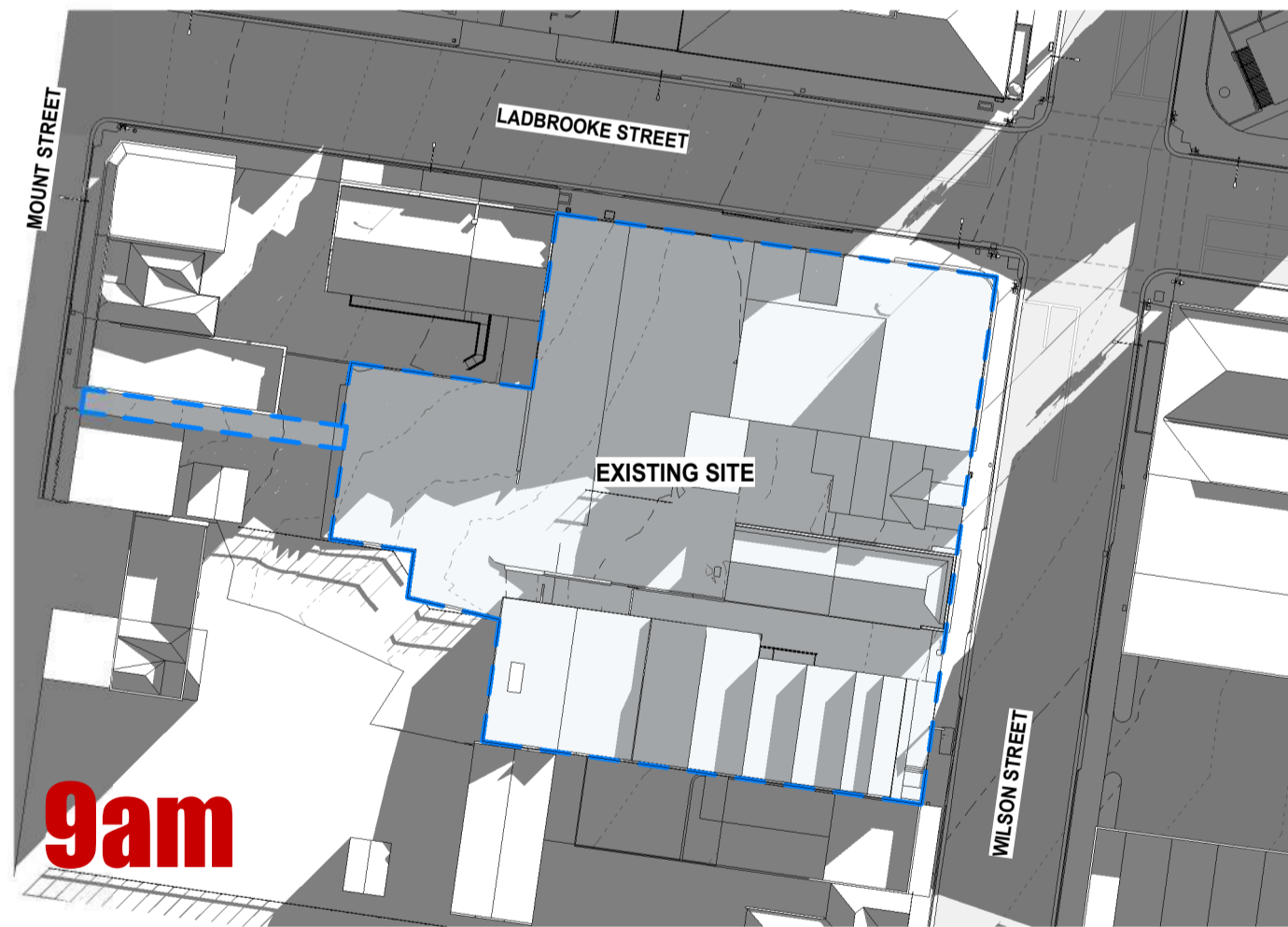


12pm



3pm

WINTER SOLSTICE: 21 JUNE



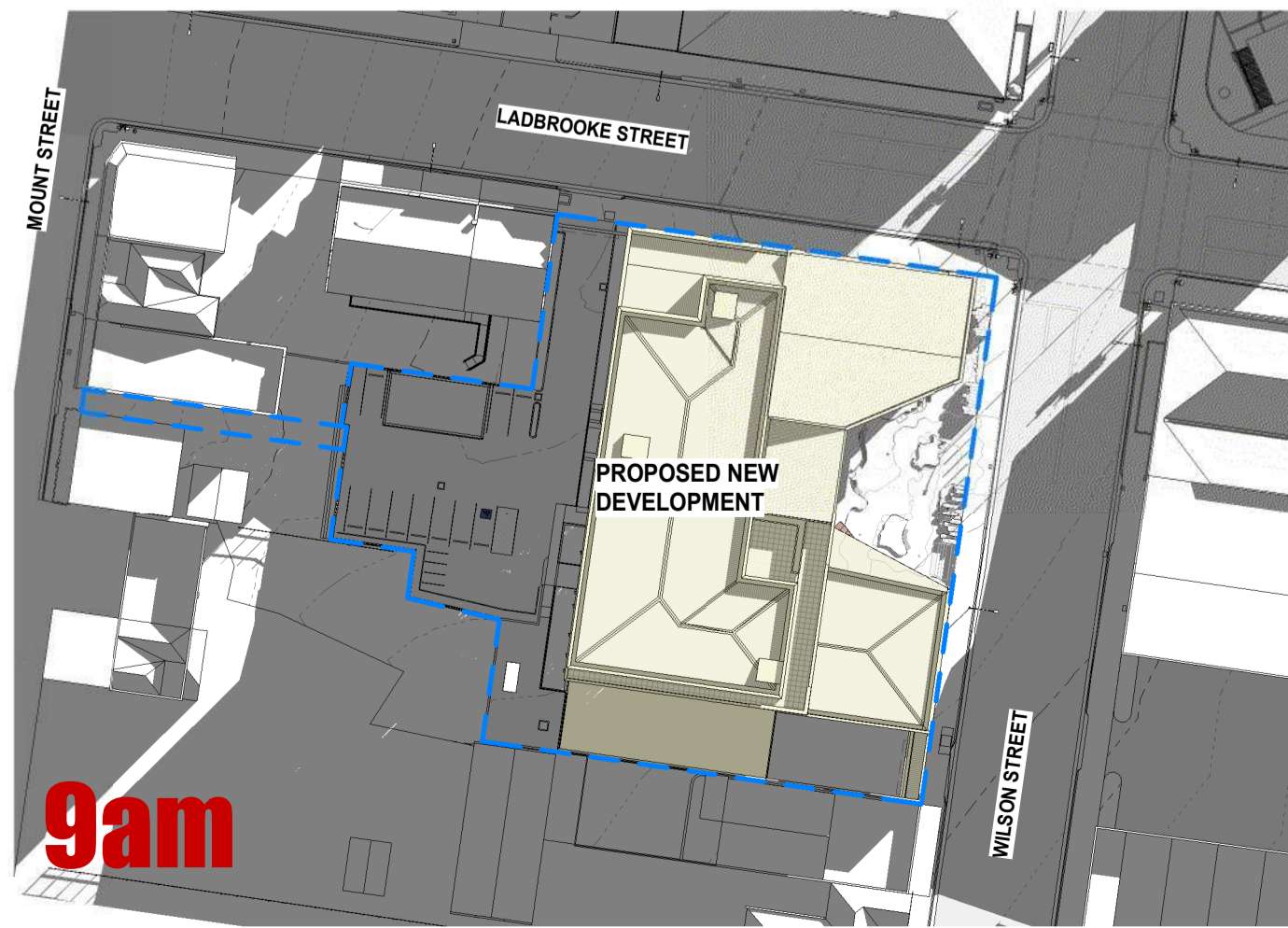
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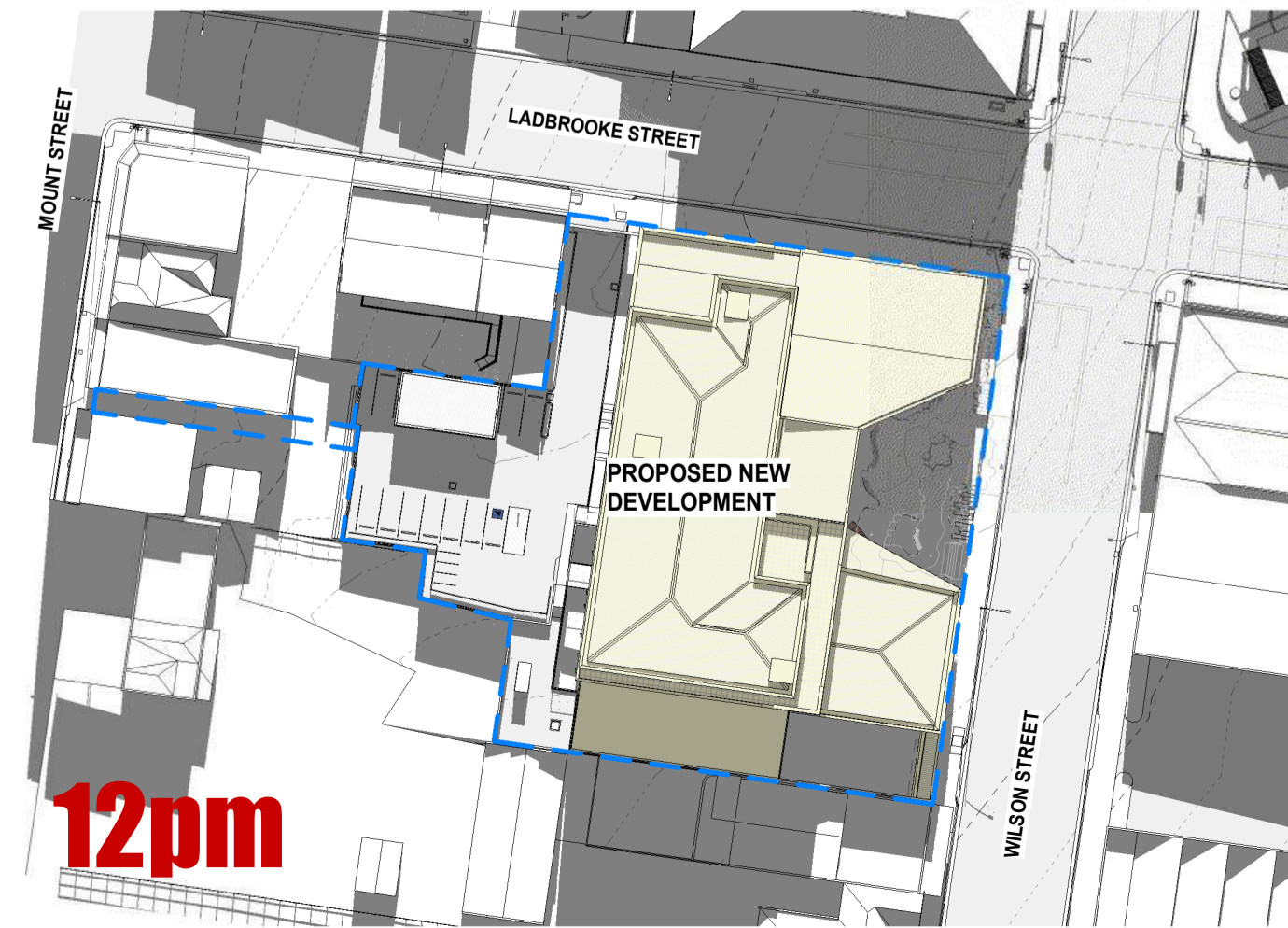
12pm



3pm



9am



12pm



3pm

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
SHADOW DIAGRAMS

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 NTS	DRAWN MM
	CHECKED AM

A105 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED

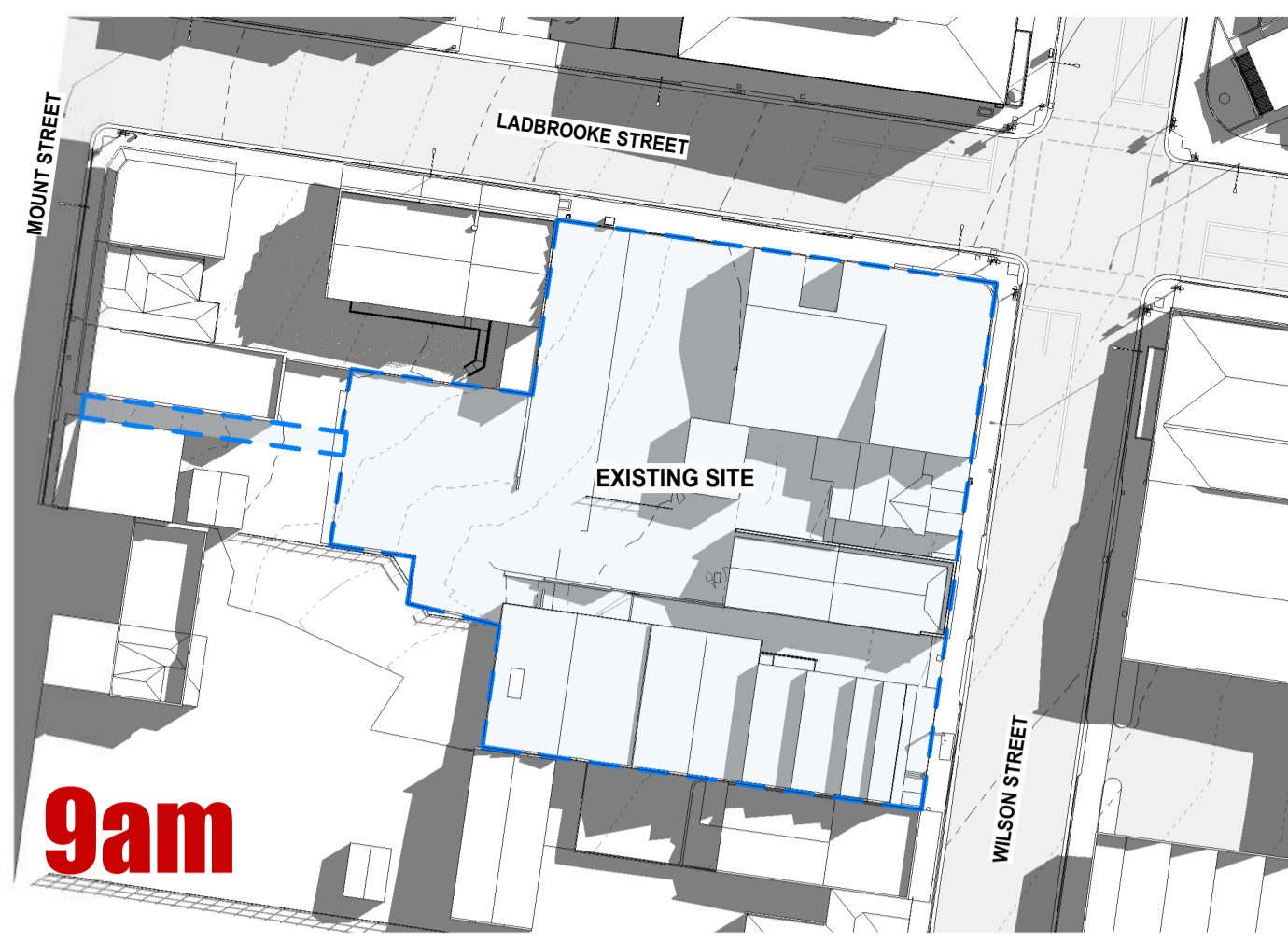
xSquared
ARCHITECTS

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SPRING EQUINOX: 22 SEPTEMBER



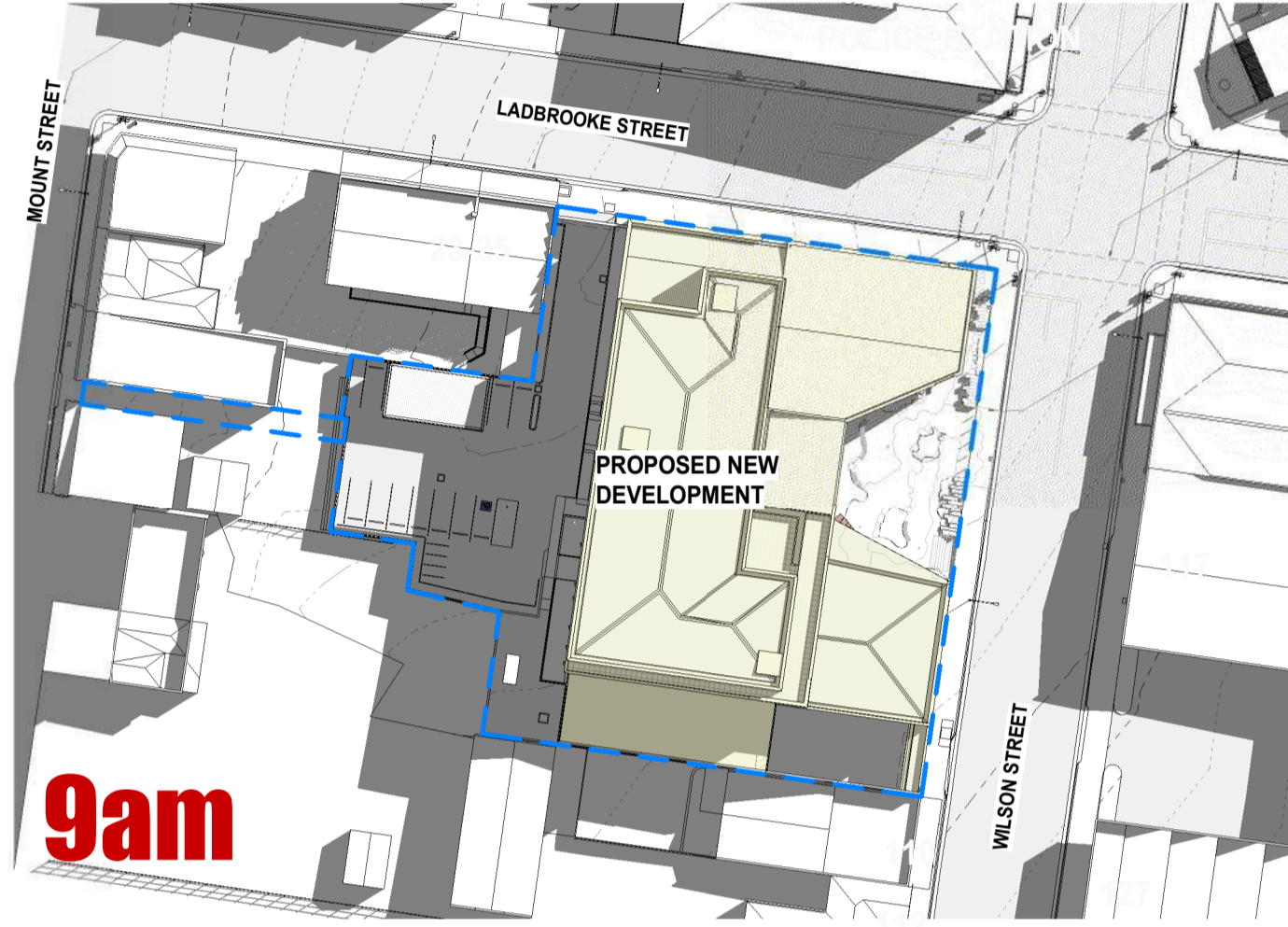
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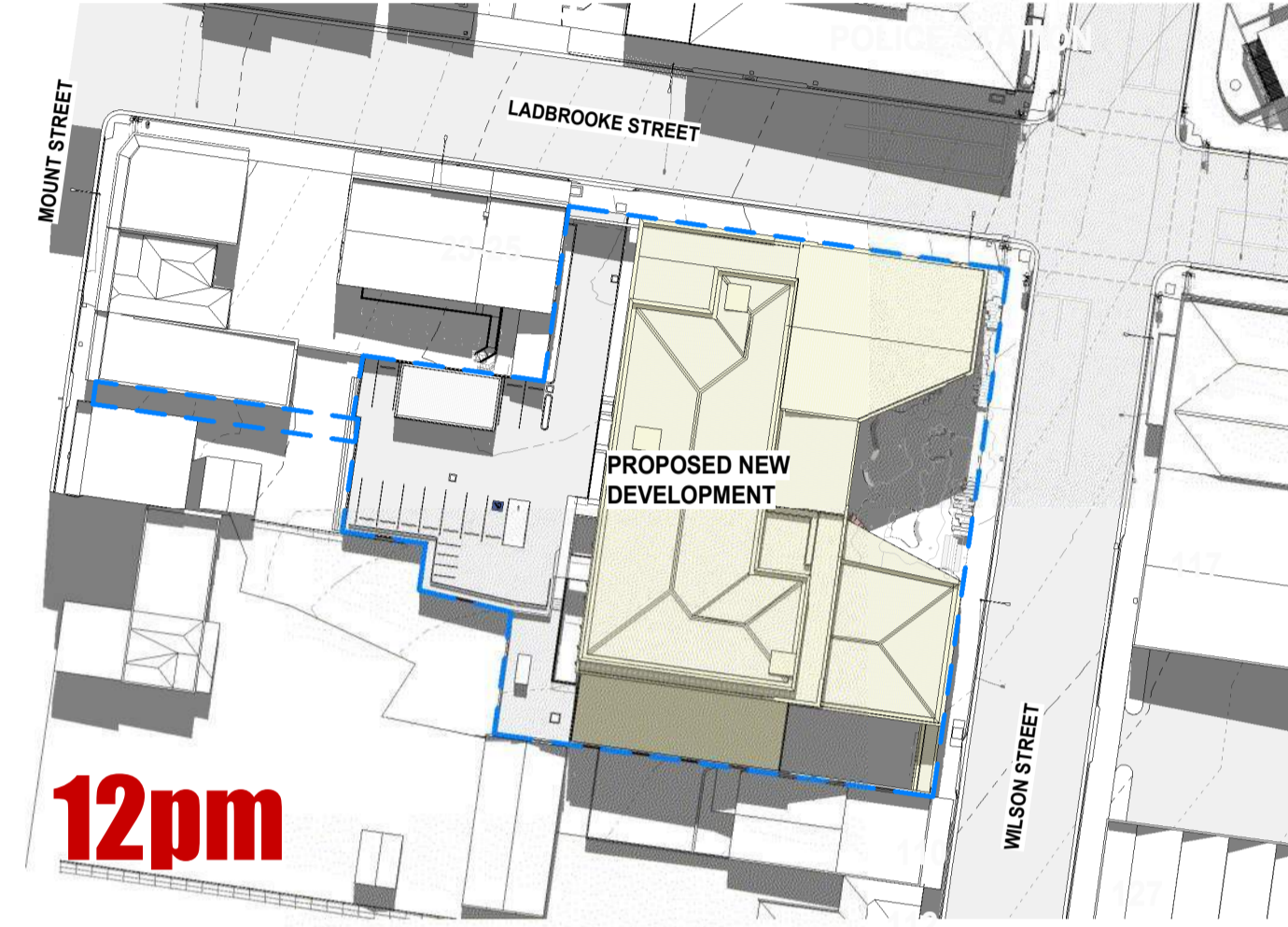
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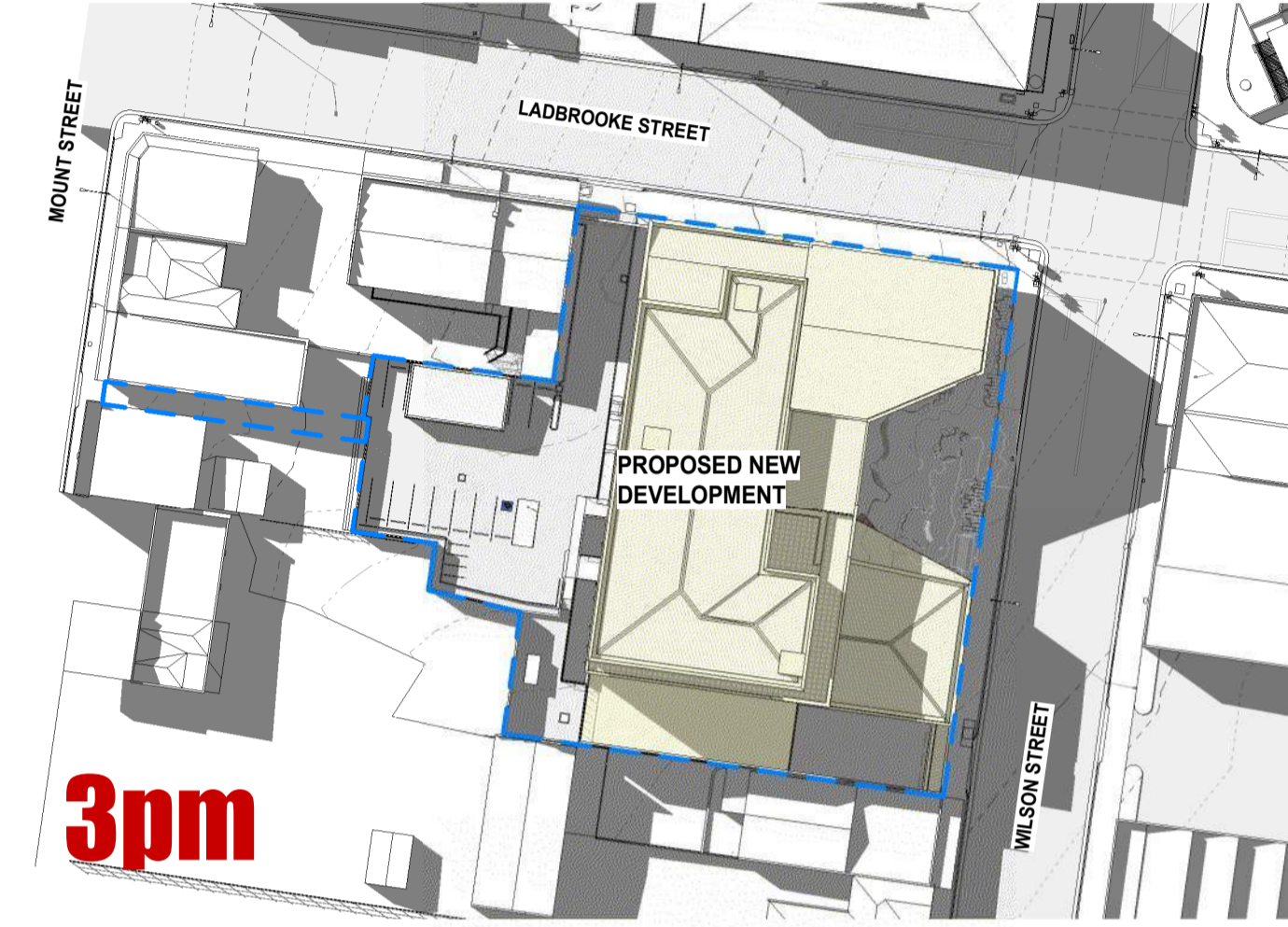
3pm



9am

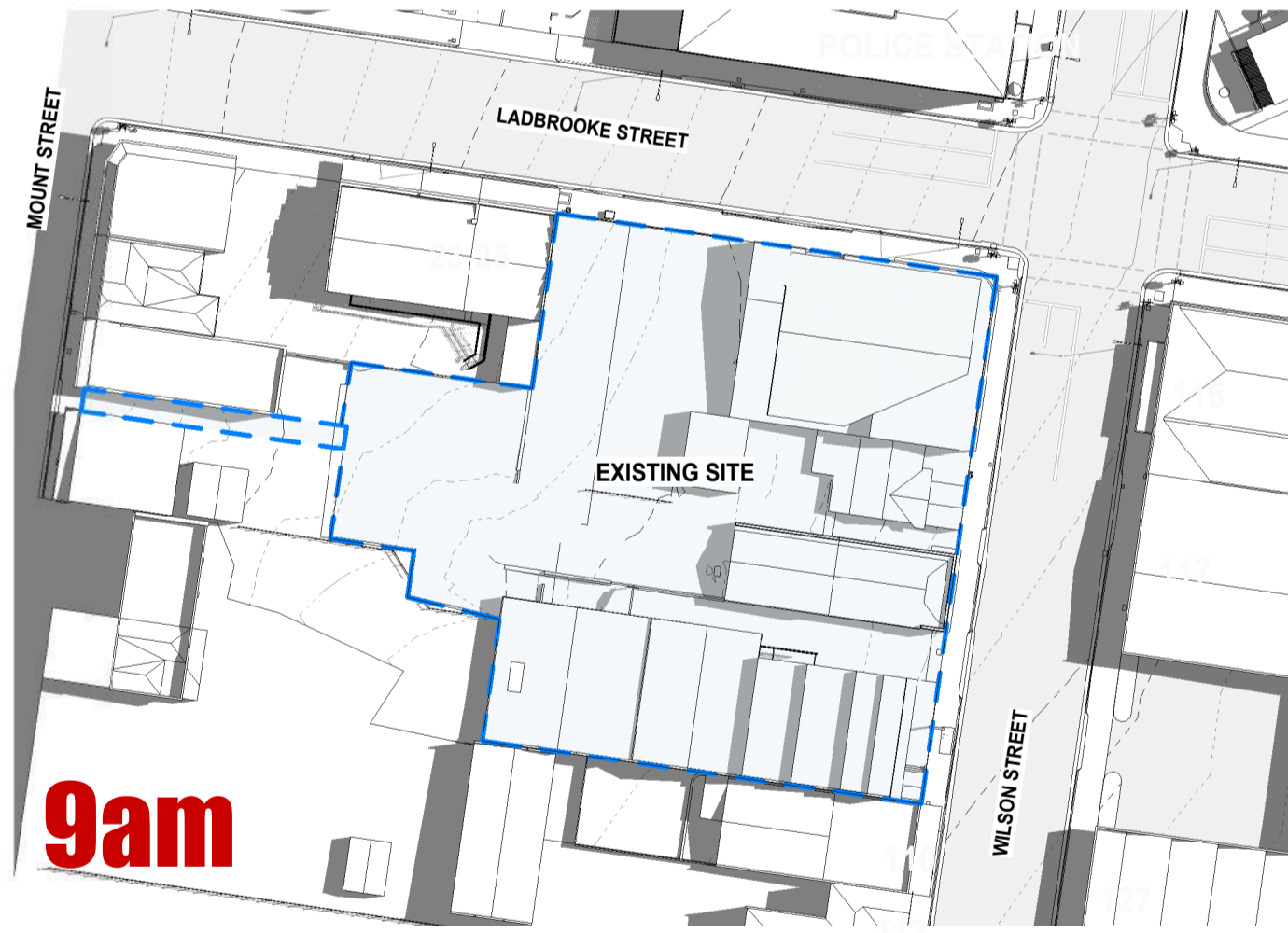


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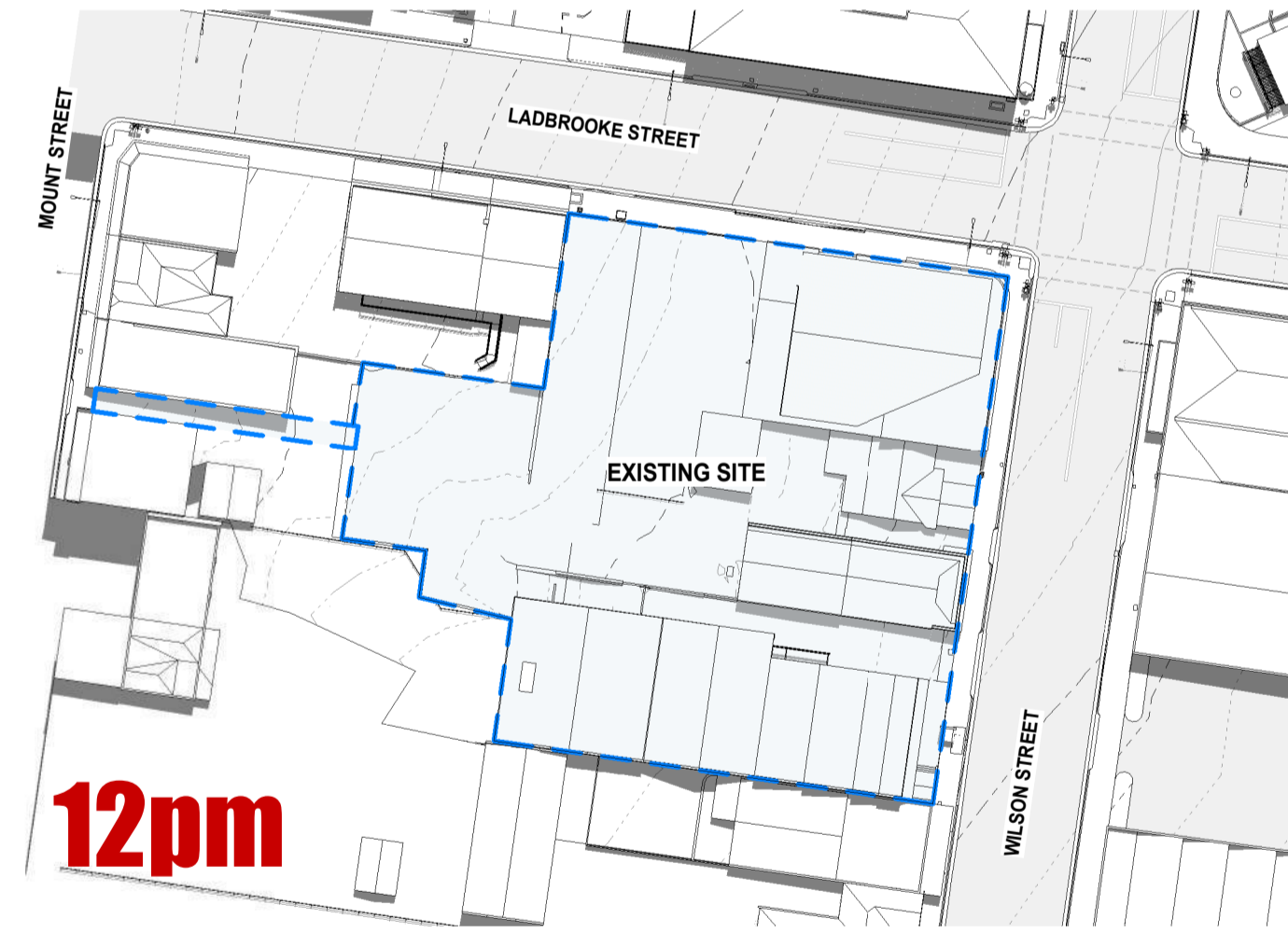


3pm

SUMMER SOLSTICE: 21 DECEMBER



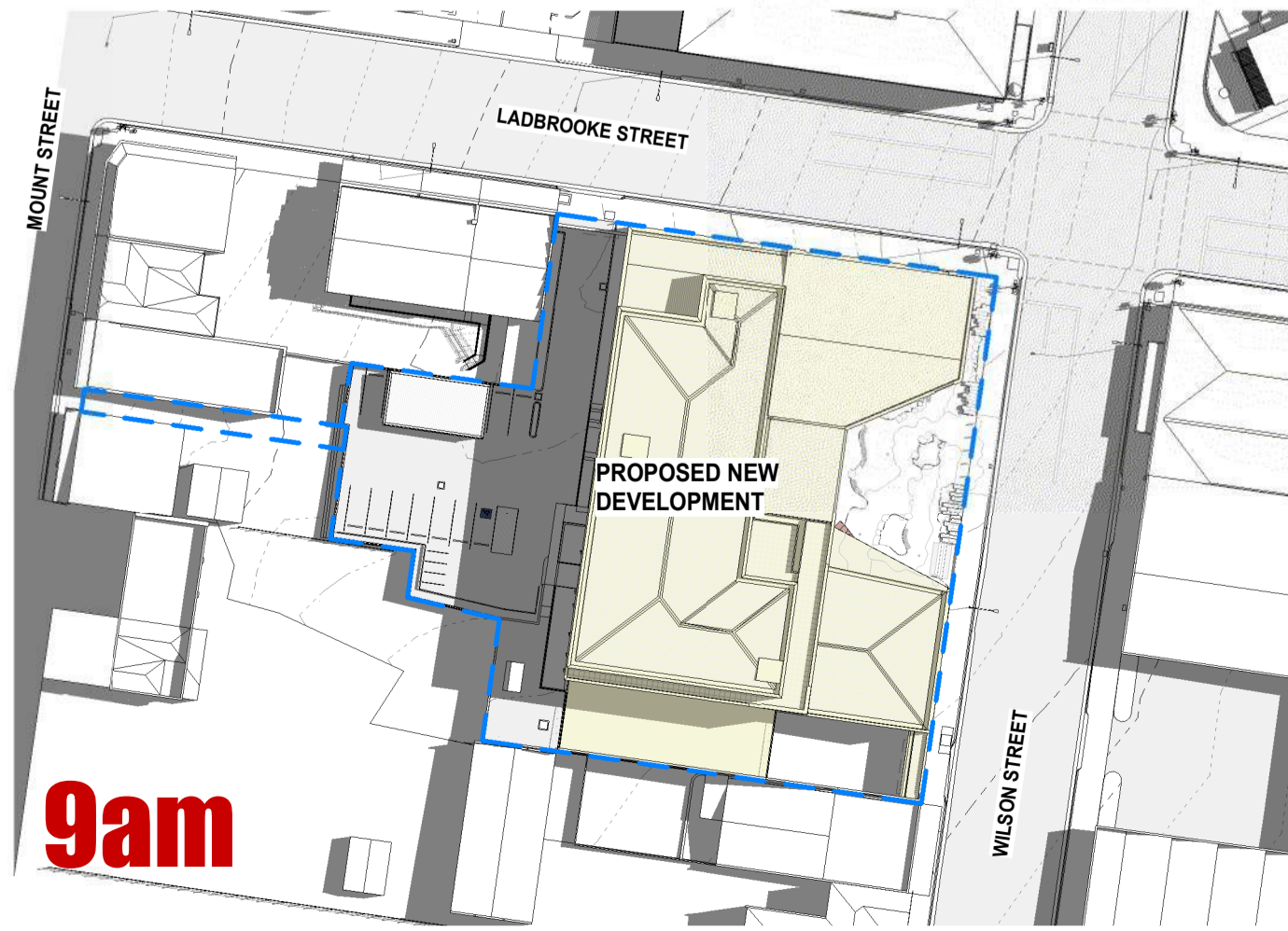
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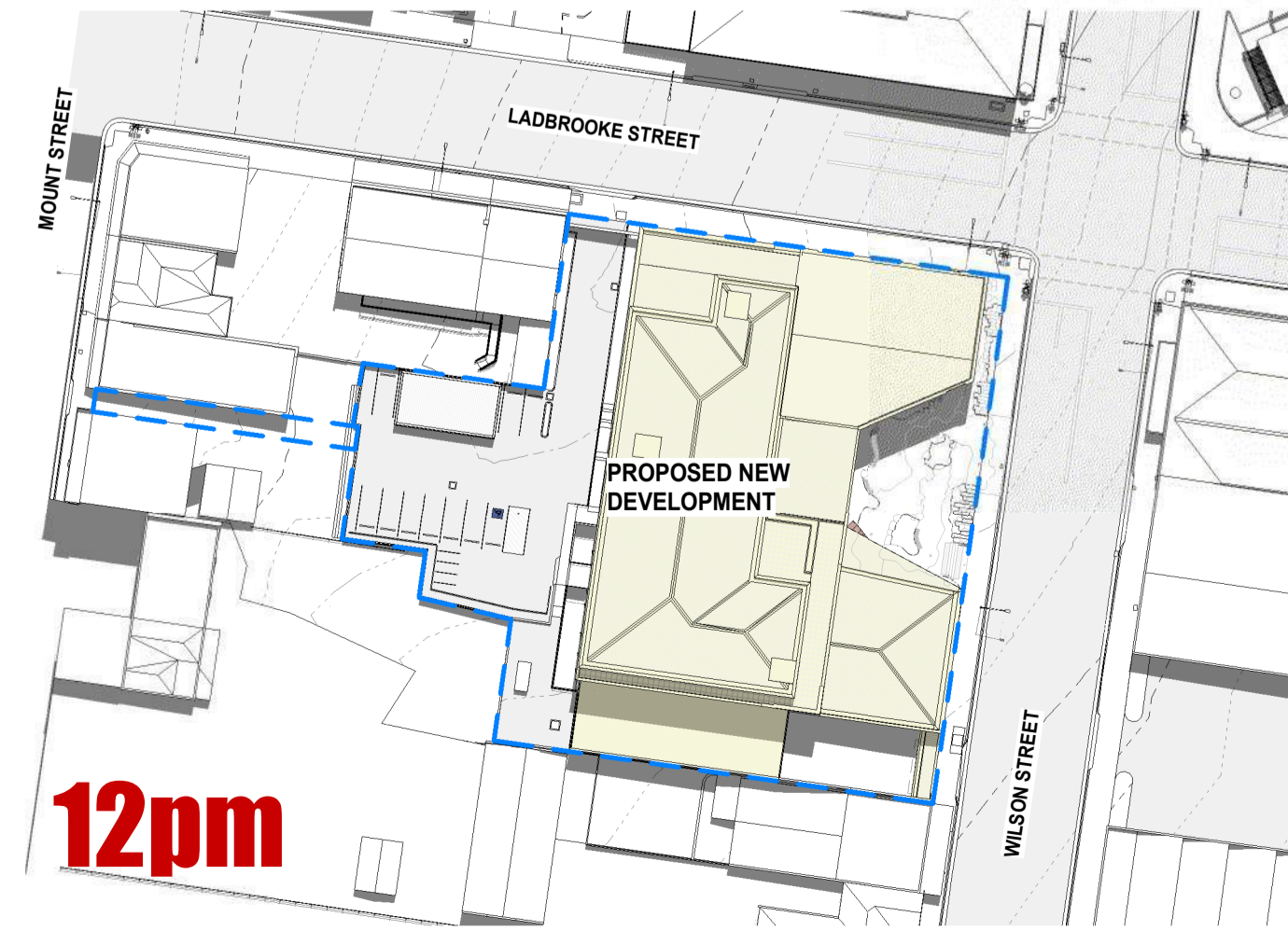
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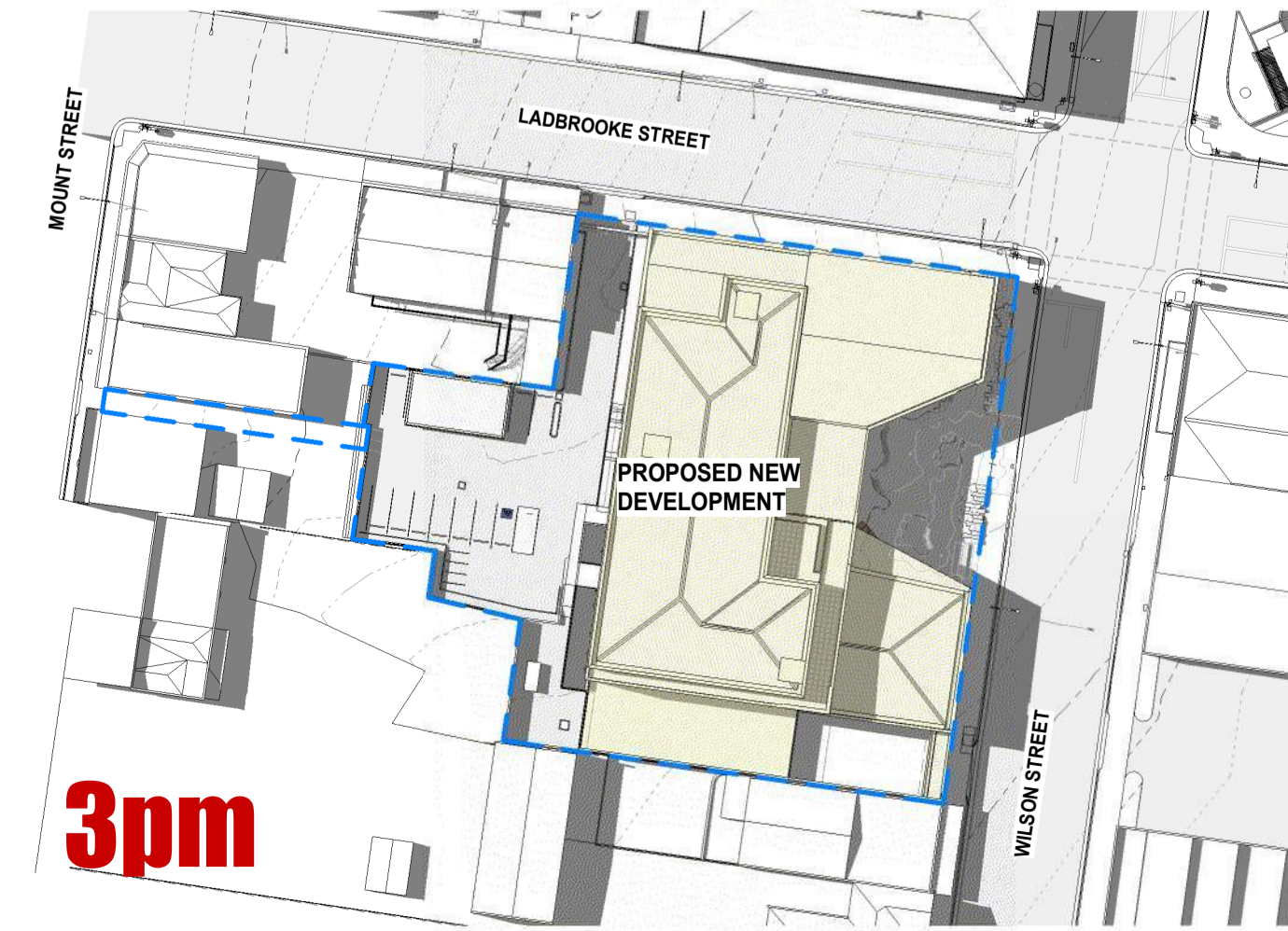
3pm



9am



12pm



3pm

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
SHADOW DIAGRAMS

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 NTS	DRAWN MM
	CHECKED AM

A106 A

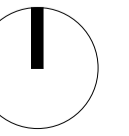
GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED



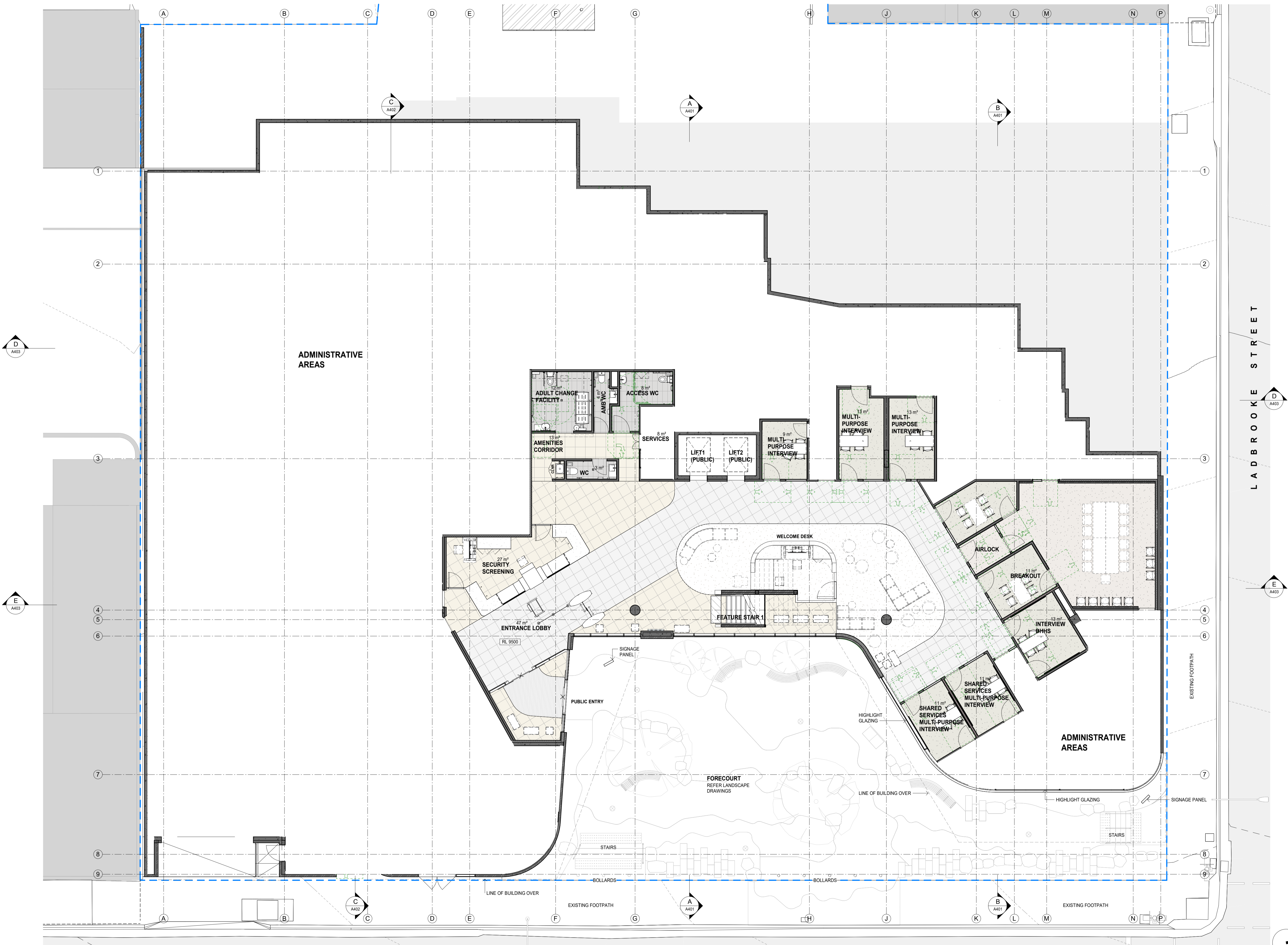
xsquared
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1 GROUND FLOOR PLAN
1:100

WILSON STREET

LADBROKE STREET

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
GROUND FLOOR PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:100	DRAWN MM
	CHECKED AN

A201 A

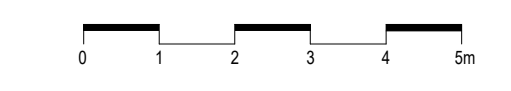


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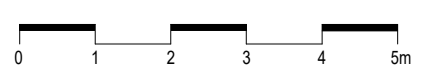
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1 FIRST FLOOR PLAN
1:100

WILSON STREET



DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
FIRST FLOOR PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324 DATE JUL 2025
SCALE @ A1 1:100 DRAWN MM CHECKED AN

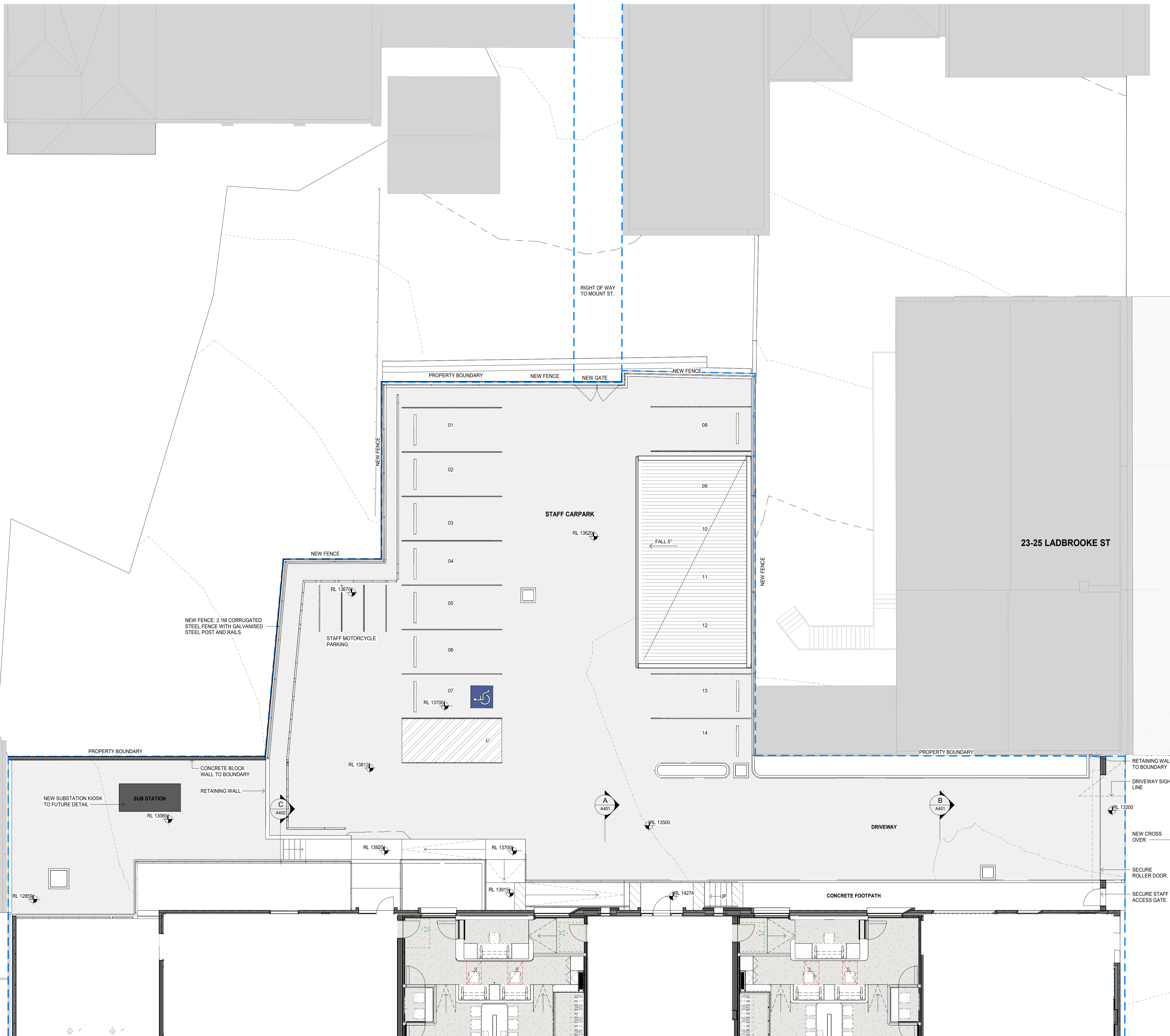
A202 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED



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1 FIRST FLOOR PLAN
1:100

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
FIRST FLOOR - CARPARK PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:100	DRAWN MM
	CHECKED AN

A203 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED

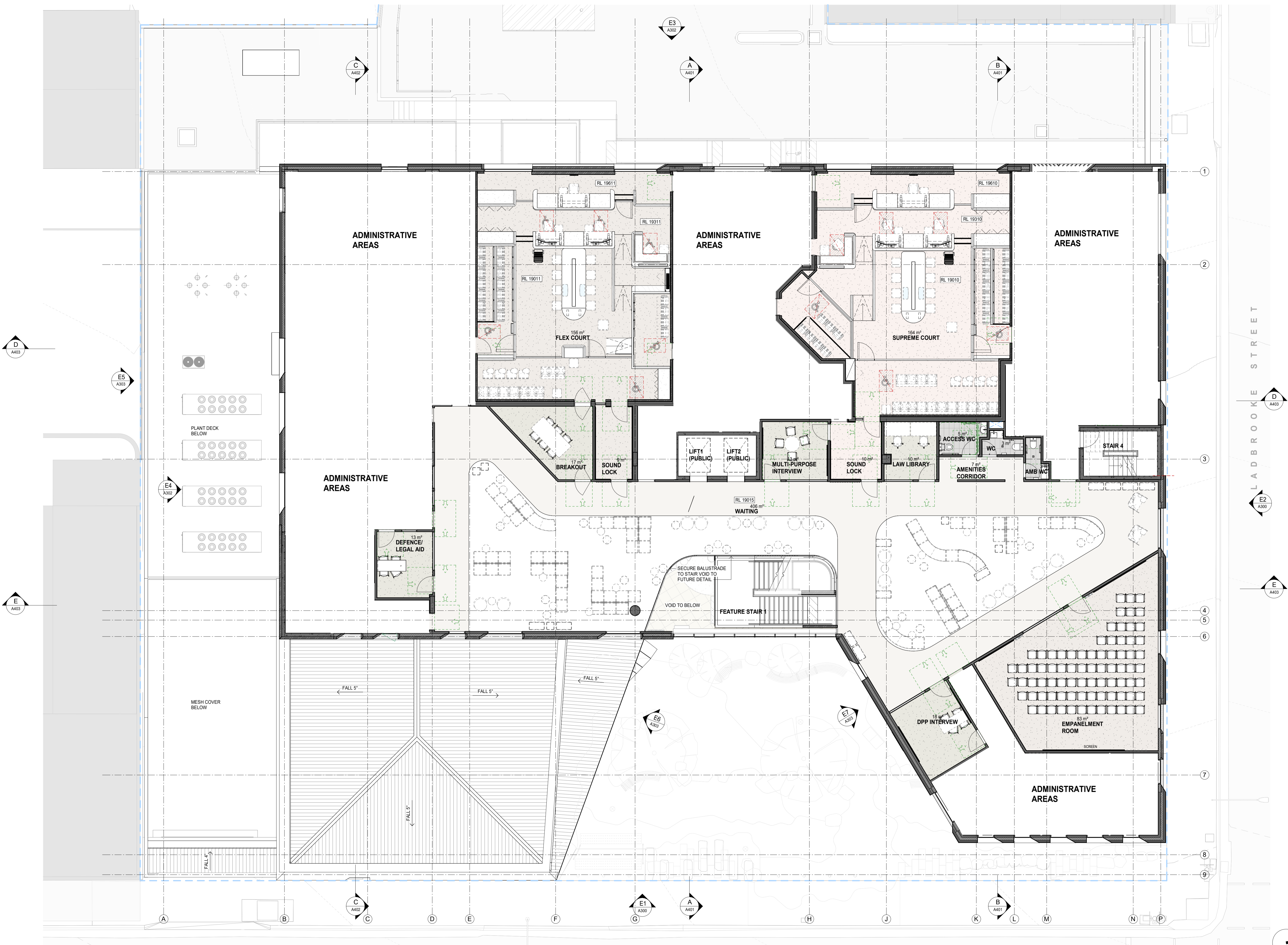


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1 SECOND FLOOR PLAN
1:100

WILSON STREET



DEVELOPMENT APPLICATION

REVISIONS
A 16/02/25 DEVELOPMENT APPLICATION

DRAWING
SECOND FLOOR PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:100	DRAWN MM
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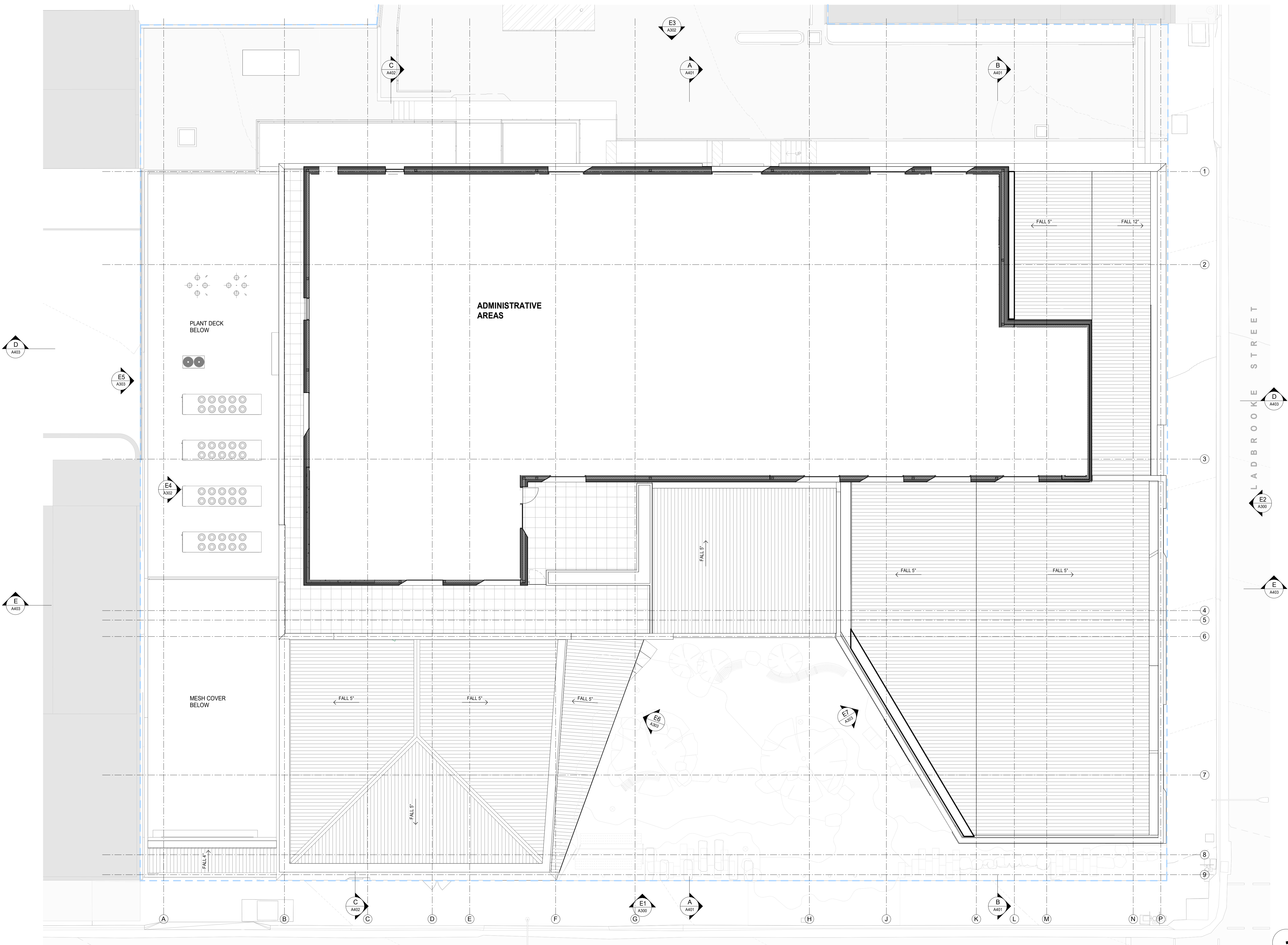
A204 A



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1 THIRD FLOOR PLAN
1:100

WILSON STREET

LADBROOKE STREET

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
THIRD FLOOR PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:100	DRAWN MM
	CHECKED AN

A205 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED

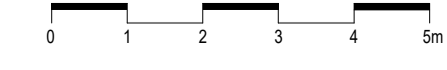


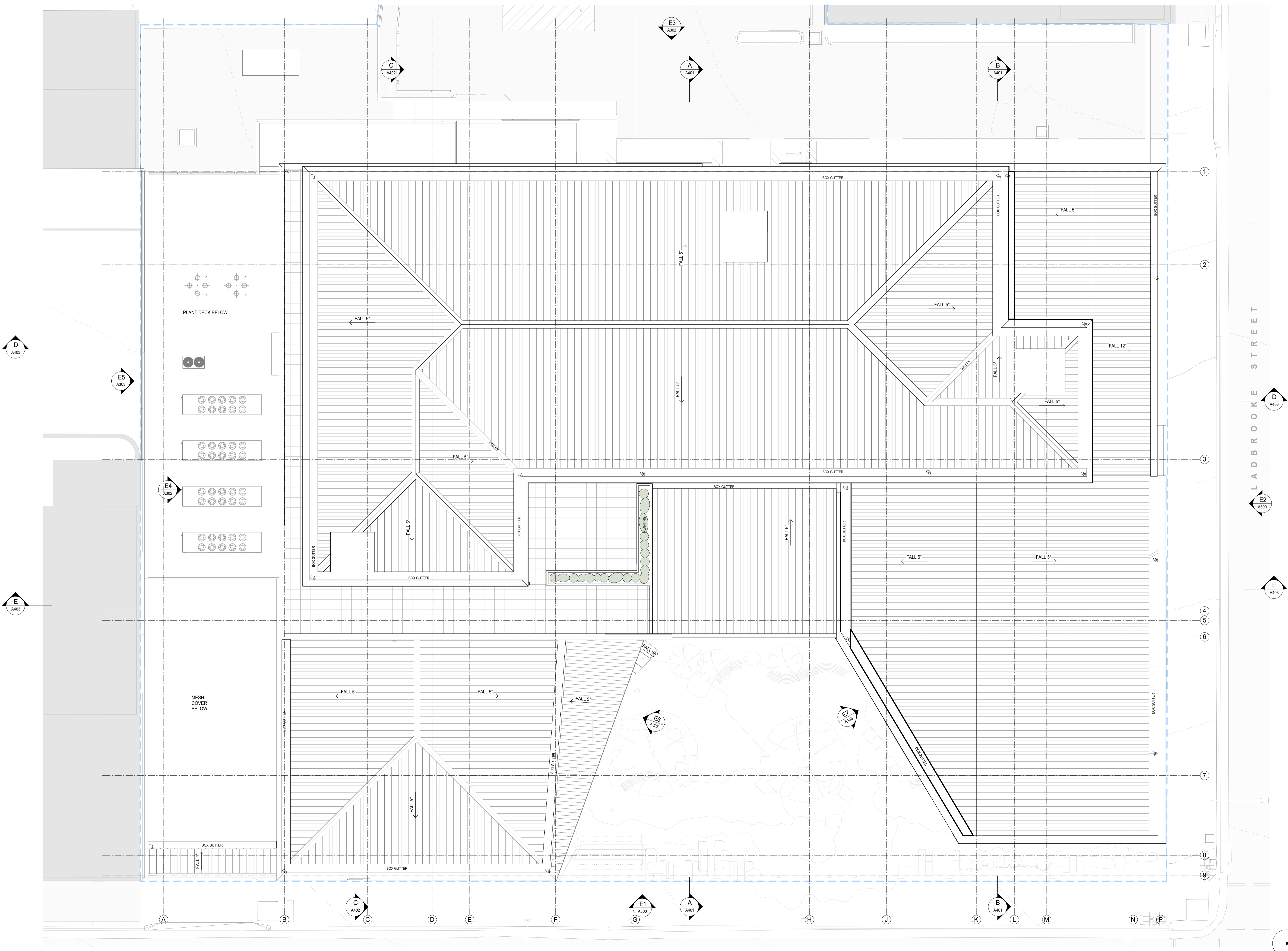
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DEVELOPMENT APPLICATION

REVISIONS
A 16/02/2025 DEVELOPMENT APPLICATION

DRAWING
ROOF PLAN

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 – 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1 : 100	DRAWN MM
	CHECKED AN

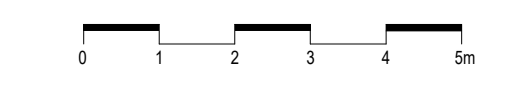
A206 A



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1 WILSON STREET ELEVATION
1:200



2 LADBROOKE STREET ELEVATION
1:200



3 MOUNT STREET ELEVATION
1:200

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION
B 09/09/2025 DEVELOPMENT APPLICATION UPDATE

DRAWING
STREET ELEVATIONS

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE SEP 2025
SCALE @ A1 1:200	DRAWN CHECKED MM AN

A300 B

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED



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1 EASTERN ELEVATION
1:100



2 NORTHERN ELEVATION
1:100



MATERIALS

DEVELOPMENT APPLICATION

REVISIONS
 A 16/07/2025 DEVELOPMENT APPLICATION
 B 27/08/2025 DEVELOPMENT APPLICATION
 C 09/09/2025 DEVELOPMENT APPLICATION UPDATE

DRAWING ELEVATIONS

PROJECT TITLE
 BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
 DEPARTMENT OF JUSTICE

PROJECT ADDRESS
 106 WILSON ST, BURNIE TAS

JOB NUMBER
 2324

DATE
 SEP 2025

SCALE @ A1
 As indicated

DRAWN
 MM

CHECKED
 AN

A301 C

GB-A GB-ARCHITECTS
 GB-LANDSCAPE
 GB-INTERIORS

LIFE FORMED

xsquared
ARCHITECTS

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3 WESTERN ELEVATION - CARPARK ENTRY
1:100



4 SOUTHERN ELEVATION - SALLY PORT
1:100

DEVELOPMENT APPLICATION

REVISIONS
A 16/02/25 DEVELOPMENT APPLICATION
B 09/09/25 DEVELOPMENT APPLICATION UPDATE

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT
PRINCIPAL
DEPARTMENT OF JUSTICE
PROJECT ADDRESS
106 WILSON ST, BURNIE TAS

JOB NUMBER 2324 DATE SEP 2025
SCALE @ A1 DRAWN MM CHECKED AN

DRAWING ELEVATIONS

ROOF 28000.00

THIRD FLOOR 24000.00

SECOND FLOOR 19000.00

FIRST FLOOR 14000.00

GROUND FLOOR 9000.00

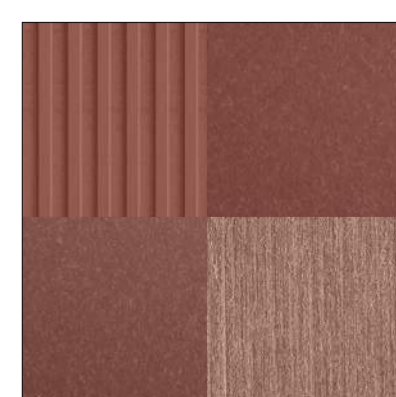
A302 B

GB-A

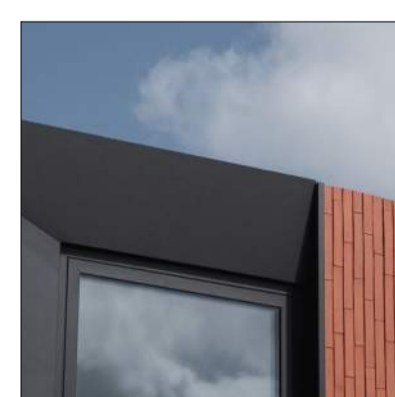
GB ARCHITECTS
GB LANDSCAPE
GB INTERIORS
LIFE FORMED



01 BRICK



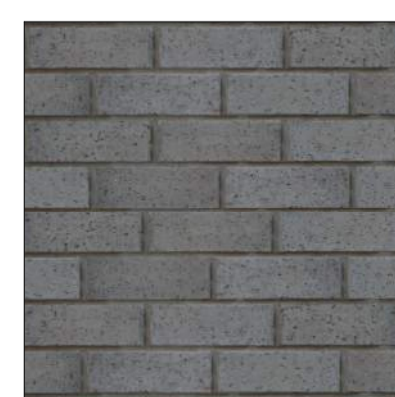
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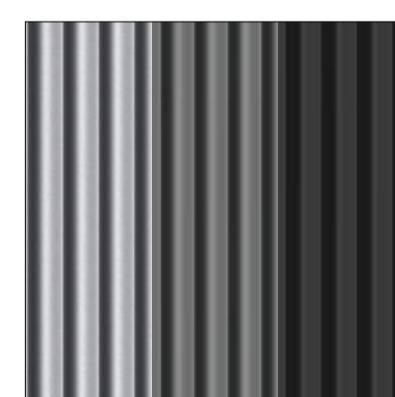
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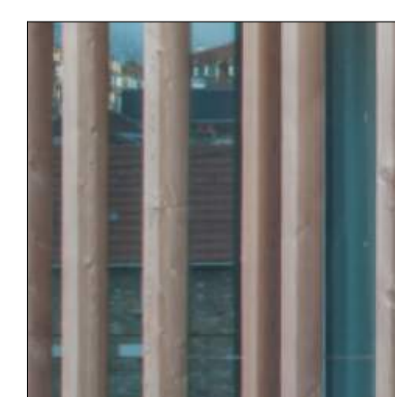
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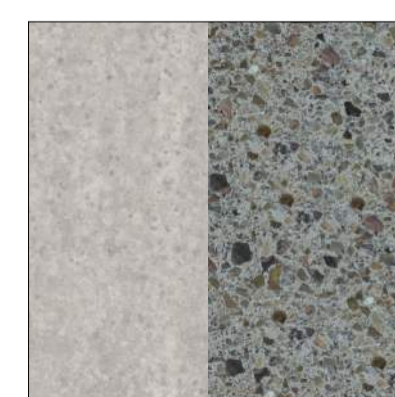
05 BRICK



06 COLORBOND COLOURED METAL SHEETING



07 TIMBER BATTENS



08 PRECAST CONCRETE - GREY & EXPOSED AGGREGATE



09 POWDERCOATED ALUMINUM LOUVRES

MATERIALS



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5 BOUNDARY ELEVATIONS - CHILDCARE YARD
1:100



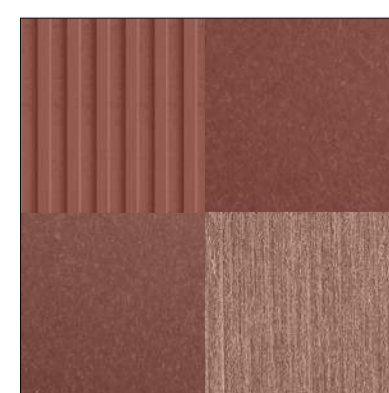
6 FORECOURT ELEVATION
1:100



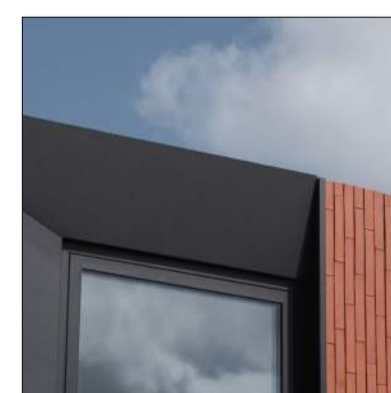
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01 BRICK



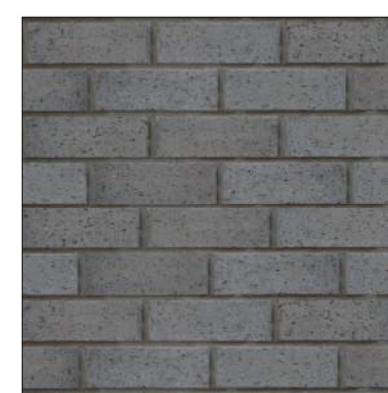
02 EQUITONE



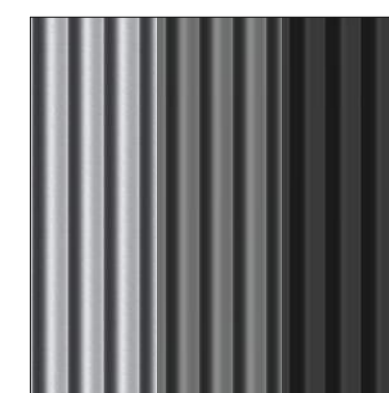
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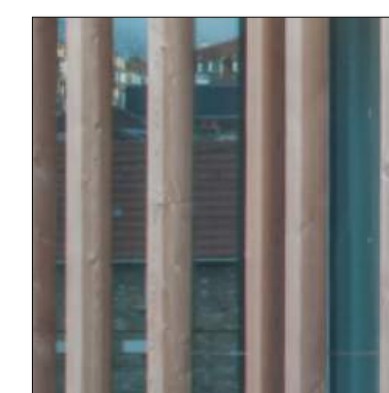
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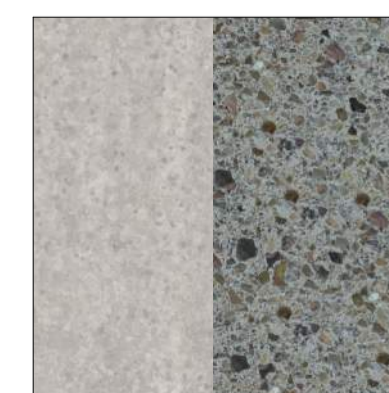
05 BRICK



06 COLORBOND COLOURED METAL SHEETING



07 TIMBER BATTENS

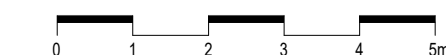


08 PRECAST CONCRETE - GREY & EXPOSED AGGREGATE



09 POWDERCOATED ALUMINIUM LOUVRES

MATERIALS



DEVELOPMENT APPLICATION

REVISIONS
A 18072025 DEVELOPMENT APPLICATION
B 27082025 DEVELOPMENT APPLICATION #11

DRAWING ELEVATIONS

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
106 WILSON ST, BURNIE TAS

JOB NUMBER
2324
SCALE @ A1
As indicated

DATE
SEP 2025
DRAWN
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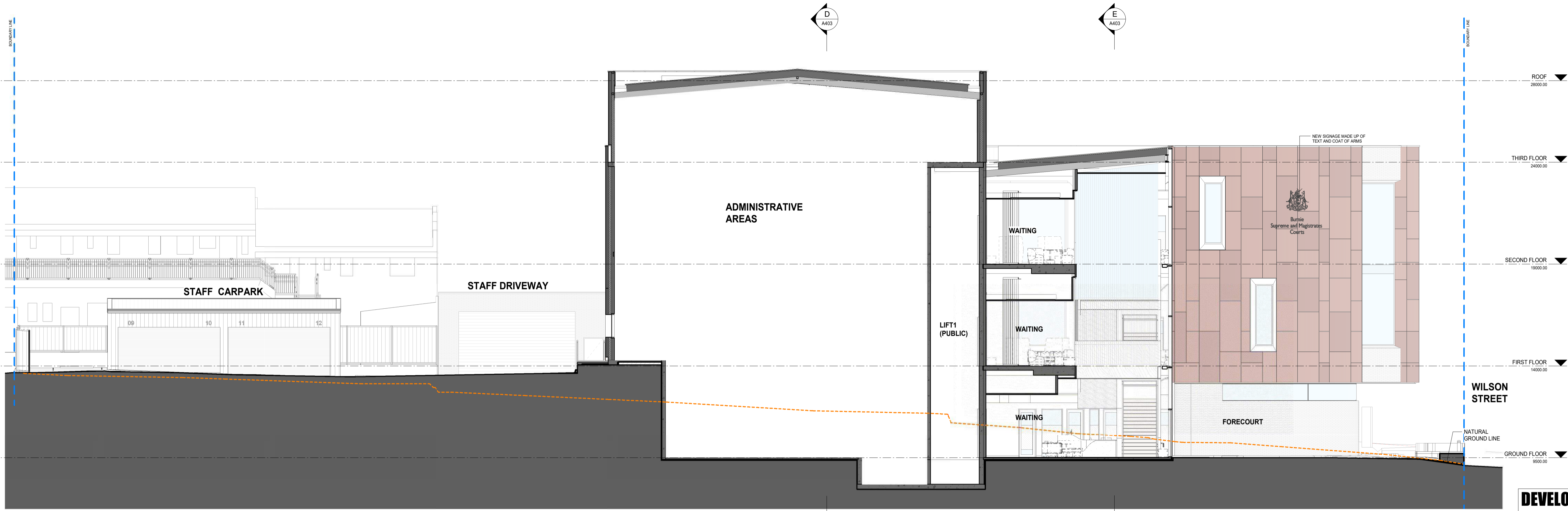
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GB-A
LIFE FORMED

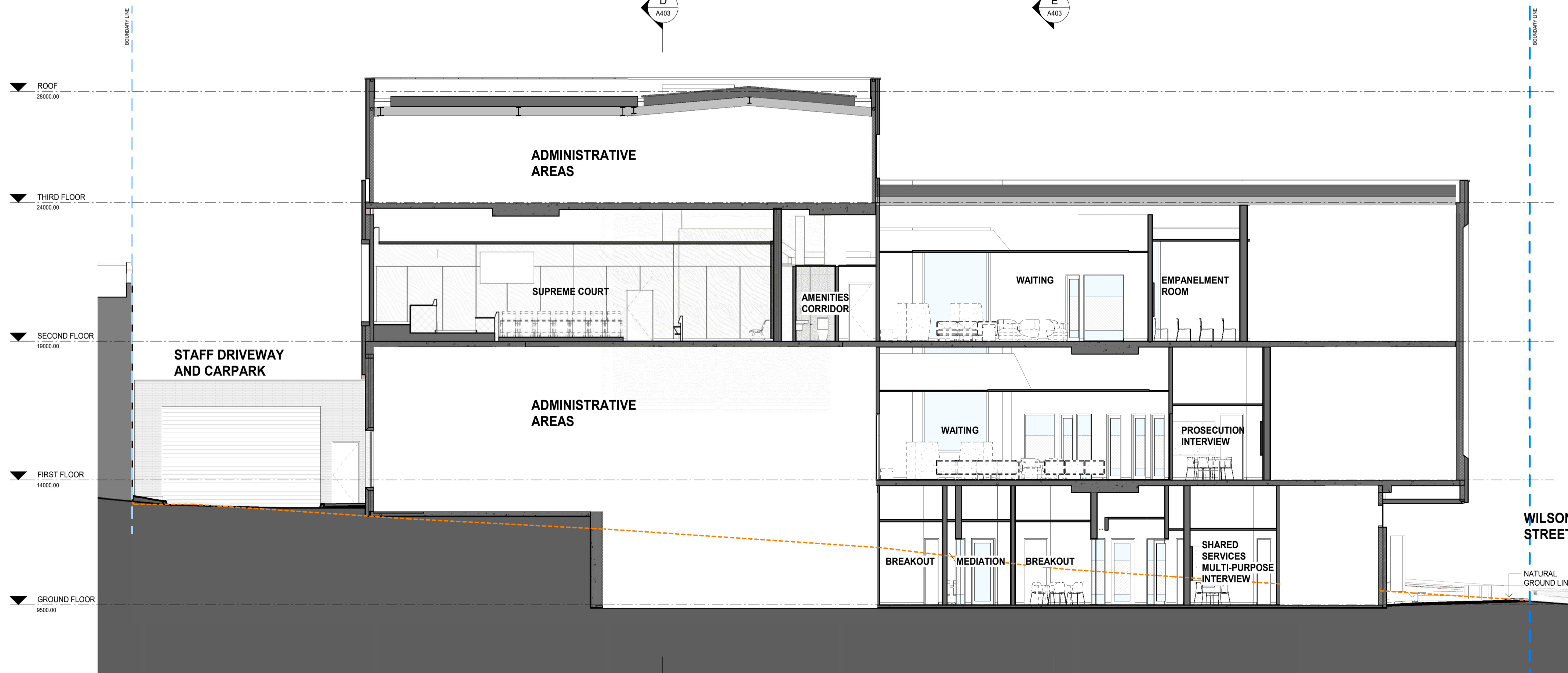


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A SECTION A
1:100



B SECTION B
1:100

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING SECTIONS

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE JUL 2025
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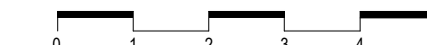
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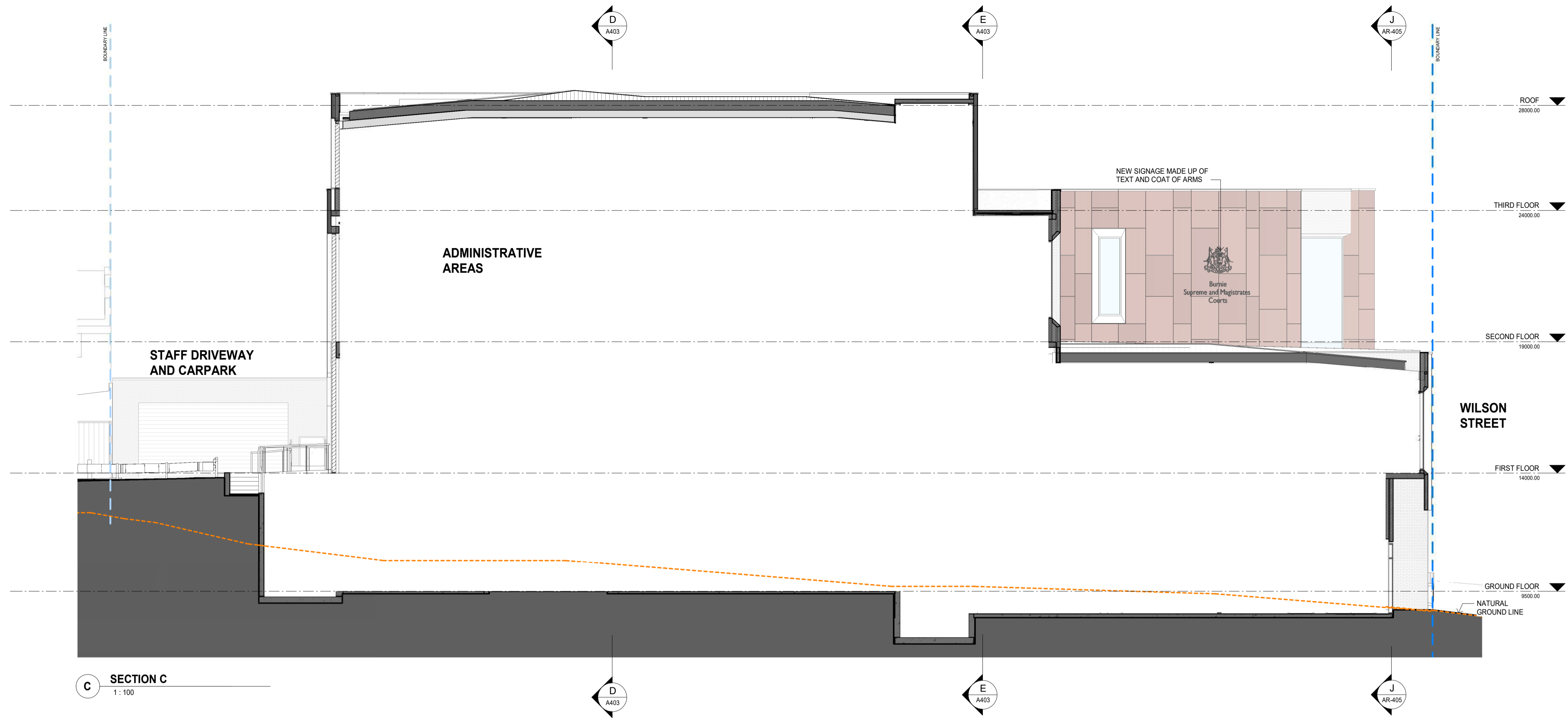


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DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING SECTIONS

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE TAS

JOB NUMBER 2324	DATE JUL 2025
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A402 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED

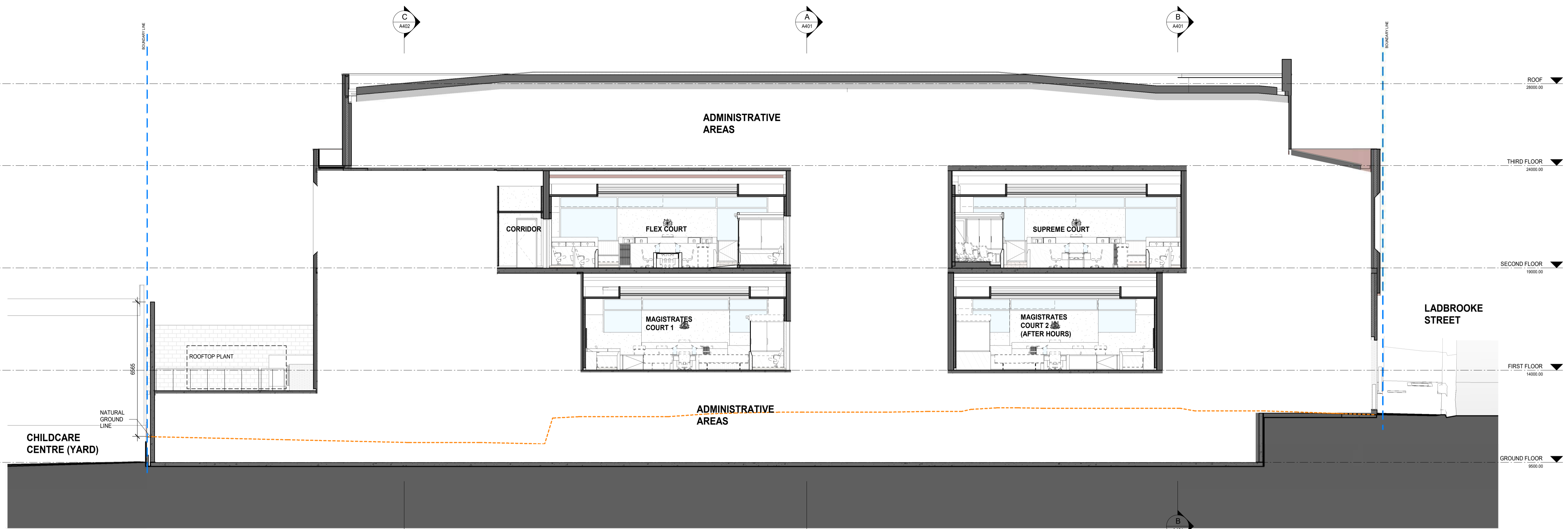
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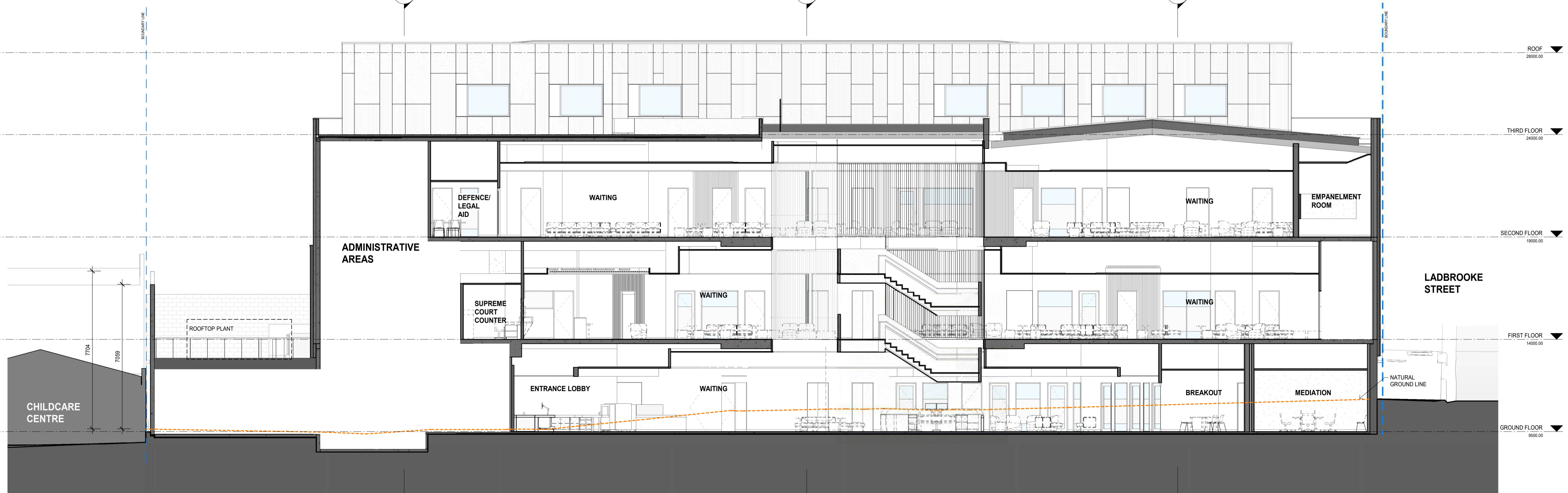
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D SECTION D
1:100



E SECTION E
1:100

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

SECTIONS

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE
TAS

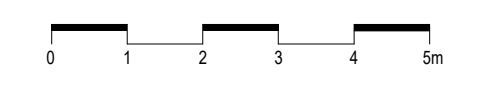
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A403 A

GB-A GB-ARCHITECTS
GB-LANDSCAPE
GB-INTERIORS
LIFE FORMED



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3D VIEW - WILSON STREET, LOOKING NORTH



3D VIEW - WILSON STREET, LOOKING NORTH



3D VIEW - LADBROOKE STREET, LOOKING WEST



3D VIEW - WILSON STREET, LOOKING SOUTH

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
3D STREET VIEWS

PROJECT TITLE
BURNIE COURTS COMPLEX
RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE
TAS

JOB NUMBER 2324	DATE JUL 2025
SCALE @ A1 1:1	DRAWN NG
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A801 A

GB-A ARCHITECTS
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3D VIEW - AERIAL VIEW



3D VIEW - AERIAL VIEW



3D VIEW - WILSON STREET, ENTRY



3D VIEW - WILSON STREET, FORECOURT

DEVELOPMENT APPLICATION

REVISIONS
A 16/07/2025 DEVELOPMENT APPLICATION

DRAWING
3D VIEWS

PROJECT TITLE
BURNIE COURTS COMPLEX RELOCATION PROJECT

PRINCIPAL
DEPARTMENT OF JUSTICE

PROJECT ADDRESS
100 - 106 WILSON ST, BURNIE TAS

JOB NUMBER
2324

DATE
JUL 2025

SCALE @ A1
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GB-A GB-ARCHITECTS GB-LANDSCAPE GB-INTERIORS

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BURNIE COURTS COMPLEX RELOCATION PROJECT

LANDSCAPE DOCUMENTATION FOR DESIGN DEVELOPMENT

Dwg No.	Drawing Title
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000 Preliminaries Series

LA-000	Cover Sheet
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100 Plan Series

LA-100	Surfaces and Finishes Plan
LA-101	Levels and Drainage Plan
LA-102	Planting Plan
LA-103	Irrigation Zone Plan
LA-104	Setout Plan

NOT IN THIS ISSUE

400 Sections Series

LA-400	Landscape Sections
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600 Details Series

LA-600	Landscape Landscape Details 01 of 03
LA-601	Landscape Landscape Details 02 of 03
LA-602	Landscape Landscape Details 03 of 03

NOT IN THIS ISSUE

NOT IN THIS ISSUE

700 Schedule Series

LA-700	Schedules
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Coordination

Surveyor: PDA Surveyors (ph: 03 6431 4400)
 Architect: X Squared Architects (ph: 03 6224 9370)
 Structural Engineer: Gandy and Roberts (ph: 03 6215 8600)
 Civil, Electrical, Fire, Hydraulic & Mechanical Engineer: JMG Engineers (ph: 03 6231 2555)

These drawings are to be read in conjunction with Landscape Materials Schedule (LA-700), Landscape Specification (LA-800), and Architect's and Engineer's documentation and specifications.

Details of structural elements including walls, footings, pavements and structures are shown for general arrangement and coordination purposes only - refer to structural engineer's or architect's documentation for construction details. This includes required compaction rates for bases, concrete specifications, curing methods, jointing and reinforcement. Refer to engineer's documentation for all steel construction including member sizes, fixing, connections, and protective coatings.

Architect's and Engineer's documentation takes precedence.

Refer to civil engineer's documentation for all bulk earthworks and base preparation.

Refer civil engineer's documentation for all stormwater pits, pipes and agricultural drains. Agricultural drains and inspection openings shown on landscape documentation for clarity only.

Refer to electrical engineer's documentation for details of all external lighting, communications, pits and service connections.

Refer to hydraulic engineer's documentation for location and confirmation of size of backflow prevention device.

Notify the superintendent immediately of any discrepancies between landscape documentation package and architect's and/or engineer's documentation, or existing site conditions.

Services

Refer to existing conditions survey #52979-DS-1D for all existing services.

Locations of any known site services shown notionally only; contractor to conduct Dial Before You Dig enquiry and full review of architect's and engineer's documentation prior to the commencement of works.

Locate and mark all existing services prior to construction. Repair any existing services damaged during construction at contractor's expense.

Levels and drainage

Confirm all levels on site prior to construction. Ensure positive drainage to all surfaces and subgrades to pits and agricultural drains. Contractor is to notify the Superintendent immediately of any levels or drainage conflict between landscape documentation and existing site conditions.

Any agricultural drains and connections to existing stormwater infrastructure not installed by main building to be installed by landscaper's registered plumber.

Setout

Setout all works and seek approval prior to construction.

Setout drawings are provided for cross-check information only, all works to be set out digitally. A CAD (.dwg) file can be provided upon request.

Setout locations of pits, manholes, lights and services turrets per engineer's documentation and check against landscape documentation - notify the superintendent immediately of discrepancies between services positions and landscape documentation.

All pits to be oriented to be perpendicular and parallel to adjacent hardstand.

Lids of manholes, electrical, comms, or junction pits within pavements to be flush with no lippage and match grade of pavements.

Pits in garden beds or swales to be set down from adjacent pavements as per levels plan, with minimum 150mm offset to adjacent pavement to allow for shaping down of soft surface.

Soft landscaping

All soft landscape works to be carried out by a qualified landscape contractor.

Excavation

Any excavation adjacent to existing buildings, walls, or footings in order to achieve landscape design to be confirmed on site and approved by Superintendent prior to commencement of excavation works.

Pavements

Isolation joints to objects, pits and walls. Construction/expansion joints to LGAT Standards. Saw cuts every 3m minimum. Curing by an approved method.

Subgrade

Ensure approved compacted subgrade prior to constructing pavements.

Refer to engineer's documentation for subgrade CBR %. Compaction of all subgrades under pavements or granular surfaces to engineer's specification. Refer landscape documentation for treatment of subgrades under soft landscape treatments.

Base course (FCR)

FCR compaction to a minimum of 95% MMDD.

FCR shall be compacted to a minimum 95% MMDD or as specified by the engineer. Engineer's specification takes precedence.

Subsoil

Rip parallel to the final contours. Do not rip when the subsoil is wet or plastic.

Cultivate to a minimum depth of 100mm. Do not disturb services or tree roots - if required, cultivate these by hand. Cultivate manually within 300mm of paths or structures. Remove stones exceeding 25mm, clods of earth exceeding 50mm, and weeds, rubbish or other deleterious material brought to the surface during cultivation. Trim the surface to design levels after cultivation.

Subsoil additives

Apply additives after ripping and incorporate into the upper 75mm layer of the subsoil.

Gypsum: incorporate at the rate of 0.25kg/m2.

Topsoil

Imported topsoil to AS 4454, potting mixes to AS 3743, composts, soil conditioners and mulches to AS 4454.

Confirm stockpile locations with client prior to construction.

Spread topsoil on prepared subsoil and grade evenly, making allowances for finished levels after light compaction. Topsoil to 300mm depth in garden beds. Dispose of surplus topsoil off-site.

Planting

Do not plant in unsuitable weather conditions, including extreme heat, cold, wind or rain. In other than sandy soils, suspend excavation when the soil is wet, or during frost periods. Thoroughly water the plants before planting, immediately after planting, and as required to maintain growth rates free of stress.

Place plants as follows:

- Remove the plant from the container with minimum disturbance to the rootball. Make sure that the rootball is moist.
- If required, root prune to make sure all circling roots have been either severed or aligned radially into the surrounding soil.
- Place the plant in its final position, in the centre of the hole and plumb, and with the topsoil level of the plant rootball level with the finished surface of the surrounding soil.

In planting beds and individual plantings, place fertiliser pellets around the plants at the time of planting. Refer to manufacturer's specifications for application rate.

Backfill with topsoil mixture. Lightly tamp and water to eliminate air pockets. Make sure that topsoil is not placed over the top of the rootball, so the plant stem remains the same height above ground as it was in the container. Avoid mixing mulch with topsoil.

New trees

Proposed tree specimens to be inspected by landscape architect prior to delivery to site.

Rockwork

Ensure no foot entrapment exists between boulders, or boulders and other elements. Gaps deeper than 30mm shall be EITHER less than 30mm wide, OR greater than 300mm wide.

Surfaces intended for running and walking (or gaps between rocks) shall not contain gaps likely to cause foot or leg entrapment. Gaps in the main direction of travel shall not be greater than 30mm when measured across the direction of travel.

Plant procurement

Confirm availability of specified plants at awarding of contract. Notify the superintendent of any unavailable stock sizes or species immediately. Provide substitutions for approval for any unavailable stock sizes or species.

Plant species

Supply trees with the following properties:

- Free from injury
- Self-supporting, with calliper at any given point on the stem greater than the calliper at any higher point on the stem
- Health: foliage size, texture and colour at time of delivery consistent with that of healthy specimens for the nominated species
- Vigour: extension growth consistent with that exhibited in vigorous specimens of the nominated species
- Damage: free from damage and from restricted habit due to growth in nursery rows
- Stress: free from stress resulting from inadequate watering, excessive shade or excessive sunlight experienced at any time during their development
- Site environment: grown and hardened off to suit anticipated site conditions at the time of delivery
- Root development: grown in their final containers for the following periods:
 - plants <25L size: more than 6 weeks
 - plants >25L size: more than 12 weeks
- Pests and disease: free from attack by pests or disease

Root systems

Supply plant material with a root system that is:

- Well proportioned in relation to the size of the plant material
- Conducive to successful transplantation
- Free of any indication of having been restricted or damaged
- Root ball proportionate and to be expected of the specified stock size

Do not provide root bound stock.

Do not provide stock grown less than 6 months in the specified stock size.

Notify superintendent prior to planting to allow for inspection of plant material prior to installation.

Irrigation design and installation guidelines

Confirm extent of any existing infrastructure on site prior to construction and salvage for re-use where possible.

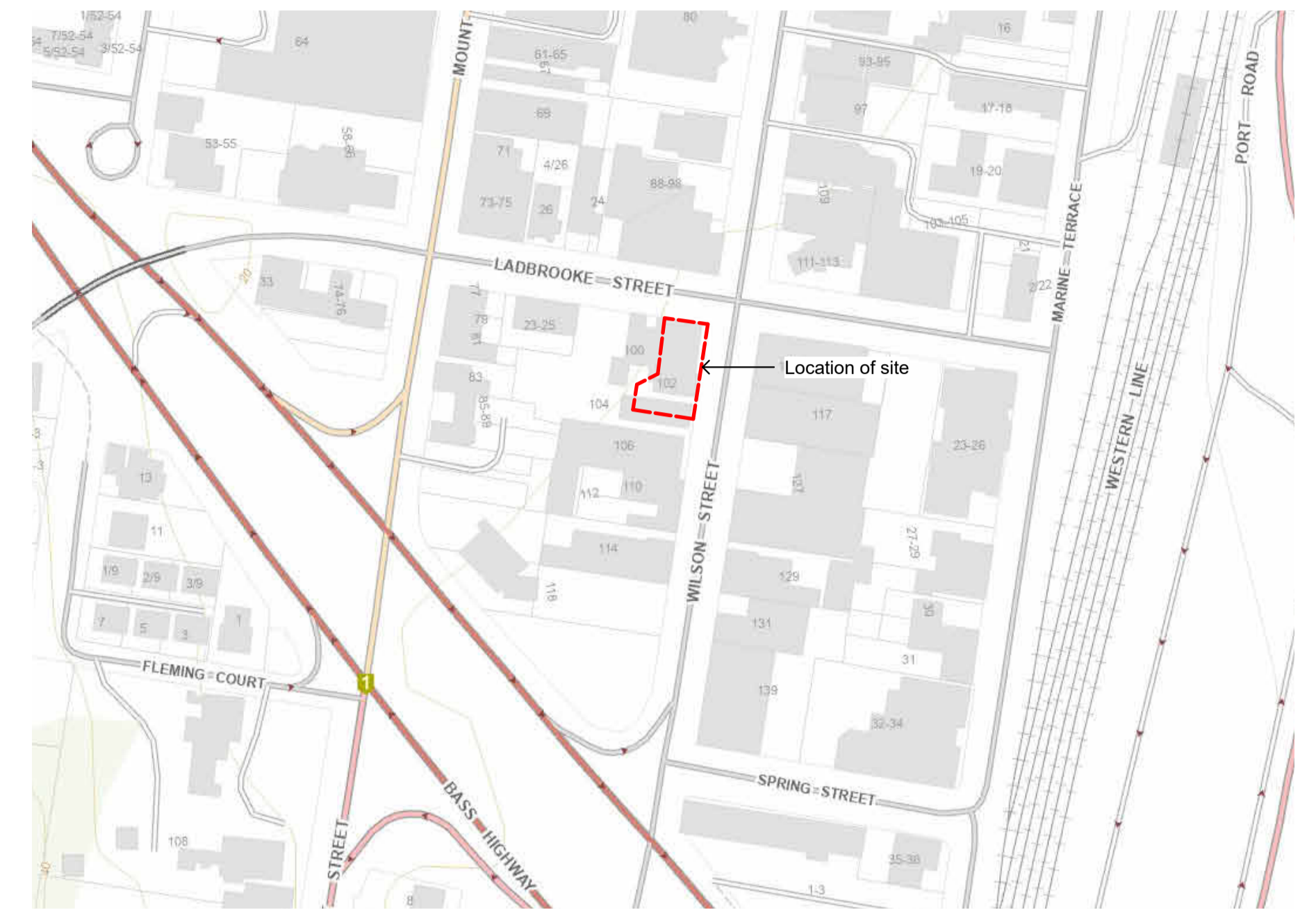
- Irrigation to be design and constructed by a recognised irrigation tradesperson.
- Irrigation schematic design to be provided by the irrigation installer to the Superintendent for client approval prior to construction.
- Layout of irrigation zones with valve boxes suitable to achieve effective irrigation to all garden bed and trees to courtyard areas and street frontages shown. Run suitable sized arterial irrigation pipes to all irrigation zones. All irrigation pipes and fixtures to be of a high quality suitable for commercial irrigation purposes. All dripline to be Tetafim Techline AS XR 13dia 1.6L/hr @ 0.3 dripper spacings in 600mm rows, to be buried 100mm below surface. Alternative irrigation methods can be provided by the irrigation installer to the Superintendent for client approval.
- Alternative irrigation approach can be provided to approval.
- Coordination will include integration of conduits within slabs and through walls as required, final locations and levels of irrigation boxes.
- Isolation valves to be located at junction/feed to each zone.
- Ensure sufficient backflow prevention is in place.
- Battery operated controllers within irrigation box unless client requests central master controller.
- As built diagram of zones, box and irrigation layout to be provided at completion.
- Basic maintenance manual to be provided by the installer for the above layout.
- Induction of system to be provided to the caretaker at end of the maintenance period.

Defects period

26 week defect period for soft landscape items starting at practical completion. Defects period for all other items - 12 months.

Maintenance period

13 weeks maintenance period for all soft landscape works.



01 Location Plan

1 : 2000

ISSUE	STATUS	DATE	REVIEWED
0	Issue for Design Development	04-07-2025	PST

LA-000

SHEET TITLE
 000 Preliminaries Series
 Cover Sheet

CLIENT

X Squared Architects

PROJECT NAME

Burnie Courts Complex Relocation Project

SITE ADDRESS

100-106 Wilson St
 Burnie TAS

PROJECT NO.

23-093

STAGE

Design Development

DESIGNED

MS / ALA

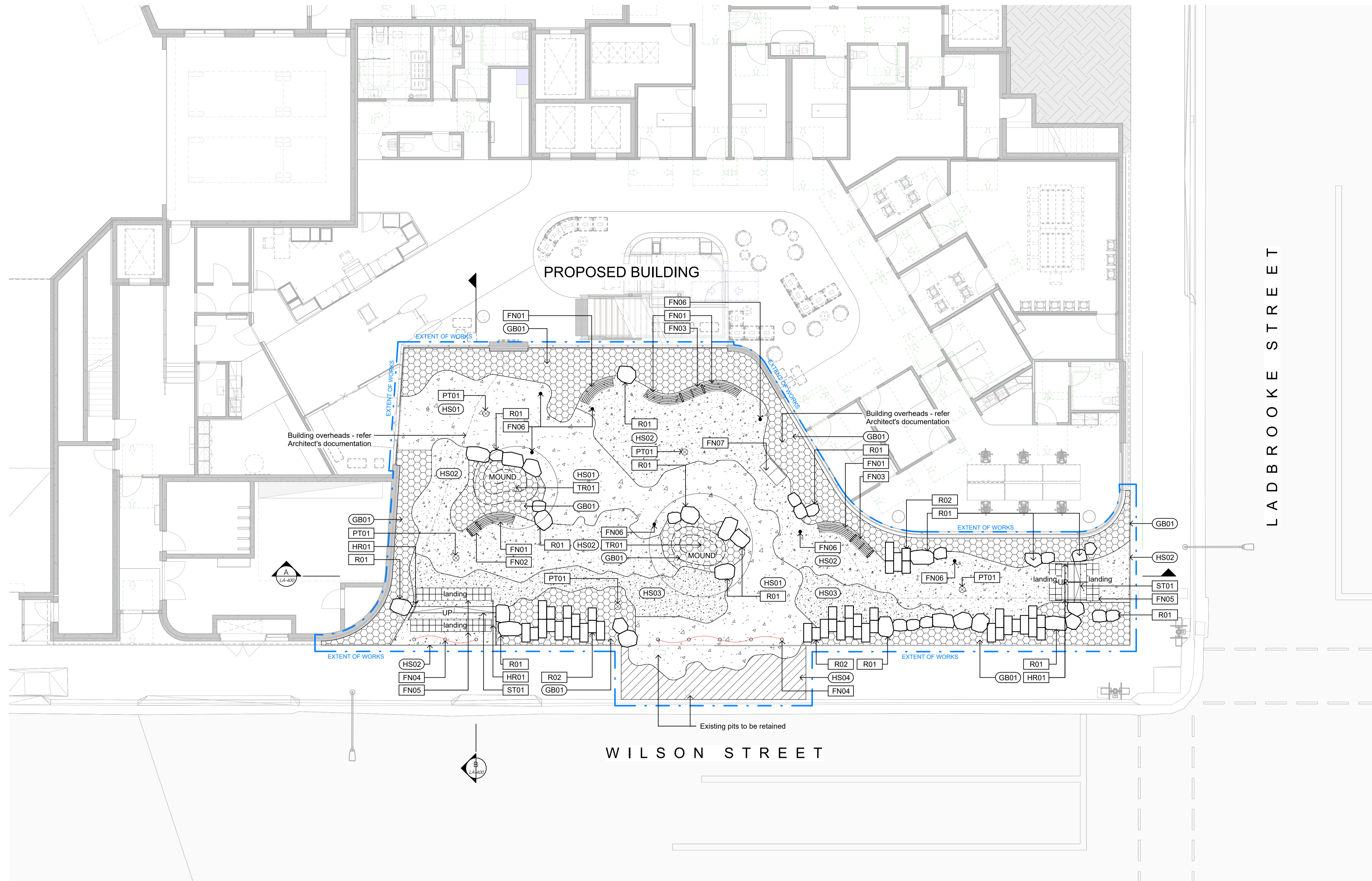
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SCALE

as shown

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MATERIALS KEY

REFER MATERIALS SCHEDULE (L001)

WW00 Surfaces tag

FN00 Element / item tag

HARD SURFACES

- HS01 Hardstand - type 01
- HS02 Hardstand - type 02
- HS03 Hardstand - type 03
- HS04 Hardstand - type 04

SOFT SURFACES

- GB01 Garden bed - type 01

HANDRAILS

- HR01 Handrail - type 01

STAIRS

- ST01 Stairs - type 01

FURNITURE & FIXTURES

- FN01 Furniture element - type 01
- FN02 Furniture element - type 02
- FN03 Furniture element - type 03
- FN04 Furniture element - type 04
- FN05 Furniture element - type 05
- FN06 Furniture element - type 06
- FN07 Furniture element - type 07

PITS

- PT01 Stormwater pit - type 01

ROCKWORK

- R01 Rockwork - type 01
- R02 Rockwork - type 02

TREES

- TR01 Advanced tree - type 01

ISSUE STATUS	DATE	REVIEWED
0 Issue for Design Development	04-07-2025	PST

LA-100

SHEET TITLE
100 Plan Series
Surfaces and Finishes Plan

CLIENT
X Squared Architects

PROJECT NAME
Burnie Courts Complex
Relocation Project
SITE ADDRESS
100-106 Wilson St
Burnie TAS

PROJECT NO.
23-093
STAGE
Design Development

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- ### LEVELS KEY
- REFER ENGINEER'S DOCUMENTATION FOR PIT/PIPE CONNECTIONS & INVERT LEVELS
- 00.00 Design level spot height (+/- to existing grade)
 - Notes surface level adopted from architect's documentation
 - Direction of fall on surface
 - Agricultural drain
 - Agricultural drain in PVC conduit
 - Flush Flush with adjacent surface
 - TS Top of step
 - BS Base of step
 - TP Top of pit
 - BR Base of rock / boulder
 - IO Inspection opening
 - EXPT Existing pit
- NOTES
- 1% crossfall on all paths
 - Max 2% fall on landings
 - All ag drains to have continuous positive drainage (1% min)
 - Fall continuous between spot heights
 - Max fall height from top of rock to ground

ISSUE STATUS	DATE	REVIEWED
0 Issue for Design Development	04-07-2025	PST

LA-101

SHEET TITLE
100 Plan Series
Levels and Drainage Plan

CLIENT
X Squared Architects

PROJECT NAME
Burnie Courts Complex
Relocation Project
SITE ADDRESS
100-106 Wilson St
Burnie TAS

PROJECT NO.
23-093
STAGE
Design Development

DESIGNED
MS / ALA
DRAWN
AK

SCALE
1:100@A1
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PLANTING KEY
REFER TO PLANTING SCHEDULE (LA-700) FOR PLANT SPECIES INFORMATION AND QUANTITIES

NOTES
Symbols show mature spread of plant species, not size at planting.
Protect existing trees to be retained throughout the course of the works. No machining over structural root zone. Avoid compaction of tree protection zone.
Hand excavation only within driplines of existing trees. If roots are exposed, notify the client. Ensure they are maintained moist and covered with hessian or similar to prevent damage during the course of the works.

CODE	NAME
TREES	
Bin	<i>Banksia integrifolia</i>
Epu	<i>Eucalyptus pulchella</i>
SHRUBS	
Dan	<i>Dicksonia antarctica</i>
Lbr	<i>Leucophyta brownii</i>
Rca	<i>Rhagodia candolleana</i> ssp. <i>candolleana</i>
Tia	<i>Tasmannia lanceolata</i>
GRASSES / GROUNDCOVERS	
Abu	<i>Asplenium bulbiferum</i>
Ast	<i>Austrospiza stipoides</i>
Bnu	<i>Blechnum tetraphyllum</i>
Bte	<i>Blechnum nudum</i>
Bnu	<i>Casuarina glauca</i> 'Cousin It'
Cgl	<i>Clematis aristata</i>
Car	<i>Dianella tasmanica</i>
Dta	<i>Grahnia grandis</i>
Ggr	<i>Mycoporum parvidolium</i>
Mpa	<i>Poa labillardieri</i>
Pla	<i>Viola hederacea</i>
Vhe	

ISSUE	STATUS	DATE	REVIEWED
0	Issue for Design Development	04-07-2025	PST

LA-102

SHEET TITLE
100 Plan Series
Planting Plan

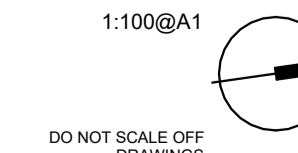
CLIENT
X Squared Architects

PROJECT NAME
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Relocation Project
SITE ADDRESS
100-106 Wilson St
Burnie TAS

PROJECT NO.
23-093
STAGE
Design Development

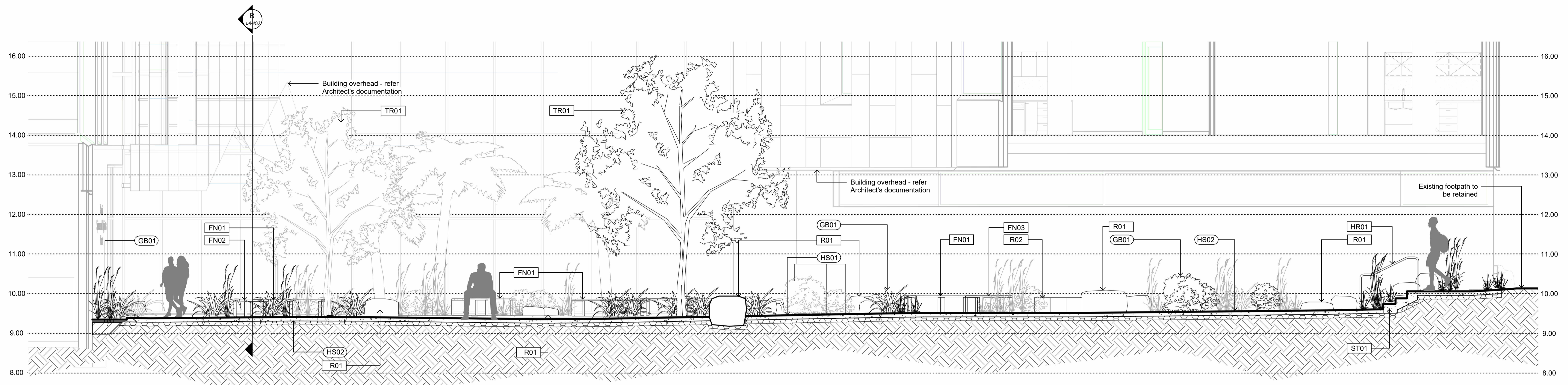
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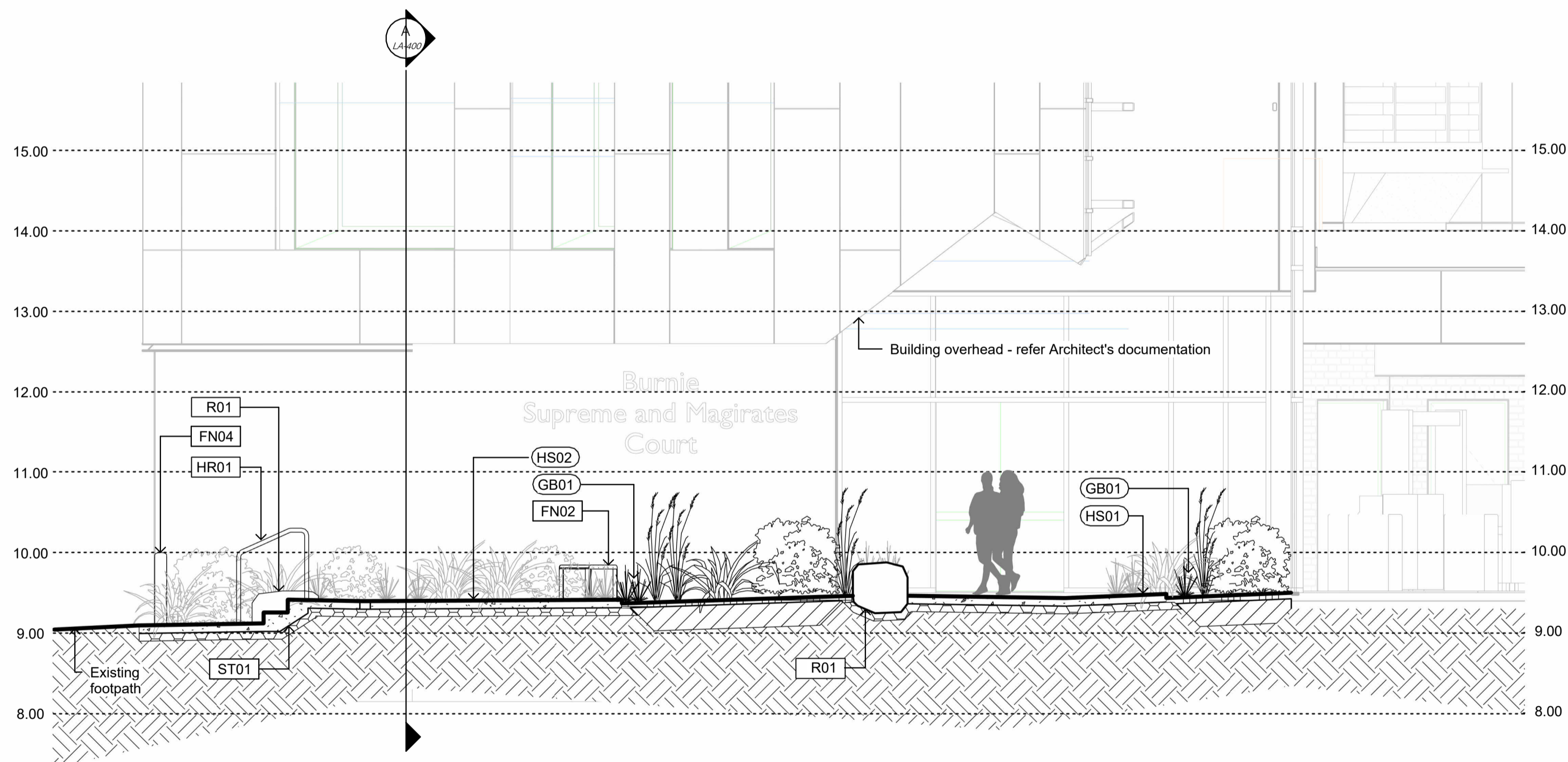


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1 A - Forecourt Section A
1 : 50



2 B - Forecourt Section B
1 : 50

ISSUE	STATUS	DATE	REVIEWED
0	Issue for Design Development	04-07-2025	PST

LA-400

SHEET TITLE
400 Sections Series
Landscape Sections

CLIENT
X Squared Architects

PROJECT NAME
Burnie Courts Complex
Relocation Project
SITE ADDRESS
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Burnie TAS

PROJECT NO.
23-093
STAGE
Design Development

DESIGNED
MS / ALA
DRAWN
AK

SCALE
as shown
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DRAWINGS



MATERIALS SCHEDULE

1 HARDSTAND				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
HS01	Hardstand - type 01	Lightly exposed aggregate concrete, pedestrian grade. 'Bakers Beach' mix or approved equivalent, standard grey cement. Exposed to remove picture frames, isolation joints to objects, walls and pits. Finish flexible joints with a polyurethane bead (sikaflex or approved equivalent), colour-matched to concrete. Ensure all corners and edges are rounded to a minimum radius of 5mm.	Refer to Engineer's documentation for typical details and specification including construction, base preparation, curing methods, jointing specification and base compaction rates	Boral Concrete (ph: 03 6274 1628)
HS02	Hardstand - type 02	Coloured concrete, pedestrian grade, 'swirly' plate finish. Colormix 'Everglade' oxide in grey cement. Remove picture frames, isolation joints to objects, walls and pits. Finish flexible joints with a polyurethane bead (sikaflex or approved equivalent), colour-matched to concrete. Ensure all corners and edges are rounded to a minimum radius of 5mm.	Refer to Engineer's documentation for typical details and specification including construction, base preparation, curing methods, jointing specification and base compaction rates	Boral Concrete (ph: 03 6274 1628)
HS03	Hardstand - type 03	Coloured concrete, pedestrian grade, 'swirly' plate finish. Colormix 'Everglade' oxide in grey cement. Pattern to be sandblasted/etched into 50% of total surface of concrete, pattern and location to be confirmed. Price pattern sandblasting as a separate allowance to concrete. Remove picture frames, isolation joints to objects, walls and pits. Finish flexible joints with a polyurethane bead (sikaflex or approved equivalent), colour-matched to concrete. Ensure all corners and edges are rounded to a minimum radius of 5mm.	Refer to Engineer's for typical details and specification including construction, base preparation, curing methods, jointing specification and base compaction rates	Boral Concrete (ph: 03 6274 1628)
HS04	Hardstand - type 04	Make good existing pavement.		

2 SOFTSTAND				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
GB01	Garden bed - type 01	Black well composted gum bark over 300mm topsoil profile. Planting to occur within this zone - refer planting plan.	Refer xx/LA-600	Prospect Timber and Landscape Supplies (ph: 03 6340 1518)

3 HANDRAILS				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
HR01	Handrail - type 01	Stainless steel handrail to match architectural building handrails.	Refer Architect's documentation for detail, specification and installation.	

4 STAIRS				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
ST01	Stairs - type 01	Insitu concrete stairs to AS1428 including luminance contrast nosings. Finish to match HS02.	Refer to Engineer's for typical details and specification including construction, base preparation, curing methods, jointing specification and base compaction rates	

5 FURNITURE & FIXTURES				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
FN01	Furniture element - type 01	'Linea' curved bench #LIB6-C, standard configuration, standard leg - surface fixed, battens and frame aluminium powdercoated, colour to be confirmed.	Refer to manufacturer's specification and installation information	Street Furniture Australia (ph: 1800 027 799)
FN02	Furniture element - type 02	'Linea' bench #LIB6, 600mm length, standard configuration, standard leg - surface fixed, battens and frame aluminium powdercoated, colour to be confirmed.	Refer to manufacturer's specification and installation information	Street Furniture Australia (ph: 1800 027 799)
FN03	Furniture element - type 03	'Linea' bench #LIB6, 1200mm length, standard configuration, standard leg - surface fixed, battens and frame aluminium powdercoated, colour to be confirmed.	Refer to manufacturer's specification and installation information	Street Furniture Australia (ph: 1800 027 799)
FN04	Furniture element - type 04	Securapost bollard #HBR150FC using shallow mount bollard system #SMF1425, flat top steel, powdercoat finish colour: Monument	Refer to manufacturer's specification and installation information	Leda Security (ph: 03 8399 8150)
FN05	Furniture element - type 05	Latham TI Series Tactile Indicator Studs, spiral top stainless steel finish, installed with Latham Thixotropic Stud Adhesive.	Refer to manufacturer's specification and installation information	Latham Safety Products (ph: 02 9879 7888)
FN06	Furniture element - type 06	Stainless steel inlay into concrete slab, 150x150mm, shape to be confirmed.	Refer xx/LA-600	
FN07	Furniture element - type 07	'Linea' bin, 1x general waste and 1x recycling, 120L, stainless steel powdercoat finish, colour to be confirmed, surface fixed.	Refer to manufacturer's specification and installation information	Street Furniture Australia (ph: 1800 027 799)

6 PITS				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
PT01	Stormwater pit - type 01	300dia SPS grated pit, heelguard and lockable.	Refer Engineer's documentation.	

7 ROCKWORK				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
R01	Rockwork - type 01	Feature and retaining. Flat-topped weathered dolerite boulder.	Refer xx/LA-600	
R02	Rockwork - type 02	Feature and retaining. Flat-topped sandstone blocks, split face on all sides. Use sling, chocks and mud points as required to achieve neat joint and level surface. Mortar between all joints.	Refer xx/LA-600	Castle Stone (ph: 0417 706 422)

7 TREES				
CODE	NAME	DESCRIPTION	DETAIL REFERENCE	SUPPLIER
TR01	Advanced Tree - type 01	Advanced tree in garden bed. HDPE root barrier must be installed to 600mm depth where tree is within 1.5m of underground services.	Refer xx/LA-600	

PLANT SCHEDULE

CODE	BOTANICAL NAME	COMMON NAME	MATURE HEIGHT	MATURE SPREAD	POT SIZE	QUANTITY
Trees						
Bin	Banksia integrifolia	Coastal Banksia	3 - 6m	3.5 - 6m	75L	1
Epu	Eucalyptus pulchella	White peppermint	10 - 15m	3.5 - 6m	75L	1
Shrubs and Features						
Dan	Dicksonia antarctica	Tasmanian Tree Fern	3 - 5m	3.5 - 6m	8' trunk height	3
Lbr	Leucophyta brownii	Cusion bush	0.75 - 0.9m	1.2 - 2.0m	200mm	7
Rca	Rhagodia candolleana ssp. candolleana	Seaberry Saltbush	0.6 - 0.75m	0.5 - 1.2m	200mm	2
Tla	Tasmannia lanceolata	Tasmanian Pepperberry	2.5 - 3m	1.5 - 3m	200mm	4
Grasses and Groundcovers						
Abu	Asplenium bulbiferum	Mother Fern	0.75 - 0.9m	0.9 - 1.2m	140mm	19
Ast	Austrostipa stipoides	Speargrass	0.30 - 0.45m	0.3 - 0.6m	140mm	42
Bte	Baloskion tetraphyllum	Tassel Rush	0.0 - 0.3m	0.6 - 0.9m	140mm	18
Bnu	Blechnum nudum	Fishbone Water Fern	0.75 - 0.9m	0.3 - 0.6m	140mm	15
Cgl	Casuarina glauca 'Cousin It'	Cousin It	0.5 - 0.8m	0.6 - 1m	140mm	14
Car	Clematis aristata	Old Man's Beard	0.75 - 0.9m	0.0 - 0.3m	140mm	18
Dta	Dianella tasmanica	Tasmanian Flax Lily	0.3 - 0.45m	0.3 - 0.6m	140mm	30
Ggr	Grahnia grandis	Brickmaker's Sedge	0.5 - 0.8m	0.5 - 0.6m	140mm	18
Mpa	Myoporum parvifolium	Creeping Boobialla	0.45 - 0.6m	0.9 - 1.2m	140mm	11
Pla	Poa labillardieri	Tussock Grass	0.90 - 1.50m	0.3 - 0.6m	140mm	59
Vhe	Viola hederacea	Native Violet	0.2 - 0.4m	0.4 - 0.8m	140mm	20

PRECEDENT IMAGES



Image 01: HS03 - concrete sandblasting



Image 02: FN01 - 'Linea' curved bench



Image 02: FN02-03 - 'Linea' bench



Image 04: FN06 - steel inlay



Image 05: R02 - sandstone blocks

ISSUE	STATUS	DATE	REVIEWED
0	Issue for Design Development	04-07-2025	PST

LA-700

SHEET TITLE
700 Schedule Series
Schedules

CLIENT
X Squared Architects

PROJECT NAME
Burnie Courts Complex
Relocation Project
SITE ADDRESS
100-106 Wilson St
Burnie TAS

PROJECT NO.
23-093
STAGE
Design Development

DESIGNED
ALA
DRAWN
AK

SCALE
nts

DO NOT SCALE OFF
DRAWINGS